

APPENDIX 1

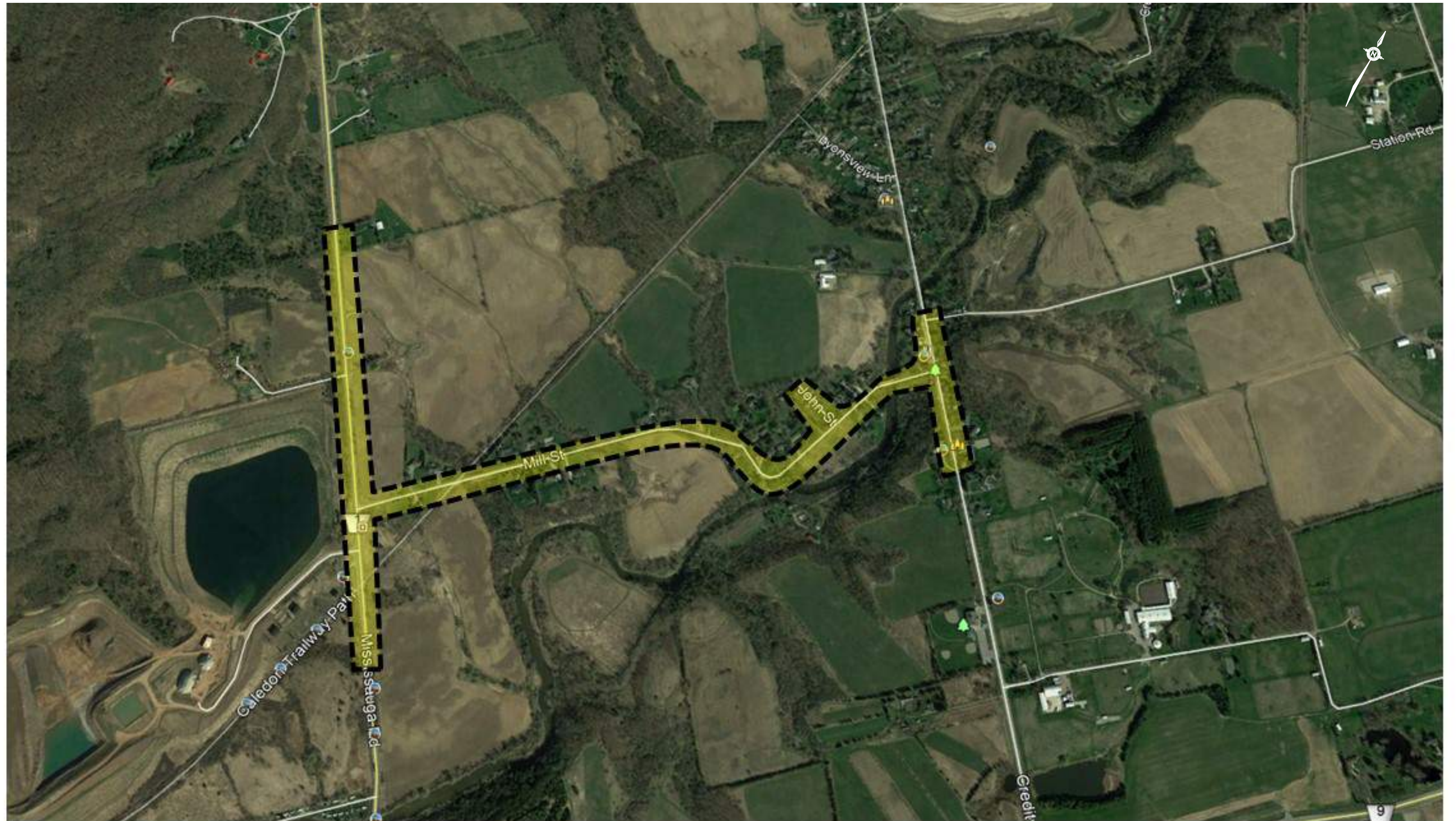
CONSULTATION MATERIAL



APPENDIX 1-1

Notices





MILL STREET (MISSISSAUGA ROAD TO CREDITVIEW ROAD)



October 22, 2020

RVA 205388

Attention: Owner/Occupant

Re: Notice of Study Commencement
Municipal Class Environmental Assessment Study
2020-97 Mill Street (Between Mississauga Road and Creditview Road)

The Town of Caledon has initiated a Municipal Class Environmental Assessment (Class EA) study for improvements to Mill Street between Mississauga Road and Creditview Road as part of the 2022 Growth Related Roads project (2020-97). The purpose of this letter is to inform you of the study and to invite your participation. A copy of the Notice of Commencement with a key plan of the study area can be found on the back of this letter.

The Town is considering a range of options for the roadway including road rehabilitation, road and intersection operational and geometric improvements, active transportation (pedestrian and cyclist) amenities, and drainage improvements. The study is being conducted in accordance with the requirements for a Municipal Class EA, which is approved under the *Ontario Environmental Assessment Act*.

The Town has retained R.V. Anderson Associates Limited to undertake this study. Over the next few months, the project team including a number of environmental sub-consultants, may be on-site collecting information related to physical roadway and environmental conditions for the study area.

A key component of the study will involve consultation with interested stakeholders, the public and regulatory agencies. You are encouraged to provide your comments to be incorporated into the project planning. Please contact either one of the following project managers if you would like further information on the project, have any questions or comments, or wish to be added to the study mailing list.

Yours very truly,

Town of Caledon
Shun H. Cheung, P.Eng., PMP
Project Manager, Engineering Services
Finance and Infrastructure Services
Tel: 905.584.2272 x 4040
E-mail: shun.cheung@caledon.ca

R.V. Anderson Associates Limited
Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
Tel: 416.497.8600 x 1471
E-mail: winnie.wong@rvanderson.com

Encl.: Notice of Study Commencement



R.V. Anderson Associates Limited
engineering • environment • infrastructure

MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

The Study

In response to continual road deterioration associated with population growth, the Town of Caledon has identified a need for improvements to Mill Street between Mississauga Road and Creditview Road in 2022. Subsequently, the Town has initiated a Municipal Class Environmental Assessment (Class EA) to review and identify required road, intersection and drainage improvements along the corridor. Within the study limits, Mill Street will retain the two general-purpose lanes, however various options will be evaluated to enhance safety, improve traffic operations and to better accommodate all users.



The Process

The purpose of the study is to identify a recommended design for Mill Street that addresses the increasing wear and tear associated with population growth while enhancing road safety, improving traffic operations and connectivity for all users. In identifying the road improvements, the study will consider the technical and aesthetic requirements; as well as socio-economic, cultural heritage, and natural environment factors.

The project is being completed as a **Schedule “B”** project in accordance with the *Municipal Class Environmental Assessment (October 2000, amended in 2007, 2011 & 2015)*.

Contact

To be added to the project mailing list, and for any questions, comments or concerns, please contact:

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Engineering Services
Finance and Infrastructure Services
Tel: 905.584.2272 x 4040
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Project Manager
Tel: 416.497.8600 x 1471
E-mail: winnie.wong@rvanderson.com

This notice was first issued October 22, 2020.

Connor MacIsaac

From: Connor MacIsaac
Sent: October 22, 2020 3:09 PM
To: Connor MacIsaac
Cc: Andrew McGregor; Shun Cheung; John P. Does; Winnie Wong
Subject: Mill Street EA - Notice of Commencement
Attachments: 2020-97 -MillStreetEA-NOC-vf.pdf

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Notice of Study Commencement for the **Mill Street Class Environmental Assessment Study (Schedule B MCEA)**. The study will review opportunities for improvements to the road between Mississauga Road and Creditview Road, including improvements to: road geometry and intersection safety; pedestrian and cyclist connectivity; and stormwater management.

At this point in the study, we are requesting any relevant background information from you as it relates to the study area. Information that would be of interest includes:

- **Contact information** and the identification of individuals that represent your agency or group that we should include as a primary contact throughout the study process;
- **Description of the existing conditions or sensitivities** within the study area as they relate to your interests; and
- Specific **issues, concerns and/or expectations** that your agency or group may have.

Please respond to this email with your comments, or contact directly the project team members listed on the attached notice for further information on this project.

Regards,



RVA IS GROWING!

Our NEW Halton and Halifax
offices are now open.



Connor MacIsaac, EPt

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

R.V. Anderson Associates Limited

43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

www.rvanderson.com



**MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD**

The Study

In October 2020, as part of the 2022 Growth Related Roads project, the Town of Caledon initiated a Schedule “B” Municipal Class Environmental Assessment (Class EA) for improvements to Mill Street between Mississauga Road and Creditview Road. The purpose of the study is to identify the required improvements to the roadway including considerations for pedestrians, cyclist activities and drainage. Since initiating the study, the Project Team has collected information on the existing physical and environmental conditions and received comments from residents within the study area.



Community Outreach Meeting

As part of the alternative solutions development for the Mill Street corridor, the Town wishes to consult with area residents through a Community Outreach Meeting, to be held electronically on **May 27, 2021 at 6 p.m.**

To listen to the Meeting, you may call 1-416-216-5643, Meeting Access Code: 132 248 0812. You can also watch live from the Town’s website. For more information on how to access the meeting details, including background on the project, please visit <https://www.caledon.ca/en/news/mill-street-ea.aspx>.

The development and evaluation of alternative solutions for the Mill Street corridor will be completed in consideration of comments received at the Community Outreach Meeting and will be presented to the public for input at an upcoming Public Information Center (PIC), tentatively scheduled for Summer 2021.

Contact

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This notice was first issued May 6, 2021.

PUBLIC NOTICE



NOTICE OF COMMUNITY OUTREACH MEETING

Mill Street Class Environmental Assessment Study Mississauga Road To Creditview Road

THE STUDY

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This notice was first issued May 6, 2021.



6311 Old Church Road
Caledon, ON L7C 1J6
www.caledon.ca
T. 905.584.2272 | 1.888.225.3366 | F. 905.584.4325

Size: 1/2 Vertical

Color: YES

Date: May 06, 2021

Distribution: Caledon Enterprise

Department: Engineering & Capital Projects

Run X1



Celebrate Seniors Month in Caledon



Seniors month is an annual celebration that is observed across Canada in honor of older adults throughout June.

The Town of Caledon and local community support agencies have partnered up to offer a variety of COVID-safe, engaging and free activities for our older residents to participate in. These activities are being offered as self-guided, virtual, or outdoor with accessibility accommodations to encourage participation.

RESERVE YOUR FREE SWAG BAG.

Each free bag, for residents 55+, includes information, resources and more. Reserve yours online by May 21: caledon.ca/adult55 or call 905.584.2272 x.7750.



Coming Soon

For agendas, minutes, live stream and call-in details, visit caledon.ca/agenda.

MAY

- 18 | General Committee, 2:30 p.m.
- 18 | Planning and Development Committee, 7 p.m.
- 19 | Committee of Adjustment, 1 p.m.
- 24 | Victoria Day Holiday (offices closed)
- 25 | Council, 6 p.m.
- 26 | Special Town Council Meeting for the Development Charges Public Meeting, 5 p.m.
- 27 | Virtual COVID-19 Town Hall, 6 p.m.

PUBLIC NOTICE

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Project Manager
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winnie.wong@rvanderson.com

This notice was first issued May 6, 2021.

COVID-19 Virtual Town Hall Meeting May 27, 7 p.m.



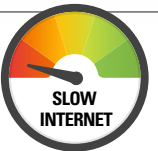
Join Dr. Lawrence Loh, Peel Medical Officer of Health and Mayor Allan Thompson for the latest on COVID-19.

View the livestream at caledon.ca/agenda, or dial 1-833-311-4101, access code 132 685 6269# for the audio-only version.

Have a COVID-19 question? Send your question in advance to mayor@caledon.ca.

LAUNCHING MAY 17

COVID-19 Internet Grant Program



Do you live in an under-served area?

As a result of the COVID-19 pandemic, a \$200 one-time grant will be available to reimburse residents for the cost of hardware required to increase internet service during the COVID-19 pandemic, in under-served areas of Caledon. Learn more at caledon.ca/internet.

*** If you do not have access to the internet, please call Service Caledon 905.584.2272 x.7750 to see if you are eligible.



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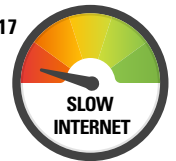
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Mill Street EA

Posted on Wednesday, May 05, 2021



Public Notice

For more information
visit caledon.ca/notices



The Town of Caledon has initiated a Municipal Class Environmental Assessment (Class EA) to look into improvements to Mill Street between Mississauga Road and Creditview Road as part of the 2022 Growth Related Roads project (2020-97). The purpose of the study is to identify a recommended design for Mill Street that addresses the increasing wear and tear associated with population growth while enhancing road safety, improving traffic operations and connectivity for all users.

In developing a recommended design, the study will consider a range of options for the road including but not limited to road rehabilitation, road and intersection improvements, active transportation (pedestrian and cyclist) amenities, and drainage improvements. Within the study limits, Mill Street will retain the two general-purpose lanes, however various options will be evaluated to enhance safety, improve traffic operations and to better accommodate all road users.

The study is being completed as a Schedule "B" project in accordance with the Municipal Class Environmental Assessment (October 2000, amended in 2007, 2011 & 2015).

Notice of Virtual Community Outreach Meeting

Event Details: Thursday May 27, 2021 | 6 - 7 p.m.

A virtual Community Outreach Meeting will be held to provide an opportunity to consult with the area residents related to this project. The session will consist of a presentation by Town's consultant followed by a moderated Q&A period. Questions can be submitted prior to the event to the contact below or through the chat feature during the live event.

Watch the [livestream](#) or call in to listen to the meeting (416-216-5643; Access code: 1322480812).

Potential Benefits

The potential benefits may include:

- Addressing deteriorating road conditions
- Improving vehicular traffic safety and operations
- Enhancing the safety and connectivity of cyclists and pedestrians
- Enhancing livability and promoting healthy living in the community
- Creating attractive and vibrant public spaces
- Supporting economic vitality
- Increasing resilience to climate change by improving environmental sustainability and stormwater management

Key Considerations

The following considerations will be applied:

- Preserving the number of lanes and ensuring sufficient capacity for vehicular traffic
- Possible improvements at intersections
- Implementation of active transportation infrastructure and facilities
- Possible addition of improved pedestrian crossings and trailway entrances at the Caledon Trailway, improving the safety and access to the trail
- Possible stormwater management improvements

- Slope stability and road grade improvements where required

Project Steps

There are a number of steps that the Town is taking to ensure that all aspects of the project are being considered:

- Analysis of existing conditions and constraints
- Consultation with the public and key technical agencies
- Identification of alternative improvements and analysis of expected impacts
- Selection of the recommended measures to implement

Notice of Study Commencement

The study

In response to continual road deterioration associated with population growth, the Town of Caledon has identified a need for improvements to Mill Street between Mississauga Road and Creditview Road in 2022. Subsequently, the Town has initiated a Municipal Class Environmental Assessment (Class EA) to review and identify required road, intersection and drainage improvements along the corridor. Within the study limits, Mill Street will retain the two general-purpose lanes, however various options will be evaluated to enhance safety, improve traffic operations and to better accommodate all users.



The Process

The purpose of the study is to identify a recommended design for Mill Street that addresses the increasing wear and tear associated with population growth while enhancing road safety, improving traffic operations and connectivity for all users. In identifying the road improvements, the study will consider the

technical and aesthetic requirements as well as socio-economic, cultural heritage, and natural environment factors.

Contact

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Town of Caledon

Shun H. Cheung, P.Eng., PMP
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416.497.8600 X.1471
winnie.wong@rvanderson.com

How can we help?

Call [905-584-2272](tel:905-584-2272) x. 7750 or [email](#) us!

Stay Informed

Sign up to get our newsletter. [Subscribe Now](#)

Council

Get involved! Learn more about [your Council](#).

Our residents make Caledon a vibrant place to live, work and play.

Contact Us

The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon ON L7C 1J6

Phone
905-584-2272

Toll Free
1-888-225-3366

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Designed by eSolutionsGroup (<http://www.esolutionsgroup.ca>).

**MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD**

THE STUDY

The Town of Caledon is currently assessing improvements to Mill Street between Mississauga Road and Creditview Road.

The purpose of this Schedule “B” Municipal Class Environmental Assessment is to identify the required improvements to the road including considerations for pedestrians, cyclist activities and drainage.

As part of the study, a Community Outreach Meeting was held in May 2021, to present and obtain information on the key issues and constraints within the study area. In reviewing comments received during and after the Community Outreach Meeting, the Town has moved forward with the development and evaluation of alternative solutions and the determination of a preliminary recommended solution.



ONLINE PUBLIC INFORMATION CENTRE

Due to the continuing efforts to contain the spread of COVID-19 and to protect individuals, we invite you to join us for an Online Public Information Centre on **Tuesday, October 5 at 6 p.m.** via WebEx webinar. The webinar will allow the project team to share information and receive input from the public on the study to date, including the problem / opportunity, existing conditions, alternative solutions, and the preliminary recommended solution. A Question-and-Answer period will follow to conclude at 7 p.m.

To listen to the meeting by phone, dial 1-416-216-5643. Use Meeting Access Code: 2632 256 0301).

To watch the meeting through WebEx online and to learn more about the project, visit: caledon.ca/notices.

CONTACT

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This notice was first issued September 9, 2021.

MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY

Mississauga Road to Creditview Road

THE STUDY

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caledon.ca/notices.

Meeting date: Tuesday October 5

Time: 6 p.m. | Followed by a question and answer period.

CONTACT

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Caledon, ON L7C 1J6
www.caledon.ca
T: 905.584.2272 | 1.888.225.3366 | F: 905.584.4325

Size: 1/2 Vertical

Color: YES

Date: September 9, 2021

Distribution: Caledon Enterprise

Department: Engineering & Capital Projects

Account#:02-09-265-20023-090-69001

Run X1

MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY

Mississauga Road to Creditview Road

THE STUDY

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Size: 1/2 Vertical

Color: YES

Date: September 16, 2021

Distribution: Caledon Enterprise

Department: Engineering & Capital Projects

Account#: 02-09-265-20023-090-69001

Run X1

Connor MacIsaac

From: Connor MacIsaac
Sent: September 17, 2021 2:54 PM
Cc: 'Shun Cheung'; Winnie Wong; Andrew McGregor (AMcGregor@rvanderson.com)
Subject: Town of Caledon: Mill Street Class EA - Notice of Online Public Information Centre
Attachments: MillStreetEA-PIC Notice-FINAL.pdf

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Notice of Online Public Information Centre (PIC) for the **Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA)**. The purpose of this meeting is to share information and receive public input on the to study date, including the problem / opportunity, existing conditions, alternative solutions, and the preliminary preferred solution. The meeting will be held on **October 5, 2021 at 6 p.m.**, and will consist of a presentation by the Town followed by a Question and Answer period concluding at 7 p.m.

Due to continuing efforts to contain the spread of COVID-19 and to protect individuals, this meeting will be held electronically. Refer to the attached notice, or visit the Town of Caledon's project website, <https://www.caledon.ca/en/news/mill-street-ea.aspx>, for further information on how to participate. The meeting link will also be posted to the project website closer to the meeting date. Questions can be submitted prior to the event to the contacts below.

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The project team values the participation of all stakeholders and wishes to ensure that the community's interests and concerns are taken into consideration. Please contact the project team members listed above if you require a hard copy of the online materials or for further information on this project.

If you would like to be removed from the project mailing list, please let us know by responding to this email.

Regards,



Connor MacIsaac, EPt

JUNIOR ENVIRONMENTAL PLANNER, EA & APPROVALS

☎ 905 685 5049 ext. 4218

📍 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



**MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD**

The Study

The Town of Caledon is considering options to enhance safety, improve traffic operations and better accommodate pedestrians and cyclists on Mill Street, to serve the community for years to come. The project is being completed as a **Schedule “B”** project in accordance with the *Municipal Class Environmental Assessment (October 2000, as amended in 2007, 2011 & 2015)*. As such, public and technical agency consultation will play a key role in developing the study recommendations.



Public Input

Preliminary recommendations were presented to the public for input at the first Public Information Centre (PIC), held online October 5, 2021. In consideration of feedback received during and following PIC #1, the project team has developed new alternatives for pedestrian accommodations along Mill Street.

A second PIC is being held to garner feedback on the options for pedestrian accommodations. The PIC will be held as an informal “Open House” with materials pertaining to the study on display, with members of the project team on hand to answer questions and discuss issues related to the project. Attendees will be asked to review the materials on display and provide their preferences for pedestrian accommodations. A final recommendation for Mill Street will be developed following the PIC, in consideration of the input received.

Public Information Centre #2:

Date: Wednesday June 15, 2022

Time: 5:30 pm to 7:00 pm

Location: Cheltenham Community Centre (14190 Creditview Rd, Caledon, ON L0P 1N0)
Entrance is located at the south side of the building

If you would like more information prior to the PIC, or to be added to the study mailing list, please contact one of the following members of the Project Team:

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure
Engineering Services
Tel: 905.584.2272 x 4040
E-mail: shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
Tel: 416.497.8600 x 1471
E-mail: winnie.wong@rvanderson.com

This notice was first issued May 26, 2022.

*Information collected will be used in accordance with the Freedom of Information and Protection Privacy Act. With the exception of personal information, all comments will become part of the public record

Affordability, climate: Future Majority

respond."

"In the Federal election last year, Future Majority was able to run some local youth events to remind them that election is coming up and they should register to vote and get out to vote. They did things like pledge drives, reminding people of the date they have to vote, the steps they have to take to vote, and also making clear the importance of voting for young people."

"I think a lot of people think their vote doesn't matter, they are uninterested or apathetic to the political process, but young people make up 30 per cent of Ontario's electorate and they are concentrated in key ridings that are needed to speak to [in order] to have a majority government, particularly in Mississauga, Brampton and Newmarket-Aurora."

To challenge the idea that the youth vote might not "matter," Santana says it isn't hard to find examples of action when "young people come together to push for change."

"It does actually influence policy platforms in subsequent elections," he says. "For example, in the last Federal election, young Canadians raised the issue of climate change over and over and now we're seeing all parties include climate action on their platforms. It is clear that when we show up to the poll in that regard we do hold some power to push for better and young people participating can influence elections [and] they can really make themselves heard among political leaders."

"I think for quite a while politics has been dominated by the middle-aged to older crowd who tend to show up to vote more, but politicians look for votes. If young people are voting, that is where their policies will shift to."

For more information on Future Majority, including how to get involved, visit futuremajority.ca.

Councillor Kiernan wants to see a community pool built in Ward 1

BY ZACHARY ROMAN

A Caledon councillor would like municipal staff to dive into research on the feasibility of building a community and therapeutic pool in Caledon's Ward 1.

Councillor Lynn Kiernan brought forward a motion at Council's May 17 General Committee meeting, asking that "staff be directed to investigate the feasibility of constructing and operating a community/therapeutic pool in Ward 1 and report back on potential locations, proposed budget and projected usership."

In her motion, she explains that the Caledon Central Pool was demolished in 2014 due to structural issues and mold growth. She added that a Northwest Caledon Indoor Recreation Facility Reserve Fund was created to meet community needs following the closure of the pool, and that \$1,038,350.16 is currently in that fund.

Kiernan's motion goes on to note Caledon's 2017 Facility Needs Assessment Study recommends one indoor aquatic centre per 25,000 population and that Caledon is expected to grow to 160,000 residents by 2041. It also notes that the Town's Age Friendly Caledon Plan says the Town will build, support and advocate for local programs that promote social participation with an emphasis on health and friendship.

Kiernan said her constituents have expressed a need and desire to have an indoor aquatic centre such as can be found in other areas of Caledon. She said Caledon community service staff have been seeing the same thing, reporting an increase in requests for therapeutic programs such as therapy classes in an aquatic setting.

Speaking to her motion at the meeting, Kiernan said she was very happy to have her motion come forward. She said even when she was campaigning to be Ward 1 area councillor in 2018, she heard the need for a pool from residents in her ward.

"It's been a big issue for the residents in Northwest Caledon, we have families that go to Orangeville and Georgetown to take their children to swimming lessons, the seniors have definitely lost their pool and really did miss some of that aquatherapy or aquafit," said Kiernan.

She said she put together a business plan in November of 2021 and met with Town Staff to discuss making the pool financially viable. She said she explored potential partnerships with parties such as Hills of Headwaters Collaborative (an Ontario Health Team), sports clubs, or private partners. Kiernan said she learned it's not about the cost of building the pool, rather the cost to maintain it. She explained she's not seeking a high-level athletic pool, rather a warm, accessible community pool that all Ward 1 residents can enjoy.

Councillor Ian Sinclair supported Kiernan's motion and said it was well-researched. He suggested a friendly amendment to it be proposed a concrete deadline for staff to report back by being added.

An amendment to Kiernan's motion to give Town staff a deadline of the third quarter of 2023 was passed unanimously. The motion itself was also passed unanimously.



Mayor Allan Thompson and Vaughan Mayor Maurizio Bevilacqua are pictured touting the GO plan earlier this year.

FILE PHOTO

Advisory firm to continue working for Caledon's GO line

Continued from FRONT

Councillor Nick deBoer put forward an amendment to the motion before it passed, a requirement to ask York Region and the City of Vaughan to share proportionately in the costs of the business case work. The motion was passed as amended.

Councillor Ian Sinclair said he had a number of questions about the Town's involvement in funding the business case for GO Train service.

"I don't understand the role of Metrolinx, do they not do anything? What do they do?" he asked.

"Caledon seems to be undertaking the technical work to justify the business plan for it... why on earth are we paying for Metrolinx's work? Surely that's their job to work on the business plan," said Sinclair.

He suggested getting in touch with Metrolinx before spending any money on the project.

Andrew Pearce, Caledon's Director of Engi-

neering services, said as the GO Rail is a strategic priority for the Town, staff have been doing all they can to advance it. Pearce said staff have been in touch with Metrolinx and that Metrolinx staff have a full workload; thus, the Caledon-Vaughan GO Rail is not an immediate priority for them at this time.

Pearce said Metrolinx's next regional transportation plan is expected in 2023 and the Town is trying to advance its case to be a priority in that plan.

He said everything he knows about York Region and City of Vaughan staff leads him to believe they will support Caledon in cost-sharing business case work.

"What we're suggesting is investing a bit now, to advance that delivery of the GO service," said Pearce.

He explained York Region and the City of Vaughan have expressed verbally their intent to support Caledon in the cost of required studies

for advancing the business case, such as a capacity analysis.

Councillor Tony Rosa said he'd like to see more than verbal intent, as he said the work that needs to be done will benefit both regions and municipalities.

Councillor Lynn Kiernan echoed his concerns.

A motion to refer the item back to staff until they could receive confirmation from York Region and the City of Vaughan that they'd be willing to cost-share was put forward and supported by councillors Annette Groves, Tony Rosa, Sinclair, and Christina Early.

However, the motion was lost as Mayor Allan Thompson and the rest of Caledon Council opposed it.

Caledon's Chief Administrative Officer, Carrey Herd, said Town staff believe the work on the business case is critical for Caledon's future and needs to be done as expeditiously as possible; and that's why Caledon is tak-

ing the lead.

Mayor Allan Thompson said he believes in the experts from StrategyCorp as they have experience in rail planning. "If we want to change the way... communities thrive, we're going to have to do a bit of investment here and these are the experts that know how to do this," he said.

Councillor Groves said she agreed with Sinclair, and that Metrolinx should be doing the planning work. "Why are we doing Metrolinx's work? We have an MPP here, she should really be advocating for the advancement of our GO train service," said Groves.

She also said she didn't like that the staff report requesting funding for the business case was added as an addendum to the May 17 meeting's agenda, and that residents who check committee meeting agendas when they are posted on Fridays may not have had a chance to read it and be involved in the decision-making process.



Development Charges Study

the municipality of the Town of Caledon if the Development Charges Act, 1997, as a municipality's proposed area-specific ground study. The area under the Bolton Parkway, it will be imposed on the Secondary Plan.

Members of Council participating in the public are invited to view the meeting by joining the Webex.

in person, via phone or Webex Meeting for email finance@caledon.ca to facilitate

TS: @caledon.ca

able to the public, the background study is

ound study are available from the Municipal

nts at the public meeting or in writing, e address prior to June 9, 2022.

PUBLIC NOTICE

Notice of Intention to pass amending bylaw to remove Holding Symbol (H)

FILE NUMBERS: RZ 2021-0016

Please be advised that the Town of Caledon is now offering a Hybrid Committee and Council Meeting model offering many ways for you to engage with Council, including in-person or virtual opportunities.

Earliest Planning and Development Committee Meeting Date: June 21, 2022
Earliest Town Council Meeting Date: June 28, 2022

HOW TO PARTICIPATE:

1. Attend In-Person: Members of the public may register to delegate in-person and speak to an item listed on the agenda or sit in the gallery to watch the meeting. To do so, please complete the Participation Request form on the Town's website.
2. Virtually: Members of the public are welcome to participate via phone or WebEx Meeting upon request. For more information on how to participate, please visit caledon.ca/agenda or contact the Planning Department. Please note that there are deadlines by which the request to participate need to be made.

APPLICANT:

The Altin Development Inc.

PROPOSED CHANGES:

The applicant has applied for a Zoning By-law Amendment (ZBA) of Holding Symbol ("H") to remove the Holding Symbol ("H") and "H10" from the subject lands zoned Village Commercial Exception 35 - Floodplain (CV-35-F-H) by Zoning By-law 87-200, as amended, and Village Commercial Exception 401 (CV-401-H) by Zoning By-law 2006-50, as amended. These site-specific zones and Holding Symbol restrictions were approved by Council in 2008 and the removal of the Holding Symbol ("H") and "H10" will permit the place of assembly, place of entertainment and restaurant uses on the subject lands.

LEAD PLANNER:

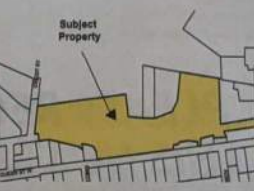
Adam Westland, MCP, CPP
Community Planning, Development and Design,
Planning Department
905-584-2272 ext. 4039
adam.westland@caledon.ca

ADDITIONAL INFORMATION:

For more information about this matter, including a copy of the proposed Zoning By-law Amendment application, additional information and material, please visit caledon.ca/openhouse or contact the Lead Planner at 905-584-2272 ext. 4039 from Monday to Friday 9:00 a.m. to 4:00 p.m.

WARD 1 LOCATION:

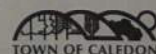
1402 Queen Street West
Part of Lot 23, Concession 4 WNS (Caledon) Part of Mill Privilege No. 4 in Block 5 on CALS (Village of Altin)
Part of Lot 23, Concession 4 WNS (Caledon) Part of Mill Privilege No. 5 in Block 5 on CALS (Village of Altin)
West side of Amelia Street, north of Queen Street West and east of Credit Street



ACCESSIBILITY:

If you require an accessibility accommodation to attend or participate in these meetings, or to access any materials related to this item in an alternate format please contact Legislative Services by phone at 905-584-2272 x286 or via email at legislative.services@caledon.ca. Requests should be submitted at least 10 days before the meeting.

Notice Date: May 26, 2022



6211 Old Church Road
Caledon, ON L7C 1J6
caledon.ca
T. 905.584.2272 | 1.888.225.2366 | F. 905.584.4325

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Caledon, ON L7C 1J6
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T. 905.584.2272 | 1.888.225.2366 | F. 905.584.4325

PUBLIC NOTICE

NOTICE OF 2ND PUBLIC INFORMATION CENTRE

Mill street class environmental assessment study Mississauga road to Creditview road

THE STUDY

The Town of Caledon is considering options to enhance safety, improve traffic operations and better accommodate pedestrians and cyclists on Mill Street, to serve the community for years to come. The project is being completed as a Schedule "B" project in accordance with the Municipal Class Environmental Assessment (October 2000), as amended in 2007, 2011 & 2015). Public and technical agency consultation will play a key role in developing the study recommendations.



PUBLIC INPUT

Preliminary recommendations were presented to the public for input at the first Public Information Centre (PIC), held online October 5, 2021. In consideration of feedback received during and following PIC #1, the project team has developed new alternatives for pedestrian accommodations along Mill Street.

A second PIC is being held to gather feedback on the options for pedestrian accommodations. The PIC will be held as an informal "Open House" with materials pertaining to the study on display, with members of the project team on hand to answer questions and discuss issues related to the project. Attendees will be asked to review the materials on display and provide their preferences for pedestrian accommodations. A final recommendation for Mill Street will be developed following the PIC, in consideration of the input received.

PUBLIC INFORMATION CENTRE #2:

Date: Wednesday June 15, 2022
Time: 5:30 to 7 p.m.
Location: Cheltenham Community Centre
(14190 Creditview Rd., Caledon, ON L7P 1N0)
Entrance is located at the south side of the building

If you would like more information prior to the PIC, or to be added to the study mailing list, please contact one of the following members of the project team:

TOWN OF CALEDON
Shun H. Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure
Engineering Services
Tel: 905.584.2272 x4040
E-mail: shun.cheung@caledon.ca

R.V. ANDERSON ASSOCIATES LIMITED
Winnie Wong, P.Eng., PMP, M.Eng.
Project Manager
Tel: 416.497.8800 x1471
E-mail: winnie.wong@randerson.com

This notice was first issued May 26, 2022.



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Caledon, ON L7C 1J6
caledon.ca
T. 905.584.2272 | 1.888.225.2366 | F. 905.584.4325

Town of Caledon e-News (June
2, 2022)

No images? [Click here](#)



Celebrate Caledon

This free community event brings together the best of the beloved Caledon Day and TRCA's Caledon Canada Day events. Newly named 'Celebrate Caledon' organizers say it will provide an opportunity to celebrate as a community again. Visit [Celebrate Caledon](#) for details.



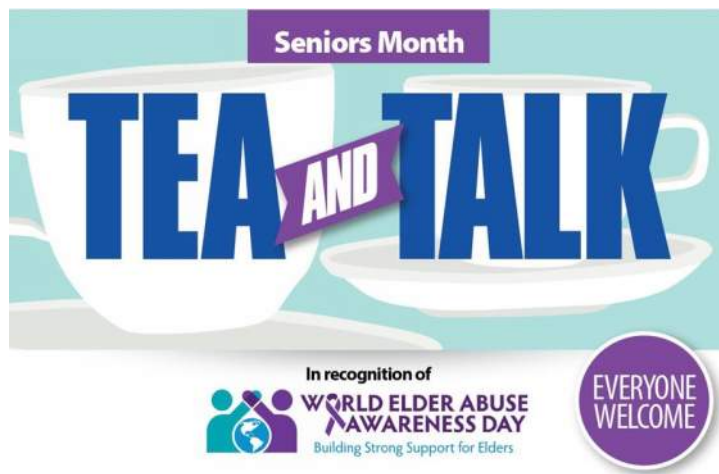
June is Pride Month

The Town of Caledon raised the Progress Pride Flag that celebrates the diversity and history of the LGBTQ2SIA+ community with the addition of black, brown, pink, light blue and white stripes to represent people of colour, Transgender, Gender Non-Binary, Intersex and the gender spectrum.



Summer Recreation Guide is now Available

Our [Summer Registration Guide](#) is here! Having summer fun, learning a new skill and staying active has never been more affordable through programs and classes offered by Caledon Recreation. Browse and [register](#) for a Program.



Tea and Talk

In recognition of World Elder Abuse Awareness day

June 15, 2022: 12-3 p.m.

Albion Bolton Community Centre, Auditorium

12 p.m. Welcome

12:30 p.m. Presentations by local community services organizations

1 p.m. Webinar with Elder Abuse Prevention Ontario

2:30 p.m. Tea & Talk Giveaway

Registration is open at caledon.ca/adult55 or by calling 311

caledon.ca/adult55



Mill Street

Class Environmental Assessment Study

Mississauga Road to Creditview Road

Public Information Centre #2:

Date: Wednesday June 15, 2022
Time: 5:30 to 7 p.m.
Location: Cheltenham Community Centre
(14190 Creditview Rd, Caledon, ON L0P 1N0)

Entrance is located at the south side
of the building

[Details](#)



Forward

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6311 Old Church Road | Caledon ON L7C 1J6
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Town of Caledon e-News (May
27, 2022)

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Bike Month

June is Bike Month and the opportunity to explore the many benefits of biking using the network of bike routes throughout Caledon.

Caledon is a well-known destination for cyclists, with [both recreational trails and challenging road routes](#). Explore our many trails during Bike Month and get active while cycling in Caledon.

Learn more about [Bike Month](#)



Mill Street

Class Environmental Assessment Study

Mississauga Road to Creditview Road

Public Information Centre #2:

Date: Wednesday June 15, 2022
Time: 5:30 p.m. to 7 p.m.
Location: Cheltenham Community Centre
(14190 Creditview Rd, Caledon, ON L0P 1N0)

Entrance is located at the south side
of the building

[Details](#)



Spongy Moth Caterpillars

Here are three things you can do to manage spongy moth caterpillars on your property:

1. Add a burlap band to sticky barrier to your tree

FREE burlap kits with a [how-to-guide](#) are available for pick-up (while supplies last) at: Town Hall, Southfields Community Centre, Caledon Recreational & Wellness Centre, Mayfield Community Centre, Caledon Public Library - Alton Branch and Caledon Public Library - Caledon Village Branch.

2. Contact a professional tree care provider for the possibility of a biopesticide treatment.

If you have several trees or a woodlot on your property, view our [Spongy Moth Caterpillar 2022 Management FAQ](#) to learn what you should consider when hiring a contractor for large-scale spongy moth management and questions to ask before hiring a licensed tree provider.

3. Report spongy moth sightings on public and private property using our [Spongy Moth Tracker Form](#).

caledon.ca/spongy



June is Seniors Month

Visit caledon.ca/adult55 and get a free swag bag with

- a calendar of events,
- promotional items, and
- information from local service providers.

caledon.ca/adult55



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Connor MacIsaac

From: Connor MacIsaac
Sent: May 30, 2022 10:57 AM
Cc: Shun Cheung; Winnie Wong
Subject: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2
Attachments: Mill.St.PIC.No.2.Notice.pdf

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Public Information Centre (PIC) #2 for the **Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA)**. The purpose of this meeting is garner feedback on the options for pedestrian accommodations developed for Mill Street in consideration of the feedback received during and following PIC #1, held online October 5, 2021.

The PIC will be held as an informal "Open House" with materials pertaining to the study on display, with members of the project team on hand to answer questions and discuss issues related to the project. Attendees will be asked to review the materials on display and provide their preferences for pedestrian accommodations. A final recommendation for Mill Street will be developed following the PIC, in consideration of the input received.

The details of the PIC are below:

Date: Wednesday June 15, 2022
Time: 5:30 to 7:00 pm
Location: Cheltenham Community Centre - 14190 Creditview Rd, Caledon, ON L0P 1N0
Entrance is located at the south side of the building

The project team values the participation of all stakeholders and wishes to ensure that the community's interests and concerns are taken into consideration. Please contact the project team members listed below if you require a hard copy of the materials or for further information on this project.

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure Engineering Services
905.584.2272 X.4040
shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
416.497.8600 X.1471
winnie.wong@rvanderson.com

If you would like to be removed from the project mailing list, please let us know by responding to this email.

Regards,



Connor MacIsaac, ENV SP, EPT
ENVIRONMENTAL PLANNER, EA & APPROVALS

☎ 905 685 5049 ext. 4218

APPENDIX 1-2

Technical Agency and Stakeholder List



Mill Street Class EA
Technical Agency Stakeholder Contact List

Agency	Contact	Title	Date Added to List	Removed from List	Notice of Study (date sent)	Notice of PIC1 (date sent)	Notice of PIC2 (date sent)	Notice of Study Recommendations (date Sent)
Provincial Ministries, Agencies and Departments								
Ministry of the Environment, Conservation and Parks (MECP)	Central Region							
Ministry of the Environment, Conservation and Parks (MECP)	General (Notices)							
Ministry of the Environment, Conservation and Parks (MECP)	Trevor Bell	Environmental Planner/Environmental Assessment Coordinator				2021-09-17	2022-05-30	2023-01-12
Ministry of Citizenship and Multiculturalism (MCM)	Karla Barboza	Team Lead(A), Heritage Heritage Planning Unit			2020-10-10	2021-09-17	2022-05-30	2023-01-12
Ministry of Citizenship and Multiculturalism (MCM)	Dan Minkin	Heritage Planner (Culture Services Unit)			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Ministry of Citizenship and Multiculturalism (MCM)	Joseph Harvey	Heritage Planner			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Infrastructure Ontario	Frank Dieterman				22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Ministry of Transportation	Jason White	Manager			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Ministry of Natural Resources and Forestry (MNRF)	Steven Strong	District Planner - Aurora District			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Ministry of Indigenous Affairs		Consultation Unit			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Ministry of Aboriginal Affairs	Camille Assam	Executive Assistant to ADM			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Environmental Assessment and Permissions Branch		Director			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Federal Agencies								
Fisheries and Oceans Canada					22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Environment Canada	Rob Dobos	Manager, Environmental Assessment Section			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
GO Transit	Elise Croll	Director, Environmental Programs and Assessments		12-Jan-23	22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Municipal Departments, Committees and Services								
Region of Peel	Joe Avsec	Manager, Engineering and Infrastructure Planning			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Region of Peel	Andrew Farr	Commisioner of Public Works		12-Jan-23	22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Region of Peel	Sean Carrick	Manager - Transportation			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Region of Peel	Neal Smith	PM - Sustainable Transportation			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Region of Peel	Sally Rok	Project Manager, Transportation Division			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Region of Peel	Asha Saddi	Technical Analyst, Infrastructure Programming	2022-05-30					2023-01-12
Caledon Fire & Emergency	Dave Peyalo	Fire Chief			22-Oct-20	2021-09-17	2022-05-30	2023-01-12

Mill Street Class EA
Technical Agency Stakeholder Contact List

Agency	Contact	Title	Date Added to List	Removed from List	Notice of Study (date sent)	Notice of PIC1 (date sent)	Notice of PIC2 (date sent)	Notice of Study Recommendations (date Sent)
Town of Caledon	Ryan Grodecki	Manager of Engineering			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Town of Caledon	Kant Chawla	Senior Transportation Planner, Development		12-Jan-23	22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Town of Caledon	Arash Olia	Engineering, Finance &			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Town of Caledon - Heritage Committee	Sally Drummond	Heritage Resource Officer			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
School Boards								
Peel District School Board	Stan Cameron	Trustee - Caledon			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Peel District School Board	Suzanne Blakeman	Manager, Planning & Enrolement / Planning &			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Peel District School Board	Bianca MV Bielski	Manager, Planning & Enrolement			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Dufferin-Peel Catholic District School Board	Frank Di Cosola	Trustee - Caledon/Dufferin			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Dufferin-Peel Catholic District School Board	Krystina Koops	Planner			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Conservation Authority								
Credit Valley Conservation Authority (CVC)	Tyler Slaght	Regulations Officer				2021-09-17	2022-05-30	2023-01-12
Niagara Escarpment Commission	Joe Muller (NDMNRF) Haleema Sarwar	Senior Strategic Advisor				2021-09-17	2022-05-30	2023-01-12
Utilities								
TransCanada Pipelines Inc.	Darlene Quilty	Project & Planning Coordinator			22-Oct-20	2021-09-17	2022-05-30	
Hydro One Networks					22-Oct-20	2021-09-17	2022-05-30	
Hydro One Networks	Matt Casey				22-Oct-20	2021-09-17	2022-05-30	
Hydro One Caledon		Planning and Scheduling Department			22-Oct-20	2021-09-17	2022-05-30	
Hydro One Caledon					22-Oct-20	2021-09-17	2022-05-30	
Bell Canada	Ahmed Saeed	Bell Implementation Mgr.			22-Oct-20	2021-09-17	2022-05-30	
Bell Canada	Diego Tobias	GT Fiber Services Inc.			22-Oct-20	2021-09-17	2022-05-30	
Bell Canada Municipal Operations Centre	Diana Velez	c/o Netricom			22-Oct-20	2021-09-17	2022-05-30	
Cogeco Cable Systems Inc.	Craig Krueger	System Planner			22-Oct-20	2021-09-17	2022-05-30	
Rogers	Roy Holland				22-Oct-20	2021-09-17	2022-05-30	
Rogers	Graham McPherson	System Planner (Bloton Area)			22-Oct-20	2021-09-17	2022-05-30	
Rogers Telecon					22-Oct-20	2021-09-17	2022-05-30	
Rogers Cable TV Limited	Marian Wright	Planning Coordinator			22-Oct-20	2021-09-17	2022-05-30	
Rogers Cable TV Limited	Travis Graham	Municipal & Utility Relations GTA West			22-Oct-20	2021-09-17	2022-05-30	
Rogers Cable	Richard Humpage	EA Coordinator - Region Representative East of Georgetownville Creek Road			22-Oct-20	2021-09-17	2022-05-30	

Mill Street Class EA
Technical Agency Stakeholder Contact List

Agency	Contact	Title	Date Added to List	Removed from List	Notice of Study (date sent)	Notice of PIC1 (date sent)	Notice of PIC2 (date sent)	Notice of Study Recommendations (date Sent)
Rogers Cable					22-Oct-20	2021-09-17	2022-05-30	
Enbridge Gas Distribution Incorporated	Emilio Labra	Advisor Planning – Central Region West			22-Oct-20	2021-09-17	2022-05-30	
Enbridge Gas Distribution Incorporated	Meetpal Chhina	Planning Technical Specialist			22-Oct-20	2021-09-17	2022-05-30	
Enbridge Gas Distribution Incorporated					22-Oct-20	2021-09-17	2022-05-30	
Alectra	Paul Sidhu	Senior Manager, Systems Planning			22-Oct-20	2021-09-17	2022-05-30	
Student Transportation of Peel Region	Marcy Macina	Operations Officer, East Area		2023-01-12	22-Oct-20	2021-09-17	2022-05-30	
MTS Allstream	Ian Fleming	EA Coordinator			22-Oct-20	2021-09-17	2022-05-30	
Indigenous Groups								
Metis Nation of Ontario					22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Metis Nation of Ontario	Justin Hunt	Consultation Advisor	21-Jan-21			2021-09-17	2022-05-30	2023-01-12
Mississaugas of the Credit First Nation	Mark LaForme	Director, Department of Consultation and Accommodation	2022-05-27				2022-05-30	2023-01-12
Mississaugas of the Credit First Nation	Adam LaForme	Archaeological Operations Supervisor	2022-05-27				2022-05-30	2023-01-12
Mississaugas of the Credit First Nation	Abby LaForme	Acting Consultation Coordinator	2022-05-27				2022-05-30	2023-01-12
Six Nations of the Grand River	Lonny Bomberry	Lands & Resource Director		12-Jan-23	22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Six Nations of the Grand River	Robbin Vanstone	Land Use Office, Lands and Research				2021-09-17	2022-05-30	2023-01-12
The Haudenosaunee Confederacy	Haudenosaunee Development Institute	General Contact					2022-05-30	2023-01-12
Huron-Wendat Nation					22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Huron-Wendat Nation	Marie-Sophie Gendron	Archealogy Analyst					2022-05-30	2023-01-12
Huron-Wendat Nation	Maxime Picard	Coordinateur de projets - Ontario			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Elected Officials								
Ontario Government	Sylvia Jones	MPP - Dufferin Caledon			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Federal Government	Kyle Seeback	MP - Dufferin Caledon			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Town of Caledon	Allan Thompson	Mayor			22-Oct-20	2021-09-17	2022-05-30	2023-01-12

Mill Street Class EA
Technical Agency Stakeholder Contact List

Updated: 2023-02-27

Agency	Contact	Title	Date Added to List	Removed from List	Notice of Study (date sent)	Notice of PIC1 (date sent)	Notice of PIC2 (date sent)	Notice of Study Recommendations (date Sent)
Town of Caledon	Ian Sinclair	Ward 1 Regional Councillor			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Town of Caledon	Lynn Kiernan	Ward 1 Area Councillor			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Town of Caledon	Johanna Downey	Ward 2 Regional Councillor			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Town of Caledon	Christina Early	Ward 2 Area Councillor			22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Other								
Caledon Cycling Club					22-Oct-20	2021-09-17	2022-05-30	2023-01-12
Cheltenham Area Residents' Association								

APPENDIX 1-3

Residents and General Public Consultation



Community Outreach Meeting Summary Report





Mill Street (Mississauga Road to Creditview Road) Municipal Class Environmental Assessment

Community Outreach Meeting Summary Report

Town of Caledon

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RVA 205388

July 21, 2021

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1.0 INTRODUCTION

In October 2020, as part of the 2022 Growth Related Roads project, the Town of Caledon initiated a Schedule “B” Municipal Class Environmental Assessment (Class EA) for improvements to Mill Street between Mississauga Road and Creditview Road. The purpose of the Study is to identify the required improvements to the roadway including considerations for pedestrians, cyclist activities and stormwater drainage. In identifying improvements to the corridor, the Study is considering technical, aesthetic, socio-economic, cultural heritage, and natural environment factors.

R.V. Anderson Associates Limited (RVA) was retained by the Town to complete the Class Environmental Assessment (EA) for this project. This is a Schedule “B” project that covers phases 1 and 2 of the planning process, in accordance with the *Municipal Class Environmental Assessment (October 2000, amended in 2007, 2011 & 2015)*. As part of the Class EA planning and design process, extensive public and technical agency consultation is required in developing the Study recommendations. In addition to this Community Outreach Meeting, one (1) Public Information Centre (PIC) will also be held as part of the Study.

1.1 Community Outreach Meeting

As part of the alternative solutions development for the Mill Street corridor, the Town held a Community Outreach Meeting to consult with area residents on May 27, 2021, from 6 p.m. to 7 p.m. The meeting was attended by both Town staff and RVA’s project team. Residents were invited to participate in the Community Outreach Meeting by calling in via phone, or by registering for the live WebEx event.

The Community Outreach Meeting included an approximately 20-minute presentation on the MCEA process, existing conditions, consultation completed to date, and next steps, followed by an approximately 40-minute question and answer period.

Residents were encouraged to submit their questions using the WebEx Q & A function, or by using the “Raise Your Hand” feature, to ask a question directly to the project team. Questions submitted using the Q & A feature were read aloud to the project team by an RVA moderator and answered during the meeting by the RVA technical staff. Residents who called in were also able to ask questions to the project team directly.

Following the Community Outreach Meeting, the recording of the meeting and the presentation slides were available for review and comment until June 11, 2021 on the Town’s website.

Materials posted to the project website were available in an alternate format (i.e., hard copy) upon request, however no requests were received.

Refer to **Appendix 2** for copies of the materials made available for comment during the Community Outreach Meeting.

2.0 STAKEHOLDERS AND NOTIFICATION

In accordance with the MCEA, notification to the public and stakeholders is required well in advance for public consultation. Notification of the PIC was provided through various methods and media, as outlined below.

- **General Public:**
 - Notice posted on the Town's website on February 8, 2021
 - Notice published in *Caledon Citizen* (local newspaper) on February 11, 2021 and February 18, 2021
 - Posts on social media (Facebook and Twitter) leading up to the PIC
- **Local Residents & Businesses:**
 - Notice mailed to all property owners within the Study area
- **Technical Agencies, Local Interest Groups and First Nations Communities:**
 - Emails containing the notice and details of the PIC were sent on February 8, 2021
- **Project Mailing List (other parties who previously submitted comments or indicated interest in the project)**
 - Email containing the notice and details of the PIC was sent on February 8, 2021

Refer to **Appendix 1** for copies of the published notifications and stakeholder contact list.

3.0 CONSULTATION MATERIAL

The purpose of the Community Outreach Meeting was to introduce the project to the public and share and receive input from the public as part of the development of alternative solutions to be considered for the Mill Street corridor.

The display boards presented at the Community Outreach Meeting are included in **Appendix 2**. The display materials presented information on the following:

- Study Area & Project Background
- Municipal Class EA Process & Study Organization
- Consultation Completed to Date
- Existing Conditions
- Next Steps

Copies of the display boards were available on the project website, and by alternative format (i.e., Hard copy) by request, however no requests for hard copy materials were received.

4.0 PARTICIPATION AND COMMENT SUMMARY

4.1 Comments and Questions Received – During Community Outreach Meeting

A total of 77 comments and questions were received through the Q & A function, with one resident providing their comments / question directly to the project team using the “Raise Your Hand” feature. Several common categories were identified within the comments provided, including concerns related to vehicle operations & safety, property impacts and maintenance, active transportation accommodation & safety, cultural heritage & village character, natural environmental impacts including drainage, the Class EA process and utility impacts. Comments / questions received are summarized in the table below, by these categories, in no particular order.

COMMENT / QUESTION CATEGORY	COMMENT / QUESTION RECEIVED	ANSWER TO QUESTION / CONSIDERATION IN CLASS EA
Vehicle Operations & Safety	What is the width of the existing right-of-way?	Existing road allowance (or right-of-way) varies between 10.5m to 26.5m, this is different than what was said at the meeting but confirmed by checking the property boundaries after the meeting.
	You say the curve is too tight for a 40km/h speed. I understand that you are considering expropriations. Is it not more cost effective to lower the speed limit to match the curve rather than maintain the speed and re-engineer the curve?	<p>The design approach aims to minimize property impacts.</p> <p>The operating speeds of vehicles are typically more a function of road design, and less the posted speed limit. Road curves can be a form of traffic calming measure; however, they still need to meet design requirements necessary to maintain an acceptable level of safety for all road users in the vicinity of the curves.</p> <p>RVA is currently undertaking a Traffic Study to evaluate current operating speeds along Mill Street and through the curves, which will help inform the design of proposed solutions.</p>
	What posted speed limit would the curve currently allow ?	The current curve is designed to safely accommodate operating speeds well below 40km/h.
	What does your research indicate is the expected impact on traffic speeds and volumes of the proposed widening of the road?	The operating speeds of vehicles are typically more a function of road design, and less the posted speed limit. Road curves can be a form of traffic calming measure; however, they still

	need to meet design requirements necessary to maintain an acceptable level of safety for all road users in the vicinity of the curves.
What is the expected impact of improving/standardizing the road width with respect to diverting traffic off of Mississauga Rd. i.e. creating a route to by-pass the Miss Rd./King intersection?	One of the Study's objectives is to improve road user safety and traffic operations on Mill Street for existing users and potential future growth. Mill Street is a public road and provides a functional link between Mississauga Road and Creditview Road.
The observational Study was conducted in October 2020, a peak time for pedestrian traffic (fall colours) and additional pandemic traffic. How will this be adjusted in your evaluations?	Further studies of traffic operations will be undertaken in Summer 2021.
Do current design standards require curbs?	The current Town Road Design Standard (Standard No. 23) includes curb and gutter for local urban/ rural roads with 20m right-of-way; however, we recognize the impact on adjacent properties. Therefore careful consideration is required to determine if appropriate.
You mentioned there are certain areas of the road that are hazardous is there ever been a thought of dealing with those hazardous areas making them safer instead of making it a standard through the whole Road	Currently the road does not meet the Town's or TAC design standards and is considered unsafe. The Study will consider alternatives to accommodate pedestrian, cyclist, and vehicular traffic.
Why consider traffic calming measures that don't work?	The Study will review a variety of traffic calming options for Mill Street which have been shown to be effective based on previous studies.
Have you considered traffic calming such as sections reduced to 1 lane as is done in many places in Ontario?	Traffic calming will be considered in developing the alternative solutions for the corridor.

We live right on the "s" curve. There is absolutely no problem passing two cars on any section of the road.	The existing road width (6m) does not meet the Town or TAC (Transportation Association of Canada) design standards for a two lane road (7.9m to 8.9m wide). There are also no facilities to accommodate pedestrian and cyclist traffic.
Is it correct to say that the impact of sightlines and road width are proportionate to the speed of the traffic. If so, wouldn't slowing the traffic mitigate the concerns significantly?	Sight distance requirements varies with the posted speed of the road based on TAC. Sight distance available is dependent on the road geometry and surrounding environment. The Study will include review of various traffic calming measures.
You mentioned the current road width is 6 metres. What is the TAC standard road width?	TAC standard road width for roads similar to Mill Street is 6.6m to 7.4m. Town's standard road width for roads similar to Mill Street is 7.9m to 8.9m.
People park along the road and have picnics, launch canoes etc. They are parked for long periods of time.	Noted.
What are traffic operation improvements? Is that the lingo for accommodating the snow-plows?	Traffic operation improvements can include traffic signs, pavement markings, traffic calming measures, and roadside safety improvements.
There was a comment that safety is priority 1. I look at the widening of Creditview to the point that you cannot park a vehicle at the General Store without blocking the sidewalk or sticking out into the road. This same issue is prevalent in the new section of Valleywood subdivision. How is that making safety a priority.	Based on site observations there is enough space in front of the General Store for vehicles to parallel park without parking on the sidewalk. This Study focuses on Mill Street between Mississauga Road and Creditview Road. Issues with the store parking is noted for Town's future Study.

Property
Impacts and
Maintenance

What is the current property allowance from the road to property lines? What is the worst case scenario for expropriation?	Distance between edge of road to property line varies along the road. Existing road allowance (or right-of-way) is typically 13m and varies between 10.5m to 26.5m. Currently we are gathering feedback from public before developing alternative solutions. Before property impacts are identified, alternative solutions need to be considered. The goal is to minimize property impact. Owners whose property that are impacted by the preferred alternative, subsequently selected after the PIC, will be consulted with at future meetings to review the alternative.
In order for you to create a legal size road with 2 lanes and some sort of walking area, you will have to take property and cut trees, it is ignorant for the town to think otherwise..	Our design approach is to minimize property and tree impacts as much as possible.
What is the proposed road allowance?	Town standard for local rural/ urban road has a road allowance of 20m. Existing road allowance is narrower than that in some sections of the road. Our design approach is to maintain the improvement works as much as possible within the existing road allowance.
Who will be responsible for snow clearing of the proposed sidewalks?	Town to provide comment.
The majority of homeowners are 55+ and even 65+ on Mill St. adding a concrete sidewalk will burden the homeowners during in the winter months to remove snow from sidewalks. Some of this	Town to provide comment.

	road includes farmland. It is unreasonable to expect residents to manage the snow removal.	
	Is the Town expecting the respective resident to maintain the sidewalks (winter snow-ice clearing) or is the Town budgeting to provide services?	Town to provide comment.
	Are you taking into consideration the negative impact on residents regarding being responsible to shovel sidewalks they don't want, especially significant lengths of sidewalks	Sidewalk maintenance will be one of the evaluation criteria for all the alternatives considered in the Study.
	What will you do to ensure all potentially impacted property owners are consulted? We asked for a meeting and received no response	After this Community Outreach Meeting, there will be a Public Information Center (PIC) scheduled for Summer/ Fall 2021. Public feedback and comments can be submitted anytime throughout the Study. Owners whose property may be impacted by the preferred solution, selected following the PIC, will be invited to subsequent meetings to review the specifics of the preferred solution.
Active Transportation Accommodation	Regarding sidewalks, there is not sidewalk on Creditview or Mississauga rd? Why mill st which is a narrower rd rarely used by anyone other than residents?	With the existing sidewalk along Creditview Road (west side starting at Mill Street intersection and continuing north) and the Caledon Trailway Path, the Town requires safe pedestrian access to and from the trail from Creditview Road.
	Could a sidewalk only in the dangerous zones such as blind spots demonstrated be considered. i.e. from the top of the hill to the dam? The rest of the road once to standard would be suitable.	The Study is still in the early steps of gathering feedback from the community to develop alternatives. A sidewalk is one of the alternatives for pedestrian facility. Pedestrian access needs to be continuous to provide connectivity and safe passage throughout the corridor and not force pedestrians onto the road

Similar roads in Cheltenham do not have sidewalks ie Kennedy road and Credit road...please explain the need for a sidewalk on Mill where there is less pedestrian traffic than the other roads	With the existing sidewalk along Creditview Road (west side starting at Mill Street intersection and continuing north) and the Caledon Trailway Path, the Town requires safe pedestrian access to and from the trail from Creditview Road.
Presenter noted 'pedestrian traffic' concerns. How many resident complaints?	No resident complaints were received so far in the Study. Some verbal comments received include difficulty in seeing on-coming cars at the "S" curve. With the existing sidewalk along Creditview Road and the Caledon Trailway Path, the Town requires safe pedestrian access to and from the trail from Creditview Road .
We walk our dogs on this road. Putting in sidewalks will actually increase traffic.	Currently the Study has no preferred solution as we are still in the early stages of gathering feedback from the community. Sidewalks and other dedicated pedestrian facilities aim to improve safety for pedestrian traffic.
If a sidewalk is your preferred solution, is it expected to have a boulevard between the road and sidewalk or will the sidewalk be at the curb? What is the mandatory width of a sidewalk?	Currently the Study has no preferred solution as we are still in the early stages gathering feedback from the community. Sidewalk can be separated from the curb with a boulevard or placed next to the curb. Minimum sidewalk width is 1.5m per the Ontario Traffic Manual and AODA standard.
In talking about pedestrians, the vast majority are residents of Mill Street, not the public	Mill Street is a public road that is open for access to residents within and outside of the Cheltenham community.
It is a safe street that I let my children ride their bikes on and i have never felt they were in danger as there is barely any traffic	Noted.

	I prefer paved shoulder to minimize the long-term costs to the town which minimizes taxes to homeowners.	The provision of a paved shoulder (as opposed to a sidewalk) to accommodate pedestrians and cyclists will be considered in the development and evaluation of the alternative solutions
	Why are people using mill street go to the trail when the trail has parking lots everywhere	Noted.
	There is no reason to encourage pedestrians down a residential street by installing a sidewalk when visitors come here to use the trail not the road.	Mill Street is a public road that is open for access to residents within and outside of the Cheltenham community. With the existing sidewalk along Creditview Road (west side starting at Mill Street intersection and continuing north) and the Caledon Trailway Path, the Town requires safe pedestrian access to and from the trail from Creditview Road.
	What would need to be done to the corner space at the corner of Mill st and the Caledon Trail. You mentioned adding a guardrail, what changes would that entail?	This section of road will require a new guiderail which will be installed in the shoulder/ boulevard area.
Cultural Heritage / Village Character	There are a number of comments from the presenters about "standards". When you talk about integrating the plan with the character of the village, what does that actually mean?	To integrate the plan with the character of the village, our design approach will aim to minimize tree removals and property impacts while improving road safety for pedestrian, cyclist, and vehicular traffic.
	We moved from the city to the country for country living and DO NOT want this to become a main rd for tourists	Noted.
	The statement of "It is a lovely corridor" speaks to the manner in which the engineers view the village. It is a community.	We recognize Mill Street is a unique community within Cheltenham.
Natural Environment	Would CVC and NE allow for the ditch to have a sidewalk over top of it with a culvert for drainage?	Mill Street design will be submitted to CVC and NEC for review and comment. Currently we are still

Including Drainage		gathering feedback from the community and have not developed alternatives.
	ok so if the road is too narrow now, and our properties are almost at the road, then you will be taking property and removing trees to improve.	Our design approach will aim to minimize tree removals and property impacts.
	You'll have to remove trees as they are close to the road. a lot of them	Our design approach will aim to minimize tree removals and property impacts.
Class EA Process	You have identified safety and traffic operations for all users as your mandate. How have you broken down each the factors within those categories, and how are these ranked within your evaluation process?	More details on the evaluation criteria and process will be available during the Public Information Center, scheduled for 2021 Summer/ Fall.
	A project letter does not allow us to question / comment on how we may be specifically impacted. We asked for a meeting. We want to be heard before all alternate proposals are finalized. WE don't want to only be notified once decisions are already made.	<p>The Notice of Study Commencement was intended to notify residents of the upcoming Mill Street Study and included contact information where residents and public can submit their comments.</p> <p>The Notice of Community Outreach Meeting was intended to invite all residents to this meeting as a forum for initial comments and feedback. The comment sheet for the public to comment on this Community Outreach Meeting can be found on the project website https://www.caledon.ca/en/news/mill-street-ea.aspx, link was provided at the beginning of the meeting.</p> <p>At any time of the Study, residents are welcomed to submit their comments and questions to the contact persons listed on the project website above.</p>
	Why has the Town not directly contacted each property owner on Mill Street? Why, in such a small community, am I only learning	Sidewalk is one of the pedestrian facilities that can be considered on Mill Street. The Study is

<p>about sidewalks on Creditview tonight? Basic concerns about the lack of communication.</p>	<p>at the early stages of collecting feedback from the community and no alternatives have been developed or selected yet. The purpose of this Community Outreach Meeting is to gather comments from the residents. The comments received will help develop and evaluate the alternative solutions, all of which will be presented at a Public Information Center scheduled for 2021 Summer/ Fall. In the meantime, the public can also submit their comments regarding the Mill Street EA Study to the contact persons listed on the Town's project website: https://www.caledon.ca/en/news/mill-street-ea.aspx. More information on the project background and Study updates can also be found here. Link was provided at the beginning of the meeting.</p>
<p>As a landowner of approximately 80 acres of land that is presently zoned residential with frontage both on Mill St. & Creditview Rd please ensure that the road will meet future change. Change will happen in the future, just not in the current plan.</p>	<p>Study will take into consideration known future plans.</p>
<p>What is the procedure to get the resident's needs to take precedence?</p>	<p>Consultation with the public is an important component of all EA Studies. The intent of the Municipal Class EA is to develop a balanced alternative that considers engineering and traffic needs, as well as impacts to the adjacent social, natural and cultural heritage environments. Due to the heightened level of interest shown from the residents along the corridor, additional focus has been allocated to obtaining input from the</p>

		residents prior to developing the alternative solutions for the corridor.
Utility Impacts	What will happen with the hydro poles? Will these be pushed further back toward resident homes? Will these be buried?	The Study is in its early stages and we are still gathering feedback from the community. No alternatives have been developed yet at this point. Our design approach will aim to minimize utility impacts but utilities may be relocated if required.
	What are you proposing for the hydro poles and fire hydrants	We are still gathering feedback from the community and have not developed alternatives yet. Our design approach will aim to minimize utility impacts. Utilities may be relocated if required.
General Comments / Other	Is it possible to make all of the submitted questions visible to everyone on the call? We currently can only see 1 question	All questions and comments submitted during and after the Community Outreach Meeting will be posted on the Town's website. All consultation correspondence during the Study, included these questions and responses, will be documented included in the Mill Street EA Study Project File, which will be submitted for public review and comment following the selection of the preferred design for the corridor.
	What is the % of roads in Caledon that you repave & fix without doing the 'upgrade' - my perception is that there are many roads that are repaved without doing this 'upgrade'. When is Kennedy Road going to be paved? You mentioned Creditview but not Kennedy itself & they were repaved around 3 years ago!	Currently Mill Street does not meet Town or TAC design standards and when it is rehabilitated, safety improvements need to be considered. Town to provide comment re paving schedules.

	Most of us only wanted the town to fix the road, why does this have to be a huge process. The only major structural problem is the creek adjacent to the trail	Noted and we will review the creek area adjacent to the trail as part of our design.
	Trespassing on private property is a common problem along with garbage left behind.	Town to provide comment.
	My family has been in Cheltenham since 1825	Noted.

4.2 Additional Questions and Comments Received

Following the Community Outreach Meeting, a video recording of the meeting and a comment form was posted to the Town's project website. Members of the public were invited to review the recording and submit their comments and/or questions to the project team by completing the comment form, or by providing their comments to the project team directly via mail e-mail, and/or phone.

As of July 8, 2021, a total of 6 residents provided addition comments and/ or questions to the project team following the Community Outreach Meeting. The comments and the responses are provided in the following Table.

COMMENT / QUESTION CATEGORY	COMMENT / QUESTION RECEIVED	CONSIDERATION OF COMMENT
Vehicle Operations & Safety	<p>The continued assertion that the Town is concerned for the safety of all road users rings hollow to me, when there are numerous other ongoing safety issues around Cheltenham that have never been fixed by the Town. In my opinion, the Town is more likely concerned with being exposed to liabilities.</p> <p>In the May 27th virtual meeting, there was continued reference made to road safety standards, guidelines and such that pushed the need to check off boxes in the proposed design, including sidewalks, speed limit signs, curbs, two lanes, minimum curve radii and all that..... but not one Town representative that I recall, stood out and said, in this situation, those do not apply or are not appropriate in this unique situation, the residents don't want all that and as a Town, we are over thinking this. In my opinion it would be the necessary way to move forward and get the residents what they want and pay taxes for.</p> <p>Post the speed at 15-20 km/h for the <u>whole</u> street. Forget the 2 lane road with curbs idea, nobody wants that. Serpentine the long straight sections to slow down the drivers. Tighten up the curves and sight lines <i>even more</i> so drivers have no choice but to slow down, put TRUE traffic calming measures that actually work and leave no choice but to slow down. Fix the road base properly and pave it over, paint a bike lane along the side. Get rid of those awful industrial parking lot LED street lights.</p>	<ul style="list-style-type: none"> Road design in Canada is guided by design standards which were developed based on theoretical research, empirical data, and extensive study on driver behaviour. Our approach is not to apply the design standards indiscriminately along Mill Street, but to review the existing conditions and provide a design that is practical and safe for all road users – pedestrians, cyclists, and drivers. Preserving the unique cultural features of Cheltenham is an important consideration of this EA Study. Mill Street is also a public road open for access to both local residents and users outside of the community. Road safety for all road users is to be considered. Lowering the speed alone will not resolve the limited sight distance at the “S” curve and lack of pedestrian/ cycling facilities on Mill Street To serpentine the long straight sections will require significant property acquisition from adjacent properties. Our design approach is to minimize property impacts

	<ul style="list-style-type: none"> The Study will review traffic calming options that have been applied in previous studies. Traffic has to be slowed down in a safe and controlled manner
<p>The engineers referred to a number of safety concerns</p> <ul style="list-style-type: none"> The guiderail near the rail trail was noted <ul style="list-style-type: none"> There was no further comment from the engineers and no comment on re-installing the post & wire guiderail that was there previously The lack of any attention suggests that it is not a significant concern Sightlines at the top of the “S” curve was identified as a safety concern by the engineers <ul style="list-style-type: none"> The engineers presented no information on any risk analysis There was no solution presented other than to install sidewalks This presumably is to make way for the motor vehicles The engineers did not discuss the potential for addressing this issue specifically (i.e. cutting the grade down or specific localized pedestrian access up the hill) This was used more as a justification for sidewalks the full length of Creditview 	<ul style="list-style-type: none"> Guiderail issues will be addressed as part of the roadside safety analysis during the detailed design for the preferred alternative The limited sight distance, substandard road width, and lack of pedestrian/ cyclist facilities at the “S” curve and along Mill Street in general is a safety concern as insufficient distance is provided to accommodate vehicles, cyclists, and pedestrians. No alternative was presented at the meeting as the Study is still at its early stages gathering feedback to help develop the alternatives Sidewalk is a possible alternative for pedestrian facilities but may not be the selected preferred alternative. No alternatives have been developed yet. Sidewalks are to serve pedestrians and not motor vehicles

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| <ul style="list-style-type: none">• The engineers stated that the “S” curve does not conform to the standards for a 40km/h road and is therefore a safety concern<ul style="list-style-type: none">○ The engineers presented no information on any risk analysis○ Lowering the speed limit to match the roadway seems like a more cost-effective solution than expropriating property and enlarging the roadway○ Lowering the speed limit to match the roadway does not require a Study or investment by the Town – it is something that could have already been done if safety was the primary concern• A section of the roadway was identified as not meeting TAC standards<ul style="list-style-type: none">○ The intention of the Town is to widen the road○ The engineers presented no data on traffic volumes or speeds on the road○ There is an abundance of studies that confirm that widening the road will increase traffic speed, however the engineers did not present any ideas for corresponding traffic calming measures○ Substantive traffic calming measures were essentially dismissed as requiring additional infrastructure | <ul style="list-style-type: none">• The “S” curve does not conform to standards for 40km/hr. The horizontal and vertical alignment limits the decision and stopping sight distance for a driver traveling at 40km/h to see, react, and stop in time to avoid an accident with an object on the road (or pedestrian/ cyclist in this case). It is a safety concern because there is no room on the road for pedestrians or cyclists to move out of the way of the vehicle, or for the vehicle to safely stop to avoid the object.• Lowering the speed alone will not resolve the limited sight distance at the “S” curve, substandard road width, and lack of pedestrian/ cycling facilities on Mill Street• A traffic Study will be undertaken as part of this Study, and the findings will be incorporated into the development and evaluation of the alternative solutions. Results of the traffic study will be documented in the Project File.• One of this Study’s objectives is to improve road safety for all users, which includes upgrading/ widening the travel lanes to meet the current design standard width; however, no new lanes are being considered to increase traffic volume |
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- The engineers noted that at the intersection of Creditview and Mill Street, there was no facility to accommodate pedestrian traffic
 - There was no solution identified other than implying sidewalks as the solution
 - Many Ontario municipalities implement traffic calming measures to slow/limit traffic at residential intersections
- While the engineers cited safety concerns as an impetus for the redevelopment of Mill Street, they presented no data on pedestrian usage, accident rates or complaints to substantiate the concerns
 - Looking in Cheltenham and within the Town for examples of the Town's safety focus, one does not have to go far for both old and new examples of where safety ranks when it comes to redeveloping a road in a residential area
 - Creditview was widened
 - The Town likely has data showing that speeds have increased significantly
 - Safety was compromised as evidenced by the parking at the Cheltenham General Store – a vehicle parked at the store must either block the sidewalk or protrude out into the roadway
- Study will review traffic calming options that have been applied in previous studies. Traffic calming would need to be accomplished in a safe and controlled manner
- We note there is an existing sidewalk along the west side of Creditview Road starting from the Mill Street intersection and continuing north of Creditview Road
- At this point, the Town has not yet developed or evaluated any alternative solutions. As such, no preferred solution was presented at the meeting
- Creditview Road - Town to provide comment.
- Based on a site review, the boulevard space in front of the General Store is wide enough for parallel parking, similar to the east side of the road. Vehicles currently park straight in and onto the existing sidewalk forcing pedestrians onto the road

<ul style="list-style-type: none"> ▪ Pedestrians must often walk out onto the roadway to get by ▪ This is neither the fault of the General Store or its patrons; it falls squarely on municipal planners and its politicians ▪ Evidence suggests that the priority was not pedestrian safety ▪ New Valleywood development <ul style="list-style-type: none"> ▪ New residential neighbourhood has vehicles parked in driveways between the sidewalk and the roadway ▪ Vehicles are either protruding into the sidewalk, out onto the road or both ▪ Evidence suggests that the same priorities exist within the Town 	<ul style="list-style-type: none"> • New Valleywood development - Town to provide comment.
<ul style="list-style-type: none"> • Resurfacing issues raised involve a very small portion of the road limited to the “S” curve • Sightline issues raised by the engineers are limited to the same “S” curve section • The road is generally lightly used by any of vehicles, cyclists or pedestrians <ul style="list-style-type: none"> ○ The Town has provided no data on vehicle usage in terms of volume or speed. The absence of any relevant data leads to a reasonable belief that such data was not considered in determining the need for the EA Study or the project generally 	<p>A traffic Study will be undertaken as part of this Study, and the findings will be incorporated into the development and evaluation of the alternative solutions. Results of the traffic study will be documented in the Project File.</p>
<p>I would like to see a traffic calming solution used at the Mississauga road end of the street. Once the road is paved, more vehicles will speed, well beyond the large percentage of vehicles that already do. The posted speed limit of 40km/h does little to deter speeders, but a physical barrier will.</p> <p>With good sight lines in both directions, traffic lights would not be needed, as was mentioned at the meeting.</p>	<p>The Study will review traffic calming options that have been applied in previous studies.</p> <p>Adequate sight distance still needs to be provided when implementing a physical barrier so traffic slows down in a safe and controlled manner. Based on existing site conditions, providing good sight lines without using traffic</p>

		lights will only be possible through extensive road realignment which will incur significant property impacts. Our design approach is to minimize property impacts.
	ONLY Road repair is needed. Please do traffic Study and provide a custom plan to address further "improvements"	A traffic Study will be undertaken as part of this Study, and the findings will be incorporated into the development and evaluation of the alternative solutions. Results of the traffic study will be documented in the Project File.
Property Impacts and Maintenance	my wife and I are not going to give up our property along Mill St	Noted.
	Specifically, I want to know the road allowance/right of way in front of my property. Winnie said the range on the street is 17-23 feet. What is it at 1140 Mill Street?	Road allowance/ right of way in front of 1140 Mill Street varies, almost half of it is 13m while the other half is typically 20m.
	<p>"A sidewalk plan would not likely include a boulevard"</p> <ul style="list-style-type: none"> The result would be that snowplows would push road snow onto the sidewalks 	A preferred solution for the corridor has not yet been developed. We are still in the early stages gathering feedback from the community. Sidewalk is one of the pedestrian facility alternatives.
	<ul style="list-style-type: none"> We would have to see whether the Town would assume responsibility for snow-clearing" Currently the respective property owners are responsible for snow-clearing Between the Creditview sidewalk implementation and the proposed sidewalk the full length of Mill Street, how many meters of sidewalk are being added to the village? 	<ul style="list-style-type: none"> Town to provide comment re snow clearing A preferred solution for the corridor has not yet been developed. We are still in the early stages gathering feedback from the community. Sidewalk is one of the pedestrian facility options to be considered. The pedestrian facility implemented will be along Mill Street.

	<ul style="list-style-type: none"> As the property owners of 14376 Creditview, which is situated on the north-west corner of Creditview and Mill, we would be personally saddled with an additional 300 feet of sidewalk (already responsible for the same corner to the Cheltenham General Store). 	<ul style="list-style-type: none"> Study will include review safety improvements at the Caledon Trailway Path crossing
Active Transportation Accommodation	<ul style="list-style-type: none"> Other than safety concerns addressed above, the active transportation accessibility is only limited by the current state of the pavement at the “S” curve. The road is very lightly used by pedestrians because most pedestrian usage is at the rail trail Parking for the trail is primarily across Mississauga Road at the Brickyards and a designated parking lot on Creditview Greater safety concerns exist with the trail at the Mississauga Rd crossing 	<ul style="list-style-type: none"> The “S” curve limits the available sight distance. The active transportation accessibility is limited by the lack of active transportation facilities on Mill Street Pedestrian traffic volumes are noted and will be confirmed through a traffic Study Design will review alternatives at the Caledon Trailway Path crossing to improve safety
	The road itself does not limit active transportation	The road currently has no active transportation facilities.
	<p>Lastly, it was mentioned several times during the meeting that there was a need for access to the trails. Who is this need for? Isn't it Caledon residents who are paying taxes to the Town? It definitely isn't tourists.</p>	<p>Both Mill Street and the Caledon Trailway Path is a public road that is open for access to residents within and outside of the Cheltenham community.</p> <p>With the existing sidewalk along Creditview Road (west side starting at Mill Street intersection and continuing north) and the Caledon Trailway Path, the Town requires safe pedestrian access to and from the trail from Creditview Road.</p>

	<p>Very safe and hardly any vehicle, pedestrian and cyclist traffic currently on road. Only used by Cheltenham residents.</p>	<p>Mill Street is a public road that is open for access to residents within and outside of the Cheltenham community. As such, the Town has an obligation to provide safe accommodation for all users, which includes upgrading the road to meet the current standards and providing pedestrian and cyclist facilities</p>
	<p>Even though my boys are 5 and 2, we allow them to ride their bikes freely on the road and walk to our next door neighbour's houses. I have no fear of them being hit by a car as our road is about 95% of the time only used by the community, whether it be driving, biking or walking.</p>	<p>Mill Street is a public road that is open for access to residents within and outside of the Cheltenham community. As such, the Town has an obligation to provide safe accommodation for all users, which includes upgrading the road to meet the current standards and providing pedestrian and cyclist facilities. The road currently does not meet the Town or TAC design standards and considered unsafe. One of the study objectives is to improve safety for all road users – within and outside of the community.</p>
<p>Cultural Heritage / Village Character</p>	<p>“This is a lovely corridor”</p> <ul style="list-style-type: none"> • This comment was made at the closing and epitomizes the thought process of the engineers • This is not viewed by the engineers as a community, it is a travel corridor and nothing more • This leads to the reasonable belief that the primary goal is to increase the volume of vehicles and people through this street and the village in general • This is different than accommodating tourists, it is about moving people through 	<p>Mill Street is a public road that is open for access to residents within and outside of the Cheltenham community. As such, the Town has an obligation to provide safe accommodation for all users, which includes upgrading the road to meet the current standards and providing pedestrian and cyclist facilities</p> <p>We recognize Mill Street is part of the unique Cheltenham Community hence we are gathering feedback through the Community</p>

		<p>Outreach Meeting and through the Public Information Center scheduled for 2021 Summer/ Fall. One of the objectives for this Study is to improve road safety for all users including pedestrian, cyclist, and vehicular traffic. Currently there are no pedestrian or cyclist facilities on the road.</p>
	<p>Cheltenham is a heritage village and lots of heritage homes and very old trees. Most of the affected properties are close to road already.</p>	<p>We recognize this and our design approach is to minimize tree removal and property impacts.</p>
	<p>My family moved from Toronto last year and as soon as we found our house, we fell in love with both the house and the community. I grew up in Caledon, my parents still reside on Boston Mills Rd and Chinguacousy Rd. I have always loved the peace and quiet that country living provides and my husband and I wanted that for our 2 little boys. In only the few months that we have been here, we have become friends with many of our neighbours on Mill St.</p>	<p>Noted.</p>
	<p>It is our view that the Town needs to incorporate “improvements” for Cheltenham generally, and the Mill Street project specifically, into the village, not the other way around. It is our view that the engineers that presented in May 2021, do not grasp the concept and their process is to simply take a given set of standards and make the community fit. In making it fit, they do not consider the consequences to the village, its streetscapes or its residents. This is not about “no improvements” it is about respecting the nature of the village in making improvements and addressing the inevitable increase in traffic as urbanization encroaches.</p>	<p>We recognize Mill Street is part of the unique Cheltenham Community, hence we are gathering feedback through the Community Outreach Meeting and through the Public Information Center scheduled for 2021 Summer/ Fall. One of the objectives for this Study is to improve road safety for all users including pedestrian, cyclist, and vehicular traffic. Currently there are no pedestrian or cyclist facilities on the road. The existing road also does not meet the current Town and TAC</p>

		design standards, and we do not intend on applying the standards indiscriminately as the full upgrade will result in significant impacts to the adjacent properties. Therefore we are reaching out to the community to gather feedback in developing the Study alternatives.
Natural Environment Including Drainage	clean and secure the existing drainage ditches. Replace/improve the culverts as needed.	Design will include ditch and culvert cleanouts and replacements where required.
	Quiet residential or rural/residential in nature	Noted.
	<ul style="list-style-type: none"> Flooding area on the “S” curve identified by Town <ul style="list-style-type: none"> The engineers confirmed that this project does not address flooding issues The engineers speculated that it would not make the flooding worse Roadway drainage <ul style="list-style-type: none"> The presenters did not address any roadway drainage issues or how this project affects roadway drainage or storm water management It is reasonable to believe that roadway drainage and storm water management are not being addressed in any material way by this project 	Flooding at the “S” curve is due to the existing topography of the land and its proximity to the river. To resolve the flooding issue will require an area wide stormwater management Study, which is beyond the scope of this project. All design will be submitted to CVC and NEC for review and comment. Stormwater drainage is an important part of the detail design of the selected preferred alternative (to be selected after PIC), and our approach is to not worsen the existing drainage issue.
	<p>“We would want to minimize the cutting of trees and disruption to property”</p> <ul style="list-style-type: none"> The engineers presented no information as to anticipated impact on the current streetscape While recognizing that an actual determination cannot be assessed until a proposal is designed, there was no comment or projections on how many trees might have to be removed to accommodate the redevelopment of the roadway 	The Study is at its early stages gathering feedback from the community. No alternatives have been developed so it is not possible to determine number of tree removals at this point. Our design approach will be to minimize tree removal and property impacts.

Class EA Process	I would like to be added to the communication list for past and future updates, as well as for staff to communicate answers to questions asked, but missed by staff during the meeting on May 27.	You have been added to mailing list.
Utility Impacts	Move those few hydro poles that are a safety issue, or bury the lines.	Noted. Alternatives will consider the impact to utilities.
General Comments / Other	<p>There was no presentation of any information on the current level of vehicle and pedestrian traffic on Mill Street</p> <ul style="list-style-type: none"> • There was no presentation of any information as to the expected volumes of vehicle and pedestrian traffic on Mill Street • The engineers mentioned that sidewalks were not a “preferred” option, however, they presented no real alternative and the audience would reasonably believe that sidewalks are in fact the solution that the engineers are focused on. • Having been involved in a similar issue in another village in the Town, the primary interest of the Town infrastructure department, as stated by the department head at the time, is getting the plows down the road • Our correspondence to the engineers prior to the meeting was met with a response that was essentially <ul style="list-style-type: none"> ○ We are in the early stages ○ We will determine the options ○ We will evaluate the options ○ We will present the options that we determine ○ Residents may then comment • This presentation appeared to be nothing more than a formality with no real consultative element • The presenters did not seem to have an appreciation of the village or its character 	<ul style="list-style-type: none"> • Traffic Study will be conducted on Mill Street as part of this Study. Findings will be included at the Public Information Center (PIC) and in the Project File. • Currently there are no pedestrian facilities on Mill Street and one of the Study’s objectives is to improve road safety for all users – pedestrians, cyclists, and vehicles. Sidewalks are one of the options that will be considered in this Study. To date no preferred alternative has been selected. • The Study is in its early stages and no alternatives have been developed yet so there were none to be presented. The focus of this Community Outreach Meeting is to gather feedback from the public and the comments received will then be used to develop and evaluate alternative solutions. The results will be presented at the PIC which is open to everyone for comment. In addition, the public can also submit their comments on the Study at any time to the contact persons listed on the Town’s project

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| <ul style="list-style-type: none">• The project appears to be an attempt to urbanize the village to new development standards – make the square peg (Cheltenham) fit the round hole (new urban development).• The widening of the roadway and implementation of sidewalks will:<ul style="list-style-type: none">○ increase the volume of traffic○ increase the speed of traffic;○ likely increase the use of Mill Street as a bypass around the Mississauga Road/King St. intersection, and the Town does not seem to be considering traffic calming○ Increasing the hardscaped space and removing a significant number of trees will contribute to pollutants entering the river, and reduce to the streetscape quality. | <p>website :
https://www.caledon.ca/en/news/mill-street-ea.aspx</p> <ul style="list-style-type: none">• This process of developing and evaluating alternatives and consultation as outlined in our previous response is following the Municipal Class EA guidelines.• Although Mill Street is a Schedule 'B' EA Study which typically requires only one point of public contact per the Municipal Class EA guidelines, we are undertaking two (Community Outreach Meeting and PIC) to solicit input from and consult with the community• The current roadway width does not meet Town or TAC design standards for a two-lane local road. One of the Study's objective is to improve road safety for all users, which includes upgrading the road to meet the current standards and providing pedestrian facilities. The road widening considered in this Study is to widen the width of the travel lanes and not adding new lanes, which may increase traffic volumes.• Mill Street is a public road that is open for access to everyone getting around in Caledon.• The Study will review a variety of traffic calming options for Mill Street which have |
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	<p>been shown to be effective based on previous studies.</p> <ul style="list-style-type: none"> • Our design approach will be to minimize tree removal and property impacts. • A traffic Study will be undertaken as part of this Study, and the findings will be incorporated into the development and evaluation of the alternative solutions. Results of the traffic study will be documented in the Project File. • Stormwater drainage is an important part of the detail design of the selected preferred alternative (to be selected after PIC), and our approach is to not worsen the existing drainage issue.
<p>If all the residents (who pay taxes) are saying no, why is this even a discussion. Fix the road and leave it at that.</p>	<p>Mill Street is a public road that is open for access to residents within and outside of the Cheltenham community. As such, the Town has an obligation to provide safe accommodation for all users, which includes upgrading the road to meet the current standards and providing pedestrian and cyclist facilities.</p>
<p>During the meeting, the word "urbanize" was constantly used by the engineering company that the Town has hired to do this assessment. To me, as well as many of the residents on this street, it is unfortunate that the Town couldn't find engineers who are local and understand the need to keep the country "rural". I understand and agree that Mill St needs to be fixed as it is deteriorating, however, why must this street, which yes is a 2 lane throughway (again something that was repeatedly</p>	<p>The Study is in its early stages and no alternatives have been developed yet. The final preferred alternative may be urbanized or rural, depending on the evaluation. Mill Street is a public road that is open for access to residents within and outside of the Cheltenham community. As such, the Town has an obligation to provide safe accommodation for all</p>

told to us) but really only used by residents, be classified as the same as say McLaughlin Rd or Creditview Rd, which both see hundreds of cars daily. The Town of Caledon must do a custom plan for our road, as well as a traffic study, otherwise, not only will we lose many trees and property, we will in turn, lose the very community feel of Cheltenham

users, which includes upgrading the road to meet the current standards and providing pedestrian and cyclist facilities. The Study is not looking to change the classification of Mill Street. A traffic Study will be undertaken as part of this EA. The design approach will aim to minimize tree removals and property impacts.

5.0 CONCLUSIONS AND NEXT STEPS

The Community Outreach Meeting provided an introduction of the key issues to the community and an opportunity for interested parties to provide their input. Comments on various topics were received, with a number of trends becoming evident:

- Road safety improvements at intersections and the “S” bend are not strongly supported.
- Residents do not desire any pedestrian and cyclist accommodation throughout the corridor, including through the “S” bend of the roadway.
- Residents only want to rehabilitate the road pavement with no other change.
- Concern over property impacts and tree removals.
- Concern over the potential for improvements to increase traffic volumes and disrupt the existing character of the corridor.
- Additional traffic calming measures are supported.

The development and evaluation of alternative solutions for the Mill Street corridor will be completed in consideration of comments received at the Community Outreach Meeting and will be presented to the public for input at an upcoming Public Information Center (PIC), tentatively scheduled for Summer / Fall 2021.

APPENDIX 1

DISPLAY MATERIALS

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT



MILL STREET (MISSISSAUGA ROAD TO CREDITVIEW ROAD)

COMMUNITY OUTREACH MEETING

May 27, 2021

6:00pm to 7:00pm

Please submit any questions you may have using the Q & A function.

Presentation materials, including a recording of the presentation will be available following the presentation for review and comment on the project website at

<https://www.caledon.ca/en/news/mill-street-ea.aspx>

PRESENTATION AGENDA



S-curve on Mill Street.

- Study Area & Project Background
- Municipal Class EA Process
- Study Organization
- Consultation Completed to Date
- What We Have Heard to Date
- Next Steps in the Project
- Question and Answer Period

STUDY AREA



The Mill Street EA study area consists of **Mill Street** from Mississauga Road to Creditview Road.



Two-lane local road under the jurisdiction of the Town of Caledon.

Town is responsible for maintaining safety and traffic operations for all users.

PROBLEM OR OPPORTUNITY STATEMENT

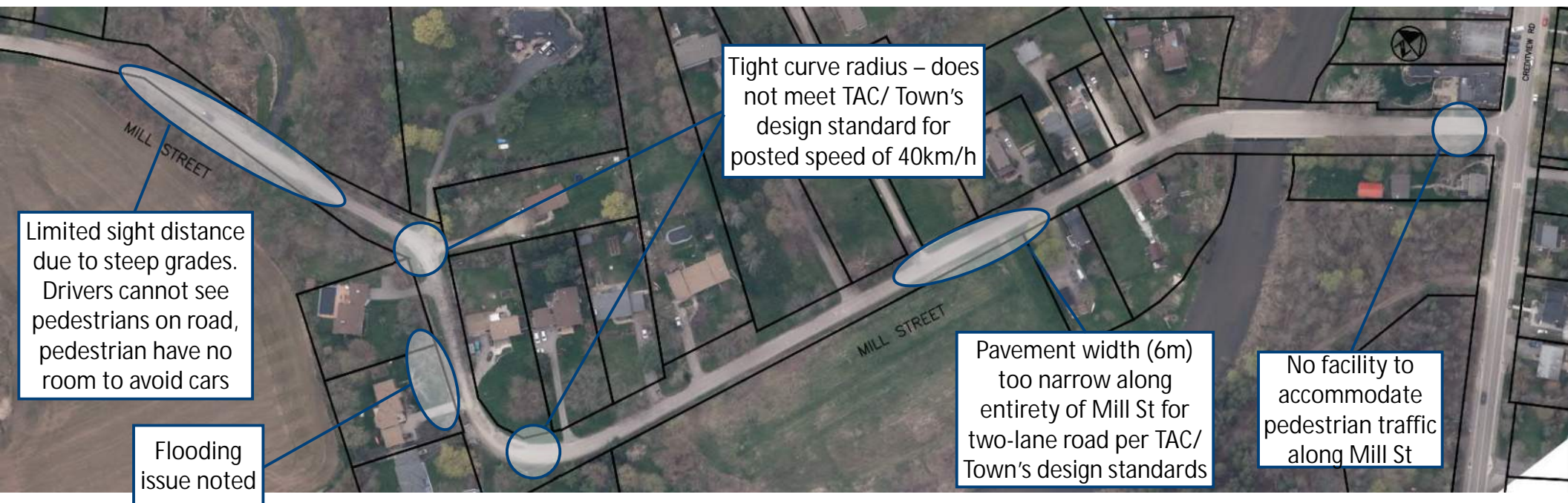


The Problem/Opportunity Statement outlines the need and justification for the overall project and establishes the general parameters, or scope, of the study

The Problem or Opportunity developed for the Mill Street Class EA, is to review opportunities within the study area to address:

- Increasing wear and tear of the roadway
- Enhanced road safety for all users
- Improved accessible active transportation (walking, cycling) including Caledon Trail crossing improvements
- Roadway drainage and stormwater management.

EXISTING CONDITIONS



EXISTING CONDITIONS



Limited sight distance due to steep profile grades and sharp curve going into the s-curve.

EXISTING CONDITIONS



EXISTING CONDITIONS



MUNICIPAL CLASS EA PROCESS



Undertaken prior to municipal **road, water, wastewater** and **other municipal** construction projects.

Consideration of all **reasonable alternatives**.

Aims to minimize **impact on the natural, cultural, social** and **economic environment**.

Input from the **public, stakeholders** and **technical agencies** is essential.

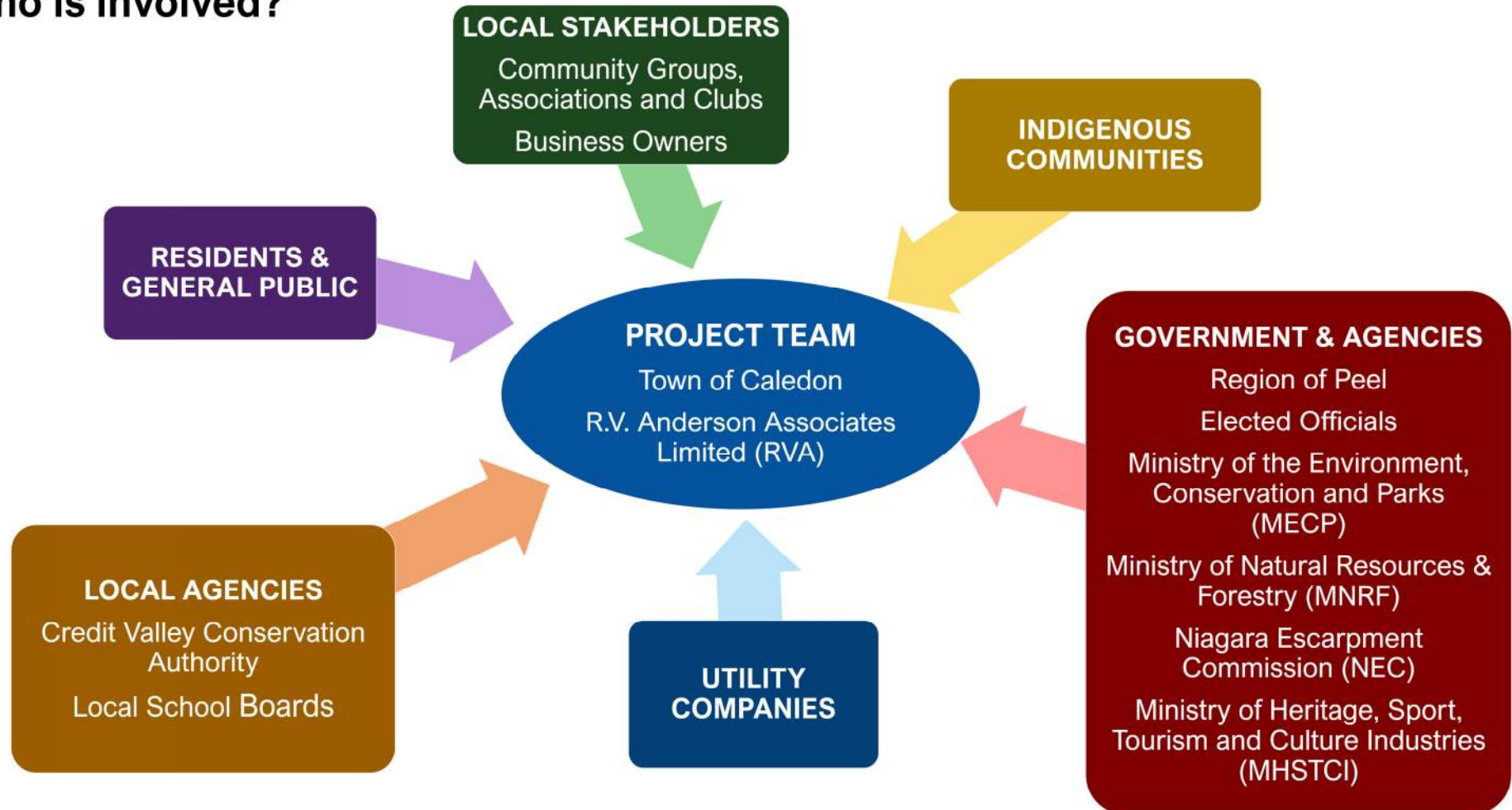


This Mill Street Class EA is classified as a **Schedule 'B' Municipal Class EA** and is subject to **Phases 1 and 2** of the **Municipal Class Environmental Assessment** prior to construction.

STUDY ORGANIZATION



Who is involved?



CONSULTATION COMPLETED TO DATE



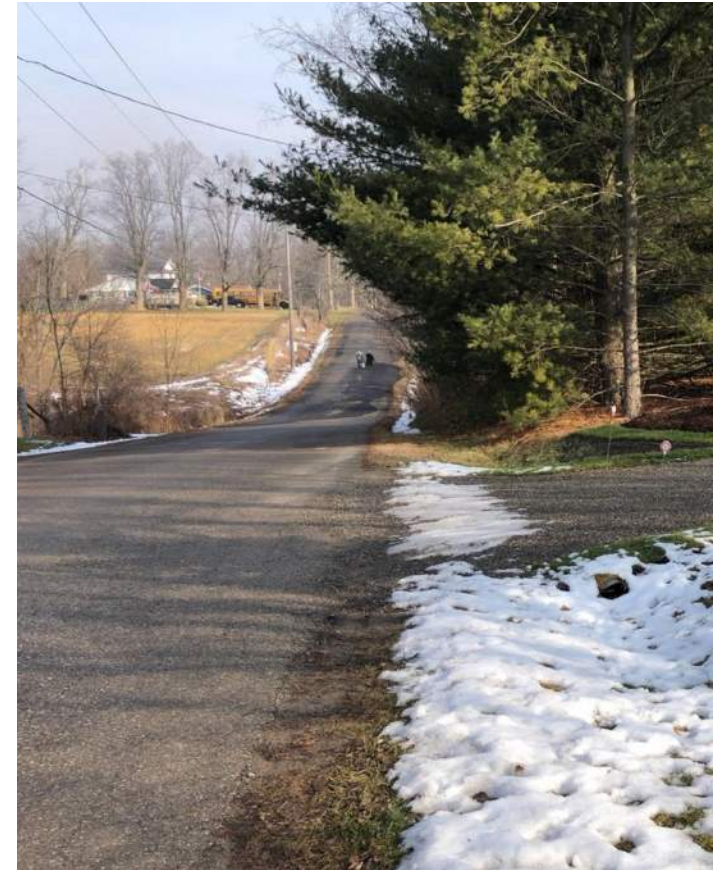
- Potentially impacted property owners
- Meetings with Technical Agencies
 - Credit Valley Conservation Authority, Niagara Escarpment Commission
- Engagement with Indigenous communities
 - Mississaugas of the Credit First Nation, Huron Wendat Nation, Metis Nation
- Cheltenham Neighbours Association
- Local utilities



WHAT WE'VE HEARD SO FAR



- I want the road repaved and everything else to remain the same.
- I don't want property acquisition.
- I don't want tree removal.
- I don't want sidewalk.
- There is flooding issue.
- I don't want to encourage speeding.



Pedestrians walk along the roadway on Mill Street.

SUMMARY OF NEXT STEPS



Roadside ditch on south side of Mill Street, looking toward Creditview Road.

- Develop and evaluate alternative solutions
- Consult with additional stakeholders and technical agencies, as required
- Present the preliminary preferred solution for feedback at the **Public Information Centre** (scheduled for Summer 2021)
- Prepare and submit a **Project File Report** for 30 Day public review
- Proceed to **detailed design and construction** (planned for 2022 pending Council approval and budget)

YOUR INPUT IS IMPORTANT



The project team is available to answer your questions until 7 P.M. Please submit any questions you may have using the Q & A feature.

You can also provide your feedback on the project, by completing a comment form on the project website, or by providing your comments directly to the project team contacts listed below by **June 11, 2021**.

THANK YOU FOR ATTENDING!

Project Team Contacts:

Town of Caledon

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Project Manager, Engineering Services
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R.V. Anderson Associates Limited Winnie Wong, P. Eng., PMP, M.Eng.

Project Manager, Transportation
Tel: (416) 497-8600 x1471
E-mail: winnie.wong@rvanderson.com

APPENDIX 2

COMMENTS RECEIVED

APPENDIX 2-1

Comments Received During Meeting

Q&A Session for MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY MISSISSAUGA ROAD TO CREDITVIEW ROAD

Session number: 1322480812

Date: May 27, 2021

Starting time: 5:42 PM

[REDACTED] - 6:04 PM

Q: First off, for your records, the Date is May 27th, not the 28th as shown on your opening screen.

Priority: N/A

[REDACTED] - 6:12 PM

Q: Regarding sidewalks, there is not sidewalk on creditview or mississauga rd. why mill st which is a narrower rd rarely used by anyone other than residents?

Priority: N/A

Ryan Grodecki - 6:17 PM

A: Thanks Emily. The construction of a sidewalk on the west side of Creditview Rd will be completed this summer from Kennedy Rd heading south to approximately the Park/Station

[REDACTED] - 6:21 PM

Q: You have identified safety and traffic operations for all users as your mandate. How have you broken down each the factors within those categories, and how are these ranked within your evaluation process?

Priority: N/A

[REDACTED] - 6:23 PM

Q: What is the width of the existing right-of-way?

Priority: N/A

[REDACTED] - 6:24 PM

Q: You say the curve is too tight for a 40km/h speed. I understand that you are considering expropriations. Is it not more cost effective to lower the speed limit to match the curve rather than maintain the speed and re-engineer the curve?

Priority: N/A

[REDACTED] - 6:28 PM

Q: In order for you to create a legal size rd with 2 lanes and some sort of walking area, you will have to take property and cut trees, it is ignorant for the town to think otherwise. We moved from the city to the country for country living and DO NOT want this to become a main rd for tourists. It is a safe street that I let my children ride their bikes on and i have never felt they were in danger as there is barely any traffic

Priority: N/A

████████████████████ - 6:28 PM

Q: what posted speed limit would the curve currently allow ?
Priority: N/A

████████████████████ - 6:29 PM

Q: What does your research indicate is the expected impact on traffic speeds and volumes of the proposed widening of the road?
Priority: N/A

████████████████████ - 6:29 PM

Q: What is the proposed road allowance?
Priority: N/A

████████████████████ - 6:30 PM

Q: We live at 1328 Mill Street. Right on the bend. We are in complete agreement with the slide that states thing that we have heard so far.
Priority: N/A

████████████████████ - 6:31 PM

Q: The observational study was conducted in October 2020, a peak time for pedestrian traffic (fall colours) and additional pandemic traffic. How will this be adjusted in your evaluations?
Priority: N/A

████████████████████ - 6:31 PM

Q: Could a sidewalk only in the dangerous zones such as blind spots demonstrated be considered. ie from the top of the hill to the dam? The rest of the road once to standard would be suitable. Would CVC and NE allow for the ditch to have a sidewalk over top of it with a culvert for drainage?
Priority: N/A

████████████████████ - 6:33 PM

Q: What is the current property allowance from the road to property lines? What is the worst case scenario for expropriation?
Priority: N/A

████████████████████ - 6:34 PM

Q: ok so if the road is too narrow now, and our properties are almost at the road, then you will be taking property and removing trees to improve.
Priority: N/A

████████████████████ - 6:36 PM

Q: Similar roads in cheltenham do not have sidewalks ie Kennedy road and Credit road...please explain the need for a sidewalk on Mill where there is less pedestrian traffic than the other roads
Priority: N/A

████████████████████ - 6:37 PM
Q: Andrew McGregor noted 'pedestrian traffic' concerns. How many resident complaints?
Priority: N/A

████████████████████ - 6:37 PM
Q: We walk our dogs on this road. Putting in sidewalks will actually increase traffic.
Priority: N/A

████████████████████ - 6:38 PM
Q: What is the expected impact of improving/standardizing the road width with respect to diverting traffic off of Mississauga Rd. i.e. creating a route to by-pass the Miss Rd./King intersection?
Priority: N/A

████████████████████ - 6:38 PM
Q: Do current design standards require curbs?
Priority: N/A

████████████████████ - 6:38 PM
Q: You mentioned there are certain areas of the road that are hazardous is there ever been a thought of dealing with those hazardous areas making them safer instead of making it a standard through the whole Road
Priority: N/A

████████████████████ - 6:39 PM
Q: Why consider traffic calming measures that don't work?
Priority: N/A

████████████████████ - 6:43 PM
Q: Is it possible to make all of the submitted questions visible to everyone on the call? We currently can only see 1 question, submitted by Emily
Priority: N/A

████████████████████ - 6:41 PM
Q: Who will be responsible for snow clearing of the proposed sidewalks?
Priority: N/A

[REDACTED] - 6:41 PM

Q: Have you considered traffic calming such as sections reduced to 1 lane as is done in many places in Ontario?

Priority: N/A

[REDACTED] - 6:41 PM

Q: What is the % of roads in Caledon that you repave & fix without doing the 'upgrade' - my perception is that there are many roads that are repaved without doing this 'upgrade'. When is Kennedy road going to be paved? You mentioned Creditview but not Kennedy itself & they were repaved around 3 years ago!

Priority: N/A

[REDACTED] - 6:43 PM

Q: sorry meant when is Kennedy road getting sidewalks !

Priority: N/A

[REDACTED] - 6:44 PM

Q: If a sidewalk is your preferred solution, is it expected to have a boulevard between the road and sidewalk or will the sidewalk be at the curb? What is the mandatory width of a sidewalk?

Priority: N/A

[REDACTED] - 6:48 PM

Q: Was the question about making it a 1-lane street (not a 1-way street)?

Priority: N/A

[REDACTED] - 6:45 PM

Q: The question is not about reducing the whole stretch to a one-lane it is about changing sections only

Priority: N/A

[REDACTED] - 6:46 PM

Q: Comment. The majority of homeowners are 55+ and even 65+ on Mill St. adding a concrete sidewalk will burden the homeowners during in the winter months to remove snow from sidewalks. Some of this road includes farmland. It is unreasonable to expect residents to manage the snow removal.

Priority: N/A

[REDACTED] - 6:47 PM

Q: there is so little traffic on this road so a one way short stretch may work!!

Priority: N/A

[REDACTED] - 6:47 PM

Q: As a land owner of approximately 80 acres of land that is presently zoned residential with frontage both on Mill St. & Creditview Rd please ensure that the road will meet future change. Change will happen in the future, just not in the current plan.

Priority: N/A

████████████████████ - 6:48 PM

Q: We live right on the "s" curve. There is absolutely no problem passing two cars on any section of the road.

Priority: N/A

████████████████████ - 6:48 PM

Q: you can't show participants your questions

Priority: N/A

████████████████████ - 6:49 PM

Q: only hosts and presenters

Priority: N/A

████████████████████ - 6:49 PM

Q: yes

Priority: N/A

████████████████████ - 6:49 PM

Q: There is no option to allow a user to make questions visible to all users. Options are Host or Presenter.

Priority: N/A

████████████████████ - 6:50 PM

Q: Is the Town expecting the respective resident to maintain the sidewalks (winter snow-ice clearing) or is the Town budgeting to provide services?

Priority: N/A

████████████████████ - 6:51 PM

Q: Are you taking into consideration the negative impact on residents regarding being responsible to shovel sidewalks they don't want, especially significant lengths of sidewalks

Priority: N/A

████████████████████ - 6:51 PM

Q: not in toronto

Priority: N/A

[REDACTED] - 6:51 PM
Q: toronto you have to clear your own sidewalk
Priority: N/A

[REDACTED] - 6:53 PM
Q: What will you do to ensure all potentially impacted property owners are consulted? We asked for a meeting and received no response
Priority: N/A

[REDACTED] - 6:56 PM
Q: In talking about pedestrians, the vast majority are residents of Mill Street, not the general public
Priority: N/A

[REDACTED] - 6:54 PM
Q: I prefer paved shoulder to minimize the long-term costs to the town which minimizes taxes to homeowners.
Priority: N/A

[REDACTED] - 6:54 PM
Q: there is no existing side walk on credit view. only outside of the store to the bridge
Priority: N/A

[REDACTED] - 6:54 PM
Q: not up to the trail
Priority: N/A

[REDACTED] - 6:55 PM
Q: There are a number of comments from the presenters about "standards". When you talk about integrating the plan with the character of the village, what does that actually mean?
Priority: N/A

[REDACTED] - 6:56 PM
Q: Can you extend this meeting past 7 pm so all questions can be answered?
Priority: N/A

[REDACTED] - 6:56 PM
Q: who rides their bike in the winter
Priority: N/A

[REDACTED] - 6:56 PM

Q: yah i bet you live in the city
Priority: N/A

████████████████████ - 6:56 PM
Q: Why has the Town not directly contacted each property owner on Mill Street? Why, in such a small community, am I only learning about sidewalks on Creditview tonight?
Basic concerns about the lack of communication.
Priority: N/A

████████████████████ - 6:56 PM
Q: What will happen with the hydro poles? Will these be pushed further back toward resident homes? Will these be buried?
Priority: N/A

████████████████████ - 6:57 PM
Q: when there are so few stakeholders involved.
Priority: N/A

████████████████████ - 6:58 PM
Q: You don't live here, therefore one or two trips down the road does not constitute a reasonable longitudinal or short-term study.
Priority: N/A

████████████████████ - 6:58 PM
Q: Is it correct to say that the impact of sightlines and road width are proportionate to the speed of the traffic. If so, wouldn't slowing the traffic mitigate the concerns significantly?
Priority: N/A

████████████████████ - 6:59 PM
Q: Or if they didn't get the CARA email it comes from hollymonkman2@hotmail.com. I had sent the notice twice also
Priority: N/A

████████████████████ - 7:00 PM
Q: A project letter does not allow us to question / comment on how we may be specifically impacted. We asked for a meeting. We want to be heard before all alternate proposals are finalized. WE don't want to only be notified once decisions are already made.
Priority: N/A

████████████████████ - 7:00 PM
Q: The question about "how many complaints / accidents have occurred" has not been

answered

Priority: N/A

[REDACTED] - 7:00 PM

Q: What is the procedure to get the resident's needs to take precedence?

Priority: N/A

[REDACTED] - 7:01 PM

Q: Why are people using mill street go to the trail when the trail has parking lots everywhere

Priority: N/A

[REDACTED] - 7:03 PM

Q: there is no reason to encourage pedestrians down a residential street by installing a sidewalk when visitors come here to use the trail not the road.

Priority: N/A

[REDACTED] - 7:04 PM

Q: Winnie, you mentioned the current road width is 6 metres. What is the TAC standard road width?

Priority: N/A

[REDACTED] - 7:04 PM

Q: theres 3 spots

Priority: N/A

[REDACTED] - 7:04 PM

Q: on the mill street

Priority: N/A

[REDACTED] - 7:04 PM

Q: Most of us only wanted the town to fix the road, why does this have to be a huge process. The only major structural problem is the creek adjacent to the trail

Priority: N/A

[REDACTED] - 7:05 PM

Q: yes

Priority: N/A

[REDACTED] - 7:05 PM

Q: John is absolutely right.

Priority: N/A

████████████████████ - 7:05 PM

Q: Also what are you propping for the hydro poles and fire hydrants
Priority: N/A

████████████████████ - 7:06 PM

Q: What would need to be done to the corner space at the corner of Mill st and the Caledon Trail. You mentioned adding a guardrail, what changes would that entail?
Priority: N/A

████████████████████ - 7:06 PM

Q: What are traffic operation improvements? Is that the lingo for accommodating the snow-plows?
Priority: N/A

████████████████████ - 7:06 PM

Q: People park along the road and have picnics, launch canoes etc. They are parked for long periods of time.
Priority: N/A

████████████████████ - 7:10 PM

Q: Previously someone asked how many complaint and/or accidents have occurred, and that wasn't really answered.
Priority: N/A

████████████████████ - 7:09 PM

Q: you'll have to remove trees as they are close to the road. a lot of them
Priority: N/A

████████████████████ - 7:09 PM

Q: Trespassing on private property is a common problem along with garbage left behind.
Priority: N/A

████████████████████ - 7:09 PM

Q: My family has been in Cheltenham since 1825
Priority: N/A

████████████████████ - 7:11 PM

Q: There was a comment that safety is priority 1. I look at the widening of Creditview to the point that you cannot park a vehicle at the General Store without blocking the sidewalk or sticking out into the road. This same issue is prevalent in the new section of Valleywood subdivision. How is that making safety a priority.
Priority: N/A

████████████████████ - 7:15 PM

Q: This is a comment. The statement of "It is a lovely corridor" speaks to the manner in which the engineers view the village. It is a community.

Priority: N/A

APPENDIX 2-2

Comments Received After Meeting

Connor MacIsaac

To: Winnie Wong
Subject: RE: Mill street environmental study

From: Shun Cheung <Shun.Cheung@caledon.ca>
Sent: June 10, 2021 7:28 AM
To: [REDACTED]
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: RE: Mill street environmental study

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Morning [REDACTED]

Thank you for your email and your feedback on the project.

Shun H. Cheung, P.Eng., PMP
Project Manager
Engineering Services Department

Office: 905.584.2272 x.4040
Cell: 416.436.0910
Email: shun.cheung@caledon.ca

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From: [REDACTED]
Sent: Wednesday, June 09, 2021 9:55 PM
To: winnie.wong@rvanderson.com; Shun Cheung <Shun.Cheung@caledon.ca>
Subject: Mill street environmental study

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Hello Winnie and Shun

I live at [REDACTED] and would like to see a traffic calming solution (see pictures below) used at the Mississauga road end of the street.

Once the road is paved, more vehicles will speed, well beyond the large percentage of vehicles that already do. The posted speed limit of 40km/h does little to deter speeders, but a physical barrier will.

With good sight lines in both directions, traffic lights would not be needed, as was mentioned at the meeting.

Regards,

[REDACTED]





[REDACTED]

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Connor MacIsaac

To: Winnie Wong
Subject: RE: Mill St

From: [REDACTED]
Sent: June 11, 2021 9:44 AM
To: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>; Christina Early <christina.early@caledon.ca>;
Subject: Mill St

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good Morning

I know the Town portal is the expected place for comments in regards to the Mill St project, but like a lot of residents I have spoken to, I too have no confidence in that format and process, covid protocols notwithstanding. So I am sending my email directly.

First off, my wife and I are not going to give up our property along Mill St.

The continued assertion that the Town is concerned for the safety of all road users rings hollow to me, when there are numerous other ongoing safety issues around Cheltenham that have never been fixed by the Town. In my opinion, the Town is more likely concerned with being exposed to liabilities.

In the May 27th virtual meeting, there was continued reference made to road safety standards, guidelines and such that pushed the need to check off boxes in the proposed design, including sidewalks, speed limit signs, curbs, two lanes, minimum curve radii and all that.... but not one Town representative that I recall, stood out and said, in this situation, those do not apply or are not appropriate in this unique situation, the residents don't want all that and as a Town, we are over thinking this. In my opinion it would be the necessary way to move forward and get the residents what they want and pay taxes for.

My input/comments are.... clean and secure the existing drainage ditches. Replace/improve the culverts as needed. Move those few hydro poles that are a safety issue, or bury the lines. Post the speed at 15-20 km/h for the whole street. Forget the 2 lane road with curbs idea, nobody wants that. Serpentine the long straight sections to slow down the drivers. Tighten up the curves and sight lines *even more* so drivers have no choice but to slow down, put TRUE traffic calming measures that actually work and leave no choice but to slow down. Fix the road base properly

and pave it over, paint a bike lane along the side. Get rid of those awful industrial parking lot LED street lights.

Thank you for your time. Respectfully

[REDACTED]
[REDACTED]
[REDACTED]

Connor MacIsaac

To: Winnie Wong
Subject: RE: Mill St EA

From: [REDACTED]
Sent: June 10, 2021 3:02 PM
To: Christina Early <Christina.Early@caledon.ca>
Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: Re: Mill St EA

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Please see attached my comment form and add to my original email.

Thank you.

[REDACTED]

On Thu, Jun 10, 2021 at 2:05 PM Christina Early <Christina.Early@caledon.ca> wrote:

Thanks for your comments [REDACTED]

Christina

Get [Outlook for iOS](#)

From: [REDACTED] >
Sent: Thursday, June 10, 2021 2:04:07 PM
To: Shun Cheung <Shun.Cheung@caledon.ca>
Cc: winnie.wong@rvanderson.com <winnie.wong@rvanderson.com>; Christina Early <Christina.Early@caledon.ca>
Subject: Re: Mill St EA

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Thank you. I wonder how many of my older neighbours will know that that is where you have hidden it.

On Thu, Jun 10, 2021 at 2:02 PM Shun Cheung <Shun.Cheung@caledon.ca> wrote:

[REDACTED]

The Comment Form is located under the Public Meeting Materials highlighted in red.

Notice of Virtual Community Outreach Meeting

A virtual Community Outreach Meeting was held on May 27, 2021 to share updates and receive inputs from the public on the study to date, including the problem/opportunity, existing conditions, project's next steps.

Public Meeting Materials

—

Materials presented at the public meeting

- [Presentation slides](#)
- [Comment form](#)

+

Public meeting recording

Questions/Comments Received (Answers will be available after June 11th)

Shun H. Cheung, P.Eng., PMP

Project Manager

Engineering Services Department

Office: 905.584.2272 x.4040

Cell: 416.436.0910

Email: shun.cheung@caledon.ca

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From: [REDACTED]
Sent: Thursday, June 10, 2021 1:53 PM
To: Shun Cheung <Shun.Cheung@caledon.ca>
Cc: winnie.wong@rvanderson.com; Christina Early <Christina.Early@caledon.ca>
Subject: Re: Mill St EA

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Thank you, but where is the form on that page? I couldn't find it, and followed the instructions to email you.

[REDACTED]

On Thu, Jun 10, 2021 at 1:51 PM Shun Cheung <Shun.Cheung@caledon.ca> wrote:

[REDACTED]

Thank you for your comments and the Town will document this for future references.

However, if you have further comments, please visit the Town's project website and complete the comment form there.

<https://www.caledon.ca/en/news/mill-street-ea.aspx>

Shun H. Cheung, P.Eng., PMP

Project Manager

Engineering Services Department

Office: 905.584.2272 x.4040

Cell: 416.436.0910

Email: shun.cheung@caledon.ca

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From: [REDACTED]
Sent: Thursday, June 10, 2021 1:48 PM
To: Shun Cheung <Shun.Cheung@caledon.ca>; winnie.wong@rvanderson.com
Cc: Christina Early <Christina.Early@caledon.ca>
Subject: Mill St EA

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Good afternoon Mr. Cheung and Ms. Wong,

I am writing to you on behalf of myself and my husband, who live at [REDACTED] in Cheltenham. We both attended the Virtual Community Outreach Meeting on May 27 2021, and left the meeting very concerned about both the Town's and the engineering company's ideas about what should happen to our lovely street.

My family moved from Toronto last year and as soon as we found our house, we fell in love with both the house and the community. I grew up in Caledon, my parents still reside on Boston Mills Rd and Chinguacousy Rd. I have always loved the peace and quiet that country living provides and my husband and I wanted that for our 2 little boys. In only the few months that we have been here, we have become friends with many of our neighbours on Mill St. Even though my boys are 5 and 2, we allow them to ride their bikes freely on the road and walk to our next door neighbour's houses. I have no fear of them being hit by a car as our road is about 95% of the time only used by the community, whether it be driving, biking or walking.

During the meeting, the word "urbanize" was constantly used by the engineering company that the Town has hired to do this assessment. To me, as well as many of the residents on this street, it is unfortunate that the Town couldn't find engineers who are local and understand the need to keep the country "rural". I understand and agree that Mill St needs to be fixed as it is deteriorating, however, why must this street, which yes is a 2 lane throughway (again something that was repeatedly told to us) but really only used by residents, be classified as the same as say McLaughlin Rd or Creditview Rd, which both see hundreds of cars daily. The Town of Caledon must do a custom plan for our road, as well as a traffic study, otherwise, not only will we lose many trees and property, we will in turn, lose the very community feel of Cheltenham.

Lastly, it was mentioned several times during the meeting that there was a need for access to the trails. Who is this need for? Isn't it Caledon residents who are paying taxes to the Town? It definitely isn't tourists.

Thank you,

[REDACTED]

[REDACTED]

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**MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD**

COMMENT FORM

NAME: _____

EMAIL (OPTIONAL): _____

MAILING ADDRESS (OPTIONAL): _____

☐ *PLEASE ADD ME TO THE STUDY MAILING LIST*

1. Please check the box that applies to you:

- ☐ Local Resident
- ☐ Resident Living Outside the Study Area
- ☐ Technical Agency/Interest Group Representative (Please Identify - _____)
- ☐ Other (Please Identify - _____)

2. Please provide your comments as they pertain to the study and/or materials presented:

a) Problem / Opportunity Statement & Study Area

b) Existing Conditions (Traffic Operations, Safety, Socio-economic, Natural, Cultural etc.)

3. Do you have any other comments regarding the study or materials presented?



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Please email your completed comment form by June 11, 2021 to:

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Engineering Services
Tel: 905.584.2272 x 4040
E-mail: shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
Tel: 416.497.8600 x 1471
E-mail: winnie.wong@rvanderson.com

Please use additional paper if required to complete your comments.

**Information collected will be used in accordance with the Freedom of Information and Protection Privacy Act. With the exception of personal information, all comments will become part of the public record*

THANK YOU FOR YOUR TIME AND EFFORT!

Connor MacIsaac

From: [REDACTED]
Sent: June 10, 2021 8:37 PM
To: Winnie Wong; Shun Cheung
Cc: Connor MacIsaac; Andrew McGregor; [REDACTED]
Subject: Re: Mill Street Improvements - Cheltenham
Attachments: Mill Street Sidewalk Letter_SchA_[REDACTED]_June10_2021.pdf;
Mill.St.Outreach.Comment.Form_[REDACTED]_June10_2021.pdf

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good Evening

Attached are our comments with regard to the presentation dated May 27th.

[REDACTED]

From: [REDACTED] >
Sent: March 2, 2021 4:09 PM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: Shun Cheung <shun.cheung@caledon.ca>; Connor MacIsaac <cmaclsaac@rvanderson.com>; Andrew McGregor <AMcGregor@rvanderson.com>; [REDACTED]
Subject: Re: Mill Street Improvements - Cheltenham

Good afternoon Winnie,

Thank you for your response.

I understood that the Town was discussing the project with households individually to some extent. I also understood that the construction was scheduled for next year (2022).

I was hoping to understand more about how the Town was approaching the project(s) as well as developing and evaluating the alternatives. My concern with seeking input to presented alternatives is that it suggests that the only choices for consideration are those presented. And if the alternatives are already evaluated at the time that they are presented, then the evaluations are based on only the Town's criteria and its weighting of that criteria. With respect, I'm not sure what comments re the study I might have as the only thing I've seen is a letter outlining the project area.

Our address is [REDACTED], which is the corner of Mill and Creditview. We are impacted by both the Mill and Creditview components of the project.

I look forward to hearing from you.

Best Regards

[REDACTED]

Sent from my iPhone

On Mar 2, 2021, at 9:46 AM, Winnie Wong <Winnie.Wong@rvanderson.com> wrote:

Morning [REDACTED]

Can you confirm your address with us please?

Currently we are in the preliminary stages of the study looking at alternatives and talking to the owners whom the Town may need to acquire property or grading easement from. There will be a public information center (PIC) in digital format to present these alternatives and our evaluation when we are ready, and you are invited to provide your comments and feedback then. Once the PIC date is confirmed it will be announced on the Town's project website:

<https://www.caledon.ca/Modules/News/en?CategoryNames=Public+Notices>

In the meantime, if you have comments re the study please email myself or the Town's Project Manager, Shun Cheung (cc'd on this email).

Best regards,

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: [REDACTED]
Sent: February 28, 2021 11:19 AM
To: Shun Cheung <shun.cheung@caledon.ca>
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: Mill Street Improvements - Cheltenham

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good morning,

We understand that discussions are currently occurring with individual home owners along the study area. As our property is probably one of the most adversely affected properties in the study area, as well as in the study area for the connectivity study for Creditview Road, my wife and I would appreciate the opportunity to hear the Town's perspective and provide our own voice to the process. We are happy to meet virtually in order to facilitate a meeting.

Sent from [Mail](#) for Windows 10

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Schedule A

Scope of Project: The entire length of Mill Street from Mississauga Road to Creditview Road and includes road widening, sidewalks/designated pedestrian walkway, cycle lanes and 2 full lanes for motor vehicles

The Problem or Opportunity identified by the Town is to review and address the following:

- Increasing wear and tear on roadway;
- Enhanced road safety for all users;
- Improved accessible active transportation (cycling, walking); and
- Roadway drainage and storm water management.

Increasing Wear and Tear

- Quiet residential or rural/residential in nature
- Resurfacing issues raised involve a very small portion of the road limited to the “S” curve
- Sightline issues raised by the engineers are limited to the same “S” curve section
- The road is generally lightly used by any of vehicles, cyclists or pedestrians
 - The Town has provided no data on vehicle usage in terms of volume or speed. The absence of any relevant data leads to a reasonable belief that such data was not considered in determining the need for the EA study or the project generally

Enhanced Road Safety for all users

- The engineers referred to a number of safety concerns
 - The guiderail near the rail trail was noted
 - There was no further comment from the engineers and no comment on re-installing the post & wire guiderail that was there previously
 - The lack of any attention suggests that it is not a significant concern
 - Sightlines at the top of the “S” curve was identified as a safety concern by the engineers
 - The engineers presented no information on any risk analysis
 - There was no solution presented other than to install sidewalks
 - This presumably is to make way for the motor vehicles
 - The engineers did not discuss the potential for addressing this issue specifically (i.e. cutting the grade down or specific localized pedestrian access up the hill)
 - This was used more as a justification for sidewalks the full length of Creditview
 - The engineers stated that the “S” curve does not conform to the standards for a 40km/h road and is therefore a safety concern
 - The engineers presented no information on any risk analysis
 - Lowering the speed limit to match the roadway seems like a more cost-effective solution than expropriating property and enlarging the roadway

- Lowering the speed limit to match the roadway does not require a study or investment by the Town – it is something that could have already been done if safety was the primary concern
- A section of the roadway was identified as not meeting TAC standards
 - The intention of the Town is to widen the road
 - The engineers presented no data on traffic volumes or speeds on the road
 - There is an abundance of studies that confirm that widening the road will increase traffic speed, however the engineers did not present any ideas for corresponding traffic calming measures
 - Substantive traffic calming measures were essentially dismissed as requiring additional infrastructure
- The engineers noted that at the intersection of Creditview and Mill Street, there was no facility to accommodate pedestrian traffic
 - There was no solution identified other than implying sidewalks as the solution
 - Many Ontario municipalities implement traffic calming measures to slow/limit traffic at residential intersections
- While the engineers cited safety concerns as an impetus for the redevelopment of Mill Street, they presented no data on pedestrian usage, accident rates or complaints to substantiate the concerns
 - Looking in Cheltenham and within the Town for examples of the Town's safety focus, one does not have to go far for both old and new examples of where safety ranks when it comes to redeveloping a road in a residential area
 - Creditview was widened
 - The Town likely has data showing that speeds have increased significantly
 - Safety was compromised as evidence by the parking at the Cheltenham General Store – a vehicle parked at the store must either block the sidewalk or protrude out into the roadway
 - Pedestrians must often walk out onto the roadway to get by
 - This is neither the fault of the General Store or its patrons; it falls squarely on municipal planners and its politicians
 - Evidence suggests that the priority was not pedestrian safety
 - New Valleywood development
 - New residential neighbourhood has vehicles parked in driveways between the sidewalk and the roadway
 - Vehicles are either protruding into the sidewalk, out onto the road or both
 - Evidence suggests that the same priorities exist within the Town

Improved accessible active transportation

- Other than safety concerns addressed above, the active transportation accessibility is only limited by the current state of the pavement at the “S” curve.
- The road is very lightly used by pedestrians because most pedestrian usage is at the rail trail
 - Parking for the trail is primarily across Mississauga Road at the Brickyards and a designated parking lot on Creditview
 - Greater safety concerns exist with the trail at the Mississauga Rd crossing
- The road itself does not limit active transportation

Roadway drainage and storm water management

- Flooding area on the “S’ curve identified by Town
 - The engineers confirmed that this project does not address flooding issues
 - The engineers speculated that it would not make the flooding worse
- Roadway drainage
 - The presenters did not address any roadway drainage issues or how this project affects roadway drainage or storm water management
- It is reasonable to believe that roadway drainage and storm water management are not being addressed in any material way by this project

Other comments made by the engineers

- “The Town determines speed limits”
 - This was in the context of expropriating property from residents in order to make the “S” curve comply with standards for a 40km/h roadway
 - It seemed completely novel to the engineers that lowering the speed limit would change the requirements
 - The engineers admitted that they had not done an evaluation of what speed the current radius permitted according to the standards
 - This is indicative of the thought process employed in this evaluation
- “We would want to minimize the cutting of trees and disruption to property”
 - The engineers presented no information as to anticipated impact on the current streetscape
 - While recognizing that an actual determination cannot be assessed until a proposal is designed, there was no comment or projections on how many trees might have to be removed to accommodate the redevelopment of the roadway
- “A sidewalk plan would not likely include a boulevard”
 - The result would be that snowplows would push road snow onto the sidewalks
- “We would have to see whether the Town would assume responsibility for snow-clearing”

- Currently the respective property owners are responsible for snow-clearing
 - Between the Creditview sidewalk implementation and the proposed sidewalk the full length of Mill Street, how many meters of sidewalk are being added to the village?
 - As the property owners of 14376 Creditview, which is situated on the north-west corner of Creditview and Mill, we would be personally saddled with an additional 300 feet of sidewalk (already responsible for the same corner to the Cheltenham General Store).
- “This is a lovely corridor”
 - This comment was made at the closing and epitomizes the thought process of the engineers
 - This is not viewed by the engineers as a community, it is a travel corridor and nothing more
 - This leads to the reasonable belief that the primary goal is to increase the volume of vehicles and people through this street and the village in general
 - This is different than accommodating tourists, it is about moving people through

Comments Generally

- There was no presentation of any information on the current level of vehicle and pedestrian traffic on Mill Street
- There was no presentation of any information as to the expected volumes of vehicle and pedestrian traffic on Mill Street
- The engineers mentioned that sidewalks were not a “preferred” option, however, they presented no real alternative and the audience would reasonably believe that sidewalks are in fact the solution that the engineers are focused on.
- Having been involved in a similar issue in another village in the Town, the primary interest of the Town infrastructure department, as stated by the department head at the time, is getting the plows down the road
- Our correspondence to the engineers prior to the meeting was met with a response that was essentially
 - We are in the early stages
 - We will determine the options
 - We will evaluate the options
 - We will present the options that we determine
 - Residents may then comment
- This presentation appeared to be nothing more than a formality with no real consultative element
- The presenters did not seem to have an appreciation of the village or its character
- The project appears to be an attempt to urbanize the village to new development standards – make the square peg (Cheltenham) fit the round hole (new urban development).
- The widening of the roadway and implementation of sidewalks will:
 - increase the volume of traffic

- increase the speed of traffic;
 - likely increase the use of Mill Street as a bypass around the Mississauga Road/King St. intersection, and the Town does not seem to be considering traffic calming
- Increasing the hardscaped space and removing a significant number of trees will contribute to pollutants entering the river, and reduce to the streetscape quality.

It is our view that the Town needs to incorporate “improvements” for Cheltenham generally, and the Mill Street project specifically, into the village, not the other way around. It is our view that the engineers that presented in May 2021, do not grasp the concept and their process is to simply take a given set of standards and make the community fit. In making it fit, they do not consider the consequences to the village, its streetscapes or its residents. This is not about “no improvements” it is about respecting the nature of the village in making improvements and addressing the inevitable increase in traffic as urbanization encroaches.

[REDACTED]

Connor MacIsaac

From: Winnie Wong
Sent: July 7, 2021 10:34 AM
To: [REDACTED]
Cc: Andrew McGregor; Connor MacIsaac; Shun Cheung
Subject: RE: Mill St project

[REDACTED]

Please see below for response to your questions.

You have been added to mailing list.

Road allowance/ right of way in front of [REDACTED] varies, almost half of it is 13m while the other half is typically 20m.

Thanks,

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Shun Cheung <Shun.Cheung@caledon.ca>
Sent: June 11, 2021 11:50 AM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: Andrew McGregor <AMcGregor@rvanderson.com>; Connor MacIsaac <cmacIsaac@rvanderson.com>
Subject: FW: Mill St project

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Winnie,

This Mill St resident spelled your name wrong in the email.

Can you please provide her with a response? Thanks!

Shun H. Cheung, P.Eng., PMP
Project Manager
Engineering Services Department

Office: 905.584.2272 x.4040
Cell: 416.436.0910
Email: shun.cheung@caledon.ca

Town of Caledon | www.caledon.ca | www.visitcaledon.ca | Follow us @YourCaledon

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From: [REDACTED]
Sent: Friday, June 11, 2021 7:28 AM
To: Shun Cheung <Shun.Cheung@caledon.ca>; winne.wong@rvanderson.com
Subject: Mill St project

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

I would like to be added to the communication list for past and future updates, as well as for staff to communicate answers to questions asked, but missed by staff during the meeting on May 27.

Specifically, I want to know the road allowance/right of way in front of my property. Winnie said the range on the street is 17-23 feet. What is it at [REDACTED]?

Best,

[REDACTED]

[Sent from Yahoo Mail on Android](#)

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PIC No. 1 Summary Report





Mill Street (Mississauga Road to Creditview Road) Municipal Class Environmental Assessment

Online Public Information Centre Summary Report

Town of Caledon

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RVA 205388

October 14, 2021

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1.0 INTRODUCTION

In October 2020, as part of the 2022 Growth Related Roads project, the Town of Caledon initiated a Schedule “B” Municipal Class Environmental Assessment (Class EA) for improvements to Mill Street between Mississauga Road and Creditview Road. The purpose of the Study is to identify the required improvements to the roadway including considerations for pedestrians, cyclist activities and stormwater drainage. In identifying improvements to the corridor, the Study is considering technical, aesthetic, socio-economic, cultural heritage, and natural environment factors.

R.V. Anderson Associates Limited (RVA) was retained by the Town to complete the Class Environmental Assessment (EA) for this project. This is a Schedule “B” project that covers phases 1 and 2 of the planning process, in accordance with the *Municipal Class Environmental Assessment (October 2000, amended in 2007, 2011 & 2015)*. As part of the Class EA planning and design process, public and technical agency consultation is required in developing the Study recommendations. One Community Outreach Meeting was held on May 27, 2021, in addition to this Online Public Information Centre (PIC).

1.1 Online Public Information Centre

The Town held an Online Public Information Centre (PIC) to consult with area residents on October 5, 2021 from 6 p.m. to 7 p.m. The meeting was attended by both Town staff and RVA’s project team. Residents were invited to participate in the PIC by calling in via phone, or by registering for the live WebEx event.

The PIC included an approximately 30-minute presentation on the MCEA process, existing conditions, alternative solutions, preliminary recommendations, and next steps, followed by an approximately 30-minute question and answer period.

Residents were encouraged to submit their questions using the WebEx Q & A function, or by using the “Raise Your Hand” feature, to ask a question directly to the project team. Questions submitted using the Q & A feature were read aloud to the project team by an RVA moderator and answered during the meeting by the RVA technical staff. Residents who called in were also able to ask questions to the project team directly.

2.0 STAKEHOLDERS AND NOTIFICATION

In accordance with the MCEA, notification to the public and stakeholders is required well in advance for public consultation. Notification of the PIC was provided through various methods and media, as outlined below.

- **General Public:**
 - Notice posted on the Town's website on September 9, 2021
 - Notice published in *Caledon Citizen* (local newspaper) on September 9, 2021 and September 16, 2021
- **Local Residents & Businesses:**
 - Notice mailed to all property owners within the Study area
- **Technical Agencies, Local Interest Groups and First Nations Communities:**
 - Emails containing the notice and details of the PIC were sent on September 17, 2021
- **Project Mailing List (other parties who previously submitted comments or indicated interest in the project)**
 - Email containing the notice and details of the PIC was sent on September 17, 2021

Refer to **Appendix 1** for copies of the published notifications and stakeholder contact list.

3.0 CONSULTATION MATERIAL

The purpose of the Community Outreach Meeting was to introduce the project to the public and share and receive input from the public as part of the development of alternative solutions to be considered for the Mill Street corridor.

The display boards presented at the Community Outreach Meeting are included in **Appendix 2**. The display materials presented information on the following:

- Study Area & Project Background
- Municipal Class EA Process & Study Organization
- Existing Conditions
- Problem or Opportunity Statement
- Incorporation of Public Comments
- Evaluation of Alternative Solutions
- Preliminary Study Recommendations
- Next Steps

Following the PIC, a recording of the meeting, and a copy of the slides presented at the PIC were available for review and comment until on the Town's website. Materials posted to the project website were available in an alternate format (i.e., hard copy) upon request, however no requests were received.

4.0 PARTICIPATION AND COMMENT SUMMARY

4.1 Comments and Questions Received – During Community Outreach Meeting

A total of 25 comments and questions were received through the Q & A function, with four residents providing their comments / question directly to the project team using the “Raise Your Hand” feature.

The comments and the responses are provided in **Appendix 1-3**.

4.2 Additional Questions and Comments Received

Following the Community Outreach Meeting, a video recording of the meeting was posted to the Town’s project website. Members of the public were invited to review the recording and submit their comments and/or questions to the project team by providing their comments to the project team directly via mail e-mail, and/or phone.

A total of 7 residents provided addition comments and/ or questions to the project team following the PIC, including a community response letter signed by several residents. The comments and the responses are provided in **Appendix 1-3**.

5.0 CONCLUSIONS AND NEXT STEPS

The PIC provided an introduction of the key elements of the preliminary preferred solutions to the community and an opportunity for key stakeholders and interested parties to provide their input. Comments on various topics were received, with trends becoming evident including:

- Road safety improvements at intersections and the “S” bend are not strongly supported.
- Residents do not desire any pedestrian and cyclist accommodation throughout the corridor, including through the “S” bend of the roadway
- Concern over property impacts and tree removals.
- Concern over the potential for improvements to increase traffic volumes and disrupt the existing character of the corridor.
- Additional traffic calming measures are supported.

APPENDIX 1

NOTIFICATION MATERIAL

APPENDIX 1-1

Notices



Classic Night at the Movies

In celebration of International Day of Older Persons on October 1, 2021 the Town is hosting a FREE Drive-In Classic Night at the Movies for Caledon residents 55+!

Come and join us to see Mary Poppins.

Gates open at 6:30 p.m.

Free snacks, prizes and fun trivia.

Located at Town Hall Campus in Caledon East.

Registration closes September 30, 2021 at 5 p.m.
Spaces are limited.



Details and registration available online:
caledon.ca/adult55 or by calling
905-584-2272 x.7750

Fall Recreation Programs

REGISTRATION NOW OPEN TO RESIDENTS



More fitness, aquatics, skating and general interest programs will be available soon for you to participate in. For more information, visit caledon.ca/RecPrograms.

Non-resident registration begins September 15 | 8 a.m.

We will continue to promote and follow public health guidelines for the safety of our staff and participants.

Municipal and Community Grants (MACG)

Is your not-for-profit community organization planning an event or working on a new project in 2022 that will enrich the lives of Caledon residents? You may qualify to receive funding through the Town's MACG program.

Additional funding is also available through the program to support the community's recovery from the pandemic. This includes assistance for not-for-profit organizations with less revenue or increased costs due to the pandemic.

For more information visit caledon.ca/MACG or call 905-584-2272 x 4521.

Deadline to apply is: October 15, 2021.



School Green Fund

Are you interested in 'greening' your classroom or school in the 2021/2022 school year?

Apply now to the School Green Fund at: caledon.ca/greenfund.

Deadline to apply is October, 1, 2021 at 5 p.m.



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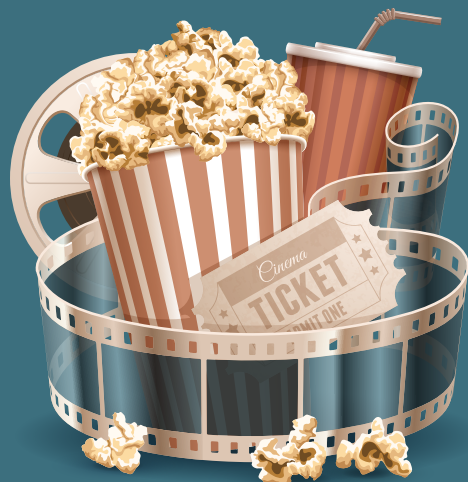
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PUBLIC NOTICE



NOTICE OF PUBLIC MEETING

Proposed Draft Plan of Subdivision and Zoning By-law Amendment

FILE NUMBER(S): 21T-21001C and RZ 2021-0005

A Public Meeting will be held to consider proposed Draft Plan of Subdivision and Zoning By-law Amendment applications. Your input and insight will inform and shape plans that meet the needs of our community.

Due to continuing efforts to contain the spread of COVID-19 and to protect individuals, Town Hall will not be open to the public to attend Public Meetings until further notice. These meetings will be held electronically.

Meeting Date: October 5, 2021

Public Meeting Start Time: 7 p.m.

To listen to the Meeting (but not participate), you may call 1-833-311-4101, Meeting Access Code: 132 586 5046#.

Members of the public are welcome to participate via phone or WebEx Meeting upon request. For more information on how to participate, please visit caledon.ca/notices or contact the Planning Department.

Please note that there are deadlines by which the request to participate need to be made.

You can also ask questions or provide comments by:

- Submitting written correspondence to agenda@caledon.ca and planning@caledon.ca for consideration during the Public Meeting. Please note that written comments will form part of the public record.
- Contacting Planning staff or the Lead Planner during office hours by phone or email.
- Booking an appointment to visit Town Hall to speak with Planning staff. Visit caledon.ca/development for more information.

APPLICANT:

KLM Planning Partners Inc. on behalf of Carantania Investments (BT) Inc.

WARD 5 LOCATION:

9229 5th Sideroad, 0 Pembroke Street, 0 Hanton Crescent, 26 Hanton Crescent, 28 Hanton Crescent, 0 Ernest Biason Boulevard South side of 5th Sideroad, south of Queensgate Boulevard, between Autumn Oak Court and Ernest Biason Boulevard, Bolton Please visit caledon.ca/notices for a location map showing all properties subject to these applications

PROPOSED CHANGES:

The applicant has applied for a Draft Plan of Subdivision and Zoning By-law Amendment to facilitate the development of 84 single detached dwelling units, a park block, and internal streets. The proposed Zoning By-law Amendment seeks to rezone the subject properties from Open Space (OS), Mixed Density Residential (RMD) and Residential One Exception Zones (R1- 69, R1-97, R1-103, R1-104, R1-107), to Open Space (OS) and Residential One – Exception XXX (R1-XXX) with site specific permissions.



The illustration is a conceptual plan for information purposes and it is subject to change. The application is currently under review by the Town of Caledon.

LEAD PLANNER:

Aleah Clarke, MHBC, on behalf of Development Review Services, Planning Department, Town of Caledon
905-584-2272 x.7338
aclarke@mhbcpplan.com

ADDITIONAL INFORMATION:

For more information about this matter, including information about appeal rights, a copy of the proposed Draft Plan of Subdivision and/or Zoning By-law Amendment, additional information and material, please visit caledon.ca/notices or contact the Lead Planner at Town Hall from Monday to Friday, 8:30 a.m. to 4:30 p.m.

ACCESSIBILITY:

If you require an accessibility accommodation to access any materials related to this item in an alternate format please contact Legislative Services by phone at 905.584.2272 x.2366 or via email at legislative.services@caledon.ca.

Multi-Tenant Landowners: Please post this notice in a visible location to all residents of the land.

Notice Date: September 9, 2021



6311 Old Church Road
Caledon, ON L7C 1J6
www.caledon.ca
T. 905.584.2272 | 1.888.225.3366 | F. 905.584.4325

Size: 1/2 Vertical

Color: YES

Date: September 9, 2021

Distribution: Caledon Enterprise

Department: Cost Recovery

Account#: Christine Halis -KLM Planning Partners Inc.

Run X1

PUBLIC NOTICE



NOTICE OF PUBLIC MEETING

Proposed Official Plan Amendment and Zoning By-law Amendment

FILE NUMBER(S): POPA 2021-0001 AND RZ 2021-0003 RELATED FILE NUMBER: SPA 2021-0004

A Public Meeting will be held to consider proposed Official Plan Amendment and Zoning By-law Amendment applications. Your input and insight will inform and shape plans that meet the needs of our community.

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- Contacting Planning staff or the Lead Planner during office hours by phone or email.
- Booking an appointment to visit Town Hall to speak with Planning staff.
Visit caledon.ca/development for more information.

APPLICANT:

KLM Planning Partners Inc. on behalf of Guiseppe Scenna and Albion-Vaughan (12148) Inc.

WARD 5 LOCATION:

12148 Albion Vaughan Road
Part Lot 1, Concession 7 (ALB)
West side of Albion Vaughan Road, north of Mayfield Road and south of Tormore Road, Bolton

PROPOSED CHANGES:

The applicant has applied for an Official Plan Amendment and Zoning By-law Amendment to facilitate the development of two six-storey apartment buildings (240 units) and two three-storey townhouse blocks (10 dwelling units) serviced by municipal water and wastewater services. The Official Plan Amendment proposes to re-designate the lands from Special Residential to Special Residential with site specific policies and Environmental Policy Area. The Zoning By-law Amendment proposes to rezone the lands from Rural Residential (RR) to Multiple Residential – Exception XXX (RM-XXX) with site specific permissions.



The illustration is a conceptual plan for information purposes and it is subject to change. The application is currently under review by the Town of Caledon.

LEAD PLANNER:

Aleah Clarke, MHBC, on behalf of Development Review Services, Planning Department, Town of Caledon
905-584-2272 x.7338
acleah@mhbcplan.com

ADDITIONAL INFORMATION:

For more information about this matter, including information about appeal rights, a copy of the proposed Official Plan Amendment and/or Zoning By-law Amendment, additional information and material, please visit caledon.ca/notices or contact the Lead Planner at

Town Hall from Monday to Friday, 8:30 a.m. to 4:30 p.m.

ACCESSIBILITY:

If you require an accessibility accommodation to access any materials related to this item in an alternate format please contact Legislative Services by phone at 905.584.2272 x.2366 or via email at legislative.services@caledon.ca.

Multi-Tenant Landowners: Please post this notice in a visible location to all residents of the land.

Notice Date: September 9, 2021



6311 Old Church Road
Caledon, ON L7C 1J6
www.caledon.ca
T. 905.584.2272 | 1.888.225.3366 | F. 905.584.4325

Size: 1/2 Vertical
Color: YES
Date: September 9, 2021
Distribution: Caledon Enterprise
Department: Cost Recovery
Account#: Grant Uyeyama-KLM Planning Partners Inc.

Run X1

NOTICE OF PUBLIC MEETING

Proposed Zoning By-law Amendment

FILE NUMBER(S): RZ 2020-0010 - RELATED FILE NUMBER: SPA 2020-0044

A Public Meeting will be held to consider a proposed Zoning By-law Amendment. Your input and insight will inform and shape plans that meet the needs of our community.

Due to continuing efforts to contain the spread of COVID-19 and to protect individuals, Town Hall will not be open to the public to attend Public Meetings until further notice. These meetings will be held electronically.

Meeting Date: October 5, 2021**Public Meeting Start Time: 7 p.m.**

To listen to the Meeting (but not participate), you may call 1-833-311-4101, Meeting Access Code: 132 586 5046#.

Members of the public are welcome to participate via phone or WebEx Meeting upon request. For more information on how to participate, please visit caledon.ca/notices or contact the Planning Department.

Please note that there are deadlines by which the request to participate need to be made.

You can also ask questions or provide comments by:

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- Contacting Planning staff or the Lead Planner during office hours by phone or email.
- Booking an appointment to visit Town Hall to speak with Planning staff. Visit caledon.ca/development for more information.

APPLICANT:

Antrix Architects on behalf of
2752324 Ontario Inc.

WARD 5 LOCATION:

12476 Highway 50 Part Lot 3,
Concession 6 (Albion),
designated as Parts 12 and 13
on Plan 43R-25176 North-west
Corner of Highway 50 and
George Bolton Parkway

PROPOSED CHANGES:

To amend the Zoning By-law for the subject lands from Bolton Highway Commercial (CHB) to Bolton Highway Commercial – Exception XXX (CHB-XXX) to introduce site specific standards, including but not limited to changing the permitted uses, increasing the permitted building height, and changing planting strip widths, exterior side yard setbacks and parking space setbacks, to facilitate the development of a 5-storey hotel on site.



The illustration is a conceptual plan for information purposes and it is subject to change. The application is currently under review by the Town of Caledon.

LEAD PLANNER:

Richard Martin, Community Development Planner
905.584.2272 x. 4283
richard.martin@caledon.ca

ADDITIONAL INFORMATION:

For more information about this matter, including information about appeal rights, a copy of the proposed Zoning By-law Amendment, additional information and material, please visit caledon.ca/notices or contact the Lead Planner at Town Hall from Monday to Friday, 8:30 a.m. to 4:30 p.m.

ACCESSIBILITY:

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Notice Date: September 9, 2021



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T. 905.584.2272 | 1.888.225.3366 | F. 905.584.4325

Size: 1/2 Vertical

Color: YES

Date: September 9, 2021

Distribution: Caledon Enterprise

Department: Cost Recovery

Account#: Shashank Chitale, Antrix Architects Inc.

Run X1

MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY

Mississauga Road to Creditview Road

THE STUDY

The Town of Caledon is currently assessing improvements to Mill Street between Mississauga Road and Creditview Road.

The purpose of this Schedule "B" Municipal Class Environmental Assessment is to identify the required improvements to the road including considerations for pedestrians, cyclist activities and drainage.

As part of the study, a Community Outreach Meeting was held in May 2021, to present and obtain information on the key issues and constraints within the study area. In reviewing comments received during and after the Community Outreach Meeting, the Town has moved forward with the development and evaluation of alternative solutions and the determination of a preliminary recommended solution.

**ONLINE PUBLIC INFORMATION CENTRE**

Due to the continuing efforts to contain the spread of COVID-19 and to protect individuals, we invite you to join us for an Online Public Information Centre via WebEx webinar. The webinar will allow the project team to share information and receive input from the public on the study to date, including the problem / opportunity, existing conditions, alternative solutions, and the preliminary recommended solution.

To listen to the meeting by phone, dial 1-416-216-5643. Use Meeting Access Code: 2632 256 0301.

To watch the meeting through WebEx online and to learn more about the project, visit:

caledon.ca/notices.

Meeting date: Tuesday October 5

Time: 6 p.m. | Followed by a question and answer period.

CONTACT

To be added to the project mailing list, and for any questions, comments or concerns, please contact:

Town of Caledon
Shun H. Cheung, P.Eng., PMP
Project Manager, Engineering Services
Tel: 905.584.2272 x 4040
E-mail: shun.cheung@caledon.ca

R.V. Anderson Associates Limited
Winnie Wong, P.Eng., PMP, M.Eng.
Project Manager
Tel: 416.497.8600 x 1471
E-mail: winnie.wong@rvanderson.com

This notice was first issued September 9, 2021.



6311 Old Church Road
Caledon, ON L7C 1J6
www.caledon.ca
T. 905.584.2272 | 1.888.225.3366 | F. 905.584.4325

Size: 1/2 Vertical

Color: YES

Date: September 9, 2021

Distribution: Caledon Enterprise

Department: Engineering & Capital Projects

Account#:02-09-265-20023-090-69001

Run X1

PUBLIC NOTICE



NOTICE OF PUBLIC MEETING

Proposed Draft Plan of Subdivision and Zoning By-law Amendment

FILE NUMBER(S): 21T-21001C and RZ 2021-0005

A Public Meeting will be held to consider proposed Draft Plan of Subdivision and Zoning By-law Amendment applications. Your input and insight will inform and shape plans that meet the needs of our community.

Due to continuing efforts to contain the spread of COVID-19 and to protect individuals, Town Hall will not be open to the public to attend Public Meetings until further notice. These meetings will be held electronically.

Meeting Date: October 5, 2021

Public Meeting Start Time: 7 p.m.

To listen to the Meeting (but not participate), you may call 1-833-311-4101, Meeting Access Code: 132 586 5046#.

Members of the public are welcome to participate via phone or WebEx Meeting upon request. For more information on how to participate, please visit caledon.ca/notices or contact the Planning Department.

Please note that there are deadlines by which the request to participate need to be made.

You can also ask questions or provide comments by:

- Submitting written correspondence to agenda@caledon.ca and planning@caledon.ca for consideration during the Public Meeting. Please note that written comments will form part of the public record.
- Contacting Planning staff or the Lead Planner during office hours by phone or email.
- Booking an appointment to visit Town Hall to speak with Planning staff. Visit caledon.ca/development for more information.

APPLICANT:

KLM Planning Partners Inc. on
behalf of Carantania Investments (BT) Inc.

WARD 5 LOCATION:

9229 5th Sideroad, 0 Pembrook Street, 0 Hanton Crescent, 26 Hanton Crescent, 28 Hanton Crescent, 0 Ernest Biason Boulevard
South side of 5th Sideroad, south of Queensgate Boulevard,
between Autumn Oak Court and Ernest Biason Boulevard, Bolton
Please visit caledon.ca/notices for a location map showing all
properties subject to these applications

PROPOSED CHANGES:

The applicant has applied for a Draft Plan of Subdivision and Zoning By-law Amendment to facilitate the development of 84 single detached dwelling units, a park block, and internal streets. The proposed Zoning By-law Amendment seeks to rezone the subject properties from Open Space (OS), Mixed Density Residential (RMD) and Residential One Exception Zones (R1- 69, R1-97, R1-103, R1-104, R1-107), to Open Space (OS) and Residential One – Exception XXX (R1-XXX) with site specific permissions.



The illustration is a conceptual plan for information purposes and it is subject to change. The application is currently under review by the Town of Caledon.

LEAD PLANNER:

Aleah Clarke, MHBC, on behalf of Development Review Services, Planning Department, Town of Caledon
905-584-2272 x.7338
aclarke@mhbcpplan.com

ADDITIONAL INFORMATION:

For more information about this matter, including information about appeal rights, a copy of the proposed Draft Plan of Subdivision and/or Zoning By-law Amendment, additional information and material, please visit caledon.ca/notices or contact the Lead Planner at Town Hall from Monday to Friday, 8:30 a.m. to 4:30 p.m.

ACCESSIBILITY:

If you require an accessibility accommodation to access any materials related to this item in an alternate format please contact Legislative Services by phone at 905.584.2272 x.2366 or via email at legislative.services@caledon.ca.

Multi-Tenant Landowners: Please post this notice in a visible location to all residents of the land.

Notice Date: September 9, 2021



6311 Old Church Road
Caledon, ON L7C 1J6
www.caledon.ca
T. 905.584.2272 | 1.888.225.3366 | F. 905.584.4325

Size: 1/2 Vertical

Color: YES

Date: September 9, 2021

Distribution: Caledon Citizen

Department: Cost Recovery

Account#: Christine Halis -KLM Planning Partners Inc.

Run X1

PUBLIC NOTICE



NOTICE OF PUBLIC MEETING

Proposed Official Plan Amendment and Zoning By-law Amendment

FILE NUMBER(S): POPA 2021-0001 AND RZ 2021-0003 RELATED FILE NUMBER: SPA 2021-0004

A Public Meeting will be held to consider proposed Official Plan Amendment and Zoning By-law Amendment applications. Your input and insight will inform and shape plans that meet the needs of our community.

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Meeting Date: October 5, 2021

Public Meeting Start Time: 7 p.m.

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- Booking an appointment to visit Town Hall to speak with Planning staff.
Visit caledon.ca/development for more information.

APPLICANT:

KLM Planning Partners Inc. on behalf of Guiseppe Scenna and Albion-Vaughan (12148) Inc.

WARD 5 LOCATION:

12148 Albion Vaughan Road
Part Lot 1, Concession 7 (ALB)
West side of Albion Vaughan Road, north of Mayfield Road and south of Tormore Road, Bolton

PROPOSED CHANGES:

The applicant has applied for an Official Plan Amendment and Zoning By-law Amendment to facilitate the development of two six-storey apartment buildings (240 units) and two three-storey townhouse blocks (10 dwelling units) serviced by municipal water and wastewater services. The Official Plan Amendment proposes to re-designate the lands from Special Residential to Special Residential with site specific policies and Environmental Policy Area. The Zoning By-law Amendment proposes to rezone the lands from Rural Residential (RR) to Multiple Residential – Exception XXX (RM-XXX) with site specific permissions.



The illustration is a conceptual plan for information purposes and it is subject to change. The application is currently under review by the Town of Caledon.

LEAD PLANNER:

Aleah Clarke, MHBC, on behalf of Development Review Services, Planning Department, Town of Caledon
905-584-2272 x.7338
acleah@mhbcpplan.com

ADDITIONAL INFORMATION:

For more information about this matter, including information about appeal rights, a copy of the proposed Official Plan Amendment and/or Zoning By-law Amendment, additional information and material, please visit caledon.ca/notices or contact the Lead Planner at

Town Hall from Monday to Friday, 8:30 a.m. to 4:30 p.m.

ACCESSIBILITY:

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Multi-Tenant Landowners: Please post this notice in a visible location to all residents of the land.

Notice Date: September 9, 2021



TOWN OF CALEDON

6311 Old Church Road
Caledon, ON L7C 1J6
www.caledon.ca
T. 905.584.2272 | 1.888.225.3366 | F. 905.584.4325

Size: 1/2 Vertical

Color: YES

Date: September 9, 2021

Distribution: Caledon Citizen

Department: Cost Recovery

Account#: Grant Uyeyama-KLM Planning Partners Inc.

Run X1

PUBLIC NOTICE



NOTICE OF PUBLIC MEETING

Proposed Zoning By-law Amendment

FILE NUMBER(S): RZ 2020-0010 - RELATED FILE NUMBER: SPA 2020-0044

A Public Meeting will be held to consider a proposed Zoning By-law Amendment. Your input and insight will inform and shape plans that meet the needs of our community.

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APPLICANT:

Antrix Architects on behalf of
2752324 Ontario Inc.

WARD 5 LOCATION:

12476 Highway 50 Part Lot 3,
Concession 6 (Albion),
designated as Parts 12 and 13
on Plan 43R-25176 North-west
Corner of Highway 50 and
George Bolton Parkway

PROPOSED CHANGES:

To amend the Zoning By-law for the subject lands from Bolton Highway Commercial (CHB) to Bolton Highway Commercial – Exception XXX (CHB-XXX) to introduce site specific standards, including but not limited to changing the permitted uses, increasing the permitted building height, and changing planting strip widths, exterior side yard setbacks and parking space setbacks, to facilitate the development of a 5-storey hotel on site.



The illustration is a conceptual plan for information purposes and it is subject to change. The application is currently under review by the Town of Caledon.

LEAD PLANNER:

Richard Martin, Community Development Planner
905.584.2272 x. 4283
richard.martin@caledon.ca

ADDITIONAL INFORMATION:

For more information about this matter, including information about appeal rights, a copy of the proposed Zoning By-law Amendment, additional information and material, please visit caledon.ca/notices or contact the Lead Planner at Town Hall from Monday to Friday, 8:30 a.m. to 4:30 p.m.

ACCESSIBILITY:

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Multi-Tenant Landowners: Please post this notice in a visible location to all residents of the land.

Notice Date: September 9, 2021



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Caledon, ON L7C 1J6
www.caledon.ca
T. 905.584.2272 | 1.888.225.3366 | F. 905.584.4325

Size: 1/2 Vertical

Color: YES

Date: September 9, 2021

Distribution: Caledon Citizen

Department: Cost Recovery

Account#: Shashank Chitale, Antrix Architects Inc.

Run X1



MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY

Mississauga Road to Creditview Road

THE STUDY

The Town of Caledon is currently assessing improvements to Mill Street between Mississauga Road and Creditview Road.

The purpose of this Schedule "B" Municipal Class Environmental Assessment is to identify the required improvements to the road including considerations for pedestrians, cyclist activities and drainage.

As part of the study, a Community Outreach Meeting was held in May 2021, to present and obtain information on the key issues and constraints within the study area. In reviewing comments received during and after the Community Outreach Meeting, the Town has moved forward with the development and evaluation of alternative solutions and the determination of a preliminary recommended solution.



ONLINE PUBLIC INFORMATION CENTRE

Due to the continuing efforts to contain the spread of COVID-19 and to protect individuals, we invite you to join us for an Online Public Information Centre via WebEx webinar. The webinar will allow the project team to share information and receive input from the public on the study to date, including the problem / opportunity, existing conditions, alternative solutions, and the preliminary recommended solution.

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To watch the meeting through WebEx online and to learn more about the project, visit:

caledon.ca/notices.

Meeting date: Tuesday October 5

Time: 6 p.m. | Followed by a question and answer period.

CONTACT

To be added to the project mailing list, and for any questions, comments or concerns, please contact:

Town of Caledon
Shun H. Cheung, P.Eng., PMP
Project Manager, Engineering Services
Tel: 905.584.2272 x 4040
E-mail: shun.cheung@caledon.ca

R.V. Anderson Associates Limited
Winnie Wong, P.Eng., PMP, M.Eng.
Project Manager
Tel: 416.497.8600 x 1471
E-mail: winnie.wong@rvanderson.com

This notice was first issued September 9, 2021.



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Size: 1/2 Vertical

Color: YES

Date: September 9, 2021

Distribution: Caledon Citizen

Department: Engineering & Capital Projects

Account#: 02-09-265-20023-090-69001

Run X1

PUBLIC NOTICE



NOTICE OF PUBLIC MEETING

Proposed Official Plan Amendment and Zoning By-law Amendment

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Visit caledon.ca/development for more information.

APPLICANT:

KLM Planning Partners Inc. on behalf of Guisepe Scenna and Albion-Vaughan (12148) Inc.

WARD 5 LOCATION:

12148 Albion Vaughan Road
Part Lot 1, Concession 7 (ALB)
West side of Albion Vaughan Road, north of Mayfield Road and south of Tormore Road, Bolton

PROPOSED CHANGES:

The applicant has applied for an Official Plan Amendment and Zoning By-law Amendment to facilitate the development of two six-storey apartment buildings (240 units) and two three-storey townhouse blocks (10 dwelling units) serviced by municipal water and wastewater services. The Official Plan Amendment proposes to re-designate the lands from Special Residential to Special Residential with site specific policies and Environmental Policy Area. The Zoning By-law Amendment proposes to rezone the lands from Rural Residential (RR) to Multiple Residential – Exception XXX (RM-XXX) with site specific permissions.



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LEAD PLANNER:

Aleah Clarke, MHBC, on behalf of Development Review Services, Planning Department, Town of Caledon
905-584-2272 x.7338
aclarke@mhbcplan.com

ADDITIONAL INFORMATION:

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Town Hall from Monday to Friday, 8:30 a.m. to 4:30 p.m.

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Notice Date: September 9, 2021



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Caledon, ON L7C 1J6
www.caledon.ca
T. 905.584.2272 | 1.888.225.3366 | F. 905.584.4325

Size: 1/2 Vertical
Color: YES
Date: September 9, 2021
Distribution: Vaughan Citizen
Department: Cost Recovery
Account#: Grant Uyeyama-KLM Planning Partners Inc.

Run X1

**MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD**

THE STUDY

The Town of Caledon is currently assessing improvements to Mill Street between Mississauga Road and Creditview Road.

The purpose of this Schedule “B” Municipal Class Environmental Assessment is to identify the required improvements to the road including considerations for pedestrians, cyclist activities and drainage.

As part of the study, a Community Outreach Meeting was held in May 2021, to present and obtain information on the key issues and constraints within the study area. In reviewing comments received during and after the Community Outreach Meeting, the Town has moved forward with the development and evaluation of alternative solutions and the determination of a preliminary recommended solution.



ONLINE PUBLIC INFORMATION CENTRE

Due to the continuing efforts to contain the spread of COVID-19 and to protect individuals, we invite you to join us for an Online Public Information Centre on **Tuesday, October 5 at 6 p.m.** via WebEx webinar. The webinar will allow the project team to share information and receive input from the public on the study to date, including the problem / opportunity, existing conditions, alternative solutions, and the preliminary recommended solution. A Question-and-Answer period will follow to conclude at 7 p.m.

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CONTACT

To be added to the project mailing list, and for any questions, comments or concerns, please contact:

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Engineering Services
Engineering Services
Tel: 905.584.2272 x 4040
E-mail: shun.cheung@caledon.ca

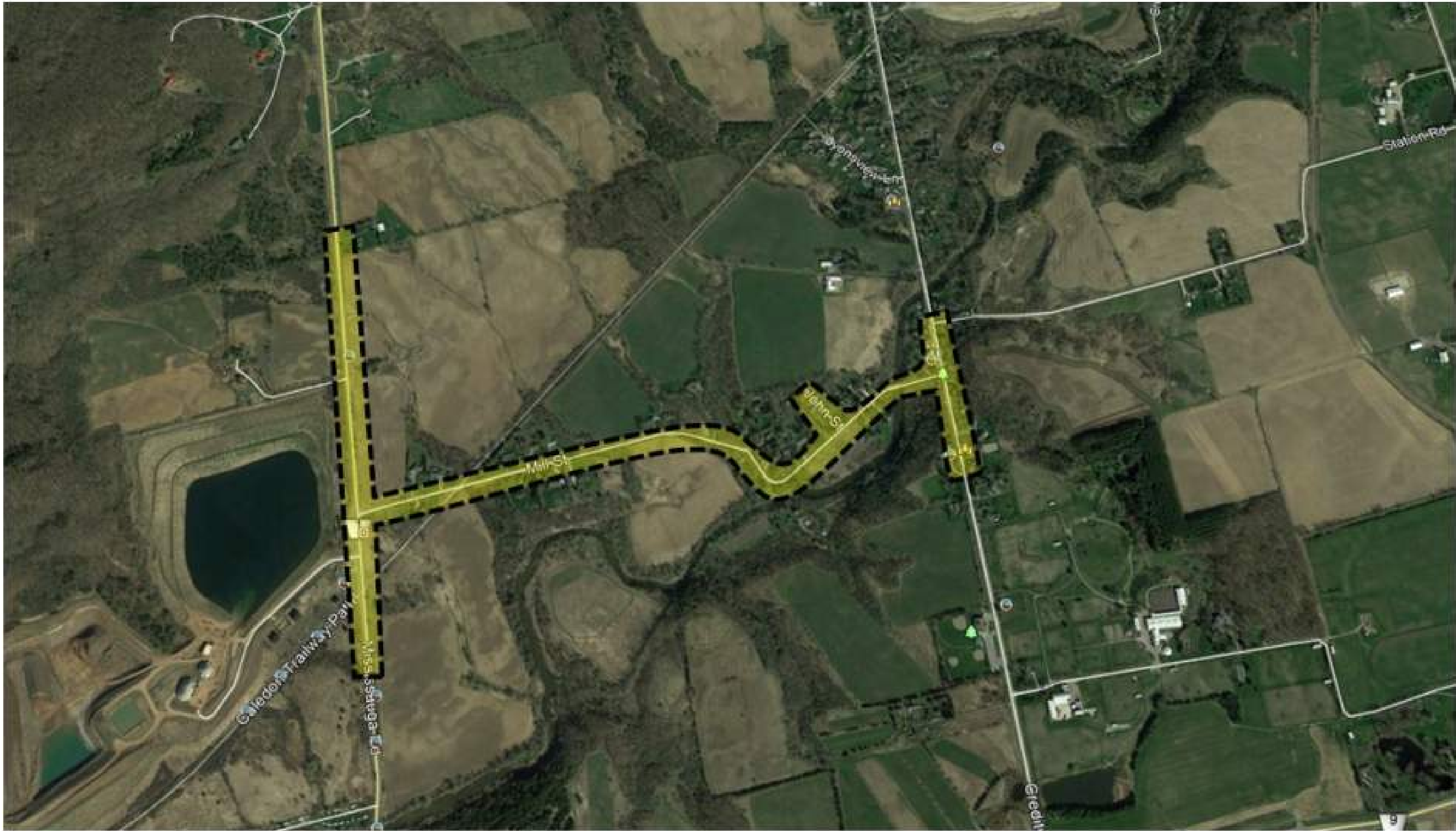
R.V. Anderson Associates Limited

Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
Tel: 416.497.8600 x 1471
E-mail: winnie.wong@rvanderson.com

This notice was first issued September 9, 2021.

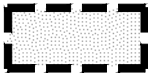
APPENDIX 1-2


Notice Distribution Area




LEGEND:

NOTIFICATION DISTRIBUTION AREA





TOWN OF CALEDON



R.V. Anderson Associates Limited
engineering • environment • infrastructure

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT
MILL STREET (MISSISSAUGA ROAD TO CREDITVIEW ROAD)

NOTIFICATION DISTRIBUTION AREA

APPENDIX 2

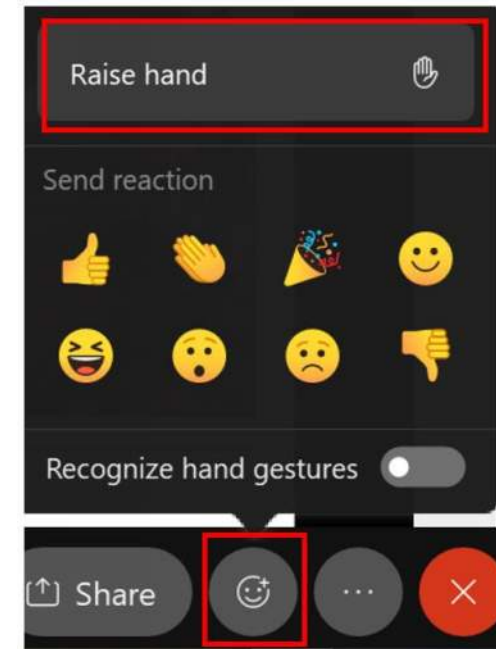
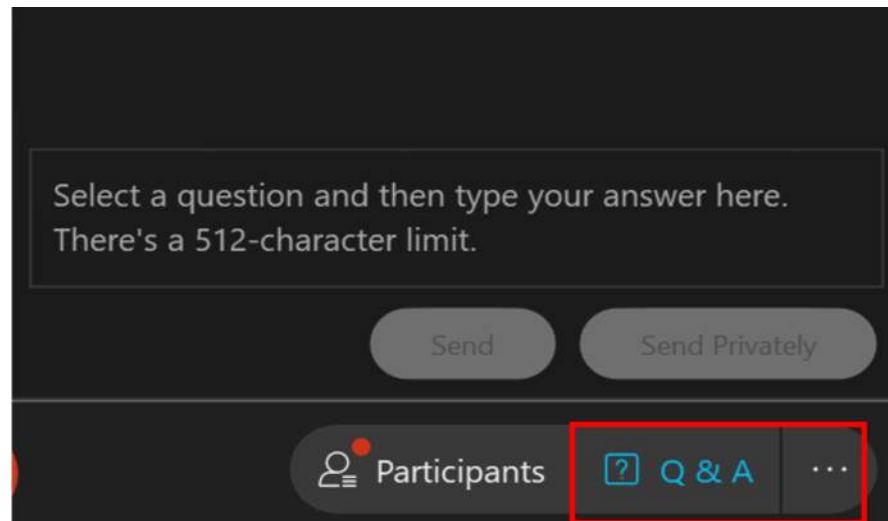
DISPLAY MATERIALS

HOW TO PARTICIPATE – Q & A



Following the presentation, a Question-and-Answer Period will be held, concluding at 7 P.M.

- Please submit any questions you may have using the Q & A feature
- If you would like to speak, raise your hand using the “raise hand” button under the “Reactions” window and you will be unmuted by a member of the project team
- Phone-in participants must dial *3 to raise your hand



MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT

MILL STREET (MISSISSAUGA ROAD TO CREDITVIEW ROAD)



ONLINE PUBLIC INFORMATION CENTRE

October 5, 2021

6:00pm to 7:00pm

Please submit any questions you may have using the Q & A function.

Presentation materials, including a recording of the presentation will be available following the presentation for review and comment on the project website at

<https://www.caledon.ca/en/news/mill-street-ea.aspx>

PRESENTATION AGENDA

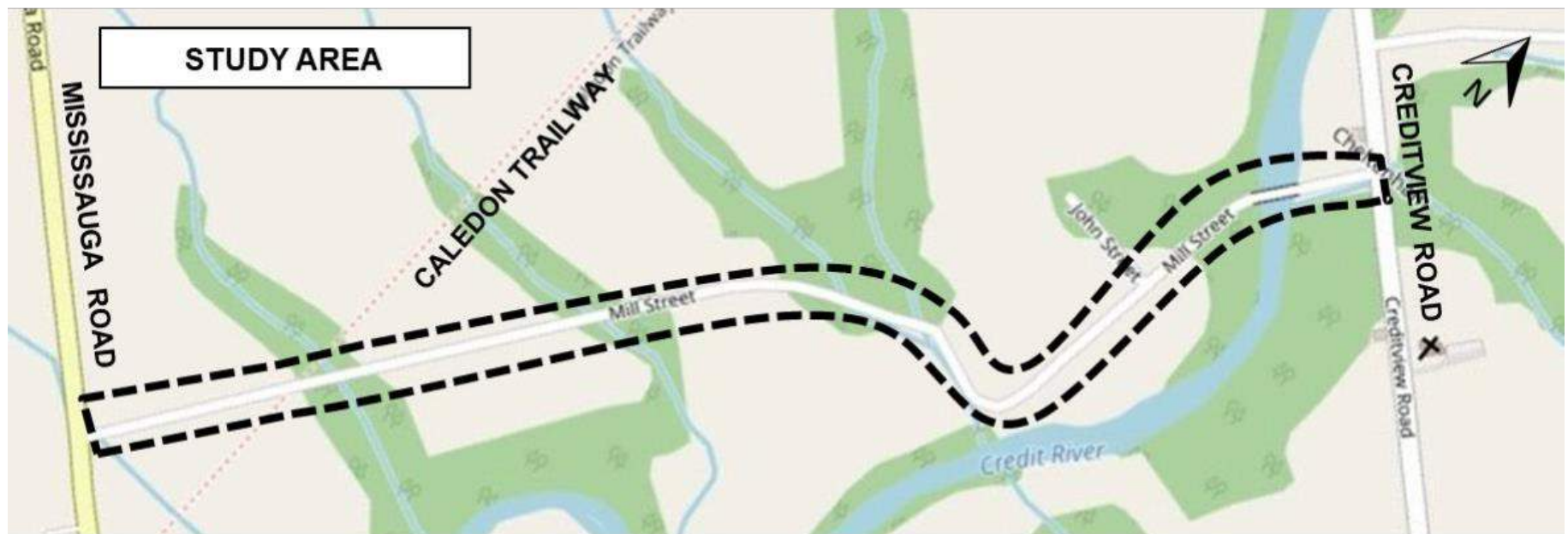


- Study Area & Project Background
- Municipal Class EA Process
- Problem or Opportunity Statement
- Incorporating Your Comments
- Evaluation of Alternative Solutions
- Preliminary Study Recommendations
- Next Steps in the Project
- Question and Answer Period

STUDY AREA



The Mill Street EA study area consists of Mill Street from Mississauga Road to Creditview Road.



Two-lane local road under the jurisdiction of the Town of Caledon.

Town is responsible for maintaining safety and traffic operations for all users within and outside of Cheltenham Community.

MUNICIPAL CLASS EA PROCESS

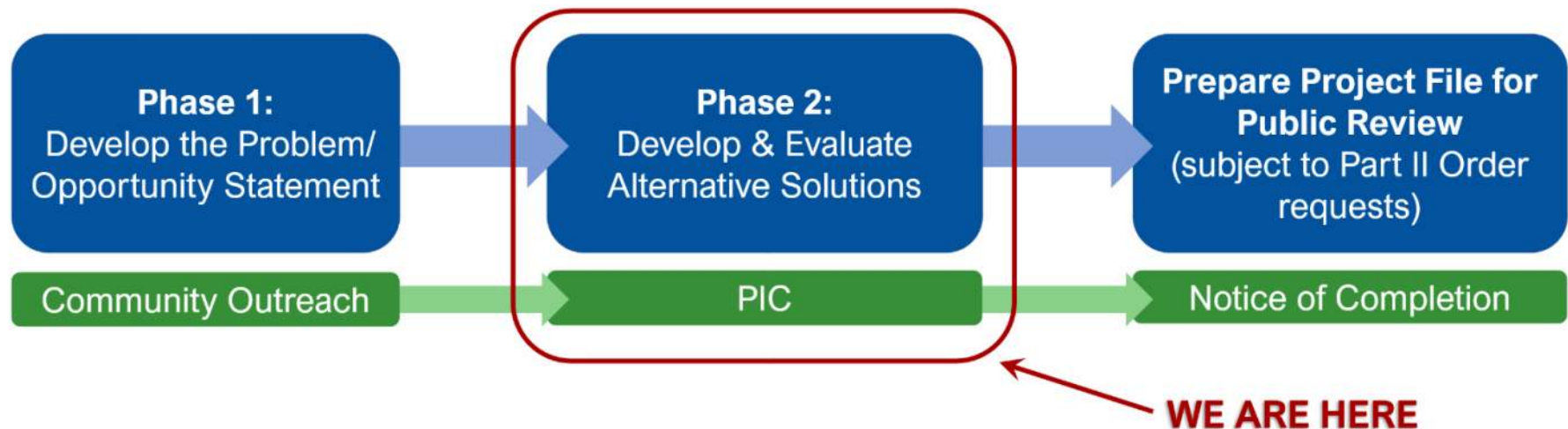


Undertaken prior to municipal **road, water, wastewater** and **other municipal** construction projects.

Requires consideration of all **reasonable alternatives**.

Aims to minimize **impact on the natural, cultural, social** and **economic environment**.

Input from the **public, stakeholders** and **technical agencies** is essential.



This Mill Street Class EA is classified as a **Schedule 'B' Municipal Class EA** and is subject to **Phases 1 and 2** of the **Municipal Class Environmental Assessment**.

PROBLEM OR OPPORTUNITY STATEMENT



The Mill Street Class EA was initiated to review opportunities within the study area to address:

- Roadway surface issues (increased wear and tear)
- Traffic operations and road design considerations
- Accessibility and safety for vulnerable road users (pedestrians and cyclists)
- Roadway drainage and stormwater management

The Problem/Opportunity Statement outlines the need and justification for the overall project and establishes the general parameters, or scope, of the study

EXISTING CONDITIONS- SUMMARY



Uncontrolled pedestrian crossing at Caledon Trail

No guiderail protecting steep embankments

Existing road is within private property

Limited sightlines due to steep roadway grades and tight curves. Drivers may not be able to stop in time to avoid pedestrians on road.

Flooding issue noted

- Existing pavement width (6m) is too narrow for two-lane road per TAC and Town's design standards
- No facility to accommodate pedestrian traffic

TRAFFIC OPERATIONS & SAFETY



- A Traffic Study to assess speed patterns and driver behaviour identified operating speeds on Mill Street to be above the 40 km/h **Posted Speed**, with the following vehicle operation speed:
 - Approximately 50-55 km/h on tangent (straight) sections; and
 - Approximately 40-50 km/h near the sharp curves.
- Sharp curves can lead to vehicles **crossing the centreline** or **encroaching into the shoulder**, especially on wet pavement.
- **Sightlines reduced** due to roadside vegetation/ topography and horizontal and vertical curvature of the roadway.
- **No separation** between vulnerable road users (pedestrians and cyclists) and vehicles through the curves.
- Space between traffic lane and obstructions i.e. trees and hydro poles, do not meet minimum separation requirements i.e. Clear Zone



NATURAL ENVIRONMENT



Located within the Credit River subwatershed, the study area includes Credit Valley Conservation Authority (CVC) Regulated Watercourses, lands designated as Natural Heritage System by the Town of Caledon, as well as lands designated under The Greenbelt Plan Area and the Niagara Escarpment Plan.



ARCHAEOLOGICAL ENVIRONMENT



Some of the undisturbed lands adjacent to Mill Street **exhibit archaeological potential** due to proximity to:

- Water Sources (**Credit River**),
- **Historic transportation routes**,
- Early settlements (**Cheltenham**); and
- Well-drained soils.



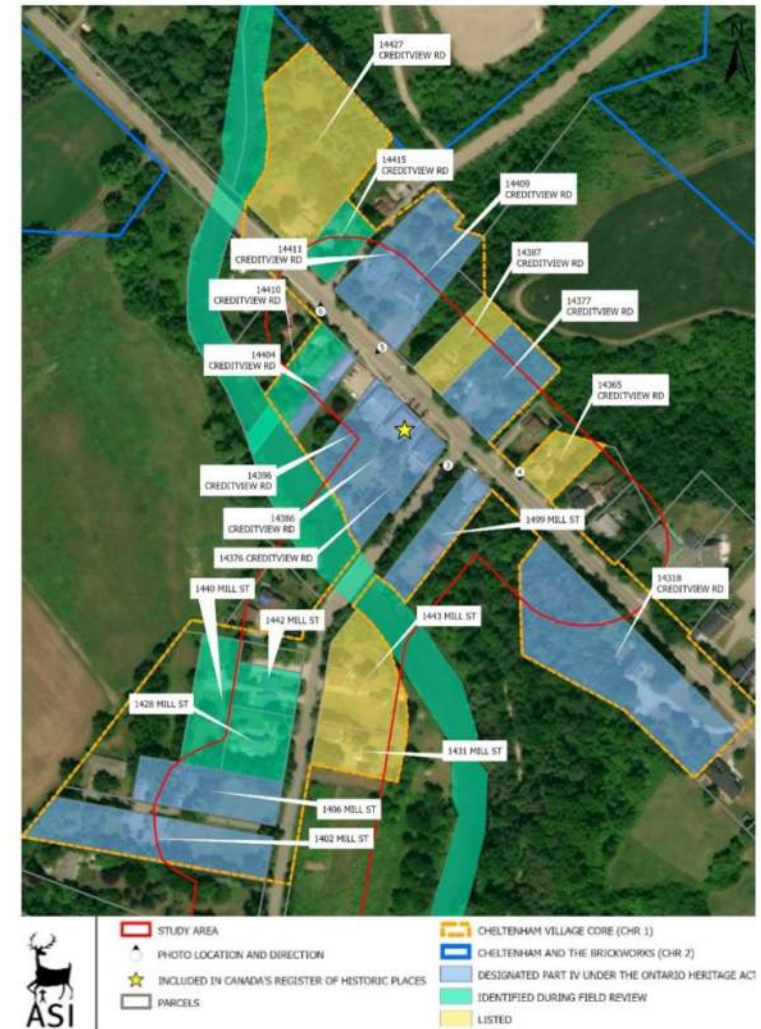
Lands that exhibit archeological potential will require a Stage 2 Archaeological Assessment, if impacted, prior to any proposed construction.

CULTURAL HERITAGE ENVIRONMENT



- 12 properties designated under the ***Ontario Heritage Act***
- 1 property on ***Canada's Register of Historic Places*** (Cheltenham Store)
- 5 properties on the Town's ***Heritage Register***
- 2 properties on the Town's **Cultural Heritage Landscapes Inventory** (Cheltenham and the Brickworks)
- 5 properties identified as **potential built heritage resources**

Construction activities should avoid impacts to identified cultural heritage resources.



DRAINAGE & STORMWATER








- Drainage features in the study area include:
 - Grass lined ditches and culverts
 - Curb and gutter east of the bridge connecting to a storm sewer on Creditview Road
- Flooding along Credit River tributary impacting adjacent residents
- Flood plain from Credit River heavily impacts capacity of the tributary and causes flooding of the tributary
- Between Credit River bridge and creek tributary drainage outlets overland with no defined path.



EVALUATION OF ALTERNATIVE SOLUTIONS



Alternative Solutions to address the **Problem / Opportunity Statement** were comparatively evaluated based on criteria that represent the broad definition of the environment, as described in the *EA Act*.

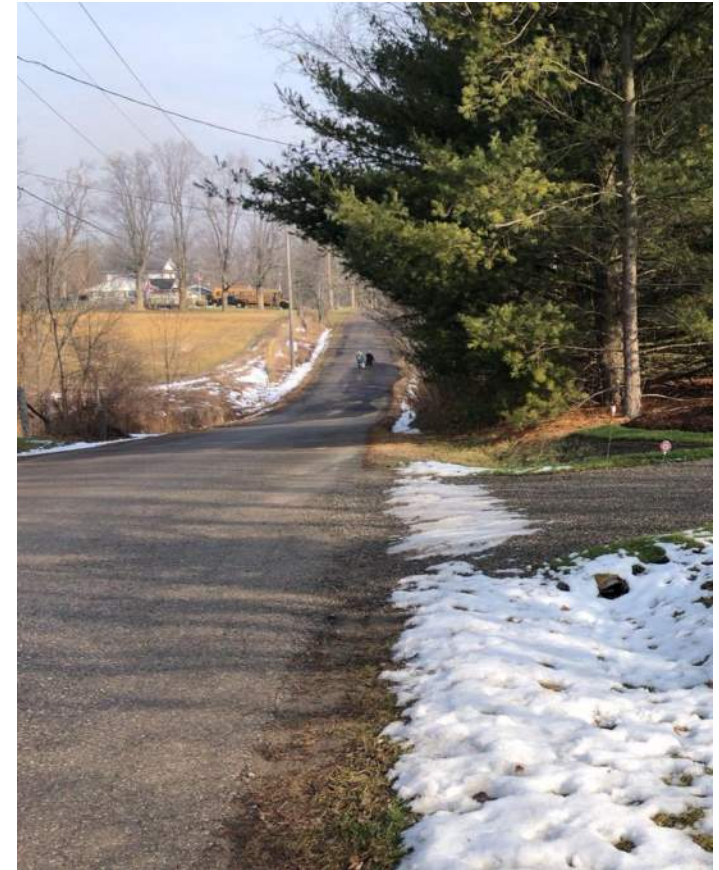
CRITERIA	DESCRIPTION
Traffic Operations and Safety 	How will the alternative serve the existing and future vehicular, pedestrian and cycling traffic needs? (<i>Safety, Road Condition, Intersection improvements, Active Transportation, Sightlines</i>)
Socio-Economic Environment 	What impacts will the alternative have on the local community (e.g., compatibility with area land use, impacts on local businesses, property requirements, access restrictions, etc.)?
Natural Environment 	How does the alternative affect existing vegetation, water quality, fisheries/wildlife and habitat? Does the alternative address climate change?
Cultural Heritage 	Will the alternative affect archaeological, cultural heritage resources or First Nations communities?
Costs 	What is the capital cost of the alternative? What is the cost for utility relocations and property acquisitions (if required)? What are the operation and maintenance cost impacts?

INCORPORATING YOUR COMMENTS



- Deteriorated roadway needs to be addressed.
- Improvements should not negatively disrupt the character of the Village.
- Support for traffic calming measures.
- Support for improvements in problem areas.
- Concern over property impacts and tree removals.
- Concern over increased traffic volumes from the road improvements.
- Formal pedestrian and cyclist facilities such as sidewalks and cyclist facilities are not desired

Alternative solutions were developed and evaluated in consideration of all comments provided through individual meetings with residents and the Community Outreach Meeting held on May 27, 2021.



ALTERNATIVE SOLUTIONS – S-CURVE ALIGNMENT



Alternative 1 – Do Nothing

- No improvements to existing conditions
- Road will continue to encroach on private property
- Safety concerns of pedestrians, cyclists, and vehicles are not addressed



Alternative 2 – Adjust Alignment Slightly

- Adjust turning radii to improve sight lines with operational improvements (pavement widening / shoulder to accommodate pedestrian, cyclist, and vehicular traffic; additional signage & pavement markings, vegetation pruning)
- Minor encroachment towards properties & natural environmental impacts





Alternative 3 – Adjust Alignment to Meet Design Standards

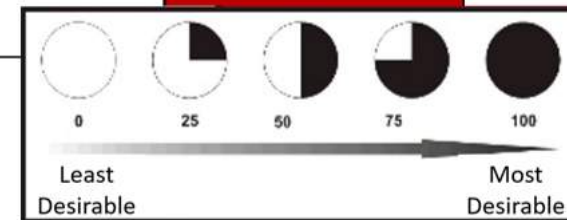
- Turning radii is realigned to meet Transportation Association of Canada (TAC) Standards.
- Significant property & natural environmental impacts
- High capital costs

EVALUATION OF ALTERNATIVE SOLUTIONS – S-CURVE ALIGNMENT



Alternative Solutions	Traffic Operations & Safety	Social Environment	Natural Environment	Cultural Heritage / Archaeological	Cost	Evaluation Summary
Alternative 1 - Do Nothing						Not Recommended
 Alternative 2 - Adjust Alignment Slightly						Recommended to be Carried Forward
 Alternative 3 - Adjust Alignment to Meet Design Standards						Not Recommended

Alternative 2 – Adjust Alignment Slightly is the recommended solution to be carried forward.



ALTERNATIVE SOLUTIONS – CROSS-SECTION

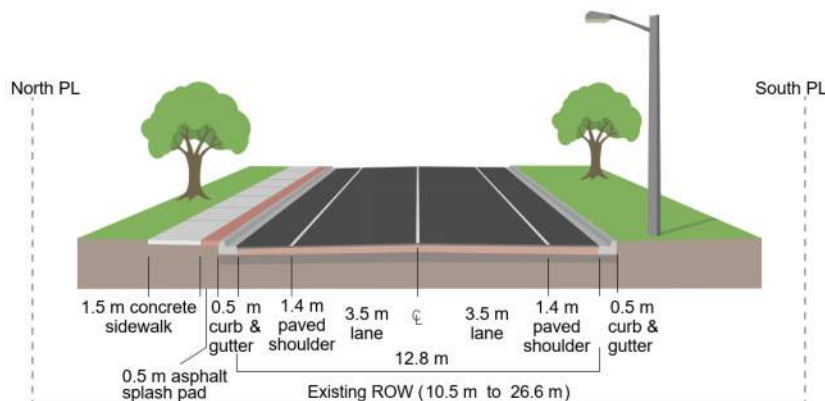


Alternative 1 – Do Nothing



- No improvements to existing conditions
- Does not meet road design standards nor align with the Town's planning studies
- Deteriorated pavement condition remains
- No accommodation for pedestrians or cyclists

Alternative 2 – Curb and Gutter with Sidewalks

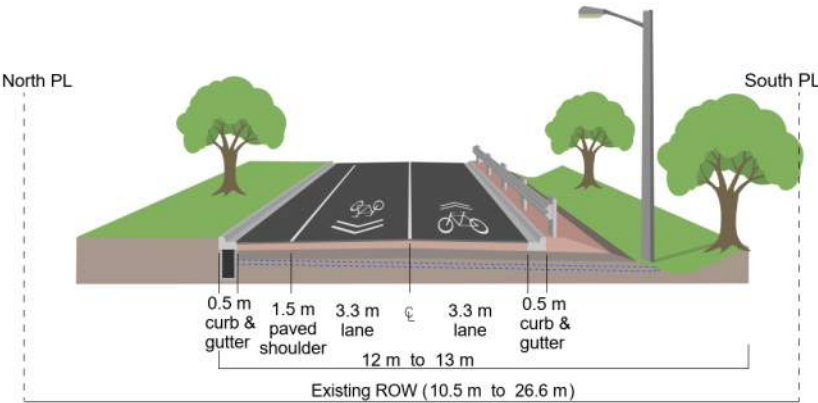


- Two 3.5m lanes with paved shoulder, curb and gutter on both sides; sidewalk on north side
- Cyclist separated from the travel lane via paved shoulders
- Significant property & natural environmental impacts
- Recommended in Town's *2019 Development Charge Study*

ALTERNATIVE SOLUTIONS – CROSS-SECTION

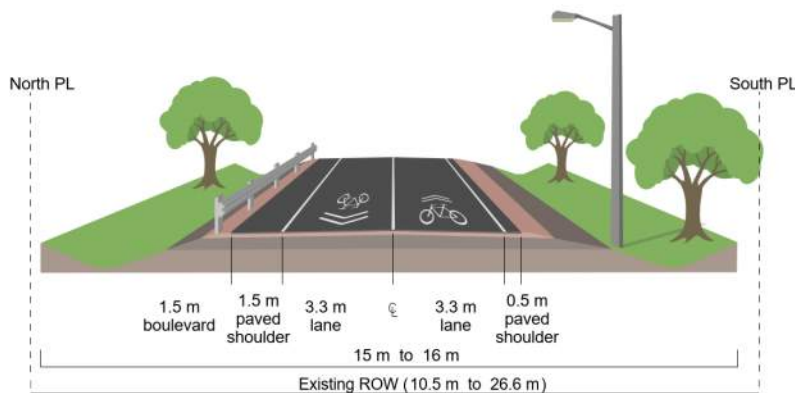


Alternative 3 – Curb and Gutter with Paved Shoulder



- Two 3.3m lanes with curb and gutter, paved shoulder on the north side, and grassed ditch on the south side
- Pavement markings for vehicles and cyclists to share the road
- Pedestrians separated from the travel lane via paved shoulder
- Narrowed pavement width and curb and gutter (traffic calming feature)
- Reduced property impacts (in comparison to roadside ditches with no curbs)
- Guiderail where required

Alternative 4 – Paved Shoulders and Re-instate Ditches



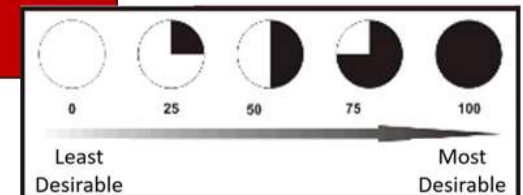
- Two 3.3m lanes with paved shoulders and grassed ditches on both sides
- Pavement markings for vehicles and cyclists to share the road
- Pedestrians separated from the travel lane via paved shoulder
- Widened pavement width can contribute to increased vehicle speeds
- Additional property required to accommodate grassed ditches
- Guiderail where required

EVALUATION OF ALTERNATIVE SOLUTIONS – CROSS SECTION



Alternative Solutions	Traffic Operations & Safety	Social Environment	Natural Environment	Cultural Heritage / Archaeological	Cost	Evaluation Summary
Alternative 1 - Do Nothing						Not Recommended
<p>Alternative 2 - Curb and Gutter with Sidewalk</p>						Recommended to be Carried Forward
<p>Alternative 3 - Curb and Gutter with Paved Shoulder</p>						Recommended to be Carried Forward
<p>Alternative 4 - Paved Shoulder and Re-insate Ditches</p>						Not Recommended

Alternative 3 –Curb and Gutter with Paved Shoulder is the recommended cross-section to be carried forward for Mill Street.



PRELIMINARY RECOMMENDATIONS - SUMMARY

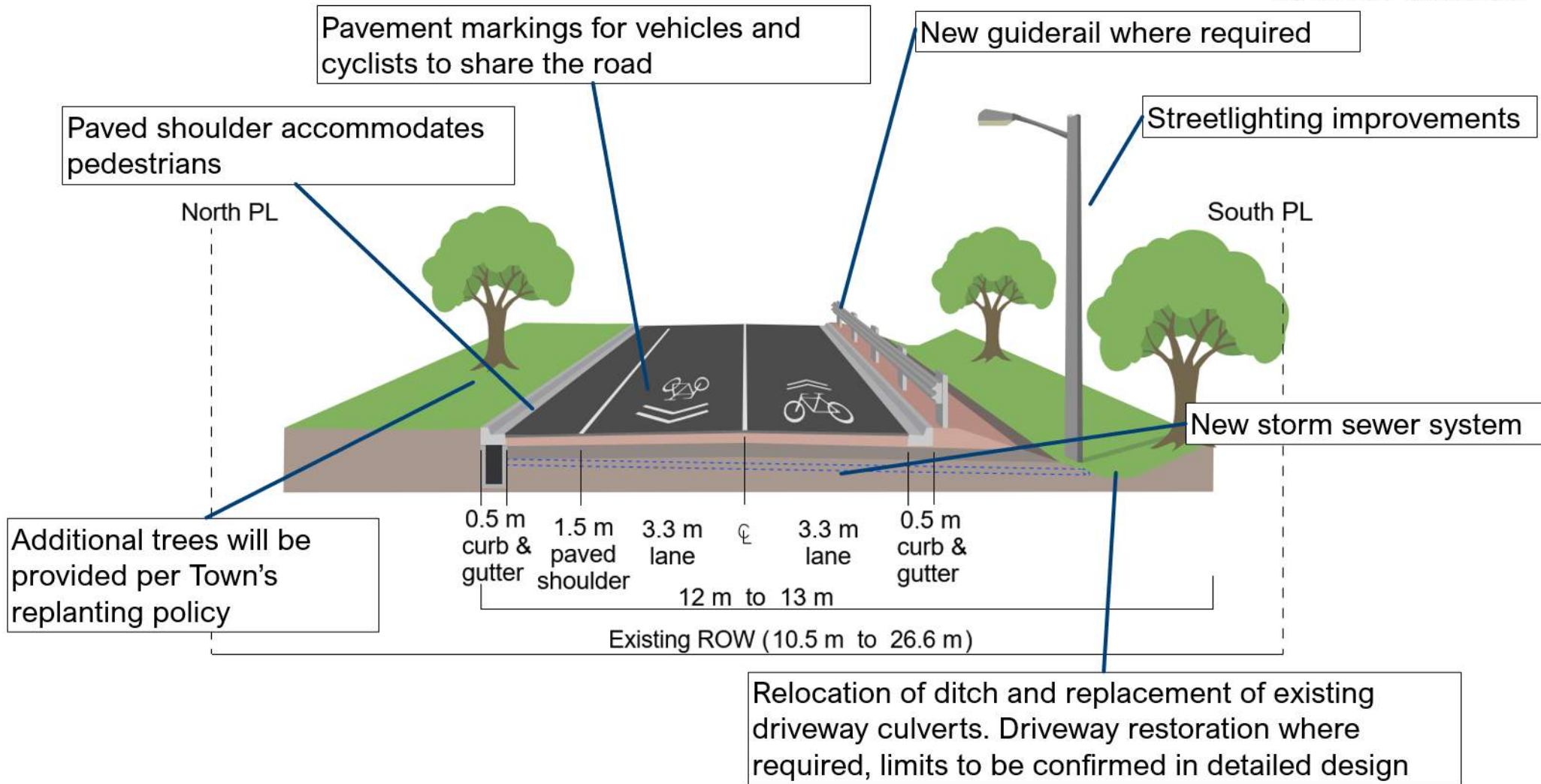


Key elements of the study recommendations are shown below. Following this PIC, the recommended solution for the corridor will be confirmed in consideration of the comments received.

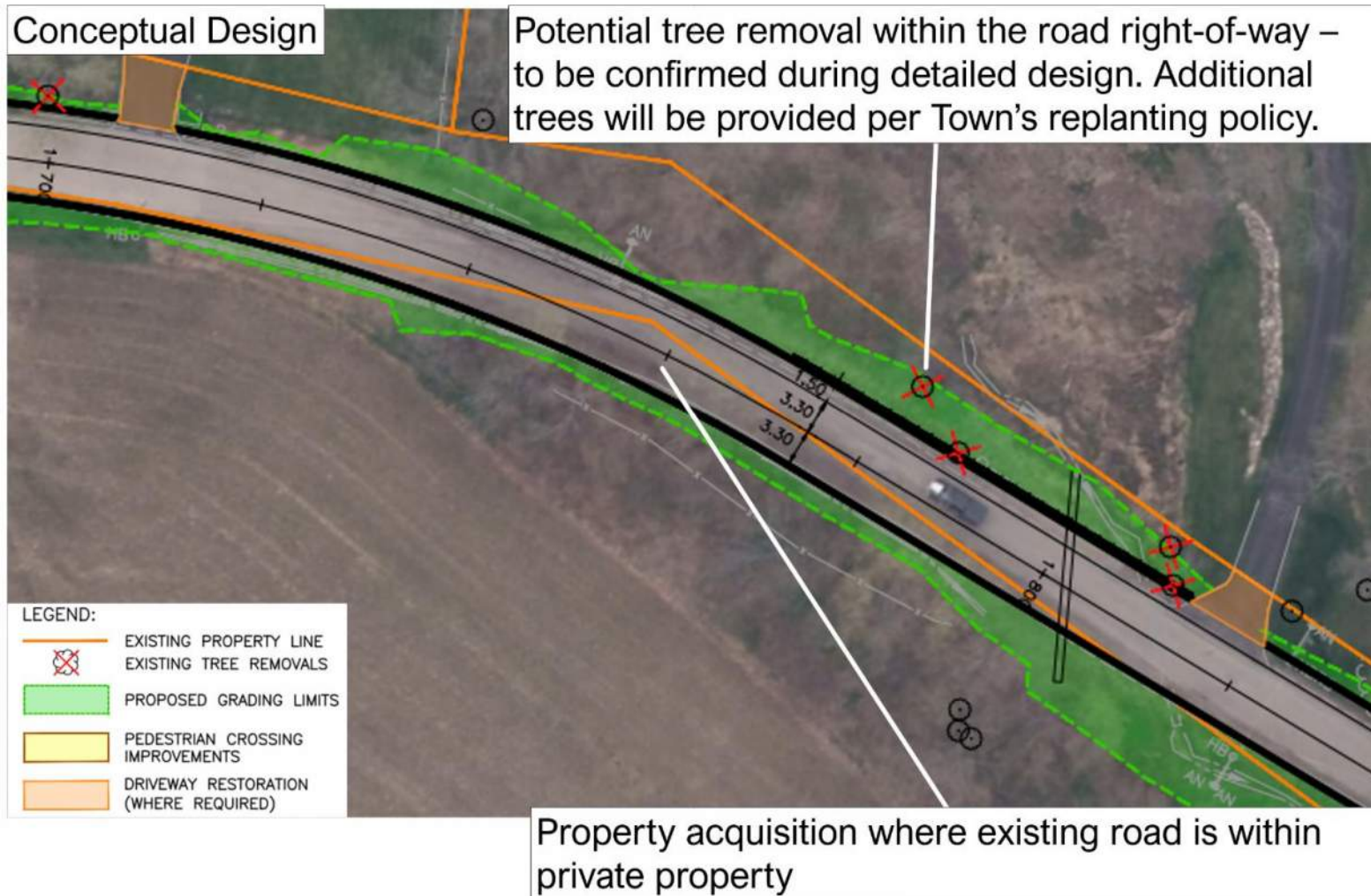
Conceptual Design



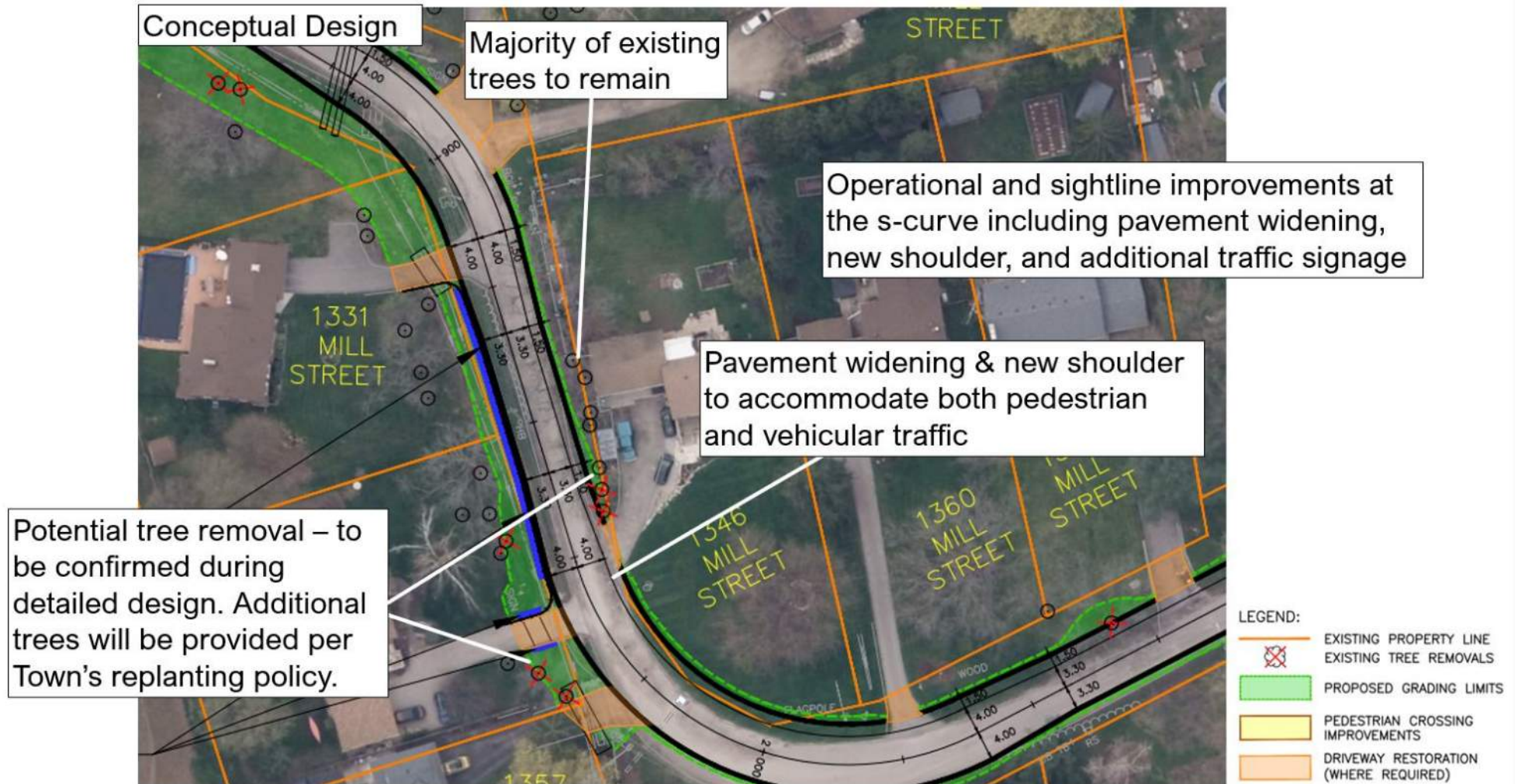
PRELIMINARY RECOMMENDATION – CROSS-SECTION



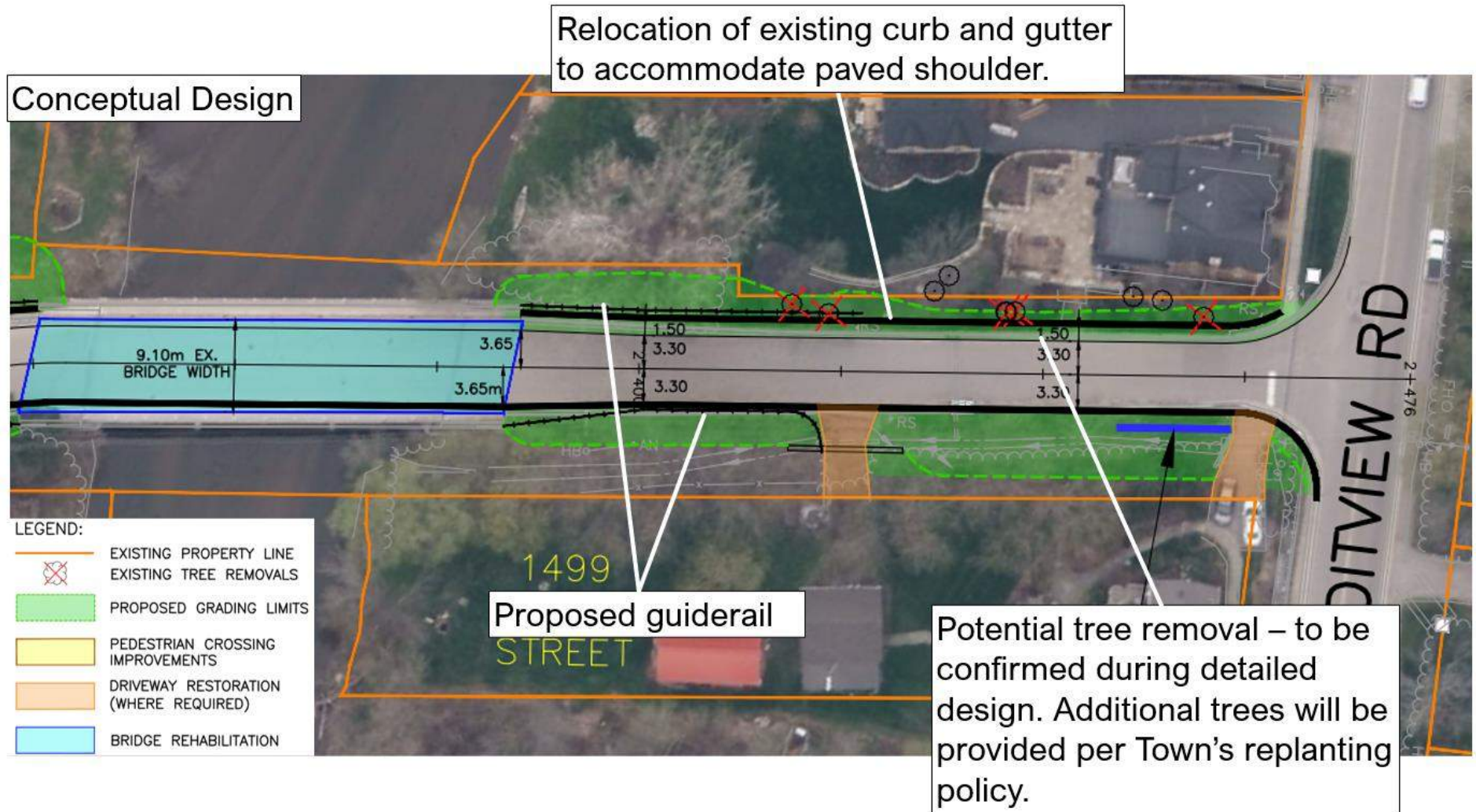
PRELIMINARY RECOMMENDATIONS



PRELIMINARY RECOMMENDATIONS



PRELIMINARY RECOMMENDATIONS



KEY IMPACTS & MITIGATION MEASURES



Mitigation measures will be further developed during detailed design to reduce the severity and duration of any impacts associated with the recommendations.

Archaeological and Cultural Heritage	<ul style="list-style-type: none">• Lands that exhibit archeological potential will require a Stage 2 Archaeological Assessment.• Construction activities and staging should avoid impacts to identified cultural heritage resources. Impacted cultural heritage resources may require a property-specific heritage impact assessment.
Property Requirements	<ul style="list-style-type: none">• Property acquisition where existing road is within private property.• Grading easement required for 7 properties adjacent to s-curve.• Driveway restoration to existing conditions where impacted.
Vehicle Traffic, Pedestrians and Cyclists	<ul style="list-style-type: none">• Town to consider additional traffic calming measures• Local and emergency traffic will be maintained during road works with alternative detour routes for non-local traffic
Natural Environment Impacts	<ul style="list-style-type: none">• Removal of street trees and vegetation adjacent to the existing ROW to facilitate grading. Construction and vegetation clearing will be conducted in appropriate seasons to limit ecological impacts (nesting birds, spawning fish, etc).• Relocation of Credit River tributaries and replacement of existing culverts. Erosion and sediment controls to protect the Credit River and its tributaries from sediment during construction
Utility Impacts	<ul style="list-style-type: none">• Relocation of utility poles will be required.

SUMMARY OF NEXT STEPS



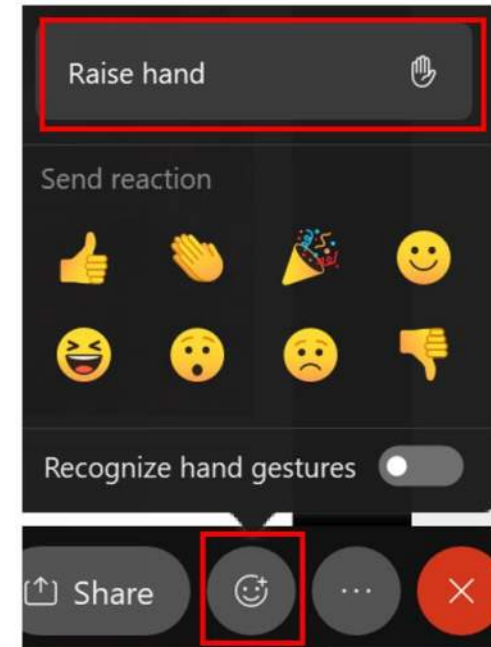
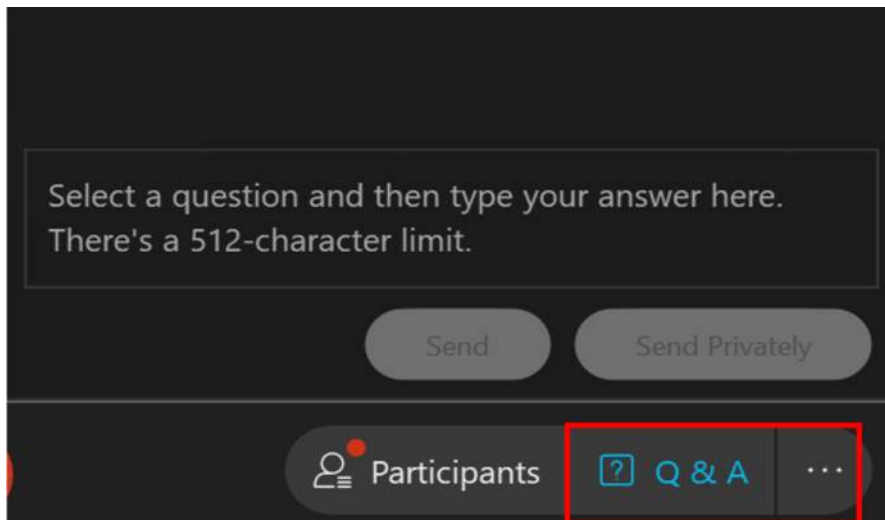
- Review and address the comments submitted at this PIC
- Consult with additional stakeholders and technical agencies, as required
- Confirm study recommendations (i.e. **preliminary preferred solutions**)
- Prepare and submit a **Project File Report** for 30 Day public review
- Proceed to **detailed design and construction** (planned for 2022 pending Council approval and budget)

HOW TO PARTICIPATE – Q & A



The project team is available to answer your questions until 7 P.M.

- Please submit any questions you may have using the Q & A feature
- If you would like to speak, raise your hand using the “raise hand” button under the “Reactions” window and you will be unmuted by a member of the project team
- Phone-in participants must dial *3 to raise your hand



YOUR INPUT IS IMPORTANT



You can also provide your feedback on the project by providing your comments to the project team contacts listed below by **October 19, 2021**.

THANK YOU FOR ATTENDING

Project Team Contacts:

Town of Caledon

Shun Cheung, P.Eng., PMP

Project Manager, Engineering Services

Tel: (905) 584-2272 x 4040

E-mail: shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P. Eng., PMP., M.Eng.

Project Manager, Transportation

Tel: (416) 497-8600 x1471

E-mail: winnie.wong@rvanderson.com

APPENDIX 3

COMMENTS RECEIVED

APPENDIX 3-1

Comments Received During Meeting

Q&A Session for Mill St - Public Information Centre

Session number: 26322560301

Date: Tuesday, October 05, 2021

Starting time: 5:38 PM

6:11 PM

Q: I have a question about the traffic study when it's the Q&A

Priority: N/A

Connor MacIsaac - 6:14 PM

A: Hi Emily, please submit your question here and we can get to it following the presentation, alternatively, you can use the "raise your hand" feature once the Q & A period periods if you prefer to say your question to the team aloud.

- 6:24 PM

Q: alternative 2 how wide are you widening the road? and the whole of mill st?

Priority: N/A

- 6:39 PM

Q: Who do I address my concerns to re current property flooding caused by the existing ditch?

Priority: N/A

6:40 PM

Q: What impact did the traffic study have on the recommendations?

Priority: N/A

- 6:40 PM

Q: Why cut back vegetation to increase sightlines when that will just invite cars to speed even more?

Priority: N/A

- 6:41 PM

Q: Can you please have the cross section options on the screen?

Priority: N/A

- 6:41 PM

Q: or the preferred option, thank you.

Priority: N/A

[REDACTED] - 6:42 PM

Q: please let me ask my question. I have my hand up
Priority: N/A

[REDACTED] - 6:45 PM

Q: Please explain what is involved to have the posted speed on Mill St to 30 km/h, or even less?
Priority: N/A

[REDACTED] 6:46 PM

Q: Why are traffic calming methods not incorporated into the design now rather than doing things now that we know will increase speeds and trying to correct it later
Priority: N/A

[REDACTED] - 6:50 PM

Q: unmute me
Priority: N/A

[REDACTED] - 6:51 PM

Q: that is not fair
Priority: N/A

[REDACTED] - 6:51 PM

Q: Are the traffic calming measures actually happening, or just proposed? Clarify please.
Priority: N/A

[REDACTED] - 6:51 PM

Q: id like to respond once hes finishes
Priority: N/A

[REDACTED] - 6:57 PM

Q: YOU SAID YOU DIDN'T HAVE HISTORICAL DATA AT THE LAST MEETING
Priority: N/A

[REDACTED] - 6:58 PM

Q: LISTEN TO THE RECORDING - THIS IS UNBELIEVABLE
Priority: N/A

[REDACTED] - 7:00 PM

Q: Thank you all for the presentation and clear responses. Tough crowd, well

handled.

Priority: N/A

[REDACTED] 7:03 PM

Q: my question has to do with speed

Priority: N/A

[REDACTED] - 7:03 PM

Q: we often use vertical bollards as a seasonal alternative to speed humps...

Priority: N/A

[REDACTED]
Q: why wasn't this done on all the other roads that have been recently improved

Priority: N/A

[REDACTED] - 7:09 PM

Q: that is not correct, they all had EA's not as extensive.

Priority: N/A

[REDACTED] 7:12 PM

Q: Is this presentation going to be available on the Town of Caledon website?

Priority: N/A

[REDACTED]
Q: what?

Priority: N/A

[REDACTED] - 7:18 PM

Q: My question please.... explain the process to get a posted speed of 30?

Priority: N/A

[REDACTED] - 7:18 PM

Q: When will the presentation be posted

Priority: N/A

APPENDIX 3-2

Comments Received After Meeting

Connor MacIsaac

To: Winnie Wong
Subject: RE: Mill Street Reconstruction EA

-----Original Message-----

From: [REDACTED]
Sent: October 5, 2021 7:54 PM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: Mill Street Reconstruction EA

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

There seems to be a power group. They are not speaking for me. I cannot believe people cannot see the many positives. The recommendations in your presentation are inviting to me. I have seen three cycle accidents and two cars in my ditch at [REDACTED]. I have seen cars playing chicken and trespassing to get to the river...I have seen two dogs get hit by cars, I have seen people parking on lawns and in no parking zones, hiding the fire hydrant. I myself have rushed to the side of the road where guard rails are needed with my dogs. I am looking forward to your reconstruction. Everyone complained about the road conditions one on the street and now we finally can have it. I have waited 25 years as living on the S curve has been problematic. The culverts took 10 years to get. My driveway was caving in. You have seen the danger at the end of my driveway with the slope going on road property..

I do not understand how people are so negative and not looking at the positives. Thank you for all your work. A new road with improvements will raise the value of our property, allow kids to safely reach the trail, dog walkers getting off the middle of the road and so much more. Safety railings are essential and I see many tire tracks off the road Some people like to hear their own voices and offer nothing for the future of our street.. it is like a power struggle in the neighbourhood. I am sick of the negativity. I thought when sidewalks were not an issue... I appreciate your patience and all your team has done to accommodate the people.

I stay out of the discussions and just want the improvements..I am sure others feel this way, but do not say it. Living in the past and not preparing for the future is disgusting Sincerely Sent from my iPad

Connor MacIsaac

From: [REDACTED]
Sent: October 7, 2021 3:47 PM
To: Connor MacIsaac
Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Online Public Information Centre

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Thanks!

Sent from my iPad

On Oct 7, 2021, at 2:39 PM, Connor MacIsaac <cmaclsaac@rvanderson.com> wrote:



Please use the link below to view / download the slides from Tuesday's meeting.

<https://www.caledon.ca/en/town-services/resources/Engineering/Mill-St-EA/Mill.St.PIC.AODA.pdf>

You can also watch the video recording of the meeting and learn more about the study on the Town's project website <https://www.caledon.ca/en/news/mill-street-ea.aspx>.

Thanks,

<image001.png> **Connor MacIsaac, EPt**

JUNIOR ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

<image002.png> <image003.png> <image004.png> <image005.png> <image006.png> <image007.png>
rvanderson.com

From: [REDACTED]
Sent: October 6, 2021 1:21 PM
To: Connor MacIsaac <cmaclsaac@rvanderson.com>
Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Online Public Information Centre

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Can you please send me the slides from last night's Mill Street meeting? I wasn't able to attend the entire meeting. Thanks..

On Fri, Sep 17, 2021 at 2:54 PM Connor MacIsaac <cmacisaac@rvanderson.com> wrote:

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Notice of Online Public Information Centre (PIC) for the **Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA)**. The purpose of this meeting is to share information and receive public input on the to study date, including the problem / opportunity, existing conditions, alternative solutions, and the preliminary preferred solution. The meeting will be held on **October 5, 2021 at 6 p.m.**, and will consist of a presentation by the Town followed by a Question and Answer period concluding at 7 p.m.

Due to continuing efforts to contain the spread of COVID-19 and to protect individuals, this meeting will be held electronically. Refer to the attached notice, or visit the Town of Caledon's project website, <https://www.caledon.ca/en/news/mill-street-ea.aspx>, for further information on how to participate. The meeting link will also be posted to the project website closer to the meeting date. Questions can be submitted prior to the event to the contacts below.

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Engineering Services
905.584.2272 X.4040
shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
416.497.8600 X.1471
winnie.wong@rvanderson.com

The project team values the participation of all stakeholders and wishes to ensure that the community's interests and concerns are taken into consideration. Please contact the project team members listed above if you require a hard copy of the online materials or for further information on this project.

If you would like to be removed from the project mailing list, please let us know by responding to this email.

Regards,

<image001.png> **Connor MacIsaac, EPT**

JUNIOR ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

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[rvanderson.com](http://www.rvanderson.com)

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Connor MacIsaac

Subject: RE: Mill St. EA

From: Andrew Pearce <Andrew.Pearce@caledon.ca>

Sent: Saturday, October 16, 2021 9:07 AM

To: Christina Early <Christina.Early@caledon.ca>

Cc: Michele Olliffe <michele.olliffe@caledon.ca>; Ryan Grodecki <Ryan.Grodecki@caledon.ca>; Shun Cheung <Shun.Cheung@caledon.ca>

Subject: Re: Mill St. EA

Good morning, Councillor Early

Thank you for bringing the residents concerns to our attention. We certainly want to get this road project right so the residents will be pleased with the end product. Learning about the property specific concerns is critical to the overall success of the project.

By copy to Michele, I am asking her to arrange a meeting with you as soon as possible to discuss the details.

Thanks, and have a great weekend.

Andrew

Get [Outlook for iOS](#)

From: Christina Early <Christina.Early@caledon.ca>

Sent: Saturday, October 16, 2021 7:20:20 AM

To: Andrew Pearce <Andrew.Pearce@caledon.ca>

Subject: FW: Mill St. EA

Hi Andrew ...hope all is well with you! I am wondering if we could have a quick conversation on the Mill street proposal that is currently underway. Shun and team have done a great job in trying to compromise for the street however there are a few issues that I want to walk through with you if possible.

I have attached pics from the house that is on the corner of Creditview and Mill ...they will be significantly impacted and I wonder if we could consider another option. Additionally I met with two other residents on the streets for smaller issues ...namely retaining walls and guard rails.

Your team has moved mountains on this project and I am very appreciative but hoping to adjust a few items to keep everyone happy.

Christina

Christina Early

Area Councillor, Ward 2

Office: 905.584.2272 x. 5130

Cell: 416.576.9366

Email: christina.early@caledon.ca

Town of Caledon | www.caledon.ca | www.visitcaledon.ca

From: [REDACTED]
Sent: Wednesday, October 13, 2021 10:53 PM
To: Christina Early <Christina.Early@caledon.ca>
Subject: Re: Mill St. EA

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Good Evening Christina

Thanks again for dropping by today, much appreciated.

I've attached the pictures you requested. As you saw in person, the towns' proposal to widen the road to accommodate a pedestrian walkway will have significant negative affects on our heritage property. Using the slides and measurements outlined in the proposal, the first staked measurement will put the widened road roughly 11' from our house at the corner of Mill and Creditview. This is just our best estimate, however the exact impact could be worse than that.

As mentioned, when we first purchased our home in 2009, the streetscape was lined with large trees and bushes. However, the ice storm several years back caused significant damage. We've been replacing & replanting since then, in an effort to rehabilitate the streetscape. These trees and bushes also provide our heritage property with some degree of privacy. If the town moves forward with the proposal currently recommended by the consultants, the amount of trees, both large and small, as well as numerous bushes and plantings that will need to come out will ruin the streetscape and impact the overall quality of our property.

Also, the proposed widening of the road at the intersection of Creditview and Mill (where we are) will promote speeding of traffic off Creditview headed west on Mill. In our view, the width of Mill at the Creditview intersection should be made narrower rather than being widened, in an effort to introduce a traffic calming measure.

We've attended both virtual meetings and have raised many questions and concerns that are not being addressed (e.g. traffic calming measures). We're also of the view that the consultants are not presenting the information on which they are basing their recommendations and are presenting options that are not plausible, so that their recommended solution is presented as the only option (also referred to as "bait and switch"). Your attention notwithstanding, this makes many of us question the sincerity of the process. Option 1 "do nothing" is clearly not an option. The road does need repair.

You mentioned today it would be ideal if someone from the town were to pay a visit and see in person the adverse impact this proposal would have on our section of the street. We would appreciate if you could make that arrangement as soon as possible. During the first virtual meeting, we mentioned that we had not been consulted directly by any town employee and were told someone would reach out, yet we've still not heard from anyone. As was recognized today, our heritage home and property would be the most negatively affected of all properties on the entire street.

Thank you
[REDACTED]

From: Christina Early <Christina.Early@caledon.ca>
Sent: October 12, 2021 2:03 PM
To: [REDACTED]
Cc: [REDACTED]
Subject: RE: Mill St. EA

Happy Belated Thanksgiving to you both!!

Thanks for the email ...I am meeting with [REDACTED] tomorrow at 10am so I will certainly drive by your place to see the stakes if visible. Let me know if anyone is around as I can gladly drop by.

Christina

Christina Early
Area Councillor, Ward 2
Office: 905.584.2272 x. 5130
Cell: 416.576.9366
Email: christina.early@caledon.ca
Town of Caledon | www.caledon.ca | www.visitcaledon.ca

From: [REDACTED]
Sent: Tuesday, October 12, 2021 10:57 AM
To: Christina Early <Christina.Early@caledon.ca>
Cc: [REDACTED]
Subject: RE: Mill St. EA

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Good Morning Christina

Hoping you enjoyed Thanksgiving weekend. The weather yesterday was so beautiful.

Wanted to let you know that [REDACTED] and I took the presentation and staked out some measurements alongside our house. The impact of the road widening is concerning us even more now. It's clear to us that more than the 5 large trees would be coming out. Perhaps only those ones were marked on the presentation due to their sizes however more trees, bushes and shrubs would need to come out if the roadway were to be widened as suggested. Visualizing the wider entrance off Creditview onto Mill is equally alarming. We do not see how this makes any sense at all.

This is going to cause significant negative impact to the enjoyment of our home and property. We will be completely exposed to the road and traffic. All efforts we've made over the past 10+ years to create some privacy for ourselves from current road will be wiped out. We will be completely exposed to both pedestrians and vehicles and the noise will be that much closer.

We've sent a note to [REDACTED] asking for a meeting later this week or Monday, or over the weekend, and asked that you attend as well. We would like to discuss our concerns in person so you both can see and appreciate the impact of the proposal to us and our property personally.

As mentioned to you during our call last week, the community along Mill are meeting as well and we will have feedback to the Town by the deadline next Tuesday. However, if that date has been extended (as requested) please let me know.

Thanks Christina. [REDACTED] and I look forward to discussing with you further and we appreciate your ongoing support.

[REDACTED]

Sent from [Mail](#) for Windows

From: [REDACTED]
Sent: October 6, 2021 11:56 AM
To: [Christina Early](#); [REDACTED]
Subject: RE: Mill St. EA

Good Morning Christina

Clearly there will be many of us extremely disappointed with the proposals made during the virtual meeting last night.

We do not believe that widening Mill St., is in the best interests of safety, nor is it an acceptable proposal in respect of maintaining the heritage and character of our street, homes and our community.

Widening the road is going to encourage more traffic, additional and increased speeding and is going to negatively impact our entire streetscape. They are proposing taking down 5 trees alongside our property alone! As you've seen from our backyard, these trees serve as buffers & privacy between our patio and backyard and the road itself. Of all the homes, ours may be the one that is already the closest to Mill St. Bringing the road closer to us and removing the trees are going to expose us even more to that additional traffic and increase in speeders & speeding. There were little to no suggestions last night with regard to traffic calming measures on the road, such as simply reducing the speed limit, so where are the safety considerations in that? The speed limit is 40 but they are proposing a solution that will meet the needs of speeds between 45 – 55, instead of proposing solutions that will force cars to travel 40 or less?

Our community simply wanted the road repaved around the areas of deterioration on the curves and hill. Why on earth this issue was escalated to an entire road re-structure that will negatively impact many of us residents and our community in general is beyond us.

The first virtual meeting was in May, the second meeting last night – a period of 5 months in between. The town is now only giving us until Oct. 19th to send in our concerns and comments = 9 business days, which also seems completely unfair, especially since the presentation won't be accessible on the town website for review for a few days. They are basically giving us 1 week to submit our detailed concerns.

Is there any way you can forward along a copy of the presentation so we can review as soon as possible? Also, could you ask for an extension on the deadline for the communities questions and concerns? Again, there were questions asked last night in the background that were not brought up during the meeting and one person felt that they were deliberately muted.

[REDACTED]

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Connor MacIsaac

Subject: RE: Mill St - Flooding Concern

From: [REDACTED]
Sent: October 18, 2021 4:03 PM
To: Shun Cheung <shun.cheung@caledon.ca>
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: Re: Mill St - Flooding Concern

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Shun and thank you for the email.

Flooding from the ditch onto our property at [REDACTED] is an ongoing concern. We have had the ditch flood 3 times to the extent that water has flowed into our garage which is some 15-20m from the ditch. We also have an issue of water flowing down our driveway and into the garage during severe weather events.

In my experience there have been two core causes related to the ditch: 1- blockage at the culvert approximately 100m west of Creditview and, 2- extra volume of water in the ditch since the town introduced catch basins and a storm water outlet into the ditch.

- 1- appears to be mostly caused by a lack of Town maintenance of the ditch.
- 2- is more problematic as the CB outflow directly faces the lowest part of our property.

Unlike the current process, when the stretch of Mill St, west of Creditview and east of the Mill St. bridge, had curbing installed and storm water diverted from its natural flow, there was no property owner consultation. I am pleased to see the current, responsive process.

The driveway flooding issue only started after the road and curbing was introduced. Natural flow off the road into the ditch was diverted to the curb and in heavy rain this volume of water flows down our drive as the gutter appears insufficient for the increased volume.

I would invite you to come by and view first hand. It would probably be more apparent than my written description.

If there is any other information that I can provide, do not hesitate to ask.

Regards

[REDACTED]

Sent from my iPhone

On Oct 18, 2021, at 15:14, Shun Cheung <shun.cheung@caledon.ca> wrote:



I would like to reach out to you regarding to your flooding concern which brought up at the PIC.

It would be appreciated if you could provide me with any details regarding to this concern. Thanks!

Shun H. Cheung, P.Eng., PMP
Project Manager
Engineering Services Department

Office: 905.584.2272 x.4040

Cell: 416.436.0910

Email: shun.cheung@caledon.ca

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Connor MacIsaac

To: Winnie Wong
Subject: RE: Mill St Cheltenham

From: Shun Cheung <Shun.Cheung@caledon.ca>
Sent: October 19, 2021 1:48 PM
To: [REDACTED]
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: RE: Mill St Cheltenham

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

[REDACTED]

Thank you for your email and the letter.

Shun H. Cheung, P.Eng., PMP
Project Manager
Engineering Services Department

Office: 905.584.2272 x.4040
Cell: 416.436.0910
Email: shun.cheung@caledon.ca

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From: [REDACTED]
Sent: Tuesday, October 19, 2021 1:46 PM
To: Shun Cheung <Shun.Cheung@caledon.ca>; winnie.wong@rvanerson.com
Subject: Fw: Mill St Cheltenham

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1443 MILL ST CHELTENHAM

Good day Shun Cheug & Winnie Wong

We are the new owners of [REDACTED] in Cheltenham

Please add us to the mailing list for information updates to the area.

We have not previously been contacted about changes that are currently being proposed.

We recently became aware of the proposed plan and I have since sent a letter to our Counselor Johanna Downey.

A copy of that letter is attached.
It voices our concerns about the proposal.
Hoping our concerns will be taken seriously
prior to final assignment of changes

Please feel free to contact me to discuss the situation further.

We look forward to an amicable solution which works for all.

Please contact me if you have any issues opening the attachment

Regards

[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

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October 18, 2021

Counsellor Johanna Downey
Town of Caledon

SUBJECT: MILL STR. CHELTENHAM ROAD WIDENING

Dear Counsellor Downey

We are new to Caledon and have just recently purchased [REDACTED] in Cheltenham in July 2021. We purchased the property in order to have a peaceful place where our family could enjoy the safety of the community of a village and the Credit River. This is the home we had planned to retire in. So far, the property is absolutely all we could have imagined it would be. The community is full of wonderful friendly people. It is a quiet street with friendly neighbours who walk or bike on the road and stop to chat as they pass by our property. Nobody seems to be in a rush. Most people who use the street, live on the street. There is not much traffic, average speed is slow. We absolutely adore being part of this community.

However, it has just recently been brought to our attention that the Town of Caledon is considering widening Mill Street from Creditview Road to Mississauga Rd., installing guard rails and pedestrian/bike lanes.

The town is reviewing proposed changes to Mill St. Cheltenham citing safety reasons. Although we were not notified of any meeting, we therefore did not have an opportunity to participate, we have read over the available information on line. We believe these proposed changes will have a negative effect both on the community, the environment and major consequences to our property. We have spoken with some of the neighbours and agree with the collective that the conclusions of the consulting firm used do not fit in with the community of Cheltenham. We received a diagram of the proposed changes (attached) and were absolutely shocked to see the devastation they have proposed for our property and the street as a whole.

We are the first property after the bridge on the south side. Our house is already located extremely close to the street and if the street is widened as per the proposal, it would dramatically affect our comfort of living. There are several trees that would have to be removed including a birch tree located directly out front of the house. We do not have the luxury of relocating the building to another part of the property and we really don't want them to bring the street any closer to us.

As it is, we do not have very much curb side appeal, but are working diligently on improving that, all the while, keeping the village atmosphere and environment first. If they move the road closer to our house and put up a guardrail, there will be absolutely no hope for any curb side appeal ever. Bringing a road closer to a home, guardrail or not, is never a good idea. It certainly does not give us a feeling of safety and will greatly impede our view while sitting on our front porch.

Widening the road and installing guard rails is not keeping with the essence of the area. It will instead convert it to a much more metropolitan area. Widening the road at the ends of the bridge will likely lead to an increase in traffic as Mill St. as it will end up being used as a throughway from Creditview to Mississauga Rd. This in turn will lead to an increase in speeds, as well as an increase in noise pollution, air pollution and light pollution. More salt will be required on the roads in the winter, more run off into the river, adding to the water pollution.

If we had been aware prior to purchasing the property that this would happen, we never would have purchased it.

Is there any help you can offer us to convince the Town of Caledon that this is not an improvement that is of benefit to the people of Cheltenham? Is there anything we can do to stop the devastation which will happen to our property? We would greatly appreciate any advice, guidance and overall help you can offer.

Looking forward to discussing this matter further at your earliest convenience.

Regards

[REDACTED]
[REDACTED]
[REDACTED]

Connor MacIsaac

To: Winnie Wong
Subject: RE: Mill St EA - Cheltenham Residents Response to R.V. Anderson Proposal

From: [REDACTED]
Sent: October 19, 2021 2:46 PM
To: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: Christina Early <Christina.Early@caledon.ca>; johanna.downey@caledon.ca
Subject: Mill St EA - Cheltenham Residents Response to R.V. Anderson Proposal

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good Afternoon

The following attached documents are submitted on behalf of the many residents of Mill St., Creditview Rd, and other addresses within our village of Cheltenham.

- Community Response to Mill St. EA
- Signature Support Document
- Mill St., Road Proposal Oct. 2021 CARA Supporting Letter

We look forward to further dialogue regarding a proposal that meets the needs of the community.

[REDACTED]

The primary concern of the undersigned Mill Street residents (the “Residents”) is the current condition of the road surface. The remaining concerns of the Residents are nominal in comparison. The Residents are the primary users of Mill Street (vehicular and pedestrian).

The Town of Caledon (the “Town”) advised residents that reside on or are adjacent to Mill Street in Cheltenham, that the Town engaged R.V. Anderson Associates Ltd. (the “Consultants”) to conduct a Municipal Class Environmental Assessment on the entire length of Mill Street (the “EA”). This document is in response to the EA and the information presented at the public meetings held on May 27, 2021 and October 5, 2021.

The Study Area: The study area for the Assessment is from Mississauga Road to Creditview Road in Cheltenham.¹

Summary of the Consultants’ Presentation

The Consultants presented what they identified as the Problem or Opportunity Statement (the “Problem Statement”), evaluation of alternative solutions, preliminary study recommendations and next steps.²

The Town purportedly initiated the EA to address the following:

1. Deteriorating roadway surface issues;
2. Identify any traffic operations and road design issues;
3. Accessibility and safety for vulnerable road users;
4. Roadway drainage and stormwater management.³

The Consultants then presented their findings on the current state which included the following:

1. No suitable road crossing at the trailway near the west end of Mill Street;
2. Need for guiderails at the steep embankments near the trailway;
3. Road is located on private property west of the curve;
4. Limited sightlines as a result of steep grades and sharp curves;
5. Flooding at the curve; and
6. Narrow roadway.⁴

As a result of the EA, the Consultants identified three options with respect to the S-Curve alignment for the Town to consider. The three options were:

1. Do nothing;
2. Adjust Alignment Slightly; and

¹ Presentation Slides R.V. Anderson Associates Ltd. dated October 5, 2021 p 4.

² Presentation Slides R.V. Anderson Associates Ltd. dated October 5, 2021 p 3.

³ Presentation Slides R.V. Anderson Associates Ltd. dated October 5, 2021 p 6.

⁴ Presentation Slides R.V. Anderson Associates Ltd. dated October 5, 2021 p 7.

3. Adjust Alignment to Meet Design Standards.⁵

The Consultants' recommendation for the alignment is #2.⁶

The Consultants then identified four alternative cross-sections as follows:

1. Do nothing;
2. Curb and gutter with sidewalks;
3. Curb and gutter with paved shoulder;
4. Paved shoulder and re-instate ditches.⁷

The Consultants recommended option 3, Curb and gutter with paved shoulder.⁸

Issues with EA as Presented

The Residents raised a number of factual questions and concerns during the presentations by the Town and the Consultants, including that "do nothing" as well as proposing sidewalks were both completely unacceptable.

The need for an EA

The Residents queried whether an EA was even necessary if the road surface was simply removed, reground and re-laid. It is the understanding of the Residents that no such EA is required in that scenario and the option to resurface the roadway is the least costly option to the Town and its taxpayers.

In the October Presentation, other road works undertaken by the Town purportedly did not require an EA. The road works included extensive road work on Chinguacousy Road north of King Street, as well as the replacement of a one-lane bridge over the Credit River on McLaren Road north of The Grange Sideroad. The Residents asked why the Town undertook an EA for Mill Street, when the other projects did not require one. There was no answer forthcoming.

Risk Assessment

Traffic Study

During the Consultants' May Presentation, the Residents queried whether the Town or Consultants conducted any traffic studies to support their position on the need for extensive reconstruction. Given that shortly thereafter, traffic monitoring equipment appeared on the road, the Residents conclude that the Consultants made their initial conclusion that Mill Street required extensive reconstruction, without any current traffic data.

It is worth noting that the traffic study was short-lived and conducted during a period of time wherein the Town knew that both north-south roads immediately to the east and west of Cheltenham were severely restricted or closed due to construction. Particularly, Mississauga Road being a busy regional

⁵ Presentation Slides R.V. Anderson Associates Ltd. dated October 5, 2021 p 15.

⁶ Presentation Slides R.V. Anderson Associates Ltd. dated October 5, 2021 p 16.

⁷ Presentation Slides R.V. Anderson Associates Ltd. dated October 5, 2021 pp 17, 18.

⁸ Presentation Slides R.V. Anderson Associates Ltd. dated October 5, 2021 p 19.

road was severely restricted to traffic immediately south of Mill Street, thereby making Mill Street an effective detour for commuters during the entire duration of the traffic study.

The Consultants presented the results of the traffic study without identifying any statistical adjustment for the inevitable increase in both volume and speed of commuter traffic. The Consultants referenced a prior traffic study in the oral presentation that identified the usage was between 100-200 trips per day, and an average speed of 40-45km/hr approximately a year ago.

The Consultants have not identified any data with respect to usage of Mill Street by pedestrians or cyclists. The Residents state that most pedestrian usage is by the Residents themselves. The Consultants confirmed that they used the data taken while commuter traffic detoured onto Mill Street, in the design of the current proposal.

Agricultural Considerations

- NEC (Niagara Escarpment Commission) have positioned that any agricultural land on the south side of Mill St., be retained as rural environment farm land. Therefore, access for farm equipment must be considered in the Mill St, reconstruction design.
-
- Agricultural activities will be ongoing for a number of years and must be accommodated.

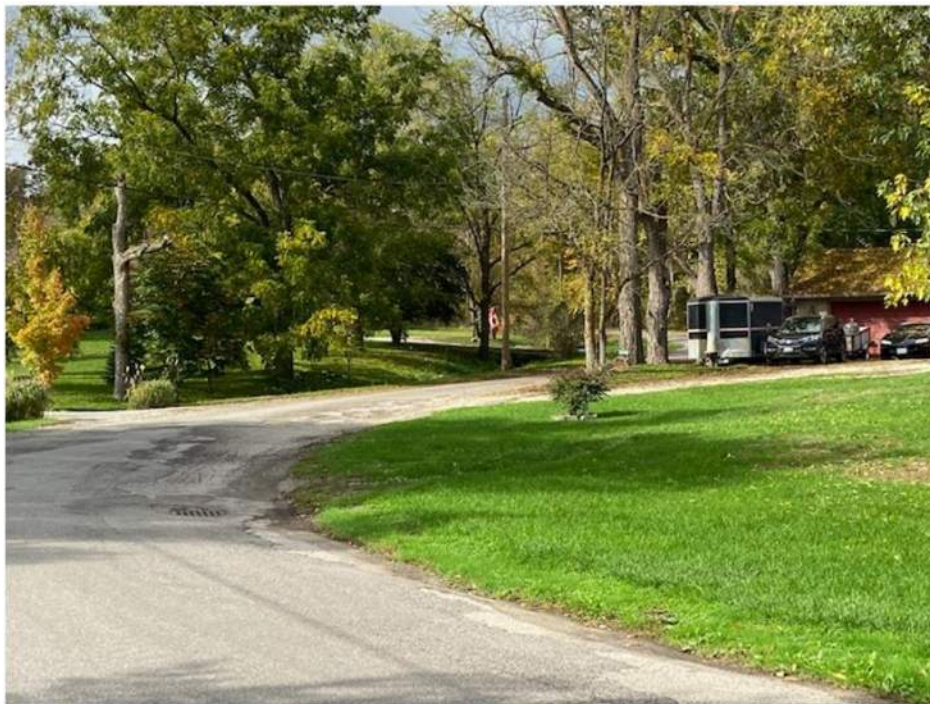
Sightlines

The Consultants identified reduced sightlines as a safety concern. The Consultants did not provide any information as to the evaluation of sightlines or minimums required. Significant stretches of Mill Street have long unobstructed sightlines. Areas in which the Consultants focused their presentation was the S-Bend, which is a reasonably small section in relation to the total length of Mill Street.

It is worth noting that even at the S-Bend, most sections have good unobstructed sightlines, particularly given the current speed and usage of the traffic. In many sections, sightline obstructions are only partial, and do not impact reaction time. See below.



Long views of curve



Second curve visible from before first curve heading west, trees on right to be removed will increase visibility further



Full view of approach to second curve westbound



Long view uphill after second curve



Long views west at top of hill



Long views towards west, before trailway



Long views approaching railway

Speeding

The posted speed limit on both Mill Street and Creditview Road is 40km/h. The Consultants identified that the current operating speeds on Mill Street are 40-50km/h at the curves and 50-55km/h on the straight sections.⁹

It is well known to the Town and admitted by the Consultants, that widening roads increases traffic speeds. The speeds of traffic on Creditview Road, serve as an immediate example. By proposing to widen Mill Street, the Town and the Consultants are effectively proposing to increase the speed of traffic.

Traffic Calming

The Town is policed by the Ontario Provincial Police (the “OPP”). The village of Cheltenham receives very little police enforcement. The OPP advised that it does not have the resources to police traffic in Cheltenham. As a result, any road design cannot rely on policing as a method of traffic calming.

Information on the Town website identifies that Mill Street does not qualify for traffic calming measures. Therefore the Residents may expect that the Town will refuse future requests for traffic calming after any construction. The Town’s current policy on traffic calming states:

⁹ Presentation Slides R.V. Anderson Associates Ltd. dated October 5, 2021 p 8.

“Daily Traffic Volume: if the average daily traffic (ADT) along the roadway section is less than 750 ADT for local roads, and 1500 ADT for collector roads the location will not be considered for implementation of traffic calming measures.”¹⁰

As a result, it is reasonable to believe that the Town and the Consultants are content to increase the speed on Mill Street, generally.

The fact that the Town replaced the previously mentioned one-lane bridge over the Credit River with another one-lane bridge is at odds with the Consultants position that Mill Street must be wider than it currently is. At no time did the Consultants identify the minimum width required, and whether there were examples wherein exceptions were recommended or implemented.

The Consultants identify that the lack of a controlled crossing at the trailway is a safety issue, however, there was no proposed remedy. It should be noted that there are no controlled crossings at the trailways, including the much busier Mississauga Road crossing.

Risk Assessment Summary

The Consultants did not identify any accident data whatsoever related to the usage of Mill Street. In general, it does not appear that either the Town, or the Consultants performed any quantifiable risk assessment to identify the need for the extensive reconstruction proposed. They do not appear to have conducted any risk assessment of the increased speed on Mill Street that they know will occur. It further appears that the notion of safety is applied inconsistently in the EA.

Drainage

In the Problem Statement, the Consultants identified flooding at the S-Bend as an issue of concern. During the May presentation, the Consultants verbally stated that they were not going to propose anything to address the flooding.

Currently the drainage for Mill Street includes storm sewers at the east end before the bridge, ditches and natural seepage west of the bridge as well as the Credit River tributary at the S-Bend.

The recommended cross-section proposed by the Consultants includes a new storm sewer system. If adopted, the Consultants’ recommended cross-section will significantly increase the amount of stormwater collected and thereby increase the volume of stormwater requiring discharge. With increased stormwater discharge, any such discharge into the Credit River or tributaries will increase the level of pollutants into the Credit. The Consultants verbally noted the sensitivity of the Credit River as a cold water fishery. For example the Credit River has trout and salmon that are sensitive to water quality.

Currently, the slope of the roadway between the bridge and the S-Bend directs water west. A new storm sewer will collect the water and have to divert transport it via gravity to a discharge point. The Consultants did not identify any drainage ditches, or stormwater easements between the bridge and the S-Bend, so the Residents have to wonder if it will drain into the Credit River tributary at the S-Bend. Alternatively, will the Town expropriate property for stormwater management? General public

¹⁰ Town of Caledon *Traffic Calming Strategy*, Schedule A to Staff Report 2020-0176 p 28.

commentary is that severe weather events are increasing. Redirecting storm sewers to the tributary increases pollutants directly entering the Credit River, as well as increasing the risk and severity of flooding at the S-Bend. The Consultants did not identify a discharge point for the increased stormwater collection west of the S-Bend.

Another concern left unaddressed, is whether reconstruction includes altering the road level (and to what extent) from current in order to create the slope necessary for the stormwater management. Altering the road level may adversely impact the Residents access to their respective properties and may require additional expense regarding driveways and landscaping on their portion of their property.

Tree Removal

The Consultants identified tree removal and grading required to implement their recommendations.¹¹ Preliminary indications are that the Consultants understate the extent of tree removal. Furthermore, it appears that the Consultants have minimized the appearance of how much will be removed by failing to identify the extent to which bushes, shrubs and other significant plantings will be removed to accommodate the reconstruction as proposed.

The Consultants did not identify the minimum distance their proposal requires as “Clear Zone”.

It is reasonable to expect that some of the grading identified by the Consultants will adversely impact trees and other plants which will then result in the need to remove dead trees and shrubs in the not too distant future. This is another way in which the presentation of the Consultants appears to understate the removal of trees and plants that currently enhance the streetscape.

Guiderails

The Town currently has post and wire guiderails along the steep embankments just east of the trailway. For unknown reasons, the Town did not install the wires when they replaced the posts recently. The Consultants propose large steel I-beam guardrails to replace the existing ones.

In addition, the Consultants propose adding large I-beam guiderails to significant residential portions of Mill Street.¹² Some of the areas in which the Consultants propose the large I-beam guardrails are already curbed, which is intended to serve a similar function as the I-beam guardrails.

The I-beam guardrails that the Consultants propose in front of, and along residences are identified in the pictures below. The Consultants did not identify the specific need for these guardrails that impact individual residences. They significantly detract from the streetscape.

¹¹ Presentation Slides R.V. Anderson Associates Ltd. dated October 5, 2021 pp 22-24.

¹² Presentation Slides R.V. Anderson Associates Ltd. dated October 5, 2021 pp 23, 24.



I-beam guardrails do not enhance the streetscape

Streetscape

Cheltenham is a small rural village on the Credit River. As noted in the EA, Cheltenham is also home to a high percentage of heritage buildings as well as natural heritage features. It is the desire of the Residents to preserve the heritage character, including the streetscape.

The Consultants propose to significantly increase the width of the road, and thereby the amount of asphalt, in addition to understating the extent of tree and shrub removal. The Consultants also identified street lighting improvements without identifying what they intended. One can reasonably presume that their proposal is to increase the amount of street lighting and thereby increase the light pollution.

The Consultants proposal significantly impacts one of the heritage properties located at the intersection of Mill St., and Creditview. The tape in the pictures below identifies the additional road width proposed by the Consultants.



Significantly more trees and shrubs proposed to be removed from streetscape than showing in the October Presentation

The effect of the Consultants' recommended course of action significantly detracts from the village streetscape, and is simply an inappropriate urbanization of the streetscape.

Proposed Remedy

The Residents identify that there is support for the following proposal and request its consideration by the Town and the Consultants.

From Creditview Road to the west end of the bridge (to the end of current curbing at 1443 Mill Street):

- Maintain the current width and style of curbing;
- Repair the sag in the road and curbing along this section;
- Aesthetically appropriate retaining wall on south side of street at 1499 Mill Street;
- Consider traffic calming measures (such as curb extensions or “bump outs”) at the intersection of Mill Street and Creditview Road. This will help slow down traffic entering Mill Street and may also deter drivers from parking alongside Mill on the north side. There already exists an issue where drivers, who are often visiting the Cheltenham General Store, park along Mill St., in the no-parking zone. A no-parking sign already exists however, due to no enforcement in this area, drivers continue to park in the no-parking zone on a regular basis.
- Maintain current drainage system;
- No designated pedestrian lane;
- No cyclist graphics on the road; and
- No guardrails as the current curbing is sufficient;
 - Alternatively, aesthetically compatible barriers (with modification to permit property access according to current practices)

From the bridge to the S-Bend:

- Lane width of 3.30m;
- Rollover gutters;
- No dedicated pedestrian lane
- No cyclist graphics on the road; and
- Appropriate stormwater management.

The S-Bend:

- Modified lane width to suit curve based on a lane with of 3.30m entering and exiting the curve and reduced speed;
- Rollover gutters;
- Cut hill slope to improve sightlines;
- Aesthetically acceptable retaining wall at 1331 and 1347 Mill Street;
- No guardrail at 1331 and 1347 Mill Street;
- No dedicated pedestrian lane;
- No cyclist graphics on the road;
- Signage at top of hill re speed reduction through S-Bend;
- Pavement paintings re speed reduction;
- Signage at top of hill re pedestrians; and
- Appropriate stormwater management.

From S-Bend to Mississauga Road:

- Lane width of 3.30m;

- Rollover gutters;
- No dedicated pedestrian lane;
- No cyclist graphics on the road; and
- Appropriate stormwater management.

Reasoning

The Town policy on traffic calming indicates that it will not support traffic calming measures after the fact. It is also understood that police enforcement is not a viable option. As a result, any reconstruction of the road must incorporate traffic calming into its design while still allowing for the efficient movement of traffic (pedestrian, vehicular and cyclists). The Residents believe that the proposed lane width of 3.30 meters is the maximum width and narrower widths may be appropriate and accomplish those goals and helps preserve the streetscape.

Narrower roads help promote slower driving speeds. They have the added benefits of reduced crossing distances, less stormwater and less construction materials to build. The curbs will help make the road appear narrower.

Mill Street is not a high volume road, and it currently has a posted limit of 40km/h. The main users of the road (vehicular and pedestrian) are the residents of the community. Out of town visitors are increasing, but the Residents believe that their proposal accommodates any increases in volume and has the benefit of discouraging the use of Mill Street as a bypass or commuter route.

The proposed rollover curbs permit pedestrians to move off of the roadway if necessary to avoid other users, and the lack of a designated pedestrian lane permits pedestrians to walk facing oncoming traffic. The rollover curbs permit easier access to property for the Residents, including those with farm equipment.

Concluding Remarks:

We, the Residents, believe that our proposed solution is a compromise that could work and should be implemented within the same planned year of 2022 as has been budgeted for. The residents need to see the road repairs completed and would not be accepting of a delay in the repairs, as this issue has been outstanding for several years now.

The Residents look forward to continuing dialogue and meeting with the Town in a collaborative process with the ultimate goal of a design that suits the Village in all respects.



Cheltenham Area Residents Association

P.O. Box 1
Cheltenham, Ontario
L7C 3L7

Dear Sir/Madam,

October 18, 2021

The Cheltenham Area Residents Association fully supports the alternate proposal put forward in this document for the rehabilitation of Mill Street in Cheltenham.

This proposal represents the opinion of the majority of the residents living on Mill Street and is far more in keeping with the historic character of this village.

Maintaining the trees and streetscape should be given the highest priority and this document is in keeping with the new paradigm adopted by many other jurisdictions who have recognised the value of their unique small villages.

We respectfully request that the Town of Caledon and R.V. Anderson Associates Ltd adopt this proposal as the way to move forward for Mill Street.

Respectfully,

Cheltenham Area Residents Association

Comments from the Community regarding the Oct. 5th Virtual Presentation:


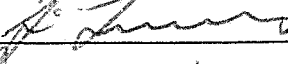

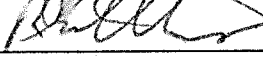


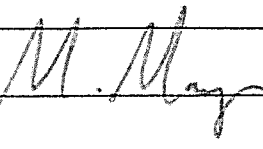
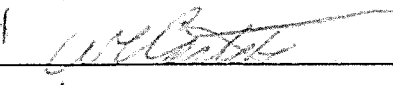

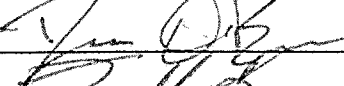
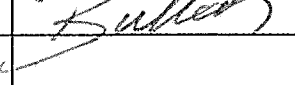
The Community was only given until Oct. 19th to respond with questions and concerns (2 weeks). A request was made that more time be allowed for the community to respond with questions and concerns, however there was no response from the Town on that request.

In the limited time available between the drafting of the Proposed Remedy and the deadline of Oct. 19th (48 hours), the following resident reviewed the Proposed Remedy and their signatures were collected. The Community is confident that many other residents would be in support of the Proposed Remedy and signatures will continue to be collected.

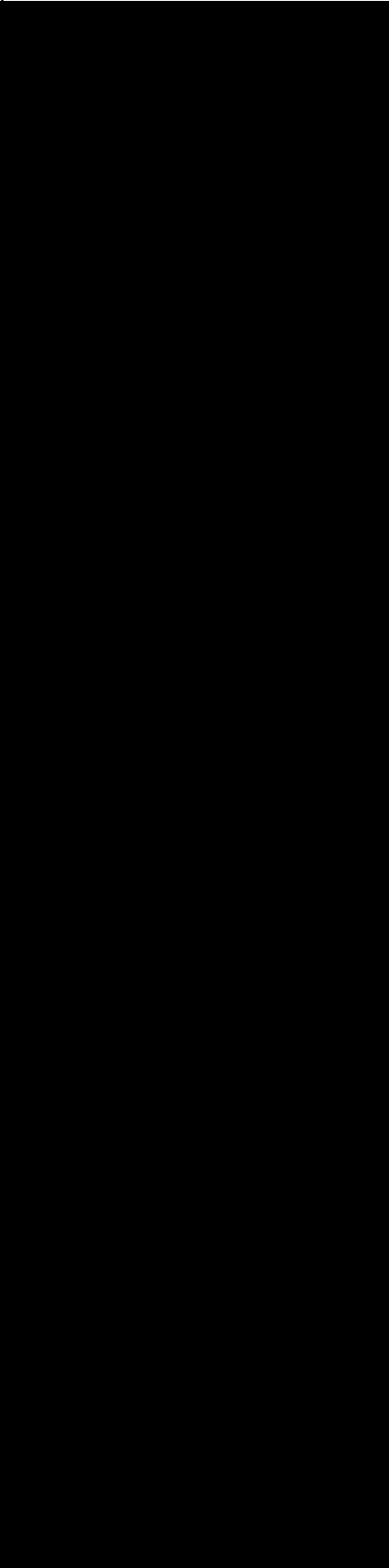
- ✓ 1039 Mill St.
- ✓ 1058 Mill St.
- ✓ 1140 Mill St.
- ✓ 1144 Mill St.
- ✓ 1164 Mill St.
- ✓ 1176 Mill St.
- ✓ 1254 Mill St.
- ✓ 1260 Mill St.
- ✓ 1326 Mill St.
- ✓ 1328 Mill St.
- ✓ 1331 Mill St.
- ✓ 1346 Mill St.
- ✓ 1347 Mill St.
- ✓ 1360 Mill St.
- ✓ 1376 Mill St.
- ✓ 1392 Mill St.
- ✓ 1402 Mill St.
- ✓ 1406 Mill St.
- ✓ 1428 Mill St.
- ✓ 1431 Mill St.
- ✓ 1440 Mill St.
- ✓ 1442 Mill St.
- ✓ 1443 Mill St.
- ✓ 1448 Mill St.
- ✓ 1499 Mill St.
- ✓ 14369 Creditview Rd.
- ✓ 14376 Creditview Rd.
- ✓ 14387 Creditview Rd.
- ✓ 14586 Creditview Rd.
- ✓ 14596 Creditview Rd.
- ✓ 14601 Creditview Rd.
- ✓ 161 Ferndale Park Rd.

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the submission to the Town:

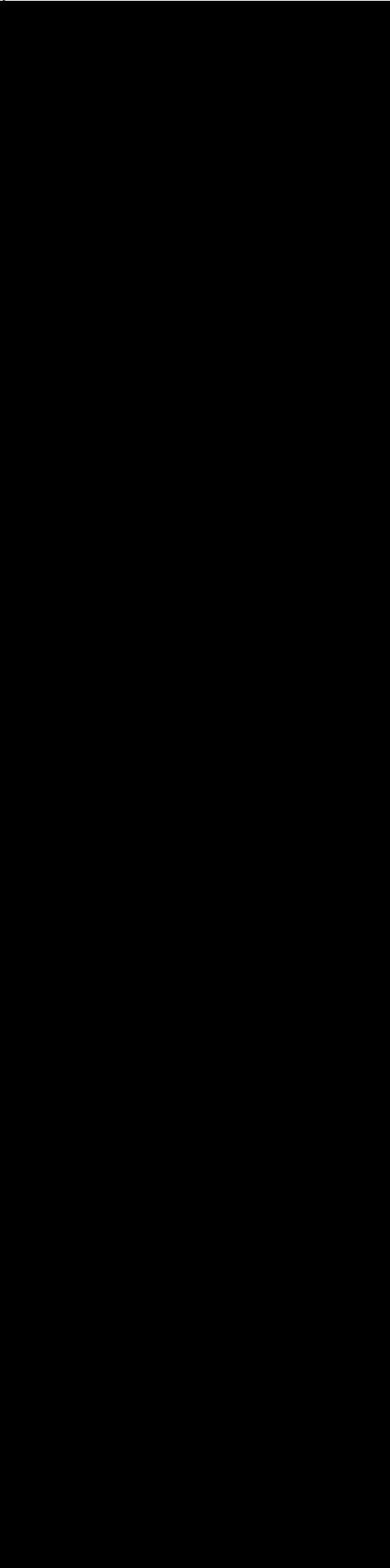

Cheltenham Resident Name	Address	Signature	Date
CARA (Cheltenham Area Residents Association)			
	1499 Mill St.		
	14376 Creditview Rd.		
	1448 Mill St.	Debbie	17/10/21
		Debbie	17/09/21
	1442 Mill St.	Debbie	Oct. 17/21
		Samuel	10/17/21
	1440 Mill St.	Kristine Malm	Oct 17/21
	1442 Mill	Debbie	10/17/21
	1443 Mill St.	Guy B	10/17/2021
	1431 Mill St.	Ken Newcombe	10/18/21
	1428 Mill St.		
	1406 Mill St.	Marcia Reid	10/17/21
	1404 Mill St.		
	1402 Mill St.		
	1392 Mill St.		

	1382 Mill St.		
	1376 Mill St.		10/17/21
			10/17/21
	1380 Mill St.		10/17/21
			10/17/21
	1346 Mill St.		10/17/21
	1346 Mill		10/17/21
	1357 Mill St.		
	1346 mill		
	1347 Mill St.		10/17/21
	1331 Mill St.		
			10/17/21
	1328 Mill St.		10/17/21
	1328		10/17/21
	1326 Mill St.		10/17/21
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	1140 Mill St.		
	1058 Mill St.		

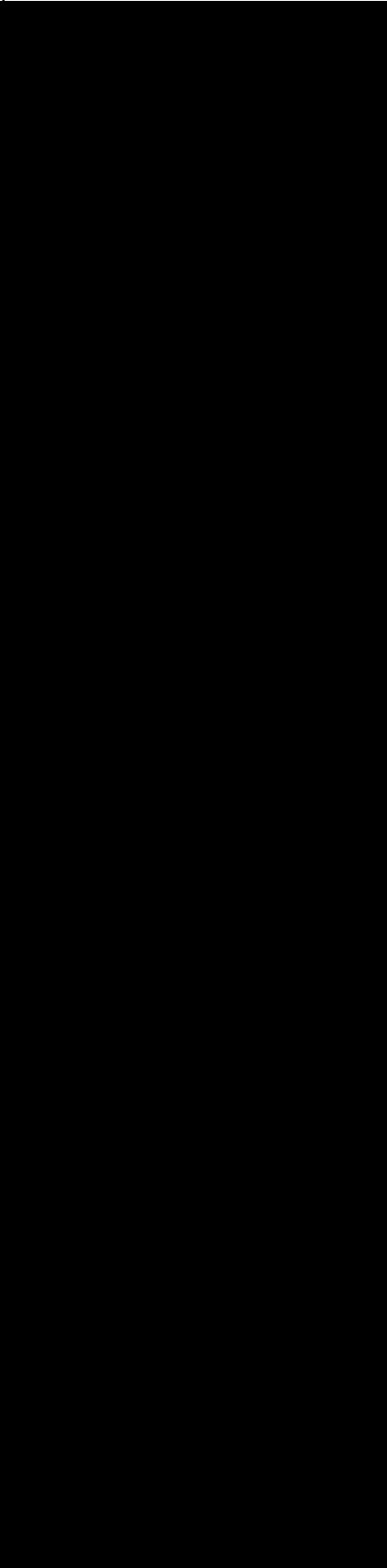
In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the submission to the Town:

Cheltenham Resident Name	Address	Signature	Date
	Association)		
	1499 Mill St.	<i>Celeen Whithed</i>	Oct 17/21
		<i>[Signature]</i>	u
	14376 Creditview Rd.		
	1448 Mill St.		
	1442 Mill St.		
	1440 Mill St.		
	1443 Mill St.		
	1431 Mill St.		
	1428 Mill St.		
	1406 Mill St.		
	1404 Mill St.		
	1402 Mill St.		
	1392 Mill St.		

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the submission to the Town:

Cheltenham Resident Name	Address	Signature	Date
	Association)		
	1499 Mill St.		
	14376 Creditview Rd.		
	14387 Creditview Rd.		Oct 18, 2021
	1448 Mill St.		
	1442 Mill St.		
	1440 Mill St.		
	1443 Mill St.		
	1431 Mill St.		
	1428 Mill St.		
	1406 Mill St.		
	1404 Mill St.		
	1402 Mill St.		
	1392 Mill St.		

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the submission to the Town:

Cheltenham Resident Name	Address	Signature	Date
	Association)		
	1499 Mill St.		
	14376 Creditview Rd.		
	1448 Mill St.		
	1442 Mill St.		
	1440 Mill St.		
	1443 Mill St.		
	1431 Mill St.		
	1428 Mill St.		
	1406 Mill St.		
	1404 Mill St.		
	1402 Mill St.		
	1392 Mill St.	M. Bishop	10/18/21

Andre Chow Leong	1392 Mill St.	1392 Od 18 th 2021	
	1382 Mill St.		
	1376 Mill St.		
	1380 Mill St.		
	1346 Mill St.		
	1357 Mill St.		
	1347 Mill St.		
	1331 Mill St.		
	1328 Mill St.		
	1326 Mill St.		
	1254 Mill St.		
	1276 Mill St.		
	1164 Mill St.		
	1140 Mill St.		
	1058 Mill St.		

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the submission to the Town:

Cheltenham Resident Name	Address	Signature	Date
	Association)		
	1499 Mill St.		
	14376 Creditview Rd.		
	1448 Mill St.		
	1442 Mill St.		
	1440 Mill St.		
	1443 Mill St.		
	1431 Mill St.		
	1428 Mill St.	DL Mrs Ki	oct 18/21
	1406 Mill St.		
	1404 Mill St.		
	1402 Mill St.	x Maynard	18/oct/21
	1392 Mill St.		

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy as outlined in the submission to the Town:

Name	Address	Signature	Date
(Residents Association)			
	1499 Mill St.		
	14376 Creditview Rd.		
	1448 Mill St.		
	1442 Mill St.		
	1440 Mill St.		
	1443 Mill St.		
	1443 Mill St.	<i>Macdonald</i>	<i>Oct 17/21</i>
	1431 Mill St.		
	1428 Mill St.		
	1406 Mill St.		
	1404 Mill St.		
	1402 Mill St.		
	1392 Mill St.		

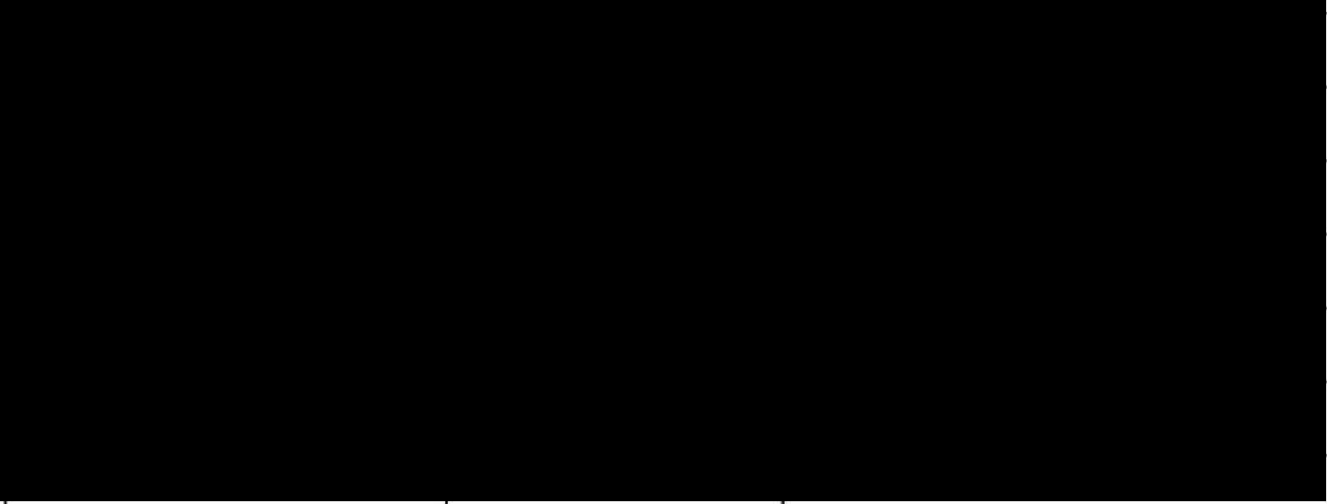
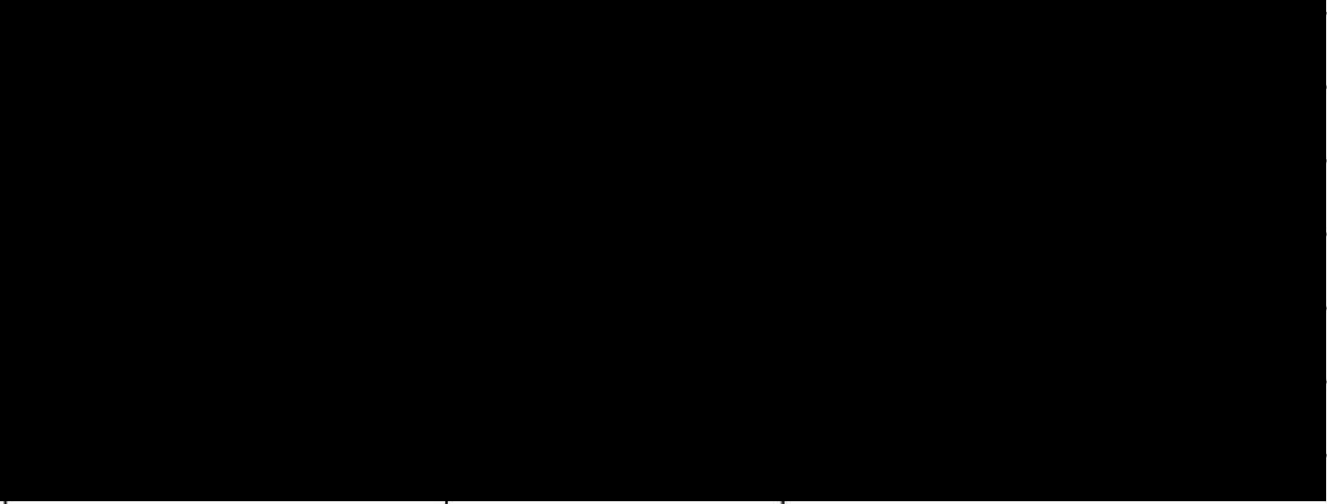
In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the submission to the Town:

Cheltenham Resident Name	Address	Signature	Date
	Association)		
	1499 Mill St.		
	14376 Creditview Rd.	Karen Keith	10/18/21
	1448 Mill St.		
	1442 Mill St.		
	1440 Mill St.		
	1443 Mill St.		
	1431 Mill St.		
	1428 Mill St.		
	1406 Mill St.		
	1404 Mill St.		
	1402 Mill St.		
	1392 Mill St.		

	1382 Mill St.		
	1376 Mill St.		
	1380 Mill St.		
	1346 Mill St.		
	1357 Mill St.		
	1347 Mill St.		
	1347 mill st.	<i>[Signature]</i>	10/18/21
	1331 Mill St.		
	1328 Mill St.		
	1326 Mill St.		
	1254 Mill St.		
	1276 Mill St.		
	1164 Mill St.		
	1140 Mill St.		
	1058 Mill St.		

	1040 Mill St.		
	1039 Mill St.		
	Others missing on south side of Mill (e.g. Dan & Nancy)		
	Creditview Rd., Station Rd., Kennedy Rd., residents?		
	14369 CREDITVIEW	per [Signature]	10/18/21

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the submission to the Town: (we are pedestrian users of Mill Street; and very rarely vehicular users)

Cheltenham Resident Name	Address	Signature	Date
			10/18/2021
			10/18/2021
			10/18/2021
			10/18/2021
			10/18/2021
			10/18/2021

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the submission to the Town:

Name(s)

Address:

Address:

Phone:

Date:

Oct 18/2021

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the submission to the Town:

Name(s)

Address:

Address:

Phone:

Date:

oct 17 / 21

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with eachother and are in support of the communities Proposed Remedy, as outlined in the attached submission to the Town:

Name(s)		
Address:		
Address:		
Phone:		
Date:		

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the attached submission to the Town:

Name(s)

Address:

Address:

Phone:

Date:

OCTOBER 17TH 2021.

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the attached submission to the Town:

Name(s)

Address:

Address:

Phone:

Date:

Oct 17 2021

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the submission to the Town:

Name(s)

Address:

Address:

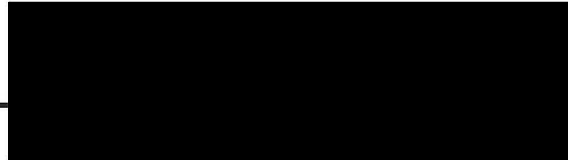
Phone:

Date:

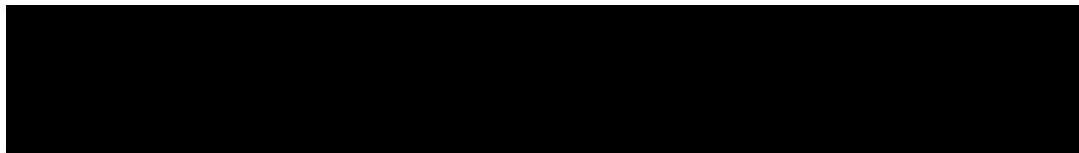
OCT 17 / 2021

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the submission to the Town:

Name(s)

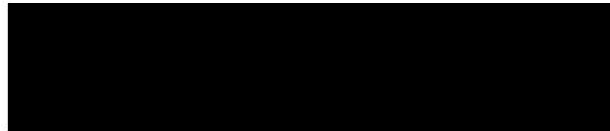


Address:



Address:

Phone:



Date:

Oct. 17 / 2001

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed Remedy, as outlined in the submission to the Town:

Name(s)

[REDACTED]

Address:

[REDACTED]

Address:

CHELTONHAM

Phone:

[REDACTED]

Date:

October 18, 2021

In response to the Town of Caledon and its Consultants re: Mill St EA and proposed solution, the following residents have consulted with each other and are in support of the communities Proposed remedy, as outlined in the submission to the Town:

[illegible]

Connor MacIsaac

To: Winnie Wong
Subject: RE: Mill St EA - 14376 Creditview Rd, Cheltenham - Karen & David Keith

From: Shun Cheung <Shun.Cheung@caledon.ca>

Sent: October 19, 2021 4:56 PM

To: [REDACTED]

Cc: Christina Early <Christina.Early@caledon.ca>; Johanna Downey <Johanna.Downey@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>

Subject: Re: Mill St EA - 14376 Creditview Rd, Cheltenham - Karen & David Keith

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

[REDACTED]. Received.

Get [Outlook for iOS](#)

From: [REDACTED]

[REDACTED] Tuesday, October 19, 2021 2:51:29 PM

To: Shun Cheung <Shun.Cheung@caledon.ca>; winnie.wong@rvanderson.com <winnie.wong@rvanderson.com>

Cc: Christina Early <Christina.Early@caledon.ca>; Johanna Downey <Johanna.Downey@caledon.ca>

Subject: Mill St EA - 14376 Creditview Rd, Cheltenham - Karen & David Keith

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Hello

In addition to the community documents submitted in a separate email, and in order to officially meet with the Towns deadline of Oct. 19th to respond to the consultants R.V. Anderson and their recommended proposal regarding Mill St., below are our concerns. Also attached:

- Pictures that detail the impact of the proposal to our streetscape and our property
- Letter of support from Melanie Daigle, Tailor Made Real Estate, dated Oct. 18, 2021

Our concerns:

- [REDACTED], Cheltenham, **north-west corner of Creditview and Mill**
- 20% of our street frontage is on Creditview Rd. The historical road widening of Creditview Rd., means there is no separation between our house and the Creditview sidewalk and little separation between our house and Creditview Rd., itself
- **80% of our street frontage is on Mill St.**
- Ours is a heritage home with our Mill St., frontage currently lined with large and small trees as well as numerous bushes, shrubs and perennials

- Our home is well known in the community, situated beside the Cheltenham General Store, and is considered a draw and attraction to the central hub of our village
- The proposal being made as part of the Mill St EA project is to widen Mill St., the full length (from Creditview to Mississauga Rd.) to accommodate a pedestrian lane
- There can be no doubt that the widening of the road will bring additional volume of traffic to Mill St., as well as increased speeding. There is no enforcement currently and no reasonable expectation that enforcement will increase in the future. The road design should not be one that will promote speeding (which widening does); the design should incorporate traffic calming measures (such as narrowing)
- The road widening will bring the new road less than 11' from the front of our house on the corner of Mill and Creditview
- 5 large trees have been earmarked for removal but the total loss alongside our property on Mill will include additional trees (both large and small) and numerous bushes, shrubs and perennials
- The proposed changes will negatively impact the streetscape that we have tried to maintain and enhance over the past 12 years and will result in a severe loss of streetscape as well as privacy and ability for us to enjoy our property along Mill St (our patio and backyard)
- Guiderails are being proposed for our section of Mill St., that make no sense in relation to risk
- The widening of the road will also promote more parking of cars / trucks alongside our property on Mill, despite any no-parking signs that may be planned. This is already an issue with no enforcement of the current no-parking zone. Neither a pedestrian line or additional no-parking signs will do anything to deter vehicles from pulling up and parking on a regular basis
- At the Oct. 5th virtual meeting, the presentation slides and the presentation itself did not accurately identify the true impact of their proposal on our property. Questionable is the fact that our house # was not made visible on the presentation slides, as were the Mill St., properties, and the final slide on page 26, an east facing view of Mill towards Creditview, captured the south side of Mill, which is not being impacted, and cut out the north side of Mill, which is the side of the road that we are on and the side being the most adversely impacted by the road-widening proposal.

These are our high-level concerns summarized. In short, we are gravely concerned with the proposal to widen the road and the negative impact it will have on the heritage aspects of this property and our heritage home itself.

We would appreciate an in-person meeting with the relevant stakeholders in this project so that we can voice our concerns in person and they can see the impact to our section of Mill St., including the streetscape and to this heritage property and our home. With stakes and caution tape, we have outlined the impact along our area of Mill. Our house is likely the most negatively affected of all the homes on the entire street.

We look forward to your follow up as soon as possible.



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October 18th, 2021

RESPONSE TO MILL STREET (CHELTENHAM) MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT AND RECOMMENDATIONS

I am a resident of Cheltenham and have lived in the village since 1997; raising my two children here. My family are frequent pedestrian users of Mill Street and very rarely do we drive on Mill Street. The current curves and speed reductions are actually a natural deterrent for regular vehicular use which takes care of traffic calming on Mill Street just as the road is today.

Additionally to being a resident, I own and run Cheltenham's only real estate brokerage based in the village from our family home on Creditview Road. My brokerage has been here since 2009 and prior to that I have conducted my own real estate sales business; working for two other real estate brokerages; also from my home in Cheltenham.

The proposal of widening Mill Street will significantly negatively impact the current property owners financially be devaluing their properties along Mill Street and most particularly 14376 Creditview Road which is at the corner of Creditview Road and Mill Street and has a long frontage of 290 feet along Mill Street to the bridge and on the Credit River.

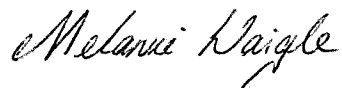
Last year I had a home on the market on Creditview Road, 14426 Creditview Road, also located at a bridge over the Credit River. Years ago, apparently Hurricane Hazel wiped out the Creditview Road bridge but that home was not impacted. When the Town reconstructed Creditview Road and the Creditview Bridge, they altered the road level by raising it, and leaving the home at 14426 Creditview Road, also a 100-year old home, looking like it had sunk. When you look at that home from the street; the south side of the front of the home used to have a drive-in garage into the existing home from the street level. As you will see, there is a significant slope on that property now. This negatively impacted this home and the financial value since most buyers would not accept that property the way that it looks now and this was during a very hot real estate market with buyers buying everything up in the entire Peel Region and Caledon. The Credit Valley Conservation Authority refuses to allow anyone to build that specific home to a higher elevation thereby leaving that home with very few interested buyers and at a much lower property value than any other homes in this area. This was all because of road adjustments made to negatively impact that home and property.

Please reconsider your plans immediately because I am very worried for all of the homeowners that will be negatively impacted and for such an unnecessary reason. Why don't you instead place a sign at Mill Street and Mississauga Road stating for "local traffic only"; keep the speed at 40 kph (or you could even lower it should you choose if you are concerned about speed) and place a speed camera at the two areas of most concern. This would likely have far less of a cost to tax payers and could create revenue if the road has anyone violating the posted speed limits. That road does not need to be a fully asphalt-covered road and could lessen the costs for maintenance if you used more of a tar and gravel or crushed asphalt product.

I invite anyone to get in touch with me directly to discuss other ideas further. Residents in our village all want to preserve the natural beauty and heritage aspects of our community. We also want to keep everyone safe, but pedestrians are safe by walking facing oncoming traffic as our entire family does along Mill Street.

Common sense should prevail in this instance and a further study is not necessary; nor is road widening on Mill Street. At the very least, I implore you to not touch the property at 14376 Creditview Road (from the Mill Street Bridge to Creditview Road on the north side of Mill Street). It should be preserved as it is now; keeping their trees and shrubs intact and not going any closer to the home than the curbs and road already is for that particular home and not placing a sidewalk there either. This property should be kept in its current state for its natural heritage in the most vibrant area of our village. If busy areas in Toronto do not have sidewalks, certainly a tiny little rural village on a small, naturally windy street does not need them either. It is part of our villages charm and character. Street lights are not needed either. Cars have very wide beams for night vision driving.

Sincerely,

A handwritten signature in cursive script that reads "Melanie Daigle".

Melanie Daigle
Broker of Record/Owner
Mobile: 416-817-4224

Resident at 14601 Creditview Road, Cheltenham, Ontario L7C 1N3

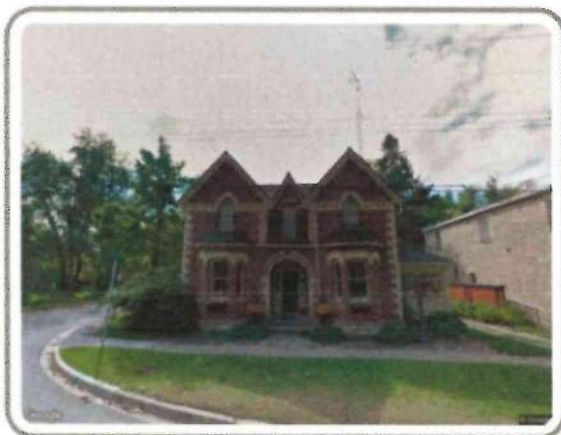
Attachments:

1. MPAC Property Detail Report for 14376 Creditview Road in Cheltenham
2. Example of how one property in Cheltenham was severely and negatively impacted based on the home's value and resale potential due to the road being altered and raised higher than this home's front door.
3. MLS Listings for two properties that I listed and sold during the same time period last year to show the home that was affected negatively due to the road widening and raising the elevation.

Property Detail Report



Date: 2021-10-18



Property Address: 14376 CREDITVIEW RD

Municipality: CALEDON TOWN

Roll Number: 212412000209400

Base Sales & Assessment Data

Property Information:

Property Type:	Residential
Property Code & Description:	301-Single-family detached (not on water)
Assessment Roll Legal Description:	PLAN CH 7 LOT 43
Legal Description:	LT 43 PL CH 7 CHINGUACOUSY EXCEPT CH32351 ; CALEDON

Year Built:	1887	Frontage(ft):	66.00
Number of Full Storeys:	2	Depth(ft):	290.00
Number of Partial Storeys:	No part storey	Site Area(sq.ft.):	0.44 Acres
		Variance:	Corner

Sale Information:

Last Sale Date:	21/08/2009
Last Sale Amount:	\$490,000

Valuation Information:

Assessed Value on January 1, 2016*:	\$722,000
Assessed Value on January 1, 2012:	\$573,000
Taxation Year:	Phased-In Assessment
2016	\$573,000
2017	\$610,250
2018	\$647,500
2019	\$684,750
2020	\$722,000
2021	\$722,000**

NOTE: Pursuant to the Ontario Assessment Act, MPAC conducts a province-wide Assessment Update every four years to reflect a new legislated valuation date. The valuation date is a fixed day on which all properties are valued. For the 2017-2020 taxation years, the assessed value was based on a January 1, 2016 valuation date. In 2020, the Assessment Update was postponed so the January 1, 2016 valuation date also applies to property assessments for the 2021 taxation year. For more information regarding assessments, visit www.mpac.ca

*Assessed Value is based on a January 1, 2016 Valuation Date.

**Phased-In Assessment reflects the phased-in portion of the Assessed Value returned to the municipality/local taxing authority on the 2020 Assessment Roll for the 2021 taxation year. Property assessments for the 2021 taxation year are based on the fully phased-in January 1, 2016 assessed values.

Enhanced Data

Bedrooms:	3
Bathrooms:	1
Half Bathrooms:	1
FirePlaces:	Yes
Garage Type:	Detached Garage
Garage Spaces:	2
Driveway:	Separate or Private Driveway
Pool:	No
Split Level:	No Split
Private Sanitary Service:	
Private Water Service:	
Zoning Code:	CV-F

Beryl Bland Parkette

PIN:142610038

Cheltenham
United Church

**EXAMPLE OF HOW ONE PROPERTY WAS NEGATIVELY IMPACTED BY ROAD
ALTERING THAT WAS RAISED HIGHER THAN THE FRONT DOOR OF THE HOME
IN CHELTENHAM**

Last year, I personally represented two properties on Creditview Road in Cheltenham;
on the market during the same time.

14426 CREDITVIEW ROAD

Days on Market: 132

Sold at: \$ 891,500

Listed: July 21, 2020

Sold: October 7, 2020

This is the property mentioned in my letter where the road had been levelled to be
higher than their front door and the road was widened where it looks like the original
home has sunk (which it has not).



This property had many valuable upgrades. It had been completely renovated in 2010 and everything was new for the interior and exterior from 2010; wiring, plumbing, hvac, insulation, walls, gas line for natural gas, municipal water, roof, soffits, eavestroughs. It is on 1.21 acres; has 238.97 feet of Credit River frontage; a 2 car massive garage with loft; no direct neighbours on any side of this home.

It had 2 offers at the initial asking price of \$997,000; both Buyers backed out when they could not rebuild or even raise a rebuilt home up to the road level per CVCA.

The person who eventually purchased it \$105,500 less than the original asking price, which was \$997,000, has no plans to rebuild on the property.

14281 CREDITVIEW ROAD

Days on Market: 9

Sold at: \$1,049,000

Listed: Sept. 28th, 2020

Sold: Oct. 7th, 2020



This property was half the size of the other one at .62 acres; no neighbours directly to the back. It was an original home but not one hundred years of age like the other two properties on Creditview Road mentioned. It had electric baseboard heating; no HVAC system, duct work or air conditioning; and this home was not on municipal water. This property did not suffer from any road level altering; nor was it located close to the road like 14266 Creditview Road or the property that could mostly be impacted by any road adjustments to Mill Street at 14376 Creditview Road on the corner of Mill Street.

During this time, the property had two offers on it and they accepted an offer within \$1000 of their asking price.



14281 Creditview Rd
Caledon Ontario L7C1N3
Sold: \$1,049,000
List: \$1,050,000

Caledon Cheltenham Peel 15-28-J
Taxes: \$5,409.44 / 2020 For: Sale % Dif: 100
SPIS: N Last Status: Sld
Pt Lt 29 Con 3 Whs Chinguacousy As In Ro694187 T/W DOM: 9

Detached Fronting On: E Rms: 7 + 5
Link: N Acreage: .50-1.99 Bedrooms: 3 + 2
Bungalow-Raised Lot: 100 x 270.5 Feet Washrms: 2
1x4xMain, 1x3xLower

Irreg: .62 Acres Per Mpac
Dir/Cross St: Creditview North Of King St

MLS#: W4930213

Seller: Rory Owen Jack

Occupancy: Owner

PIN#: 142630046

ARN#: 212412000203100

Contact After Exp: N

Holdover: 120

Kitchens:	1	Exterior:	Brick / Vinyl Siding	Zoning:	Rr
Fam Rm:	N	Drive:	Private	Cable TV:	Y Hydro: Y
Basement:	Finished / Sep Entrance	Gar/Gar Spcs:	Attached / 2.0	Gas:	A Phone: Y
Fireplace/Stv:	Y	Drive Pk Spcs:	6	Water:	Well
Heat:	Baseboard / Electric	Tot Pk Spcs:	8.0	Water Supply:	
A/C:	None	UFFI:		Sewer:	Septic
Central Vac:		Pool:	None	Waterfront:	
Apx Age:		Prop Feat:	Fenced Yard	Retirement:	
Apx Sqft:				Farm/Agr:	
Assessment:				Oth Struct:	Garden Shed
POTL:				Spec Desig:	Unknown
Laundry lev:					

#	Room	Level	Length (ft)	Width (ft)	Description		
1	Kitchen	Main	16.40	x 11.32	Hardwood Floor	Quartz Counter	Renovated
2	Living	Main	21.16	x 12.99	Hardwood Floor	French Doors	Wood Stove
3	Dining	Main	11.74	x 10.92	Hardwood Floor	W/O To Deck	Open Concept
4	Prim Bdrm	Main	16.24	x 11.58	Laminate	Semi Ensuite	Ceiling Fan
5	2nd Br	Main	13.09	x 10.33	Laminate		
6	3rd Br	Main	10.50	x 9.68	Laminate		
7	Mudroom	Ground	7.35	x 6.17	Ceramic Floor	French Doors	W/O To Garage
8	Family	Lower	26.99	x 23.26	Broadloom	Fireplace Insert	Above Grade Window
9	4th Br	Lower	13.25	x 9.58	Ceramic Floor	Ceiling Fan	Above Grade Window
10	5th Br	Lower	10.66	x 9.81	Ceramic Floor	Above Grade Window	
11	Office	Lower	12.66	x 9.41	Broadloom	Above Grade Window	
12	Laundry	Lower	13.15	x 4.76	Tile Floor	Above Grade Window	Double Sink

Client Remks: Spacious Raised Bungalow Has 2,989 Sq Ft Of Living Space With 3+2 Bedrooms Along With An Office; Updated Kitchen'19 With Quartz Counters, Plenty Of Storage, Glass Display Cabinets, Under-Cabinet Lighting & Led Pot Lts. Amazing Back Yard With Multi-Level Decks, Tiki Bar, Firepit, Mature Trees, Gardens; Backing Onto Lothlorien Horse Farm. Finished Bsmt W. Lg Above Grade Windows. Pub-Style Fam Rm. Mud Rm. Dog Run. Natural Gas Line & Town Water At Property Line.

Extras: Tiki Bar, Firepit, 2 Fireplaces "As Is", Water Softener, Hwt (R)'19. Siding & Roof '19. Windows/Doors '05. Well Pump '15. Exclude: Custom Bar & Molson Sign. Bsmt Sep Entrance Would Need To Be Unsealed By Buyer. Quaint Village; 10 Mins To 410; 15 Mins To Go.

Brkage Remks: Legal Description Cont'd: Vs111634. 4 Hours Notice Required For Sellers To Remove Dogs For Showings. Please Read Attached Covid Protocol Policy Prior To Booking A Showing. Masks Required & Please Leave Lights On. Form 801 With Offers Please; Email To Melaniedaigle@Rogers.Com.

List: TAILOR MADE REAL ESTATE INC., BROKERAGE Ph: 416-876-7355 Fax: 866-824-5687
MELANIE DAIGLE, Broker of Record 416-817-4224 DANIELLE BULARIO, Salesperson 905-299-8518
Co-Op: RE/MAX REALTY SPECIALISTS INC., BROKERAGE

Satwant Sandhu, Broker

Contract Date: 9/28/2020

Sold Date: 10/07/2020

Leased Terms:


Expiry Date: 2/28/2021


Closing Date: 1/06/2021

Original: \$1,050,000

Last Update: 10/08/2020

CB Comm: 2.5% + Hst. Thank You.

	14426 Creditview Rd		Sold: \$891,500		
	Caledon Ontario L7C1N5		List: \$897,000		
	Caledon Cheltenham Peel 15-28-J				
	Taxes: \$5,170.43 / 2020		For: Sale % Dif: 99		
SPIS: Y Last Status: Sld		DOM: 62			
Lot 58 Plan 7 Chinguacousy, Caledon					
Detached		Fronting On: W	Rms: 7 + 4		
Link: N		Acreage: .50-1.99	Bedrooms: 3		
2-Storey		Lot: 241 x 218 Feet	Washrms: 2		
			1x4xMain, 1x3x2nd		
Irreg: 238.97 Feet River Frontage-1.21 Ac Irreg					
Dir/Cross St: Creditview North Of King St					
MLS#: W4933250 Seller: Rodney William Seabrooke Occupancy: Owner					
PIN#: 142610045 ARN#: 212412000208600 Contact After Exp: N					
Holdover: 120					
Kitchens: 1	Exterior: Alum Siding	Zoning: Epa2			
Fam Rm: Y	Drive: Private	Cable TV: Y Hydro: Y			
Basement: Crawl Space	Gar/Gar Spcs: Detached / 2.0	Gas: Y Phone: Y			
Fireplace/Stv: N	Drive Pk Spcs: 6	Water: Municipal			
Heat: Forced Air / Gas	Tot Pk Spcs: 8.0	Water Supply:			
A/C: Central Air	UFFI:	Sewer: Septic			
Central Vac:	Pool: None	Waterfront: Direct			
Apx Age:	Prop Feat: Grnbelt/Conserv, Park,	Retirement:			
Apx Sqft:	Ravine, River/Stream, School Bus Route,	Farm/Agr:			
Assessment:	Waterfront	Oth Struct: Garden Shed Workshop			
POTL:		Spec Desig: Unknown			
Laundry lev:					
Water Name: Credit, River Shoreline: Sandy, Shallow Shoreline Exp: W					
Water Front(m): 72.84					
Water Features: Dock, Riverfront					
#	Room	Level	Length (ft)	Width (ft)	Description
1	Kitchen	Main	20.17	x 9.84	Breakfast Bar Granite Counter Pantry
2	Dining	Main	20.17	x 8.43	Open Concept Pot Lights Ceramic Floor
3	Family	Main	16.66	x 7.51	Heated Floor 4 Pc Bath W/O To Yard
4	Living	Main	16.83	x 9.91	Hardwood Floor Pot Lights Separate Rm
5	Mudroom	Main	11.74	x 5.08	Large Window W/O To Yard
6	Prim Bdrm	2nd	17.42	x 10.00	Hardwood Floor Pot Lights Ceiling Fan
7	Br	2nd	10.82	x 9.51	Hardwood Floor Ceiling Fan W/I Closet
8	Br	2nd	10.82	x 9.84	Hardwood Floor Ceiling Fan
9	Laundry	Main			4 Pc Bath
10	Furnace	Main			
11	Loft				
Client Remks: Live/Work From Home. Stunning View Thru All 4 Seasons On The River. Cottage-Like Setting On 1.21 Acres In The Gta! Private-No Neighbours On Any Side Or Back. \$200K Complete Reno W/I 10 Yrs. Town Water. Natural Gas. Hi-Speed Internet. Walk/Bike To The Trails In The Village. Only 1 Traffic Light To Downtown Toronto Via Old School Rd/Hwy 10. 15 Mins To Go Train. Extras: Private Dock. Renos Include: Open Kit/Granite, Gas Heat, Gas Stove, Gas Dryer, Gas Hwt(R), Radiant Floor Heat In Fam Rm/Bath/Laundry, Ac, Roof, Windows, Spray Insul'n, Ducts, Drywall, Pot Lts, Wiring, Plumbing! Security Cams; R/I Cvac. 2 Car Garage W/Loft. Brkage Remks: Propane (R) For Heated Workshop Only. Natural Gas In Home. Please Include Form 801 & Schedule B With Offers. See Attached Covid-19 Protocol Prior To Booking. Exclude Upright Freezer. Under Cvca. Fast Closing Possible.					
List: TAILOR MADE REAL ESTATE INC., BROKERAGE Ph: 416-876-7355 Fax: 866-824-5687 MELANIE DAIGLE, Broker of Record 416-817-4224 DANIELLE BULARIO, Salesperson 905-299-8518 Co-Op: CITYSITES REALTY INC., BROKERAGE Michael Spencer-Thompson, Salesperson Contract Date: 9/30/2020 Sold Date: 12/01/2020 Leased Terms: Expiry Date: 3/31/2021 Closing Date: 1/29/2021 Original: \$897,000 Last Update: 12/02/2020 CB Comm: 2.5 % Plus Hst Thank You!					

	14426 Creditview Rd Caledon Ontario L7C1N5		List: \$967,000 For: Sale	
	Caledon Cheltenham Peel 15-28-J Taxes: \$5,170.43 / 2020 Lot 58 Plan 7 Chinguacousy; Caledon		SPIS: N Last Status: Ter DOM: 29	
Detached Link: N 2-Storey Irreg: 238.97 Feet River Frontage-1.21 Ac Irreg Dir/Cross St: Creditview North Of King St		Front On: W Acre: .50-1.99 241 x 218 Feet		Rms: 7 + 4 Bedrooms: 3 Washrooms: 2 1x4xMain, 1x3x2nd

MLS#: W4892832 Sellers: Rodney William Seabrooke And Kim Marie Seabrooke Contact After Exp: N
Holdover: 120 Possession Remarks: Prefer Nov/Dec Occup: Owner
PIN#: 142610045 ARN#: 212412000208600

Kitchens: 1	Exterior: Alum Siding	Zoning: Epa2
Fam Rm: Y	Drive: Private	Cable TV: Y
Basement: Crawl Space	Gar/Gar Pk Spcs: Detached / 2.0	Hydro: Y
Fireplace/Stv: N	Drive Pk Spcs: 6	Gas: Y
Heat: Forced Air / Gas	Tot Pk Spcs: 8.0	Phone: Y
A/C: Central Air	UFFI:	Water: Municipal
Central Vac:	Pool: None	Water Supply:
Apx Age:	Energy Cert:	Sewer: None
Apx Sqft:	Cert Level:	Spec Desig: Unknown
Assessment:	GreenPIS:	Farm/Agr:
POTL:	Prop Feat: Park, Place Of Worship,	Waterfront: Direct
Elevator/Lift:	Ravine, River/Stream, School Bus Route,	Retirement:
Laundry Lev: Main	Waterfront	Oth Struct: Workshop
Phys Hdcp-Eqp:		

Water Name: Credit, River
Water Front(m): 72.84
Water Features: Dock

#	Room	Level	Length (ft)	Width (ft)	Description
1	Kitchen	Main	20.17	x 9.84	Breakfast Bar Granite Counter Pantry
2	Dining	Main	20.17	x 8.43	Open Concept Pot Lights Ceramic Floor
3	Family	Main	16.66	x 7.51	Heated Floor 4 Pc Bath W/O To Yard
4	Living	Main	16.83	x 9.91	Hardwood Floor Pot Lights Separate Rm
5	Mudroom	Main	11.74	x 5.08	Large Window W/O To Yard
6	Prim Bdrm	2nd	17.42	x 10.00	Hardwood Floor Ceiling Fan
7	Br	2nd	10.82	x 9.51	Hardwood Floor Ceiling Fan W/I Closet
8	Br	2nd	10.82	x 9.84	Hardwood Floor Ceiling Fan
9	Laundry	Main			4 Pc Bath
10	Furnace	Main			
11	Loft				

Client Remks: Live/Work From Home & Enjoy A Coffee Break On Your Own Dock On The River; Your Home Is Within An Amazing Cottage Setting On 1.21 Acres; Private-No Neighbours On Any Side Or Back. Walk To The General Store To Pick Up Your Mail & Lunch Or Dinner. Walk/Bike To The Trails In The Village. Only 1 Traffic Light To Downtown Toronto Via Old School Rd/Hwy 10. 15 Mins To Go Train.


Extras: Reno'd W/I 10 Yrs: Open Kit/Granite Counters, Windows, Gas Heat, Gas Stove, Gas Dryer, Gas Hwt(R), Radiant Floor Heat In Fam Rm/Bath/Laundry, Ac, Roof, Windows, Insulation, Ducts, Drywall, Pot Lts, Wiring, Plumbing; Security Cams; R/I Cvac.

Brkage Remks: On Town Water. Rogers Hi-Speed Internet. Rustic Garage W Loft/Propane (R) Heated Workshop. Under Cvc/Nec Watershed/Green Belt. Form 801 & Sched B With Offers/24 Hr Irrev Req'd. Covid Form Protocol Attached.

TAILOR MADE REAL ESTATE INC., BROKERAGE Ph: 416-876-7355 Fax: 866-824-5687

14601 Creditview Road Caledon L7C1N3
MELANIE DAIGLE, Broker of Record 416-817-4224
DANIELLE BULARIO, Salesperson 905-299-8518

Contract Date: 8/31/2020	Condition:	Ad: Y
Expiry Date: 2/26/2021	Cond Expiry:	Escape:
Last Update: 9/29/2020	CB Comm: 2.5% With Thanks	Original: \$984,700

	14426 Creditview Rd		List: \$997,000 For: Sale
	Caledon Ontario L7C1N5		
	Caledon Cheltenham Peel 15-28-J		
Taxes: \$5,170.43 / 2020		SPIS: N	Last Status: Ter
Lot 58 Plan 7 Chinguacousy; Caledon		DOM: 41	
Detached		Front On: W	Rms: 7 + 4
Link: N		Acre: .50-1.99	Bedrooms: 3
2-Storey		241 x 218 Feet	Washrooms: 2
Irreg: 238.97 Feet River		1x4xMain, 1x3x2nd	
Frontage - 1.21 Acres			
Dir/Cross St: Creditview North Of King St			

MLS#: W4839409 Sellers: Rodney William Seabrooke And Kim Marie Seabrooke Contact After Exp: N

Opn Hse Note: Virtual Tour

Holdover: 60

Possession Date: 10/30/2020

Possession Remarks: 30-60 Days

Occup: Owner

Tba

PIN#: 142610045

ARN#: 212412000208600

Kitchens: 1	Exterior: Alum Siding	Zoning: Irregularly Shaped
Fam Rm: Y	Drive: Private	Cable TV: Y
Basement: Crawl Space	Gar/Gar Pk Spcs: Detached / 2.0	Hydro: Y
Fireplace/Stv: N	Drive Pk Spcs: 6	Gas: Y
Heat: Forced Air / Gas	Tot Pk Spcs: 8.0	Phone: Y
A/C: Central Air	UFFI:	Water: Municipal
Central Vac:	Pool: None	Water Supply:
Apx Age:	Energy Cert:	Sewer: None
Apx Sqft:	Cert Level:	Spec Desig: Unknown
Assessment:	GreenPIS:	Farm/Agr:
POTL:	Prop Feat: Park, Place Of Worship,	Waterfront: Direct
Elevator/Lift:	Ravine, River/Stream, School Bus Route,	Retirement:
Laundry Lev: Main	Waterfront	Oth Struct: Workshop
Phys Hdcp-Eqp:		

Water Name: Credit, River
Water Front(m): 72.84

#	Room	Level	Length (ft)	Width (ft)	Description
1	Kitchen	Main	20.17	x 9.84	Breakfast Bar Granite Counter Pantry
2	Dining	Main	20.17	x 8.43	Open Concept Pot Lights Ceramic Floor
3	Family	Main	16.66	x 7.51	Heated Floor 4 Pc Bath W/O To Yard
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8	Br	2nd	10.82	x 9.84	Hardwood Floor Ceiling Fan
9	Laundry	Main			4 Pc Bath
10	Furnace	Main			
11	Loft				

Client Remks: Buy The View! This Home&Property Is All About The Lifestyle Of What Living On The Credit River Brings. 238 Ft River Frontage & Private Dock To Sit & Relax While Enjoying Nature. Go On River Tubing Journeys On The Winding Lazy River Or Canoe/Kayak&Fish. On 1.21 Acres.No Neighbours On Sides Or Back. Extensive Updates Done W/I Past 10 Yrs Incl. Windows, All Roofs, Spray Foam Insul'n,, Elect, Plumbing, Ducts&More. Lg Fire Pit. Sunny/West Back Yard. On Town Water.

Extras: Gas Heat, Gas Stove, Gas Dryer& Gashwt(R). Heated Floors In Fam Rm & Mn Flr Bath/Laundry. Pot Lights. Open Kitchen W Granite Counters. Det'd Rustic Garage With Loft&Propane(R) Heated Workshop.Security Cam's. R/I C/Vac. Mayfield Hs.

Brkage Remks: Pls. Complete Covid Form/Email To Melaniedaigle@Rogers.Com Or Avail. On Site Prior To Showing. Gloves/Mask/Shoe Covers (Provided). No Overlapping Appts/Pls Be On Time. Form 801 With Offers.Under Cvc/Nec. 24 Hr. Irrev. Req'd 1 Seller Away.

Mortgage Comments: 238.97 River Frontage With Dock On 1.21 Acres Per Mpac / No Neighbours On Any Side

TAILOR MADE REAL ESTATE INC., BROKERAGE Ph: 416-876-7355 Fax: 866-824-5687

14601 Creditview Road Caledon L7C1N3

MELANIE DAIGLE, Broker of Record 416-817-4224

DANIELLE BULARIO, Salesperson 905-299-8518

Contract Date: 7/21/2020

Condition:

Ad: Y

Expiry Date: 12/31/2020

Cond Expiry:

Escape:



















PIC No. 2 Summary Report





Mill Street Municipal Class Environmental Assessment

Public Information Centre #2
Summary Report
FINAL

Town of Caledon

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RVA 205388

October 14, 2022

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APPENDICES

APPENDIX 1	- NOTIFICATION MATERIAL
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APPENDIX 3	- COMMENTS RECEIVED

1.0 INTRODUCTION

The Town of Caledon is currently undertaking a Schedule 'B' Municipal Class Environmental Assessment for improvements to Mill Street between Creditview Road and Mississauga Road. The purpose of the study is to identify a recommended design for Mill Street that enhances road safety and improves traffic operations and connectivity for all users. In identifying improvements to the corridor, the study is considering technical requirements; as well as socio-economic, cultural heritage, and natural environment factors.

R.V. Anderson Associates Limited (RVA) was retained by the Town to complete the Class Environmental Assessment (EA) for this project. This is a Schedule "B" project that covers phases 1 and 2 of the planning process, in accordance with the *Municipal Class Environmental Assessment (October 2000, amended in 2007, 2011 & 2015)*. As part of the Class EA planning and design process, extensive public and technical agency consultation is required in developing the study recommendations. Two (2) Public Information Centre (PIC) were held as part of the study.

1.1 Public Information Centre #2

The second Public Information Centre (PIC) was held on June 15, 2022, at the Cheltenham Community Centre (14190 Creditview Rd, Caledon, ON) from 5:30pm to 7:00pm. The format of the PIC was an informal "Open House" with materials pertaining to the study on display for the public to review. Members of the project team (Town of Caledon and RV Anderson Associates staff) were on hand to discuss the project and concerns brought forward by the attendees.

2.0 STAKEHOLDERS AND NOTIFICATION

Notification of the PIC was provided through various methods and media, as outlined below.

- **General Public:**
 - Notice posted on the Town's website on May 26, 2022
 - Notice published in *Caledon Enterprise* (local newspaper) on May 26, 2022, and in the Town's e-newsletter May 27, 2022, and June 22, 2022
- **Local Residents & Businesses:**
 - Notice mailed to all property owners within the study area
- **Technical Agencies, Local Interest Groups and First Nations Communities:**
 - Emails containing the notice and details of the PIC were sent on May 30, 2022

- **Project Mailing List (other parties who previously submitted comments or indicated interest in the project)**
 - Email containing the notice and details of the PIC was sent on May 30, 2022

Refer to **Appendix 1** for copies of the published notifications and stakeholder contact list.

3.0 CONSULTATION MATERIAL

The purpose of the second PIC was to share and receive input from the public on the options for pedestrian accommodations developed for Mill Street in consideration of the feedback received during and following PIC #1, held online October 5, 2021.

The display materials presented information on the study to date including the study area and purpose of the study, public and technical agency consultation completed to date, preliminary recommendations for the roadway, and alternative solutions for pedestrian accommodation. Display materials were posted to the project website in advance of the PIC, and were available in an alternate format (i.e., hard copy) upon request.

The PIC presented three pedestrian accommodation alternatives for Mill Street. These were developed in response to the team's earlier preliminary recommendation for pedestrians to be accommodated via paved shoulders throughout the corridor.

Refer to **Appendix 2** for copies of the materials made available for comment during the PIC.

4.0 PARTICIPATION AND COMMENT SUMMARY

Attendees were requested to sign-in upon arriving at the Public Information Centre. Based on the sign-in sheet, there were 31 attendees, including representation from Town Council, Cheltenham Area Residents Association, and residents of the Town.

All attendees were encouraged to provide comments and feedback on the material presented and the study in general. Comment forms were provided for attendees to complete. Attendees were also encouraged to submit additional comments by email or mail following the open house. Comments received are summarized below and included in **Appendix 3**.

4.1 Pre-PIC Comments and Questions Received

A total of eleven (11) comments / questions were received from the public in advance of the PIC, from 4 residents, regarding the format of PIC, participation and documentation of the PIC, availability of the PIC Slides, content of the PIC, and the design of the roadway.

Comments / questions received are summarized in the table below by these categories, in no particular order.

Table 4.1 – Summary of Pre-PIC Comments Received

TOPIC OF COMMENT RECEIVED	COMMENT SUMMARY	CONSIDERATION OF COMMENTS IN CLASS EA
Format of PIC	<ul style="list-style-type: none"> - Residents requested that the PIC include a formal presentation. 	<ul style="list-style-type: none"> - The format of the PIC remained as an open house, to allow for one-on-one discussions. - Residents were advised that based on previous experience, the requested presentation format tends to result in the conversation dominated by a few vocal participants, while some individuals are reluctant to express their opinions in front of others.
Participation and Documentation of the PIC	<ul style="list-style-type: none"> - Resident inquired how the results of the PIC and input provided will be documented. - Resident inquired how those who are not able to attend can participate in the PIC. - Resident inquired how the feedback provided by residents of Mill Street will be prioritized. 	<ul style="list-style-type: none"> - Resident was advised that all attendees will be invited to provide their comments within a comment form, which will be documented in the Project File Report. - Resident was advised that the PIC display material and comment sheet would be available online following the PIC. - Residents within the study area were directly mailed the PIC notice, however all members of the general public were welcome to provide input to the study.
Availability of PIC Slides	<ul style="list-style-type: none"> - Resident requested that the PIC display boards be made available for review prior to the PIC. 	<ul style="list-style-type: none"> - Presentation slides were posted to the project website and distributed via email to the residents who requested the information prior to the PIC (June 14th).
Content of PIC	<ul style="list-style-type: none"> - Resident inquired whether the Cheltenham Area Residents Association proposal will be included as an option at the PIC. 	<ul style="list-style-type: none"> - The resident's proposal was received last Fall and was considered in the development of the pedestrian accommodation options presented at the PIC. - Details of the resident's proposal, and how it was incorporated into the study, will be included in the Project File Report, available for

TOPIC OF COMMENT RECEIVED	COMMENT SUMMARY	CONSIDERATION OF COMMENTS IN CLASS EA
		public review at the end of the study.
Design of the Roadway	- Residents expressed desire that any recommended improvements to the roadway should retain and compliment the existing rural heritage of the Village, and not include widening the roadway.	- The desire to retain a rural roadway, and maintain narrow travel lanes, was considered in the development of the additional pedestrian accommodation options presented at the PIC.

4.2 PIC Comments and Questions Received – Residents and General Public

A total of twenty (20) completed comment sheets questions were submitted, in addition to three comments provided via email. The comment sheet included six questions, requesting respondents to identify their mode and frequency of travel along Mill Street, their opinion of how well the presented alternatives accommodate pedestrians, as well as any other comments and questions they may have.

A summary of the comments received is provided below.

4.2.1 Question 1 Comment Sheet Results – Demographics

Figure 4.1 demonstrates the responses received to Question 1 on the comment sheet, which asked respondents to identify whether they are a resident of Mill Street. Of the 20 comment sheets received, 16 responses were from residents on Mill Street and 4 responses were from residents within Cheltenham community

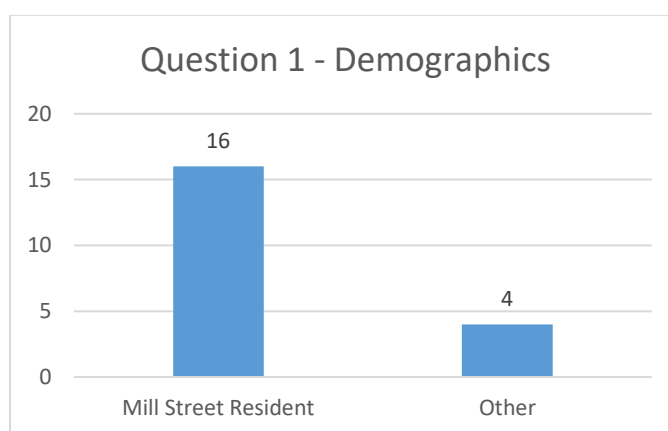


Figure 4.1 – Question 1 Results

4.2.2 Question 2 Comment Sheet Results – Primary Mode of Travel

Figure 4.2 demonstrates the responses received to Question 2 on the comment sheet, which asked respondents to identify their primary mode of travelling along Mill Street. Of the 20 comment sheets received, 14 respondents use more than one mode of transportation (walking, cycling, car, other) to travel on Mill Street.

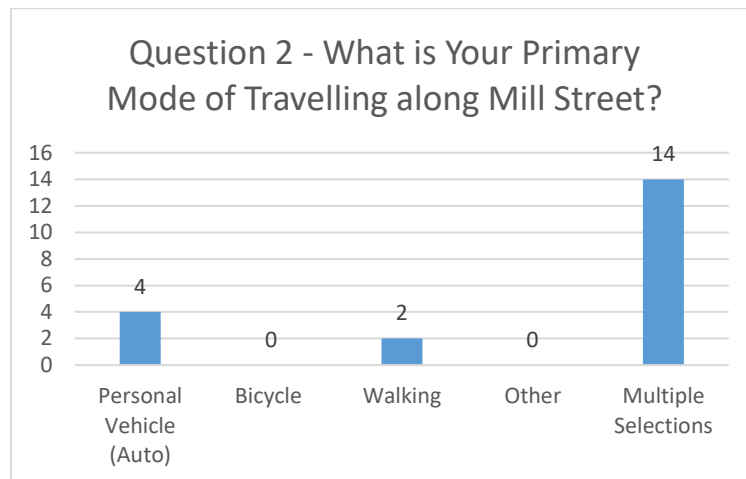


Figure 4.2 – Question 2 Results

4.2.3 Question 3 Comment Sheet Results – Primary Mode of Travel

Figure 4.3 demonstrates the responses received to Question 3 on the comment sheet, which asked respondents to identify the approximate frequency that they walk along Mill Street. Of the 20 comment sheets received, the majority of respondents (15), indicated they walk along the street daily.

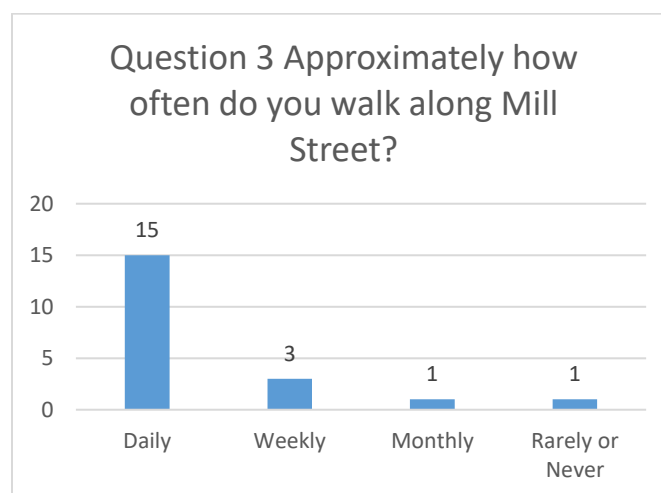


Figure 4.3 – Question 3 Results

4.2.4 Question 4 Comment Sheet Results – Likelihood of Increased Walking

Figure 4.4 demonstrates the responses received to Question 4 of the comment sheet, which asked respondents whether they would be more likely to walk along Mill Street if there was a designated pedestrian facility. Of the 20 comment sheets received, the majority of respondents (15), indicated that they would not be more likely to walk on Mill Street if there was a dedicated pedestrian facility.

Supplementary comments provided in response to the question largely indicated that residents feel safe walking along the roadway. 3 residents identified that they would be more likely to walk along Mill Street if there was a designated pedestrian facility, due to feeling safer with a designated facility than walking along the road. Of the 3 respondents who expressed that they would be more likely to walk along Mill Street if there was a designated pedestrian facility, 1 was a resident of Mill Street, while the 2 others were residents within the Cheltenham community, not residing directly on Mill Street.



Figure 4.4 – Question 3 Results

4.2.5 Question 5 Comment Sheet Results – Rank of Pedestrian Accommodation

Figures, Figure 4.5, Figure 4.6, and Figure 4.7 demonstrate the responses received to Question 5 on the comment sheet, which asked respondents to rank how they feel each of the alternatives presented accommodate pedestrians. Of the 20 comment sheets received, 4 respondents expressed support for sidewalk, however, the majority of respondents expressed a strong preference for not introducing pedestrian accommodations. Of the 4 respondents who expressed support for sidewalk, 1 was a resident of Mill Street, while the 3 others were residents within the Cheltenham community, not residing directly on Mill Street.

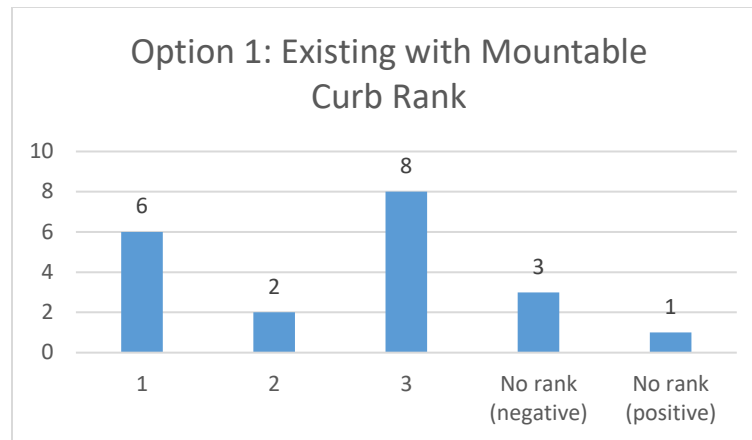


Figure 4.5 – Question 5: Option 1 Rank

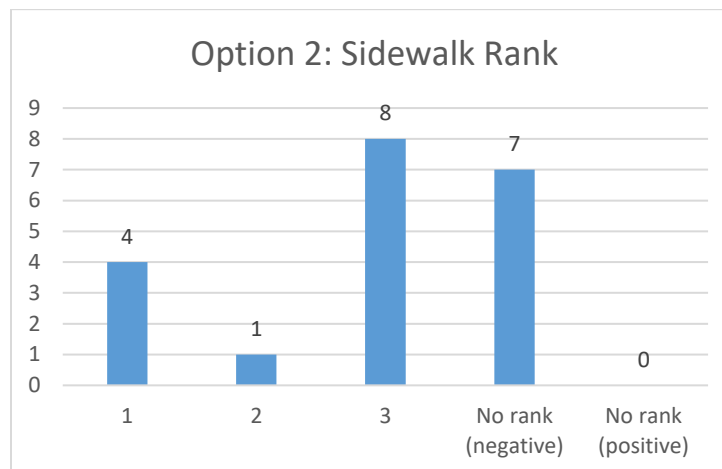


Figure 4.6 – Question 5: Option 2 Rank

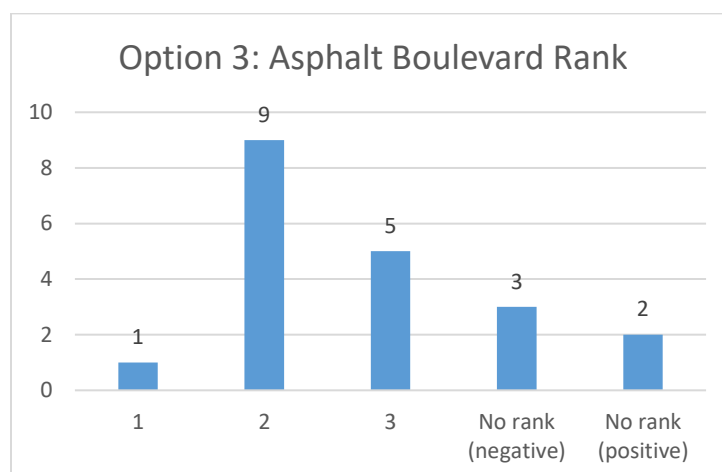


Figure 4.7 – Question 5: Option 3 Rank

4.2.6 Question 6 Comment Sheet Results – General Comments and Questions

Several common categories were identified within the comments provided, including concerns related retaining the existing cultural heritage, reducing speeding along the corridor, pedestrian accommodation, roadway drainage and stormwater management

Illumination on Mill Street utility services, property impacts & driveway access, natural environmental impacts including drainage, and the Class EA planning process & construction timeline. Comments / questions received are summarized in the table below by these categories, in no particular order.

Table 4.2 – Summary of Residents and General Public Comments Received

TOPIC OF COMMENT RECEIVED	COMMENT SUMMARY	CONSIDERATION OF COMMENTS IN CLASS EA
Retaining the Heritage of the Street	<ul style="list-style-type: none"> - Residents made clear that any recommended improvements to the roadway should retain and compliment the existing rural heritage of the Village. Residents generally defined rural heritage as no pedestrian facility, preserve trees, no guiderail, no pavement widening, no curb and gutter or semi-mountable curb and gutter and streetlighting appropriate for a rural road. 	<ul style="list-style-type: none"> - The preferred alternative maintains the existing road alignment and pavement width, semi-mountable curb and new guiderail where required for safety. Detailed design to review existing lighting levels and provide recommendation
Reducing Speeding along the corridor	<ul style="list-style-type: none"> - Residents stated concerns related to speeding within the study area, expressing support for traffic calming features. 	<ul style="list-style-type: none"> - Traffic calming to be achieved through maintaining existing lane widths, new curb and gutter to visually narrow the road, and maintaining the existing road alignment.
Pedestrian Accommodation	<ul style="list-style-type: none"> - Many residents stated a strong preference for not introducing pedestrian accommodations. - Some residents expressed support for a sidewalk. 	<ul style="list-style-type: none"> - Due to significant push back from residents on Mill Street, designated pedestrian facilities were not incorporated into the study recommendations.
Roadway Drainage and Stormwater Management	<ul style="list-style-type: none"> - Support for improved stormwater management and drainage improvements were expressed. 	<ul style="list-style-type: none"> - Recommendations for improvement stormwater management and drainage include: - New semi-mountable curb and gutter with catch basins to outlet to existing ditches

TOPIC OF COMMENT RECEIVED	COMMENT SUMMARY	CONSIDERATION OF COMMENTS IN CLASS EA
		<ul style="list-style-type: none"> - Replace and upsizing culverts that do not meet drainage capacity - Ditch clean out to improve ditch capacity.

4.3 PIC Comments and Meetings – Technical Agencies and Stakeholders

An email containing the notice and details of the PIC was sent to relevant technical agencies and stakeholders on May 30, 2022. The complete list of Technical Agencies and Stakeholders who received the PIC Notification is provided in **Appendix 2**.

Comments on the PIC display materials were received from the Region of Peel. Table 4.3 - Summary of Technical Agency Comments Received summarizes the comments received from various technical agencies during the PIC.

Table 4.3 - Summary of Technical Agency Comments Received

AGENCY/ GROUP	COMMENT SUMMARY	CONSIDERATION OF COMMENTS IN CLASS EA
Region of Peel	<ul style="list-style-type: none"> - The Region noted the presence and location of watermain along Mill Street, noting that any impact to Regional underground infrastructure, Valve, chambers and fire hydrants will need to be taken into consideration during this project. - Region requested PIC display boards. 	<ul style="list-style-type: none"> - Study recommendations were developed in consideration of Regional input and infrastructure. - The project team provided the Region with the PIC display boards June 14th
	<ul style="list-style-type: none"> - In review of the PIC displays, the Region identified Option 2 – Sidewalk, as the Region's preferred alternative, noting that it provides the safest option for pedestrians. The Region also inquired why the focus of the alternatives presented at the PIC are only on pedestrians and not active transportation (cyclists)? 	<ul style="list-style-type: none"> - Designated pedestrian and cyclist facilities were not incorporated into the study recommendations in consideration of comments received from the public.
	<p>Region of Peel staff made the following inquiries:</p> <ul style="list-style-type: none"> - Does this EA cover the entire intersection of Mississauga Road and Mill Street? - Please provide soft copies of the materials for reference after the PIC Meeting #2 	<ul style="list-style-type: none"> - The Mill Street Class EA does not include the intersection of Mississauga Road. - The PIC display slides were previously provided to the Region on June 14, 2022.

AGENCY/ GROUP	COMMENT SUMMARY	CONSIDERATION OF COMMENTS IN CLASS EA
	<ul style="list-style-type: none"> - Will Town of Caledon be conducting a Trail Crossing Treatment Warrant for the Trail Crossing at Mill Street? - Would the Town of Caledon consider a connection between Mill Street and the Trail crossing as well along Mississauga Road? 	<ul style="list-style-type: none"> - Pedestrian crossing improvements including new traffic signs and pavement markings are proposed at the Caledon Trailway Crossing on Mill Street, with details to be confirmed during detailed design. - The Caledon Trailway currently intersects both Mill Street and Mississauga Road.

All comments received, meeting agendas and follow-up discussions involving Technical Agencies and Stakeholder Groups are included in **Appendix 3**.

5.0 CONCLUSIONS, STUDY RECOMMENDATIONS AND NEXT STEPS

The PIC provided an opportunity for residents to provide input to the development of the study recommendations, with a focus on pedestrian accommodations. Comments on various topics were received, with a number of trends becoming evident:

- Little support for road safety improvements at intersections and the “S” bend.
- Pedestrian and cyclist accommodation throughout the corridor is not supported by Mill Street residents.
- General support for rehabilitating the road pavement only, with no other changes.
- Residents expressed a desire to maintain the rural character of the roadway.
- Concern over property impacts and tree removals.
- Support for drainage improvements.
- Concern over road improvements will potentially increase traffic volumes and attract other road users to Mill Street.
- Support for additional traffic calming measures.

5.1 Study Recommendations

Preliminary recommendations were presented at the first Public Information Centre (PIC), October 5, 2021, with additional alternatives for pedestrian accommodations presented at the second PIC on June 15, 2022. In consideration of the feedback received during and following the second PIC, the following recommendations have been developed for the Mill Street corridor:

- Maintain the existing road alignment, including the S-curve;
- New semi-mountable curb and gutter with catch basins, maintain existing ditches;
- No pedestrian accommodation (Option #1 presented at PIC #2);

- Pedestrian crossing improvements at the Caledon Trailway;
- New guiderail where required; and
- Streetlighting improvements (types and locations to be confirmed).

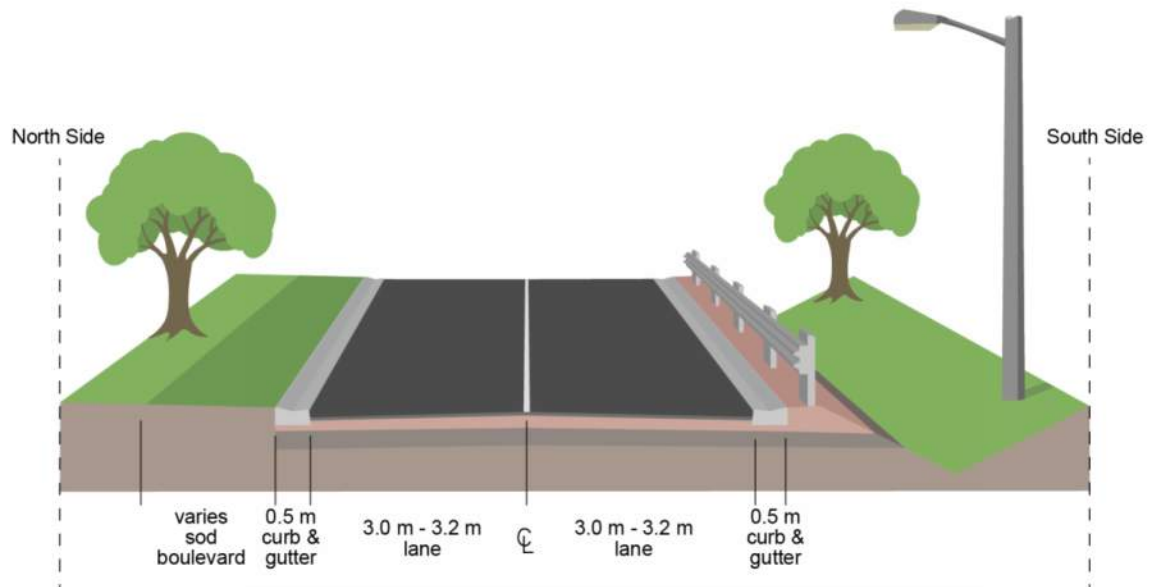


Figure 5.1 - Option #1 Existing with Mountable Curb (Presented at PIC #2)

5.2 Next Steps

In accordance with the requirements of the Municipal Class Environmental Assessment, the public is to be contacted following the identification of the recommended solution(s), prior to issuing the Notice of Study Completion. As such, a Notice of Study Recommendations has been prepared, along with this memorandum, to notify the public of the study recommendations described above.

The next opportunity for public input will be in Late Fall 2022 or Early Winter 2023 when the Project File Report is submitted for public review. The Project File Report will document, in detail, the planning and public consultation process undertaken throughout the EA, as well as details regarding the study recommendations. The report will be made available to the public for review, through issuance of a Notice of Study Completion, in accordance with the requirements of the Municipal Class Environmental Assessment.

The Notice of Study Completion will also advise the public that during the 30-day review period, a request may be made to the Ministry of the Environment, Conservation and Parks (MECP) for an order requiring a higher level of study (i.e. requiring an individual/comprehensive EA approval before being able to proceed), or that conditions be imposed (e.g. require further studies), on the grounds that the requested order may

prevent, mitigate or remedy adverse impacts on constitutionally protected Aboriginal and treaty rights.

Following the close of the 30-day public review period, the MECP has an additional 30 days to consider the project and review any potential Section 16 Order requests submitted during the 30-day public review period. The Town may not proceed with the project for at least these 30 days following the end of the public review period.

Following this 30-day MECP review period, the project may proceed to detailed design and construction, provided the ministry is not reviewing Section 16 Order requests related to the project, and subject to any other permits and approvals that may be required.

Subject to comments received on the Project File Report, the Town intends to proceed with the detailed design of this project in 2023.

APPENDIX 1

PIC MATERIALS

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT



MILL STREET (MISSISSAUGA ROAD TO CREDITVIEW ROAD)

WELCOME PUBLIC INFORMATION CENTRE #2

June 15, 2022
5:30pm to 7:00pm

PLEASE SIGN IN

**Please review the materials and provide your comments using the sheets provided.
Your feedback is important to us.**

Staff are available to answer your questions.

WHY ARE WE HERE?



In consideration of the comments received to date, the Town has developed additional pedestrian accommodation options for Mill Street. The Town is holding this PIC to allow residents and the public to review these options and provide input.

No recommended option has been selected.

The following preliminary recommendations presented at PIC #1 will be carried forward:

- Additional trees will be provided per Town's replanting policy
- Curb and gutter with catch basins to capture stormwater runoff
- Streetlighting improvements to enhance community safety
- Guiderail where required
- Slope stability (to be confirmed)
- Implement additional traffic calming measures (type and location to be confirmed)

Please provide your input on the pedestrian accommodation options by completing the comment sheet provided.

Please complete and submit the comment sheet by July 6th

STUDY AREA AND PURPOSE OF STUDY



The Study is being completed to review opportunities within the study limits to address:

- Increasing wear and tear of the roadway
- Enhanced road safety for all users
- Improved accessible active transportation (walking, cycling) including Caledon Trail crossing improvements
- Roadway drainage and stormwater management needs.

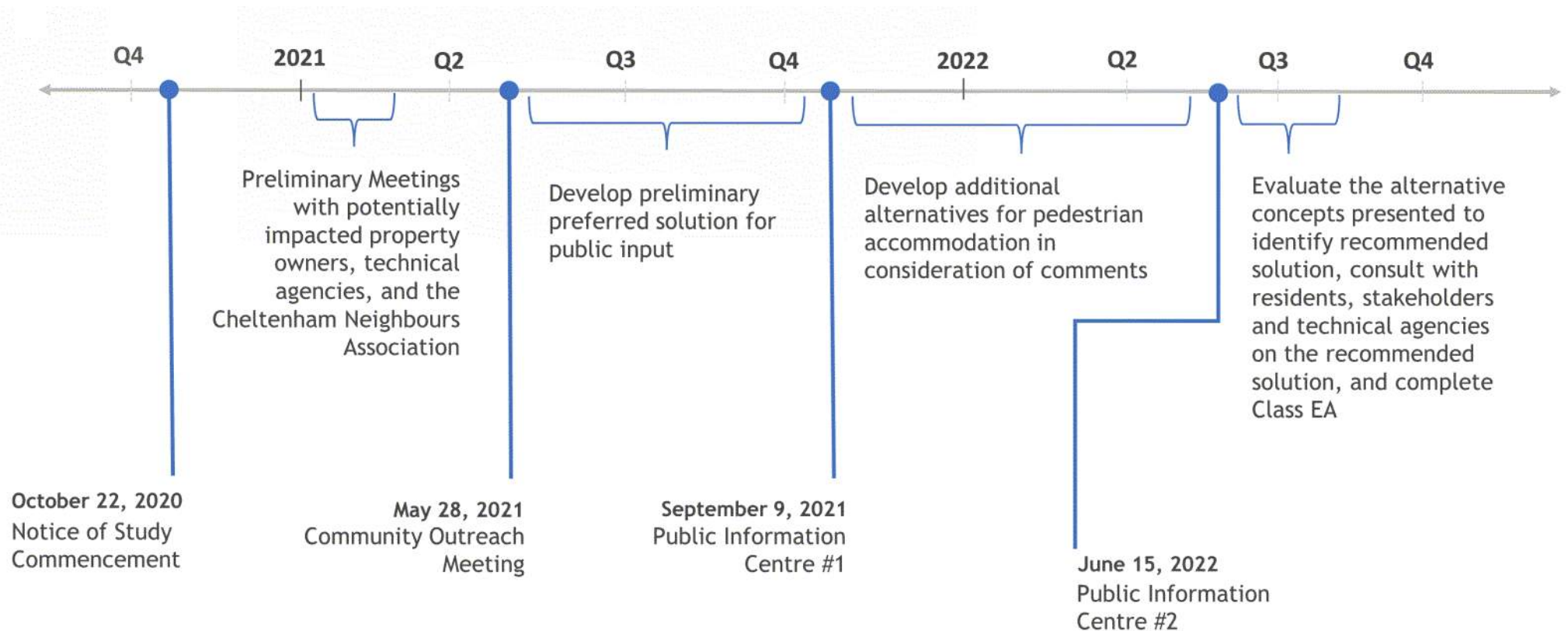


The project is being completed as a Schedule “B” project in accordance with the *Municipal Class Environmental Assessment* (October 2000, amended in 2007, 2011 & 2015).

PUBLIC AND TECHNICAL AGENCY CONSULTATION



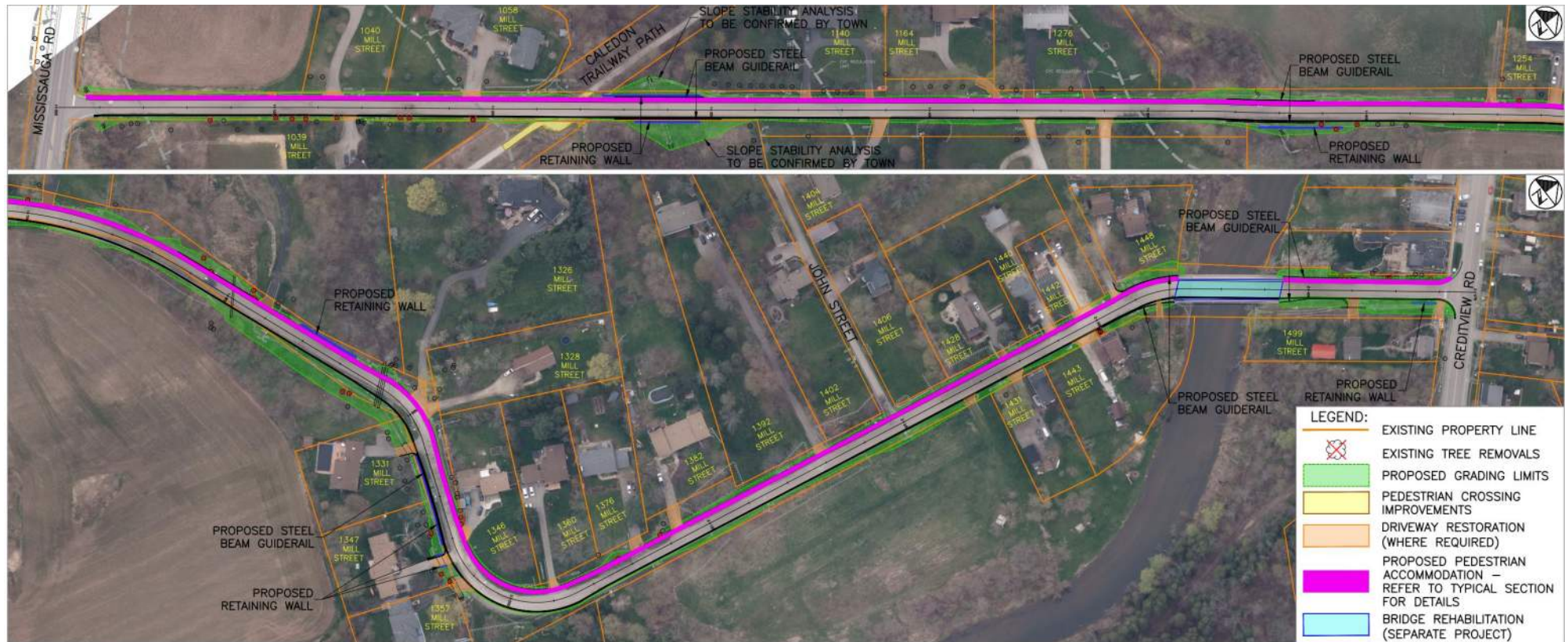
Significant public and technical agency consultation has been completed so far, but the Town wants additional input before identifying the recommended pedestrian amenities.



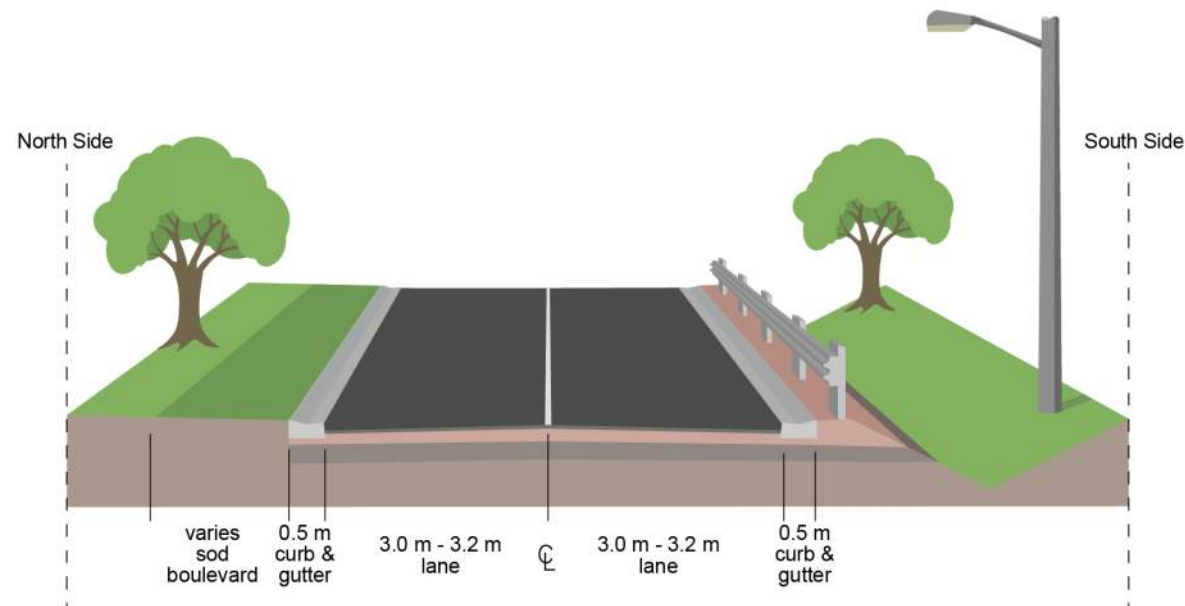
PEDESTRIAN ACCOMMODATION FOR MILL STREET



New pedestrian accommodation options have been developed for input from residents and the public (highlighted in **purple** below). All options have similar property and tree impacts.



OPTION 1 – EXISTING WITH MOUNTABLE CURB



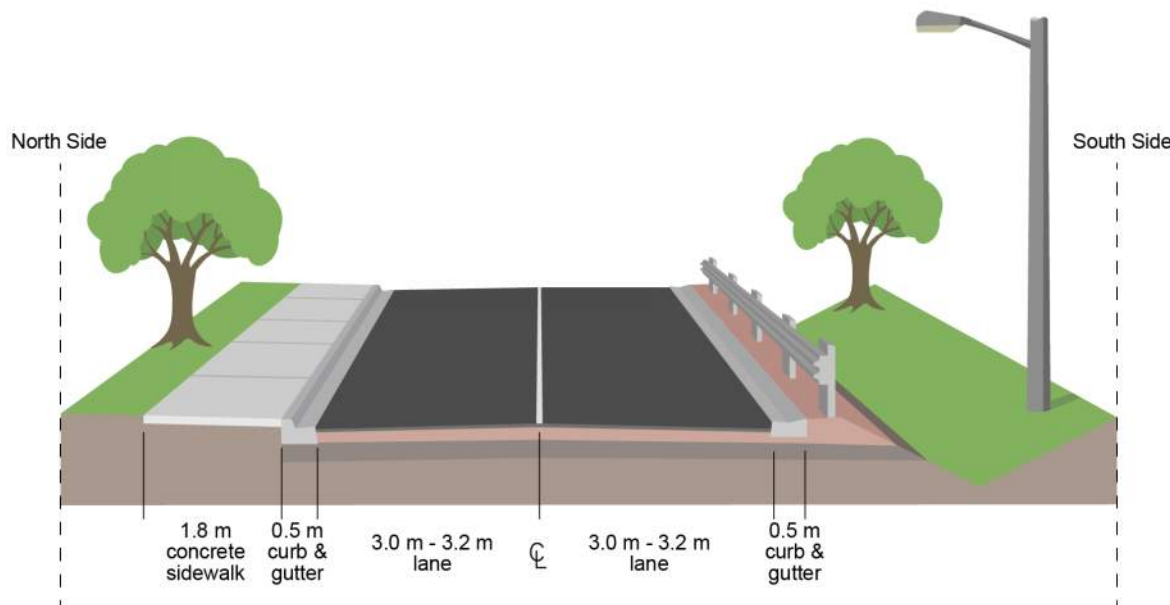
Advantages

- ✓ Maintains general rural characteristic of road (mountable curbs and no sidewalk)
- ✓ Less impervious area (less stormwater runoff)

Disadvantages

- ✗ Does not address study objectives in enhancing road safety for all users and improving accessible active transportation
- ✗ Does not accommodate pedestrians
- ✗ No separation between pedestrians, cyclists and vehicular traffic
- ✗ Boulevard becomes snow storage area in winter

OPTION 2 – SIDEWALK



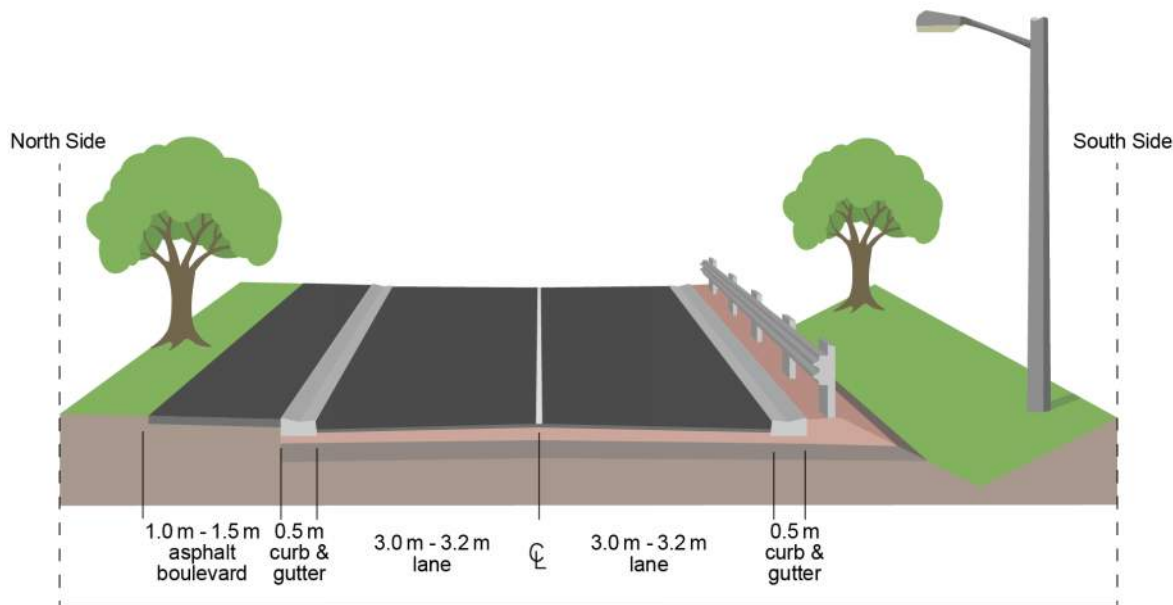
Advantages

- ✓ Provides dedicated pedestrian facility - Accessibility for Ontarians with Disabilities Act (AODA) compliant, accommodates mobility aids (ex. wheelchairs, wheeled walkers)
- ✓ Provides even surface to walk on
- ✓ Full barrier curb separates pedestrians from roadway and provides traffic calming benefits
- ✓ Will be maintained by the Town in the winter

Disadvantages

- ✗ Does not maintain general rural characteristic of road (barrier curbs and sidewalk)
- ✗ No separation between cyclists and vehicular traffic

OPTION 3 – ASPHALT BOULEVARD



Advantages

- ✓ Maintains general rural characteristic of road (mountable curbs and no sidewalk)
- ✓ Provides even surface to walk on (seasonal)

Disadvantages

- ✗ Not a dedicated pedestrian facility – not AODA compliant
- ✗ No separation between cyclists and vehicular traffic
- ✗ Boulevard becomes snow storage area in winter
- ✗ Mountable curb allows vehicles to drive onto boulevard, creating potential conflict with pedestrians

NEXT STEPS



- Review comment sheets received from this PIC
- Evaluate the alternative concepts presented and identify the preferred solution – check website for updates
- Consult with residents, stakeholders and technical agencies on the preferred solution
- Prepare and submit a Project File Report for 30 Day public review
- Proceed to detailed design (2022) and construction (2023) pending Council approval and budget

WE WANT YOUR INPUT!



Please submit your input on the pedestrian accommodation options in the comment sheet provided and place it in the comment box or submit via email by **July 6, 2022**.

THANK YOU FOR ATTENDING!
PLEASE FILL IN A COMMENT SHEET

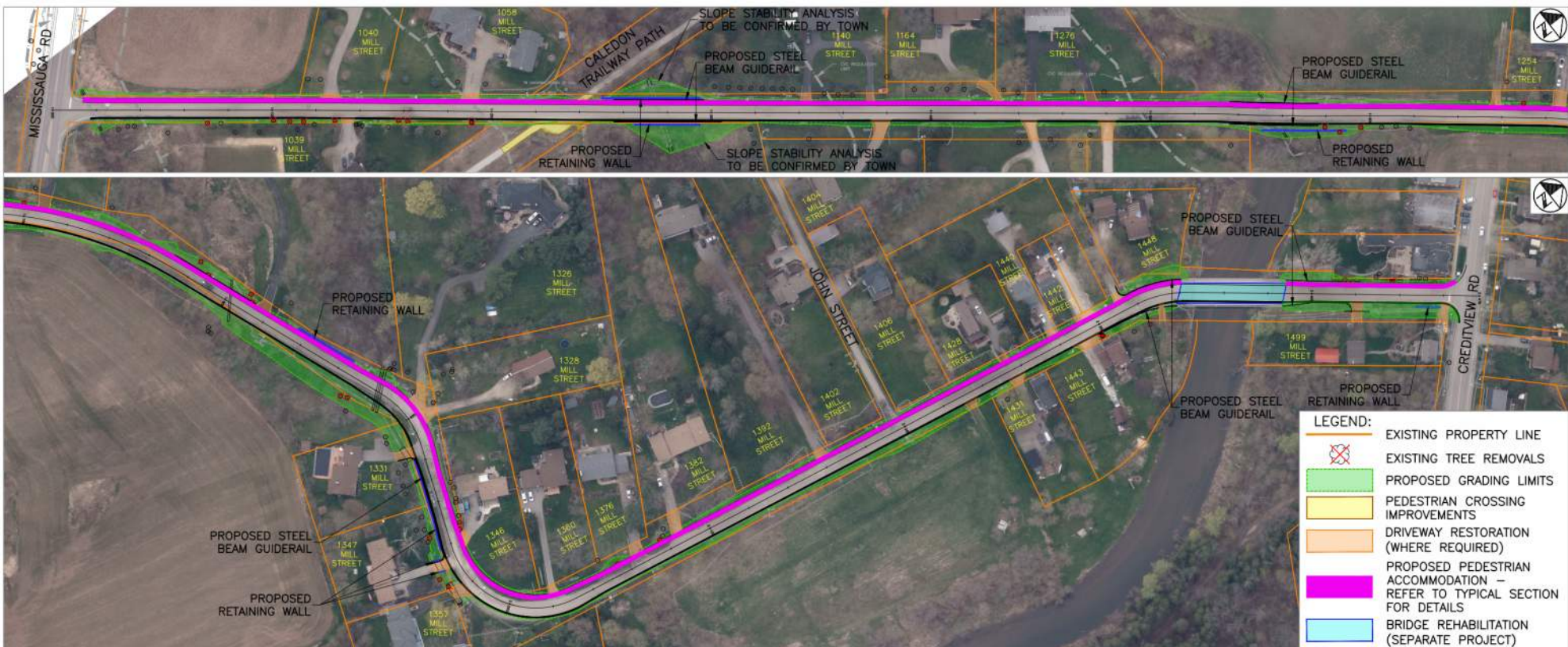
Project Team Contacts:

Town of Caledon

Shun Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure Engineering
Services
Tel: (905) 584-2272 x 4040
E-mail: shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P. Eng., PMP., M.Eng.
Project Manager, Transportation
Tel: (416) 497-8600 x1471
E-mail: winnie.wong@rvanderson.com



APPENDIX 2

COMMENTS RECEIVED

APPENDIX 2-1

Comments Received: General Public and Residents

Comments Received: Prior to PIC

Connor MacIsaac

From: [REDACTED]
Sent: June 14, 2022 2:52 PM
To: [REDACTED]
Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2
Attachments: IMG_5150.MOV

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Thank you [REDACTED]. We all want to retain the rural feel of this neighborhood. We bought here because we love it.

[REDACTED]
Sent from my iPhone

On Jun 14, 2022, at 2:42 PM, [REDACTED] wrote:

Seeing as I'm still unsure how this meeting will be on record, I will send this through here as most of the residents have been included.

I was in Prince Edward County this weekend and they have figured out how to keep the countryside, the countryside and yet still accommodate the hundreds of tourists that visit every year. The pictures attached so a road, VERY similar to Mill st in width, usage, hills and bends with blind spots etc and this road was the main road that takes pedestrians, cyclists, ebikes, cars, buses, limos, taxis etc to all the wineries and breweries that are scattered along the way. There was no asphalt (just chip & tar and NO pot holes!), no dedicated pedestrian/cyclist lane, no guide rails, just appropriate and beneficial signage, tall native grasses and hundreds of trees! As you can see, there are parts of our province that strive to maintain a rural landscape, but also provide safety to all who use the roads and visit the "countryside". If parts of Ontario can figure out a way to do this, why can't we be a little bit more inventive?





On Tue, Jun 14, 2022 at 1:01 PM [REDACTED] wrote:

Thank you for sharing the slides with us.

As far as the community feeling like they are being heard and that the Town and the engineers are eager to receive comments, we are very skeptical, as my email above was never answered and after briefly looking at the slides, it is clear that the Town of Caledon is choosing to urbanize our rural areas regardless of resident support.

See you all tomorrow.

[REDACTED]

On Tue, Jun 14, 2022 at 12:11 PM Winnie Wong <Winnie.Wong@rvanderson.com> wrote:

All,

The PIC slides are posted on the Town's website and can be accessed here:

<https://www.caledon.ca/en/news/mill-street-ea.aspx>

We look forward to meeting everyone tomorrow.

Regards,

Winnie Wong, P. Eng., PMP, M.Eng.

Associate, Project Manager

R.V. Anderson Associates Limited

From: [REDACTED]
Sent: June 8, 2022 2:31 PM
To: Shun Cheung <shun.cheung@caledon.ca>
Cc: [REDACTED]

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hello,

I didn't mean if someone wants to write something down. Obviously that would be recorded. I meant if someone has a verbal question. How is that being documented and recorded?

[REDACTED]

On Wed, Jun 8, 2022 at 2:13 PM Shun Cheung <Shun.Cheung@caledon.ca> wrote:

Hi [REDACTED]

Comment sheets will be available at the Public Information Centre (PIC) meeting and on the project website to submit your questions. It would be appreciated to submit any project related questions by July 6th.

All the questions and answers submitted from the comment sheets will be documented and filed in the Project File Report.

Shun Cheung, P.Eng., PMP

Project Manager, Capital Infrastructure

Engineering Services

Office: 905.584.2272 x.4040

Cell: 416.436.0910

Email: shun.cheung@caledon.ca

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From: [REDACTED]

Sent: Wednesday, June 08, 2022 1:55 PM

To: Andrew Pearce <Andrew.Pearce@caledon.ca>

Cc: [REDACTED]

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Hello,

Please advise on the suggestion and question below regarding how all the questions and answers, asked and answered personally to each resident at the open house, will be recorded and documented.

Thank you,

[REDACTED]

On Mon, Jun 6, 2022 at 5:41 PM [REDACTED] wrote:

Perhaps there could be a sit down question/answer/comment after the “open house” for those residents who prefer and believe the more formal format is much more appropriate.

On Mon, Jun 6, 2022 at 5:37 PM [REDACTED] wrote:

How will it be documented?

On Mon, Jun 6, 2022 at 5:36 PM Andrew Pearce <Andrew.Pearce@caledon.ca> wrote:

[REDACTED]

Very good question, thanks.

All information presented at the PIC including any questions and answers will be documented and included in the Environmental Study Report for the public record.

Thanks,

Andrew Pearce,

Director, Engineering Services

Engineering Services Department

Office: 905.584.2272, Ext 4003

Cell: 416.347.4881

Email: andrew.pearce@caledon.ca

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Please consider the environment before printing.

From: [REDACTED]
Sent: Monday, June 6, 2022 5:31 PM
To: Andrew Pearce <Andrew.Pearce@caledon.ca>
Cc: [REDACTED]

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Thank you for your email.

So will there be a presentation describing and explaining the rationale behind the proposals that will be on public record? Or will the presentations be displayed and people have to ask questions individually in order to get answers? If so, how is this being recorded then? The questions being asked and the answers being given?

[REDACTED]

On Mon, Jun 6, 2022 at 5:24 PM Andrew Pearce <Andrew.Pearce@caledon.ca> wrote:

Good afternoon [REDACTED]

Thank you for your inquiry regarding the upcoming public information centre on June 15. I look forward to meeting you there.

The project team has arranged an extra PIC for this road project for the purpose of illustrating to the residents how all the input to date has been addressed and to present the recommended road and drainage design. In addition, the project team is seeking feedback from the local residents on three new design options to accommodate pedestrians on the road. The three options include; 1. curb side concrete sidewalk, 2. paved shoulder, and 3. Grassed boulevard - no dedicated pedestrian facility.

The presentation slides are being finalized with the objective of posting them on the project website before the PIC. You will be notified by email when they are available. Although the slides will be available before the meeting, it's still important to attend the PIC to hear the description and rationale behind each of the design alternatives. I appreciate this is an extra PIC but we want to get it right.

In the interim, if you have any questions, please feel free to contact me.

Regards,

Andrew Pearce,

Director, Engineering Services

Engineering Services Department

Office: 905.584.2272, Ext 4003

Cell: 416.347.4881

Email: andrew.pearce@caledon.ca

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Please consider the environment before printing.

From: [REDACTED]
Sent: Thursday, June 2, 2022 10:35 AM
To: Christina Early <Christina.Early@caledon.ca>

Connor MacIsaac

From: [REDACTED]
Sent: June 3, 2022 9:29 AM
To: Winnie Wong
Cc: [REDACTED]
Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2
Categories: Resident Comments

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good morning Ms. Wong,
I read your email to [REDACTED] and Councillor Early. In response to your comment about the public meeting process, both the process and format of the past sessions were determined by the presenters, not the community or the “vocal participants”. As you may recall, the process was that community members were required to submit their questions online or “wave” online to make a request to speak. The questions that were, or were not answered by the panel were determined entirely by the panel. The opportunity for any individual to speak was also entirely at the discretion of the panel as they controlled both the format and the technology. As a result, there is really no way for the community to know if anyone was *unable to provide any feedback* as you state in your email below. Based on your comments, I have to wonder, do you consider a vocal participant someone that spoke once; someone that spoke more than once; or perhaps someone that just spoke passionately about their concerns? Really, your comment that the public participation was *dominated by a few vocal participants* is simply “unhelpful”.

Further to [REDACTED] comment about the format, is this format mandated by the process, or simply at the discretion of the presenters?

It is my sincere hope that the next iteration of plans for Mill St. demonstrates more creativity for the genuine betterment of the community, and not some slavish adherence to standards where the project is really nothing more than an exercise in sewers, sidewalks and setbacks. We all understand that changing various factors can change the applicable standards. For example, and if I understood the explanations provided in previous presentations, doing something as simple as changing the speed limit in a section of the road can change the applicable standards for the curve radius on the road. We also all understand that widening the road **will** increase the speed of traffic. Unless meaningful design measures are included to address traffic calming for a widened road, the net result of the project will be a degradation of the community environment resulting from speeding and increased traffic volume. Simply adding more concrete to the project will not improve Cheltenham. Unless the Town simply wants to urbanize the area without consideration for the community environment and its residents, in my view, more creativity is required from the planners than was demonstrated in the past plans.

I look forward to reviewing and commenting on the next set of plans for the Mill St. project.
Sincerely,

[REDACTED]

From: Winnie Wong [mailto:Winnie.Wong@rvanderson.com]

Sent: Thursday, June 02, 2022 5:23 PM

To: [REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

Councillor Early and [REDACTED],

The open house PIC format provides us with an one on one opportunity to talk to the participants. In our previous PIC experience a presentation format tends to result in the conversation dominated by a few vocal participants, while the others stay silent/ not able to provide any feedback.

The intent of this upcoming PIC is to gather feedback from all participants regarding the pedestrian accommodation alternatives on Mill Street. We want to listen to participants individually and answer questions they may have regarding the alternatives presented, hence we proposed the open house format.

Please let me know if you have other questions. I hope this helps.

Regards,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: [REDACTED]

Sent: June 2, 2022 10:35 AM

To: Christina Early <Christina.Early@caledon.ca>

Cc: [REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

Cc:

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Good morning,

Hoping you can respond today, Christina.

Thank you,



On Tue, May 31, 2022 at 11:45 AM Christina Early <Christina.Early@caledon.ca> wrote:

Hi everyone, I am currently out of town returning late this evening. I will respond tomorrow.

Thank you

Christina

Christina Early

Area Councillor, Ward 2

Office: 905.584.2272 x. 5130

Cell: 416.576.9366

Email: christina.early@caledon.ca

Town of Caledon | www.caledon.ca | www.visitcaledon.ca

From: [REDACTED]
Sent: Tuesday, May 31, 2022 11:23:01 AM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: [REDACTED]

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Thank you.

Christina, can you please step in here and explain to RV Anderson the importance of a formal meeting for your residents.

[REDACTED]

On Tue, May 31, 2022 at 10:46 AM Winnie Wong <Winnie.Wong@rvanderson.com> wrote:

Morning everyone,

At this time our team is working to pull together the presentation slides and if they are ready prior to the PIC, they will be posted on the project website and you will be notified by email when they are available. The intent of this PIC is to gather feedback from the public regarding options for pedestrian accommodation. No recommendation has been made. Our team will be at the open house to answer any questions attendees may have related on the presented options.

The PIC slides and comment sheet will be made available on the project website after the June 15. All attendees, including residents who could not attend in person will have the opportunity to review the material and submit their comments till July 6.

The resident's proposal was received last Fall and will be included in the Project File for this EA study, which will be available for public's review at the end of the study.

Regards,

Winnie Wong, P. Eng., PMP, M.Eng.

Associate, Project Manager

R.V. Anderson Associates Limited

From: [REDACTED]
Sent: May 30, 2022 10:19 PM
To: Connor MacIsaac <cmacisaac@rvanderson.com>; Andrew Pearce <andrew.pearce@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; John P. Does <jdoes@rvanderson.com>
Cc: [REDACTED]
Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Connor and All

Further to the concerns raised below, we are again asking that the proposal(s) that will be on display at the open house on June 15 be made available to our community and the residents in advance of the open house. This will allow all the residents who will be impacted by any alternative proposal a fair opportunity to review the latest

proposals and therefore attend the open house better informed. There has been no response yet to this request that was made last week.

Also, you have mentioned below that "all attendees" will be invited to provide their comments and preferences. If there are residents who cannot attend the open house that evening, what plans have been made for those residents to have their questions and comments heard?

Finally, can you please confirm that the residents' proposal that was submitted last fall (which also included questions, comments and a supporting letter from the Cheltenham Area Residents Association) is included as part of the public record for this project?

Thank you,

[REDACTED]

From: [REDACTED]
Sent: May 30, 2022 12:49 PM
To: Christina Early <Christina.Early@caledon.ca>; Connor MacIsaac <cmacisaac@rvanderson.com>

Cc: [REDACTED]

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

Hi Connor,

Considering how much feedback you have had from our community regarding the proposals, I find it infuriating that this is the chosen forum.

A few months ago, we came together as a community and signed a petition with a counter proposal, however this was never even acknowledged by either the Town or your engineering firm.

As I said before, as this project will solely affect the residents, I am asking our councillor, Christina Early, who has been included in this thread, to step in and amend the format of this "PIC".

Thank you,

[REDACTED]

On Mon, May 30, 2022 at 11:24 AM Connor MacIsaac <cmaclsaac@rvanderson.com> wrote:

[REDACTED]

Thank you for your continued input to this study. All attendees will be invited to provide their comments and preferences for the alternatives presented, after reviewing the display boards at their own pace. All of the comment forms will be documented in the Project File Report, in accordance with the *Municipal Class Environmental Assessment Act (October 2000, amended in 2007, 2011 & 2015)*.

Thank you and kind regards,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a [43 Church Street, Suite 104, St. Catharines, ON L2R 7E1](#)



rvanderson.com

From: [REDACTED]

Sent: May 30, 2022 11:08 AM

To: Connor MacIsaac <cmacIsaac@rvanderson.com>

Cc: [REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

[REDACTED]
Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Connor,

I'd like to know how this meeting is being recorded as public record since it is in an "open house" format.

As the small community that this project affects, we request a more formal meeting with the consultants and the town, that will record the questions asked and responses given.

Please advise.

Thank you,

[REDACTED]
[REDACTED]
[REDACTED]

On Mon, May 30, 2022 at 10:57 AM Connor MacIsaac <cmacIsaac@rvanderson.com> wrote:

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Public Information Centre (PIC) #2 for the **Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA)**. The purpose of this meeting is garner feedback on the options for pedestrian accommodations developed for Mill Street in consideration of the feedback received during and following PIC #1, held online October 5, 2021.

The PIC will be held as an informal “Open House” with materials pertaining to the study on display, with members of the project team on hand to answer questions and discuss issues related to the project. Attendees will be asked to review the materials on display and provide their preferences for pedestrian accommodations. A final recommendation for Mill Street will be developed following the PIC, in consideration of the input received.

The details of the PIC are below:

Date: Wednesday June 15, 2022
Time: 5:30 to 7:00 pm
Location: Cheltenham Community Centre - [14190 Creditview Rd, Caledon, ON L0P 1N0](#)

Entrance is located at the south side of the building

The project team values the participation of all stakeholders and wishes to ensure that the community’s interests and concerns are taken into consideration. Please contact the project team members listed below if you require a hard copy of the materials or for further information on this project.

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure Engineering Services
905.584.2272 X.4040
shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
416.497.8600 X.1471
winnie.wong@rvanderson.com

If you would like to be removed from the project mailing list, please let us know by responding to this email.

Summary of Comment Sheets Received

Reference Number	Reason for Interest in Study	Description of Other	What is your primary mode of travelling along Mill Street?		Approximately how often do you walk along Mill Street		Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?		Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 4 the least) and provide comments						Please provide any additional comments on your preferred option for pedestrian accommodation.	Please include other questions you have below
						If you answered "Rarely or Never", please identify why		If you answered "No", please provide your reason(s)	Existing with mountable curb	How do you feel this option accommodates pedestrians?	Sidewalk	How do you feel this option accommodates pedestrians?	Asphalt Boulevard	How do you feel this option accommodates pedestrians?		
1	A Mill Street Resident		Multiple Selections	Selected personal vehicle and walking	Daily		No	Have been walking since 1965	1		No rank (negative)		2			
2	A Mill Street Resident		Multiple Selections	Selected personal vehicle, bicycle and walking	Daily		No	I feel comfortable walking on the current road. Have never felt unsafe.	1	Accommodating pedestrians is not in my opinion necessary. The road should be created such that it maintains its rural character.	3		2		Curbs + asphalt + sidewalks create an urban appearance which I think detracts from the rural character of the village. Cheltenham has the most heritage structures in Caledon + this character should be preserved.	Why is a retaining wall needed on the ditch by 1499 Mill Street?
3	A Mill Street Resident		Multiple Selections	Selected personal vehicle and walking	Daily	"Several days / wk"	No	the road is better for walking dogs - very little traffic - can currently walk most of the way down the street + back without seeing any vehicles	2	If we must pick an option this is the least intolerable	No rank (negative)	Not a consideration	3		Option 1 for me is just fix the road + leave everything else as is. Most of us walk on the road, even in areas where there are sidewalks. Sidewalks are too hard on body when walking distances. Pls, pls, pls, do <u>not</u> widen the road	Why the push for sidewalks - if because there is money in the budget, why not upgrade the horrible sidewalks that were put on Creditview. Pls avoid making this road any kind of major thoroughway.
4	A Mill Street Resident		Multiple Selections	Selected personal vehicle, bicycle and walking	Daily		No	Having been a resident for ten years and walking daily I have never experienced any safety issues with vehicle traffic while walking.	1	This gives pedestrians an option to walk on the boulevard or the road whichever they feel is safest. It also gives the option for cyclist or pedestrians to move off the road if need be without an obstacle to mount or step over.	3	Poorly. During winter months snow from the road will be pushed up on to sidewalk. Then the sidewalk plow will in turn plow the side walk and return the snow back on to the road. This is happening on the new sidewalk that was installed on Creditview.	2	Poorly. If you think that this type of curb is going to stop a vehicle from leaving the paved surface and crossing the sidewalk there is no sense discussing this any further. This also is applicable to the sidewalk and curb option	The list of disadvantages is very inconsistent between options they appear to be just points used to lobby rather than truly looking at the disadvantages equally between options. Lighting - When looking at streetlighting enhancements will it be a separate street light system with proper illumination but not light pollution.	Because we own property on both sides of Mill Street there is an existing 2" duct structure under Mill Street from property on north side to property on south side. The integrity of these two ducts need to remain in tact and usable once the construction has been completed.
5	A Mill Street Resident		Personal Vehicle (Auto)		Daily	Daily but only very short	No	Know how to walk at side of road. This is not the city.	3	What pedestrians	3	Not needed. Not the City. This Town just got sidewalk that does not work. Built next to road when road plow comes sidewalk is gone when sidewalk plowed one half lane on road closes.	3	Not needed	Not wanted not needed. Curbs are not the answer and catch basins equal salt in river	How much money spent so far? Who comes up with these ideas? Why spend money on things residents don't want!!
6	A Mill Street Resident		Multiple Selections	Selected personal vehicle and walking	Daily	Many times a day	No	It is nice as is, quiet safe	3	No curb is better	No rank (negative)	ridiculous	No rank (positive)	Pedestrians are already happy. Added an option "as is" ranked as 1	I'd prefer repaving. - Trail very close to for bikes alternative - Please redo traffic study as 1st one was biased due to construction - We are very fearful that the quality of any changes to Mill Street will be similar to Creditview - sidewalk ends before community centre, looks poorly and is an eye sore.	Have there been pedestrian injuries? - Have there been car accidents? - Will the same people be doing the sidewalk install? - Upgrade sidewalks on Creditview since they are unfriendly and stop before Community Centre.
7	A Mill Street Resident		Personal Vehicle (Auto)		Daily		No	I walk daily from my lane to the trail entrance (approx. 200 feet). I use the Caledon Trailway to walk my dog. The trail is better for dogs to walk than a sidewalk.	No rank (negative)	I do not want an urban road for this neighbourhood. This is a rural setting and therefore the road should continue as a rural road. I do not want mountable curbs because these are used in urban areas, not rural areas. We live in the Greenbelt, adding more concrete to the green spaces on Mill St. seems contradictory to the purpose of a Greenbelt. This option would cause an exorbitant use of funding and would lead to property disruption for many Mill St residents, such as: mature tree removal, unsightly guardrails & excessive grading. I do not want an urban. Mountable Curb because it will increase the volume & speed of vehicular traffic on Mill St, thereby increasing noise pollution and reducing road safety and quality of rural living.	No rank (negative)	I do not want an urban road for this neighbourhood. This is a rural setting and therefore the road should continue as a rural road. I do not want a sidewalk on Mill St. This urban option would lead to an increase in other urban road features such as street lights, increased road signage. Increase in street lights-an increase in light pollution. We need to protect the night scape, for the sake of our residents and for the birds/animals that dwell here. Additional road signage is unsightly. There is an excessive use of road signs and these detract from the natural beauty of the properties on this street and the surrounding area. A sidewalk option would cause an exorbitant use of funding and would lead to property disruption for many Mill St residents, such as: mature tree removal, unsightly guardrails & excessive grading. I do not want a sidewalk because these are used in urban areas, not rural areas. We live in the Greenbelt, adding more concrete to the green spaces on Mill St. seems contradictory to the purpose of a Greenbelt. I do not want the extra pedestrian traffic from non-residents spilling from the Caledon trail to the roadway. More pedestrians = more noise, more garbage and pollution and more annoyance for the residents who live here.	No rank (negative)	I do not want an asphalt boulevard, because these are used in urban areas, not rural areas. We live in the Greenbelt, adding more asphalt to the green spaces on Mill St. seems contradictory to the purpose of a Greenbelt. I do not want an asphalt boulevard on Mill St. This urban option would lead to an increase in other urban road features such as street lights, increased road signage. Increase in street lights-an increase in light pollution. Asphalt boulevards are unsightly. What happens when the asphalt breaks down from the weather conditions, or is broken up due to snow removal? These asphalt boulevards would eventually become hazardous for pedestrians, much like the road is now in its current state.	I prefer to keep this a rural road. No urban features like mountable curbs, sidewalks or asphalt boulevards. I say no to street lights, additional road signage, and other urban road features. I say no to cutting down mature trees to make way for concrete walkways. I say no to disrupting this charming neighbourhood, a place admired by many for its rural features and charm.	
8	A Mill Street Resident		Multiple Selections	Selected personal vehicle and walking	Daily		No	We, however would feel more safe with traffic calming features for cars.	No rank (negative)	Does not offer safety for pedestrians and cyclists.	No rank (negative)	It urbanizes our country setting which is not what we want.	No rank (positive)	We like this option as it accommodates pedestrians and cyclists and supports their safety. It also is more in keeping with our rural setting. Traffic calming features would help with slowing traffic to ensure public safety.	Our primary concern is with the improvement of the road, traffic volume and speed will increase significantly. Safety measures need to be put in place for pedestrians and cyclists, especially traffic calming features. E.g. like they have in Terra Cotta and Belfountain Villages.	
9	Other	CARA - Treasurer	Multiple Selections	Bicycle, Walking	Daily				3	I feel like this is the same as what we currently have on Mill Street. Can we get rid of the guard rails? Would these be all the way along Mill Street?	1	I feel this is a safer option than what we currently have on Mill Street, looks nicer than the asphalt.	2	I feel this is a safer option for pedestrians.		Where would the guard rails be located? Really wouldn't look nice to have them all the way along.
10	A Mill Street Resident		Multiple Selections	Selected personal vehicle and walking	Daily		No	There is not enough traffic to warrant a sidewalk.	1	Maybe it wont but may stop the snow plow from destroying the lawn. Not really interested in any other options as they just create more issues.	3	Multi-use path should follow St. David Road south side to St David Street south side connecting at Front Street at the flag.	2		By the way the sidewalk snow plow is not a good option. On Creditview there is not enough room for the snow. It went on to the road last year and blocked one half of the live traffic lane.	
11	A Mill Street Resident		Walking		Daily		Yes		3	Need somewhere to walk this street	2	It could be good, but don't want to take up too much space	3	The road is in terrible condition and need asphalt.		
12	A Mill Street Resident		Personal Vehicle (Auto)		Weekly			I am indifferent usually on the shoulder off to the side.	3		1	Safest option for pedestrians	1	Enough to be off to the side of road and not walk in the middle of the road - cars can avoid.	I appreciate and value the safety respect. I also value the tranquil and rural aspect of Mill Street. I moved from the city with the deliberate intention to be on a quiet country street. Trees are sacred. I want to preserve the heritage of Cheltenham as reasonably as possible.	
13	A Mill Street Resident		Multiple Selections	Selected personal vehicle, bicycle and walking	Daily		No	Walk, ride daily as is.	No rank (negative)	Not in favor of this option.	No rank (negative)	Not in favor of this option.	No rank (negative)	Not in favor of this option.	I am not in favor of any options listed. All I want is for the road to be repaired to a safe condition, install proper guide railing near trail if road repair can not resolve the current issue. Have lived in Cheltenham for 36 years and purchased this property because my wife and I wanted a nice country home with a quiet rural setting. All of a sudden the Town of Caledon wants to urbanize our community. I am in total disagreement of any curbs, sidewalks, walkways or street lighting. I don't see any of this on other streets, (Kennedy Rd., Station Rd., Grange sideroad etc.). The sidewalk on Creditview Rd. looks like hell and really took away from the original rural setting. 95% of people using the road are local residents. Just Repair the Road !!!	I continue to hear about safety but it was not until this year that signing for pedestrians at the trail were installed. There has never been any Curve Warning / Speed tabs for the curve in the 26 years I have lived here. The guide rail at the trail is useless. The signing for Pedestrians crossing the trail at Old Base Line Rd. is a disaster waiting to happen, very rarely cars stop or make any attempt to stop. Very poor design.

Reference Number	Reason for Interest in Study	Description of Other	What is your primary mode of travelling along Mill Street?		Approximately how often do you walk along Mill Street		Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?		Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 4 the least) and provide comments						Please provide any additional comments on your preferred option for pedestrian accommodation.	Please include other questions you have below
						If you answered "Rarely or Never", please identify why		If you answered "No", please provide your reason(s)	Existing with mountable curb	How do you feel this option accommodates pedestrians?	Sidewalk	How do you feel this option accommodates pedestrians?	Asphalt Boulevard	How do you feel this option accommodates pedestrians?		
14	Other	Creditview Road resident	Walking		Weekly		No	I don't feel walking on Mill Street is an unsafe activity.	1	Any option that leaves the street in the most "natural" state would be, in my opinion, the most attractive solution to repairing Mill Street. Mountable curbs help prevent breakage at the edges of the asphalt which is the only reason I see them as acceptable inside this option.	3	This option would, without doubt, ruin Mill Street. The removal of the majestic 100 year old trees to Widen Mill Street flies in the face of Caledon's claim of being an eco-friendly place.	3	All I can think about this is why oh why was this not an option for Creditview Road.	I decided to address the guardrails, NOT REQUIRED!!! There hasn't been a need for guardrails on Mill Street in 200 years. There is no need for them now. Truly there should be a legislative exemption for the small villages in Caledon in order to allow them to be upgraded without the threat of them being bastardized beyond recognition.	I am a member of CABA. People may say "not my street, not my problem" but I disagree. Chipping away at the uniqueness of Cheltenham has already begun without input from the residents there will be more unpleasant changes. Like the abomination that now spans the west side of Creditview Road. I tried, to no avail, to convince the powers that be to install a asphalt curb instead of sidewalks (UBC has this all around its ring road). Thank you for listening hopefully this time...
15	Other	Creditview Road resident	Multiple Selections	Selected personal vehicle and walking	Monthly	I love walking around my neighbourhood but find it challenging and unsafe as there is no sidewalk and road is deteriorated.	Yes	Sidewalk would be beneficial and safer for everyone	3		1	I think it is time to make neighbourhood safe for pedestrians. Creditview Road just had sidewalk built. Great idea.	2	Cyclists tend to be out of (illegible) who come to the area to ride their bikes. I believe sidewalks for pedestrians who live in the area, is best solution.	Mill Street is a winding and hilly road and is unsafe for pedestrians, wheelchairs, walkers and strollers. I understand that current residents of Mill Street may be opposed to the encroachment on their property, however the safety of the community is paramount.	I believe sidewalks are the solution for a safer community. Now that Creditview has a sidewalk, my family walks on it more often as it is finally safe.
16	A Mill Street Resident		Personal Vehicle (Auto)		Weekly		No		No rank (positive)	I feel this is the best. Just a rural road repair upgrade.	No rank (negative)	Pedestrians walk on the road daily. Never any problems. Don't need to be like the suburbs. We are a small country community and wish to stay that way. No no no	No rank (negative)	no no no	Pedestrians can walk on the road. Never been a problem. Don't want the road fixed to make cars drive faster. People on Mill walk their dogs and kids and we all know speed kills.	Do the residents on Mill get to vote on final decision.
17	A Mill Street Resident	Address isn't Mill Street	Multiple Selections	Selected personal vehicle and walking	Daily		No	The traffic is generally slow, respectful and infrequent. There is already plenty of road width and adequate sightlines (other than the crest of the hill at the S-bend. The crest level could be adjusted without installing a specific pedestrian route) to be safe for pedestrians. A dedicated pedestrian route gives motorists a sense that they are entitled to drive faster...and they do, especially more so since the installment of sidewalk on Creditview Rd.	3	This does not accommodate pedestrians. A mountable curb restricts pedestrian entry and egress. Mountable curbs funnel water and given the current topography, the discharge will likely add to flooding which will restrict access as well as damage to property. There are no proposals to deal with flooding in the current study.	3	A sidewalk categorizes pedestrians. A sidewalk instructs pedestrians that they must only walk where designated and it projects that viewpoint on motorists it speeds up traffic which is detrimental to pedestrians and is antithetical to accommodating pedestrians. Sidewalks undermine the quiet rural nature of the community which makes it it less appealing to pedestrians, thereby failing to accommodate pedestrian. To summarize, a sidewalk requires road widening, which introduces a risk to the road that does not currently exist.	2	An asphalt blvd widens the road for motorists, which increase speeds. Increased speeds do not accommodate pedestrians This removes trees and significantly alters the streetscape. Urbanizing the streetscape does not accommodate pedestrians. The ranking does not reflect my view of these proposals in the community. I originally ranked them 3-3-2	The basic premise of this form and the presentation appears to be accommodating pedestrians. No one has asked, or stated, what accommodation means for this study. What it appears to mean from the proposals, is to get pedestrians off the road in order to accommodate motorists. This is NOT what Mill St. residents want. Given the quiet rural character of the road, I suggest that the Town and its consultants are asking the question incorrectly or at least placing the emphasis in the wrong place. Perhaps it is the behaviour of the motorists that the design needs to alter in favour of the pedestrians. Perhaps such an approach may also help in maintaining the character of the street and the sense of community. The work done over the years on Creditview is evidence of how to damage a rural village. Urbanizing everything is not the answer. Maintain a rural street environment.	The EA has not communicated a need for reconstruction/realignment based on quantifiable information and the analysis is based on flawed data. The comments form is poorly drafted with leading questions. The comment form is very poorly formatted, making it less user friendly and is not a saveable pdf. The less user-friendly, the less likely people are to complete it. It restricts community engagement. Ranking poor proposals against each other creates a false ranking of the proposal, generally. Again, this can create a misrepresentation that may be used to justify a particular proposal. A ranking of "1" does not mean it is something the community likes or WANTS.
17	A Mill Street Resident	Address isn't Mill Street	Multiple Selections	Selected personal vehicle and walking	Daily		No	The traffic is generally slow, respectful and infrequent. There is already plenty of road width and adequate sightlines (other than the crest of the hill at the S-bend. The crest level could be adjusted without installing a specific pedestrian route) to be safe for pedestrians. A dedicated pedestrian route gives motorists a sense that they are entitled to drive faster...and they do, especially more so since the installment of sidewalk on Creditview Rd.	3	This does not accommodate pedestrians. A mountable curb restricts pedestrian entry and egress. Mountable curbs funnel water and given the current topography, the discharge will likely add to flooding which will restrict access as well as damage to property. There are no proposals to deal with flooding in the current study.	3	A sidewalk categorizes pedestrians. A sidewalk instructs pedestrians that they must only walk where designated and it projects that viewpoint on motorists it speeds up traffic which is detrimental to pedestrians and is antithetical to accommodating pedestrians. Sidewalks undermine the quiet rural nature of the community which makes it it less appealing to pedestrians, thereby failing to accommodate pedestrian. To summarize, a sidewalk requires road widening, which introduces a risk to the road that does not currently exist.	2	An asphalt blvd widens the road for motorists, which increase speeds. Increased speeds do not accommodate pedestrians This removes trees and significantly alters the streetscape. Urbanizing the streetscape does not accommodate pedestrians. The ranking does not reflect my view of these proposals in the community. I originally ranked them 3-3-2	The basic premise of this form and the presentation appears to be accommodating pedestrians. No one has asked, or stated, what accommodation means for this study. What it appears to mean from the proposals, is to get pedestrians off the road in order to accommodate motorists. This is NOT what Mill St. residents want. Given the quiet rural character of the road, I suggest that the Town and its consultants are asking the question incorrectly or at least placing the emphasis in the wrong place. Perhaps it is the behaviour of the motorists that the design needs to alter in favour of the pedestrians. Perhaps such an approach may also help in maintaining the character of the street and the sense of community. The work done over the years on Creditview is evidence of how to damage a rural village. Urbanizing everything is not the answer. Maintain a rural street environment.	The EA has not communicated a need for reconstruction/realignment based on quantifiable information and the analysis is based on flawed data. The comments form is poorly drafted with leading questions. The comment form is very poorly formatted, making it less user friendly and is not a saveable pdf. The less user-friendly, the less likely people are to complete it. It restricts community engagement. Ranking poor proposals against each other creates a false ranking of the proposal, generally. Again, this can create a misrepresentation that may be used to justify a particular proposal. A ranking of "1" does not mean it is something the community likes or WANTS.
19	Other	Cheltenham Resident	Multiple Selections	Walking and Golf Cart	Rarely or Never	Because there isn't anywhere really to walk. We walk to the bridge and look at the river. My kids go to the park instead.	Yes		2	I think if we aren't going to put in a sidewalk then just do the simple one. I also don't think the road needs all the city things like road paintings and it definitely doesn't need guard rails and things like that. It will look terrible for Cheltenham.	1	I personally believe if we don't put the sidewalk in now it will never get done. We waited like 20 years to get a sidewalk on Creditview. I think it will make it safer for people to walk. I do understand the people that live on Mill streets concerns though don't live on Mill street but we might walk to more if there was somewhere to walk. But no need for all the extra road stuff like guard rails and city looking things.	3	I say either go for it with the sidewalk or just don't bother.	I am also noticing this is all just focussed on pedestrian accommodation. Why is that? Shouldn't it also be about road safety? About keeping the Cheltenham Charm? About saving as many trees and much more. Why is this comment sheet only addressing somewhere for pedestrians?	Will we be notified once a decision is made? With our Creditview sidewalk we weren't told of the final decision (that I am aware of) When will we find out this answer? I hope we can come to some agreement as a community because we really just need something done with the road. Thanks for all the extra time working with the community and providing extra meetings etc.
20	A Mill Street Resident		Multiple Selections	Selected personal vehicle, bicycle and walking	Daily		No	I walk the road at least everyday, sometimes twice...doubt any "walkway" would up that!	1	The question is biased. There are many accommodations requirements. If the road had traffic calming measures, lower than 40km/h posted speeds, narrowed lanes that don't invite cars to speed, then pedestrians would be very much accommodated to walk on the road edge and sod/ grass edge.	3	A concrete sidewalk is a no brainer for accommodating pedestrians - of course it's the option if this is the only consideration. But its not just pedestrians to accommodate.	2	Again, a paved shoulder is the same as a concrete sidewalk - so it's a no brainer accommodation for pedestrians, of course! So again the question is biased.	Again this is a biased "comment" request. To suggest that pedestrian accommodation is the only issue to comment on is again slanted, biased and rather insulting and diminishing to all the concerns and needs to accommodate. I believe pedestrians need better / safer accommodations, but is the cars that need less accommodations: narrow lanes, tighter turns, slower speeds, limited sight lines, traffic calming, etc etc - slow the cars down, and the pedestrian issue takes care of itself.	A.) List specific traffic calming measures that will be implemented. B.) Explain in detail why the posted speed limit can't be lower than 40km/h. C.) What is required to allow the Town to drop this minimum 2 lane width mantra? D.) Can someone give detailed cross section dimension of the "S" bend retaining wall and guard rail?

Copy of Comments Received

Winnie Wong

From: Shun Cheung <Shun.Cheung@caledon.ca>
Sent: July 5, 2022 8:09 AM
To: [REDACTED]
Cc: Winnie Wong; Connor MacIsaac
Subject: Re: Public Information Centre #2 Comment Form

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Thank you [REDACTED] for your comments.

Get [Outlook for iOS](#)

From: [REDACTED]
Sent: Tuesday, July 5, 2022 7:35:37 AM
To: Shun Cheung <Shun.Cheung@caledon.ca>; Christina Early <Christina.Early@caledon.ca>
Subject: Public Information Centre #2 Comment Form

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Please consider this email as a formal response to the comment form. I found the questionnaire misleading and biased towards option 3 and therefore I am not responding to it .

[REDACTED]
[REDACTED]
[REDACTED]

"This message (and any associated files) is intended only for the use of the individual or entity to which it is addressed. The content of the message is the property of the Corporation of the Town of Caledon. The message may contain information that is privileged, confidential, subject to copyright and exempt from disclosure under applicable law. If the reader of this message is not the intended recipient, you are notified that any dissemination, distribution, copying, or modification of this message is strictly prohibited. If you have received this message in error, please notify the sender immediately, advising of the error and delete this message without making a copy. (Information related to this email is automatically monitored and recorded and the content may be required to be disclosed by the Town to a third party in certain circumstances). Thank you."

Winnie Wong

From: [REDACTED]
Sent: June 17, 2022 11:25 AM
To: Shun Cheung
Cc: Winnie Wong; Connor MacIsaac
Subject: RE: Mill Street, Cheltenham

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Thank you for forwarding my email . I appreciate it.

Everyone should have equal and safe access to Mill Street which is a public (not private) road.

From: Shun Cheung <Shun.Cheung@caledon.ca>
Sent: Friday, June 17, 2022 11:19 AM
To: [REDACTED]
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>; Connor MacIsaac <cmaclsaac@rvanderson.com>
Subject: RE: Mill Street, Cheltenham

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Thanks [REDACTED] The Town is appreciated for your input.

Shun Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure
Engineering Services
Office: 905.584.2272 x.4040
Cell: 416.436.0910
Email: shun.cheung@caledon.ca

Town of Caledon | www.caledon.ca | www.visitcaledon.ca | Follow us @YourCaledon

From: [REDACTED]
Sent: Friday, June 17, 2022 11:17 AM
To: Shun Cheung <Shun.Cheung@caledon.ca>
Subject: re: Mill Street, Cheltenham

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Dear Shun

I attended the meeting on Wednesday June 15, 2022 with regards to the upgrade to Mill Street. I really am confused as why the residents of Mill Street are trying to prevent any changes to the street. I believe that installing a sidewalk is a prudent idea. Having a sidewalk would make it safer and more accessible for wheel chairs, strollers, walkers and to accommodate those with mobility issues. The addition of the sidewalk on Creditview Rd in Cheltenham has made the street safer for all pedestrians. The addition of sidewalk on Mill Street will do the same. Walking on the street should not even be the only option.

During the meeting, many residence expressed their distain for the idea of having a sidewalk or asphalt boulevard. These residence do not own the street and have sense of entitlement as they do not want non residence to use the street. They were complaining that vehicular traffic will increase, cycles will use the road and people will walk in the neighbourhood if a sidewalk would be added. The more I listened the more upset I was becoming at their belief that the street belong to them and no one should have access to it. Mill Street is a public street and everyone should have safe access to the street. It is not just for the residence of Mill street.

Thanks for listening to my concerns.

[REDACTED]

[REDACTED]

[REDACTED]

[REDACTED]

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MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

PUBLIC INFORMATION CENTRE (PIC) #2 COMMENT FORM

The Town is presenting three options for the north side of Mill Street. Option 1 is existing with mountable curb, Option 2 is dedicated sidewalk with barrier curb, and Option 3 is asphalt boulevard with mountable curb. All three options allow for grass boulevard and mountable curb on the south side of Mill Street. Please provide your input on the options presented by completing this comment form.

NAME (REQUIRED): _____

MAILING ADDRESS (REQUIRED): _____

EMAIL (OPTIONAL): _____

☐ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

- ☒ A Mill Street Resident
☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

- ☒ Personal Vehicle (Auto)
☐ Bicycle
☒ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

HAVE BEEN WALKING SINCE 1965

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 1
How do you feel this option accommodates pedestrians?	

Sidewalk	Rank:
How do you feel this option accommodates pedestrians?	

Asphalt Boulevard	Rank: 2
How do you feel this option accommodates pedestrians?	

Please provide any additional comments on your preferred option for pedestrian accommodation.

6. Please include other questions you have below:

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

With the exception of personal information, all comments will become part of the public record of study. The study is being conducted according to the requirements of the Municipal Class Environmental Assessment, which is a planning process approved under Ontario's Environmental Assessment Act.

THANK YOU FOR YOUR TIME AND EFFORT!

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MAILING ADDRESS (REQUIRED): _____

EMAIL (OPTIONAL): _____

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1. You are:

☒ A Mill Street Resident

☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

☒ Personal Vehicle (Auto)

☒ Bicycle

☒ Walking

☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

☒ Daily

☐ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

1. You are:

- ☒ A Mill Street Resident
☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

- ☒ Personal Vehicle (Auto)
☒ Bicycle
☒ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

I feel comfortable walking on the
current road. Have never felt
unsafe.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 1
How do you feel this option accommodates pedestrians?	
Accommodating pedestrians is not in my opinion necessary. The road should be created such that it maintains its rural character.	

Sidewalk	Rank: 3
How do you feel this option accommodates pedestrians?	

Asphalt Boulevard	Rank: 2
How do you feel this option accommodates pedestrians?	

That it maintains its rural character.

Sidewalk

Rank: 3

How do you feel this option accommodates pedestrians?

Asphalt Boulevard

Rank: 2

How do you feel this option accommodates pedestrians?

Please provide any additional comments on your preferred option for pedestrian accommodation.

Curbs + asphalt + sidewalks
create an urban appearance
which I think detracts from
the village rural character of
the village. Cheltenham has the
most heritage structures in
Caledon + this character should
be preserved.

6. Please include other questions you have below:

Why is a retaining wall needed on
the ditch by 1499 Mill Street?

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

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MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

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NAME (REQUIRED): _____

MAILING ADDRESS (REQUIRED): _____

EMAIL (OPTIONAL): _____

☐ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

- ☒ A Mill Street Resident
☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

- ☒ Personal Vehicle (Auto)
☐ Bicycle
☒ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☒ Weekly *several days / wk.*
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

the road is better for walking dogs - very little traffic - can currently walk most of the way down the street & back without seeing any vehicles

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 2
How do you feel this option accommodates pedestrians?	
if we must pick an option	
this is the least intolerable	

Sidewalk	Rank: X
How do you feel this option accommodates pedestrians?	
not a consideration	

Asphalt Boulevard	Rank: 3
How do you feel this option accommodates pedestrians?	

option 1 for me is just fix the road & leave everything else as is

Please provide any additional comments on your preferred option for pedestrian accommodation.

most of us walk on the road, even in
areas where there are sidewalks.
Sidewalks are too hard on body when
walking distance.

Pls, pls, pls, do not widen the road

6. Please include other questions you have below:

- why the push for sidewalks - if because
there is money in the budget, why not upgrade
the horrible sidewalks that were put on
Creditview.

Pls avoid making this road any kind
of major throughway.

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EMAIL (OPTIONAL): _____

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☐ Other (Please Identify - _____)

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- ☒ Personal Vehicle (Auto)
☒ Bicycle
☒ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

HAVING BEEN A RESIDENT FOR TEN YEARS
AND WALKING DAILY I HAVE NEVER
EXPERIENCED ANY SAFETY ISSUES
WITH VEHICLE TRAFFIC WHILE
WALKING. ~~BE~~

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 1
How do you feel this option accommodates pedestrians?	
THIS GIVES PEDESTRIANS AN OPTION TO WALK ON THE BOULEVARD OR THE ROAD WHICHEVER THEY FEEL IS SAFEST. IT ALSO GIVES THE OPTION FOR CYCLIST OR PEDESTRIANS TO MOVE OFF THE ROAD IF NEED BE WITHOUT AN OBSTACLE TO MOUNT OR STEP OVER.	

Sidewalk	Rank: 3
How do you feel this option accommodates pedestrians?	
POORLY. DURING WINTER MONTHS SNOW FROM THE ROAD WILL BE PUSHED UP ON TO SIDEWALK. THEN THE SIDEWALK PLOW WILL IN TURN PLOW THE SIDE WALK AND RETURN THE SNOW BACK ON TO THE ROAD.	
* THIS IS NOW HAPPENING ON THE NEW SIDEWALK THAT WAS INSTALLED ON CREDIT VIEW.	

Asphalt Boulevard	Rank: 2
How do you feel this option accommodates pedestrians?	
POORLY. IF YOU THINK THAT THIS TYPE OF CURB IS NOT GOING TO STOP A VEHICLE FROM LEAVING THE PAVED SURFACE AND ^{CROSSING} ON TO THE SIDEWALK THERE IS NO SENSE DISCUSSING THIS ANY FURTHER.	
* THIS ALSO IS APPLICABLE TO	

THE SIDEWALK AND CURB OPTION.

Please provide any additional comments on your preferred option for pedestrian accommodation.

THE LIST OF DISADVANTAGES IS VERY
INCONSISTANT BETWEEN OPTIONS
THEY SEEM TO BE JUST POINTS
USED TO LOBBY RATHER THAN
TRULY LOOKING AT THE DISADVANTAGES
EQUALLY BETWEEN OPTIONS.

LIGHTING - WHEN LOOKING AT STREET LIGHT
ENHANCEMENTS WILL IT BE A SEPARATE
STREET LIGHT SYSTEM WITH PROPER
ILLUMINATION BUT NOT LIGHT POLLUTION

6. Please include other questions you have below:

BECAUSE WE OWN PROPERTY ON BOTH
SIDES OF MILL ST. THERE IS AN EXISTING
2 DUCT STRUCTURE UNDER MILL ST,
FROM PROPERTY ON NORTH SIDE TO
PROPERTY ON SOUTH SIDE. THE INTEGRITY
OF THESE TWO DUCTS NEED TO
REMAIN IN TACT AND USABLE
ONCE THE CONSTRUCTION HAS BEEN
COMPLETED.

Please provide your comments by July 6th, 2022 to:

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By Mail: Shun Cheung
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NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

☒ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

☒ A Mill Street Resident

☐ Other (Please Identify -

2. What is your primary mode of travelling along Mill Street?

☒ Personal Vehicle (Auto)

☐ Bicycle

☐ Walking

☐ Other (Please Identify -

3. Approximately how often do you walk along Mill Street?

☒ Daily

☐ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

Daily but only very short

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

If you answered "No", please provide your reason(s).

Know how to walk at side of Road.
This is NOT the City

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 3
How do you feel this option accommodates pedestrians?	
?	
What Pedestrians	

Sidewalk	Rank: 3
How do you feel this option accommodates pedestrians?	
Not Needed	
Not The City	
This Town Just got Sidewalk That	
Does Not work Built Next To Rd when	
Road Plow Comes Sidewalk is gone when	
Sidewalk plowed one half Lane on Road closes	

Asphalt Boulevard	Rank: 3
How do you feel this option accommodates pedestrians?	
Not Needed	

Please provide any additional comments on your preferred option for pedestrian accommodation.

Not Wanted Not Needed
Curbs Are Not The Answer.
" AND CATCH BASINS EQUAL SALT IN RIVER

6. Please include other questions you have below:

How Much Money Spent So Far?
Who Comes Up With These Ideas?
Why Spend money ON Things Residents
DON'T WANT!!

Why Not Leave Road width? Just Repair!

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MISSISSAUGA ROAD TO CREDITVIEW ROAD

PUBLIC INFORMATION CENTRE (PIC) #2 COMMENT FORM

The Town is presenting three options for the north side of Mill Street. Option 1 is existing with mountable curb, Option 2 is dedicated sidewalk with barrier curb, and Option 3 is asphalt boulevard with mountable curb. All three options allow for grass boulevard and mountable curb on the south side of Mill Street. Please provide your input on the options presented by completing this comment form.

NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

☐ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

☒ A Mill Street Resident

☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

☒ Personal Vehicle (Auto)

☐ Bicycle

☒ Walking

☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

☒ Daily

Many times a day

☐ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☒ Yes

☐ No

If you answered "No", please provide your reason(s).

- it is nice as is , quiet safe .

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: NA
How do you feel this option accommodates pedestrians?	
no curb is better	

3

Sidewalk	Rank: No
How do you feel this option accommodates pedestrians?	
rediculous	

Asphalt Boulevard	Rank: No
How do you feel this option accommodates pedestrians?	
pedestrians are already happy	
As is. please	Rank

1

Please provide any additional comments on your preferred option for pedestrian accommodation.

- I'd prefer repaving
- Trail very close to pikes alternative
- Please redo traffic study as 1st one was biased due to construction.
- we are very fearful that the quality of any changes to Mill St will be similar to Credit View - side walk ends before Community Centre, looks poorly and is an eye sore.

6. Please include other questions you have below:

- Have there been pedestrian injuries?
- Have there been car accidents?
- Will the same people be doing the side walk install?
- up grade side walks on Credit View since they are unfriendly and stop before Community Centre!

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

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☐ Bicycle
☐ Walking
☐ Other (Please Identify -)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

I walk daily from my lane to the trail entrance (approx 200 feet). I use the Caledon Trailway to walk my dog.

The trail is better for dogs to walk than a sidewalk.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank:
How do you feel this option accommodates pedestrians?	
I do not want an urban road for this neighbourhood. This is a rural setting and therefore the road should continue as a rural road.	
I do not want mountable curbs because these are used in urban areas, not rural areas. We live in the Greenbelt, adding more concrete to the green spaces on Mill St. seems contradictory to the purpose of a Greenbelt.	
This option would cause an exorbitant use of funding and would lead to property disruption for many Mill St residents, such as; mature tree removal, unsightly guardrails & excessive grading.	
I do not want an urban Mountable Curb because it will increase the volume & speed of vehicular traffic on Mill St, thereby increasing noise pollution and reducing road safety and quality of rural living.	

Sidewalk	Rank:
How do you feel this option accommodates pedestrians?	
I do not want an urban road for this neighbourhood. This is a rural setting and therefore the road should continue as a rural road.	
I do not want a sidewalk on Mill St. This urban option would lead to an increase in other urban road features such as street lights, increased road signage. Increase in street lights=an increase in light pollution.	
We need to protect the night scape, for the sake of our residents and for the birds/animals that dwell here.	
Additional road signage is unsightly. There is an excessive use of road signs and these detract from the natural beauty of the properties on this street and the surrounding area.	
A sidewalk option would cause an exorbitant use of funding and would lead to property disruption for many Mill St residents, such as; mature tree removal, unsightly guardrails & excessive grading.	
I do not want a sidewalk because these are used in urban areas, not rural areas. We live in the Greenbelt, adding more concrete to the green spaces on Mill St. seems contradictory to the purpose of a Greenbelt.	
I do not want the extra pedestrian traffic from non-residents spilling from the Caledon trail to the roadway. More pedestrians = more noise, more garbage and pollution and more annoyance for the residents who live here.	

Asphalt Boulevard	Rank:
How do you feel this option accommodates pedestrians?	
I do not want a an asphalt boulevard, because these are used in urban areas, not rural areas. We live in the Greenbelt, adding more asphalt to the green spaces on Mill St. seems contradictory to the purpose of a Greenbelt.	
I do not want an asphalt boulevard on Mill St. This urban option would lead to an increase in other urban road features such as street lights, increased road signage. Increase in street lights=an increase in light pollution.	
Asphalt boulevards are unsightly. What happens when the asphalt breaks down from the weather conditions, or is broken up due to snow removal?	
These asphalt boulevards would eventually become hazardous for pedestrians, much like the road is now in its current state.	

Please provide any additional comments on your preferred option for pedestrian accommodation.

I prefer to keep this a rural road. No urban features like mountable curbs, sidewalks or asphalt boulevards.

I say no to street lights, additional road signage, and other urban road features. I say no to cutting down mature trees to make way for concrete walkways.

I say no to disrupting this charming neighbourhood, a place admired by many for its rural features and charm.

6. Please include other questions you have below:

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MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

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MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

☒ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

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☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

☒ Personal Vehicle (Auto)

☐ Bicycle

☒ Walking

☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

☒ Daily

☐ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

If you answered "No", please provide your reason(s).

We, however would feel more
safe with traffic
calming features for cars.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank:
How do you feel this option accommodates pedestrians?	
Does not offer	
safety for pedestrians	
and cyclists.	

Sidewalk	Rank:
How do you feel this option accommodates pedestrians?	
It urbanizes our country	
setting which is not	
what we want.	

Asphalt Boulevard	Rank:
How do you feel this option accommodates pedestrians?	
We like this option as	
it accommodates pedestrians	
and cyclists and supports	
their safety.	
It also is more in	
keeping with our rural setting.	
Traffic calming features	
would help with slowing	
traffic to ensure public	
safety.	

*

Please provide any additional comments on your preferred option for pedestrian accommodation.

Our primary concern is with the improvement of the road, traffic ~~to~~ volume and speed will increase significantly. Safety measures need to be put in place for pedestrians and cyclists especially traffic calming features e.g. like they have in Terra Cotta and Belfountain Villages.

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EMAIL (OPTIONAL):

☐ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

☐ A Mill Street Resident

☒ Other (Please Identify -

CARA - Treasurer

2. What is your primary mode of travelling along Mill Street?

☐ Personal Vehicle (Auto)

☒ Bicycle

☒ Walking

☐ Other (Please Identify -

3. Approximately how often do you walk along Mill Street?

☒ Daily

☐ Weekly

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☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☐ No

If you answered "No", please provide your reason(s).

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 3
How do you feel this option accommodates pedestrians?	
I feel like this is the same as what we currently have on mill st. Can we get rid of the Guard rails? would these be all the way along mill st?	

Sidewalk	Rank: 1
How do you feel this option accommodates pedestrians?	
I feel this is a safer option than what we currently have on mill st. looks nicer than the asphalt.	

Asphalt Boulevard	Rank: 2
How do you feel this option accommodates pedestrians?	
I feel this is a safer option for pedestrians.	

Please provide any additional comments on your preferred option for pedestrian accommodation.

6. Please include other questions you have below:

Where would the guard rails
be located? Really would
look nice to have them all
the way along.

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☐ Other (Please Identify -)

2. What is your primary mode of travelling along Mill Street?

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☐ Bicycle
☒ Walking
☐ Other (Please Identify -)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

There is not enough traffic to warrant a sidewalk.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 1
How do you feel this option accommodates pedestrians?	
Maybe it wont but may stop the snow plow from destroying the lawn.	
Not really interested in any other options as they just create more issues.	

Sidewalk	Rank:3
How do you feel this option accommodates pedestrians?	

Asphalt Boulevard	Rank:2
How do you feel this option accommodates pedestrians?	



By the way.... the sidewalk snow plow is not a good option. On Creditview there is not enough room for the snow. It went on to the road last year and blocked one half of the live traffic lane.

[illegible]

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EMAIL (OPTIONAL): _____

☐ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

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☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

- ☐ Personal Vehicle (Auto)
☐ Bicycle
☒ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☒ Yes
☐ No

If you answered "No", please provide your reason(s).

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 3
How do you feel this option accommodates pedestrians?	
need somewhere to walk this street	

Sidewalk	Rank: 2
How do you feel this option accommodates pedestrians?	
It could be good, but don't want to take up much space	

Asphalt Boulevard	Rank: 3
How do you feel this option accommodates pedestrians?	
The road is in terrible condition and need asphalt	

Please provide any additional comments on your preferred option for pedestrian accommodation.

6. Please include other questions you have below:

Please provide your comments by July 6th, 2022 to:

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EMAIL (OPTIONAL): _____

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☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

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☐ Bicycle
☐ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☐ Daily
☒ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☐ No

I am indifferent usually on the shoulder off to the side.

If you answered "No", please provide your reason(s).

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: <u>3</u>
How do you feel this option accommodates pedestrians?	

Sidewalk	Rank: <u>1</u>
How do you feel this option accommodates pedestrians?	
safest option for pedestrians	

Asphalt Boulevard	Rank: <u>1</u>
How do you feel this option accommodates pedestrians?	
Enough to be off to the side ^{of road} , and not walk in the middle of the road. - cars can avoid.	

Please provide any additional comments on your preferred option for pedestrian accommodation.

I appreciate and value the safety aspect.
I also value the tranquil and rural
aspect of Mill Street. I moved from
the city w the deliberate intention to
be on a quiet ~~ex~~ - country street. ->
Trees are saved. - I want to preserve
the heritage of Cheltenham as reasonably
possible.

6. Please include other questions you have below:

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☒ Bicycle
☒ Walking
☐ Other (Please Identify -)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

Walk, ride daily as is.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank:
How do you feel this option accommodates pedestrians?	
Not in favor of this option.	

Sidewalk	Rank:
How do you feel this option accommodates pedestrians?	
Not in favor of this option.	

Asphalt Boulevard	Rank:
How do you feel this option accommodates pedestrians?	
Not in favor of this option.	

Please provide any additional comments on your preferred option for pedestrian accommodation. I am not in favor of any options listed. All i want is for the road to be repaired to a safe condition, install proper guide railing near trail if road repair can not resolve the current issue.

Have lived in Cheltenham for 36 years and purchased this property because my wife and i wanted a nice country home with a quiet rural setting. All of a sudden the Town of Caledon wants to urbanize our community. I am in total diagrement of any curbs, sidewalks, walkways or street lighting. I don't see any of this on other streets, (Kennedy Rd., Station Rd., Grange sideroad etc.). The sidewalk on Creditview Rd. looks like hell and really took away from the original rural setting. 95% of people using the road are local residents. Just Repair the Road !!!

6. Please include other questions you have below:

I continue to hear about safety but it was not until this year that signing for pedestrians at the trail were installed. There has never been any Curve Warning / Speed tabs for the curve in the 36 years i have lived here. The guide rail at the trail is useless.

The signing for Pedestrians crossing the trail at Old Base Line Rd. is a disaster waiting to happen, very rarely cars stop or make any attempt to stop. Very poor design.

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NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

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1. You are:

☐ A Mill Street Resident

☒ Other (Please Identify - My residence is on Creditview Rd.)

2. What is your primary mode of travelling along Mill Street?

☐ Personal Vehicle (Auto)

☐ Bicycle

☒ Walking

☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

☐ Daily

☒ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

If you answered "No", please provide your reason(s).

I don't feel walking
on Mill Street is an unsafe activity.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: ONE
How do you feel this option accommodates pedestrians?	
<p>ANY option that leaves the street in the most "natural" state would be, in my opinion, the most attractive solution to repairing Mill St. Mountable curbs help prevent breakage at the edges of the asphalt which is the only reason I see them as acceptable inside this option.</p>	

Sidewalk	Rank: 3
How do you feel this option accommodates pedestrians?	
<p>This option would, without doubt, ruin Mill St. The removal of the majestic 100-year old trees to widen Mill St flies in the face of Caledon's claim of being a eco-friendly place.</p>	

Asphalt Boulevard	Rank: 3
How do you feel this option accommodates pedestrians?	
<p>All I can think about this is why oh why was this not an option for Creditview Rd!!!</p>	

Please provide any additional comments on your preferred option for pedestrian accommodation.

~~n/a~~ I decided to address the guardrails. NOT REQUIRED!!!! There hasn't been a need for guardrails on Mill Street in 200 years. There is no need for them now.

Truly there should be a legislative exemption for the small villages in Caledon in order to allow them to be upgraded without the threat of them being bastardized beyond recognition.

6. Please include other questions you have below:

I am a member of ~~the~~ C.A.R.A. People may say "not my street, not my problem" but I disagree. Chipping away at the uniqueness of Cheltenham has already begun. Without input from the residents there will be more unpleasant changes. Like the abomination that now spans the west side of Creditview Rd. I tried, to no avail, to convince the powers that be to install a asphalt curb instead of sidewalks (UBC has this all around its ringrd). Thank you for listening. hopefully this time....

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MAILING ADDRESS (REQUIRED): _____

EMAIL (OPTIONAL): _____

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2. What is your primary mode of travelling along Mill Street?

☒ Personal Vehicle (Auto)

☐ Bicycle

☒ Walking

☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

☐ Daily

☐ Weekly

☒ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

I love walking around my neighbour
but find it challenging and unsafe
as there is no sidewalk and
road is deteriorated

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☒ Yes

☐ No

If you answered "No", please provide your reason(s).

Sidewalk would be beneficial
and safer for everyone.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 3
How do you feel this option accommodates pedestrians?	

Sidewalk	Rank: 1 Best
How do you feel this option accommodates pedestrians?	
I think it is time to make neighbourhood safe for pedestrians - credit view Rd just had side walk built - great idea.	

Asphalt Boulevard	Rank: 2
How do you feel this option accommodates pedestrians?	
Cyclists tend to be out of towners who come to the area to ride their bikes - I believe believe Sidewalks for pedestrians who live in the area, is best solution	

Please provide any additional comments on your preferred option for pedestrian accommodation.

Mill Street is a winding and hilly road and is unsafe for pedestrians. Side walk will make it safer for pedestrians, wheelchairs, walkers and strollers.

I understand that current residents of mill street may be opposed to the encroachment on their property. However the safety of the community is paramount.

6. Please include other questions you have below:

I believe side walks are the solution for a safer community. Now that Creditview has a side walk, my family walks on it more often as it is finally safe.

Tara M. M. M.

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

With the exception of personal information, all comments will become part of the public record of study. The study is being conducted according to the requirements of the Municipal Class Environmental Assessment, which is a planning process approved under Ontario's Environmental Assessment Act.

THANK YOU FOR YOUR TIME AND EFFORT!

**MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD**

PUBLIC INFORMATION CENTRE (PIC) #2 COMMENT FORM

The Town is presenting three options for the north side of Mill Street. Option 1 is existing with mountable curb, Option 2 is dedicated sidewalk with barrier curb, and Option 3 is asphalt boulevard with mountable curb. All three options allow for grass boulevard and mountable curb on the south side of Mill Street. Please provide your input on the options presented by completing this comment form.

NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

☐ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

☒ A Mill Street Resident

☐ Other (Please Identify -

2. What is your primary mode of travelling along Mill Street?

☒ Personal Vehicle (Auto)

☐ Bicycle

☐ Walking

☐ Other (Please Identify -

3. Approximately how often do you walk along Mill Street?

☐ Daily

☒ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

If you answered "No", please provide your reason(s).

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank:
How do you feel this option accommodates pedestrians?	
I feel this is the best. Just a rural road repair upgrade.	

Sidewalk	Rank:
How do you feel this option accommodates pedestrians?	
Pedestrians walk on the road daily. Never any problems. Don't need to be like the suburbs. We are a small country community and wish to stay that way.	
No No No.	

Asphalt Boulevard	Rank:
How do you feel this option accommodates pedestrians?	
No No No	

Please provide any additional comments on your preferred option for pedestrian accommodation.

Pedestrians can walk on the road.
Never been a problem. Don't want the
road fixed to make cars drive faster.
People on mill walk their dogs and
kids and we all know. speed kills.

6. Please include other questions you have below:

Do the residence on mill get to vote on
final decision?

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
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MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

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NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

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- ☒ A Mill Street Resident
☐ Other (Please Identify -)

2. What is your primary mode of travelling along Mill Street?

- ☐ Personal Vehicle (Auto)
☒ Bicycle
☒ Walking
☐ Other (Please Identify -)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

The traffic is generally slow, respectful and infrequent. There is already plenty of road width and adequate sightlines

(other than the crest of the hill at the S-bend. The crest level could be adjusted without installing a specific pedestrian route) to be safe for pedestrians. A dedicated

pedestrian route gives motorists a sense that they are entitled to drive faster....and they do, especially more so since the installment of sidewalk on Creditview Rd.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank:2
How do you feel this option accommodates pedestrians?	
This does not accommodate pedestrians. A mountable curb restricts pedestrian entry and egress.	
Mountable curbs funnel water and given the current	
topography, the discharge will likely add to flooding which will	
restrict access as well as damage to property.	
There are no proposals to deal with flooding in the current study.	

Sidewalk	Rank:3
How do you feel this option accommodates pedestrians?	
A sidewalk categorizes pedestrians. A sidewalk instructs pedestrians that they	
must only walk where designated and it projects that viewpoint on motorists	
It speeds up traffic which is detrimental to pedestrians and is antithetical to	
accommodating pedestrians	
Sidewalks undermine the quiet rural nature of the community	
which makes it it less appealing to pedestrians,	
thereby failing to accommodate pedestrian.	
To summarize, a sidewalk requires road widening, which introduces a risk to the road that does not currently exist.	

Asphalt Boulevard	Rank: 1
How do you feel this option accommodates pedestrians?	
An asphalt blvd widens the road for motorists, which increase speeds.	
Increased speeds do not accommodate pedestrians	
This removes trees and significantly alters the streetscape.	
Urbanizing the streetscape does not accommodate pedestrians.	
The ranking does not reflect my view	
of these proposals in the community	
I originally ranked them 3-3-2	

Please provide any additional comments on your preferred option for pedestrian accommodation. The basic premise of this form and the presentation appears to be accommodating pedestrians.

No one has asked, or stated, what accommodation means for this study. What it appears to mean from the proposals, is to get pedestrians off the road in order to accommodate motorists. This is NOT what Mill St. residents want. Given the quiet rural character of the road, I suggest that the Town and its consultants are asking the question incorrectly or at least placing the emphasis in the wrong place. Perhaps it is the behaviour of the motorists that the design needs to alter in favour of the pedestrians.

Perhaps such an approach may also help in maintaining the character of the street and the sense of community. The work done over the years on Creditview is evidence of how to damage a rural village. Urbanizing everything is not the answer. Maintain a rural street environment.

6. Please include other questions you have below:

The EA has not communicated a need for reconstruction/realignment based on quantifiable information and the analysis is based on flawed data.

The comments form is poorly drafted with leading questions.

The comment form is very poorly formatted, making it less user friendly and is not a saveable pdf. The less user-friendly, the less likely people are to complete it. It restricts community engagement.

Ranking poor proposals against each other creates a false ranking of the proposal, generally. Again, this can create a misrepresentation that may be used to justify a particular proposal. A ranking of "1" does not mean it is something the community likes or WANTS.

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

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THANK YOU FOR YOUR TIME AND EFFORT!

MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

PUBLIC INFORMATION CENTRE (PIC) #2 COMMENT FORM

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NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

☒ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

- ☒ A Mill Street Resident
☐ Other (Please Identify -)

2. What is your primary mode of travelling along Mill Street?

- ☐ Personal Vehicle (Auto)
☐ Bicycle
☒ Walking
☐ Other (Please Identify -)

3. Approximately how often do you walk along Mill Street?

- ☐ Daily
☒ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

The traffic is generally slow, respectful and infrequent. There is already plenty of road width and adequate sightlines (other than the crest of the hill at the S-bend) to provide a safe pedestrian route. A dedicated pedestrian route gives motorists a sense that they are entitled to drive faster....and they do as evidenced on Creditview Rd.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 2
How do you feel this option accommodates pedestrians?	
This does not accommodate pedestrians. A mountable curb restricts pedestrian entry and egress.	
Mountable curbs funnel water and given the current	
topography, the discharge will likely add to flooding which will	
restrict access as well as damage property.	
There are no proposals to deal with flooding in the current study.	

Sidewalk	Rank: 3
How do you feel this option accommodates pedestrians?	
A sidewalk categorizes pedestrians. A sidewalk instructs pedestrians that they	
must only walk where designated and it projects that viepoint on motorists	
It speeds up traffic which is detrimental to pedestrians and is antithetical to	
accommodating pedestrians	
Sidewalks undermine the quiet rural nature of the community	
which makes it it less appealing to pedestrians,	
thereby failing to accommodate pedestrian.	

Asphalt Boulevard	Rank: 1
How do you feel this option accommodates pedestrians?	
An asphalt blvd widens the road for motorists, which increase speeds.	
Increased speeds do not accommodate pedestrians	
This removes trees and significantly alters the streetscape.	
Urbanizing the streetscape does not accommodate pedestrians.	
The ranking does not reflect my view	
of these proposals in the community	
I originally ranked them 3-3-2	

Please provide any additional comments on your preferred option for pedestrian accommodation. The basic premise of this form and the presentation appears to be accommodating pedestrians.

No one has asked, or stated, what accommodation means for this study. What it appears to mean from the proposals, is to get pedestrians off the road in order to accommodate motorists.

Given the quiet rural character of the road, I suggest that the Town and its consultants are asking the question incorrectly or at least placing the emphasis in the wrong place. Perhaps it is the behaviour of the motorists that the design needs to alter in favour of the pedestrians.

Perhaps such an approach may also help in maintaining the character of the street and the sense of community. The work done over the years on Creditview is evidence of how to damage a rural village. Urbanizing everything is not the answer. Maintain a rural street environment.

6. Please include other questions you have below:

The EA has not communicated a need for reconstruction/realignment based on quantifiable information and the analysis is based on flawed data.

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Ranking poor proposals against each other creates a false ranking of the proposal, generally. Again, this can create a misrepresentation that may be used to justify a particular proposal. A ranking of "1" does not mean it is something the community likes.

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

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MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

PUBLIC INFORMATION CENTRE (PIC) #2 COMMENT FORM

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NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

☒ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

- ☐ A Mill Street Resident
☒ Other (Please Identify - Cheltenham Resident)

2. What is your primary mode of travelling along Mill Street?

- ☐ Personal Vehicle (Auto)
☐ Bicycle
☒ Walking
☒ Other (Please Identify - Golf Cart)

3. Approximately how often do you walk along Mill Street?

- ☐ Daily
☐ Weekly
☐ Monthly
☒ Rarely or Never

If you answered "Rarely or Never", please identify why.

Because there isnt anywhere really to walk. We walk to the bridge and look at the river. My kids go to the park instead.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☒ Yes
☐ No

If you answered "No", please provide your reason(s).

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 2
How do you feel this option accommodates pedestrians?	
I think if we arent going to put in a sidewalk then just do the simple one.	
I also dont think the road needs all the city things like road paintings	
and it definitely doesnt need guard rails and things like that. It will look terrible for cheltenham.	

Sidewalk	Rank: 1
How do you feel this option accommodates pedestrians?	
I personally believe if we dont put the sidewalk in now it will never get done.	
We waited like 20 years to get a sidewalk on creditview.	
I think it will make it safer for people to walk.	
I do understand the people that live on Mill streets concerns though.	
I dont live on mill street but we might walk to more if there was somewhere to walk.	
But no need for all the extra road stuff like guard rails and city looking things.	

Asphalt Boulevard	Rank: 3
How do you feel this option accommodates pedestrians?	
I say either go for it with the sidewalk or just dont bother.	

Please provide any additional comments on your preferred option for pedestrian accommodation.

I am also noticing this is all just focussed on pedestrian accommodation. Why is that?

Shouldnt it also be about road safety? About keeping the Cheltenham Charm?

About saving as many trees and much more. Why is this comment sheet only addressing somewhere for pedestrians?

6. Please include other questions you have below:

Will we be notified once a decision is made? With our creditview sidewalk we werent told of the final decison (that I am aware of)

When will we find out this answer?

I hope we can come to some agreement as a community becasue we really just need something done with the road.

Thanks for all the extra time working with the community and providing extra meetings etc.

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

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THANK YOU FOR YOUR TIME AND EFFORT!

MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

PUBLIC INFORMATION CENTRE (PIC) #2 COMMENT FORM

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NAME (REQUIRED): _____

MAILING ADDRESS (REQUIRED): _____

EMAIL (OPTIONAL): _____

☐ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

☒ A Mill Street Resident

☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

☒ Personal Vehicle (Auto)

☒ Bicycle

☒ Walking

☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

☒ Daily

☐ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

If you answered "No", please provide your reason(s).

I walk the road at least every day, sometimes twice doubt any "walkway" would up that!

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb

Rank: 1

How do you feel this option accommodates pedestrians?

The question is biased. There are many accommodation requirements. If the road had traffic calming measures, lower than 40 km/h posted speeds, narrowed lanes that don't invite cars to speed, then pedestrians would be very much accommodated to walk on the road edge and sod/grass edge.

Sidewalk

Rank: 3

How do you feel this option accommodates pedestrians?

A concrete sidewalk is a no brainer for accommodating pedestrians - of course it's the option if this is the only consideration. But it's not just pedestrians to accommodate.

Asphalt Boulevard

Rank: 2

How do you feel this option accommodates pedestrians?

Again, a paved shoulder is the same as a concrete sidewalk - so it's a no brainer accommodation for pedestrians of course! So again, the question is biased.

Please provide any additional comments on your preferred option for pedestrian accommodation.

Again this is a biased "comment" request. To suggest that pedestrian accommodation is the only issue to comment on is again slanted, biased and rather insulting and diminishing to ALL the concerns and needs to accommodate. I believe pedestrians need better/safer accommodations, but it is the cars that need less accommodations; narrow lanes, tighter turns, slower speeds, limited sight lines, traffic calming etc etc - SLOW the cars down, and the pedestrian issue takes care of itself.

6. Please include other questions you have below:

- A.) List specific traffic calming measures that will be implemented.
- B.) Explain in detail why the posted speed limit can't be lower than 40 km/h.
- C.) What is required to allow the Town to drop this minimum 2 lane width mantra?
- D.) Can someone give detailed cross section dimensions of the "S" bend retaining wall and guard rail?

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
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Caledon, ON L7C 1J6

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THANK YOU FOR YOUR TIME AND EFFORT!

APPENDIX 2-3

Comments Received: Technical Agency & Key Stakeholder Groups

Connor MacIsaac

From: Winnie Wong
Sent: August 31, 2022 11:01 AM
To: Saddi, Asha; Shun Cheung
Cc: Connor MacIsaac; Mannie, Sharon
Subject: RE: Town of Caledon: Mill Street Class EA

Hi Asha,

Thank you for your comments. Please see below responses in [blue](#).

Thanks,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: Saddi, Asha <asha.saddi@peelregion.ca>
Sent: July 15, 2022 8:49 AM
To: Shun Cheung <shun.cheung@caledon.ca>
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>; Connor MacIsaac <cmacIsaac@rvanderson.com>; Mannie, Sharon <sharon.mannie@peelregion.ca>
Subject: RE: Town of Caledon: Mill Street Class EA

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Shun,

Attached are comments from our Traffic Engineering teams:

Traffic Operations - Sharon Mannie

- Does this EA cover the entire intersection of Mississauga Road and Mill Street? <<RVA_20220831: EA does not include the intersection of Mississauga Road.>>
- Please provide soft copies of the materials for reference after the PIC Meeting #2 <<RVA_20220831: Town forwarded the link to PIC #2 presentation to the Region on June 14.>>

Traffic Signals - Rebecca Caughey

- Will Town of Caledon be conducting a Trail Crossing Treatment Warrant for the Trail Crossing at Mill Street? <<RVA_20220831: Pedestrian crossing improvements including new traffic signs and pavement markings are proposed and details to be confirmed during detailed design.>>
- Would the Town of Caledon consider a connection between Mill Street and the Trail crossing as well along Mississauga Road? <<RVA_20220831: Trail currently intersects both Mill Street and Mississauga Road.>>

I will be in touch again if we have additional comments from other teams and in the meantime we look forward to receiving your response comments.

Thanks,

Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca



This email, including any attachments, is intended for the recipient specified in the message and may contain information which is confidential or privileged. Any unauthorized use or disclosure of this email is prohibited. If you are not the intended recipient or have received this e-mail in error, please notify the sender via return email and permanently delete all copies of the email. Thank you.

From: Saddi, Asha
Sent: July 12, 2022 11:26 AM
To: Shun Cheung <Shun.Cheung@caledon.ca>
Cc: winnie.wong@rvanderson.com; Connor MacIsaac <cmaclsaac@rvanderson.com>
Subject: RE: Town of Caledon: Mill Street Class EA

Hi Shun,

This is to let you know that due to vacations some of our teams are still reviewing materials and I will be able to get back to you by the end of the week with any additional comments.

Thanks,

Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca



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From: Saddi, Asha
Sent: June 23, 2022 11:37 AM
To: Shun Cheung <Shun.Cheung@caledon.ca>
Cc: winnie.wong@rvanderson.com; Connor MacIsaac <cmaclsaac@rvanderson.com>; Khan, Sabrina <Sabrina.Khan@peelregion.ca>
Subject: RE: Town of Caledon: Mill Street Class EA

Hi Shun,

Our Transportation System Planning team have the following comment/question:

Transportation System Planning – Sabrina Khan

- Option 2 – Sidewalk is our preferred alternative as it provides the safest option for pedestrians
- Why is the focus only on pedestrians and not active transportation (cyclists)?

I will be in touch if other teams have comments and in the meantime we look forward to receiving your response comments.

Thanks,

Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca



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From: Saddi, Asha
Sent: June 20, 2022 3:27 PM
To: 'Shun Cheung' <Shun.Cheung@caledon.ca>
Cc: winnie.wong@rvanderson.com; 'Connor MacIsaac' <cmacisaac@rvanderson.com>; ZZG-PWI <pw@peelregion.ca>; Ponce Vanelli, Italia <Italia.Ponce@peelregion.ca>
Subject: FW: Town of Caledon: Mill Street Class EA

Hi Shun,
 Our Water and Wastewater team have the following comments:

The Region has a 250mm and 300mm watermain that runs predominantly on the north side of Mill Street. This is the only source that provides water to the residents in the area. Impact to our underground infrastructure, Valve, chambers and fire hydrants will need to be taken into consideration during this project. Furthermore, in order to comment on the solutions presented we would like the project material to be available for viewing digitally as many of us could not attend the in person meeting and the Town's website does not contain the material online.

I will be in touch again if other teams have comments and in the meantime, we look forward to your response comments.

Thanks,

Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca

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From: Connor MacIsaac <cmacisaac@rvanderson.com>

Sent: May 30, 2022 10:57 AM

Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>

Subject: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

CAUTION: EXTERNAL MAIL. DO NOT CLICK ON LINKS OR OPEN ATTACHMENTS YOU DO NOT TRUST.

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Public Information Centre (PIC) #2 for the **Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA)**. The purpose of this meeting is garner feedback on the options for pedestrian accommodations developed for Mill Street in consideration of the feedback received during and following PIC #1, held online October 5, 2021.

The PIC will be held as an informal "Open House" with materials pertaining to the study on display, with members of the project team on hand to answer questions and discuss issues related to the project. Attendees will be asked to review the materials on display and provide their preferences for pedestrian accommodations. A final recommendation for Mill Street will be developed following the PIC, in consideration of the input received.

The details of the PIC are below:

Date: Wednesday June 15, 2022
Time: 5:30 to 7:00 pm
Location: Cheltenham Community Centre - 14190 Creditview Rd, Caledon, ON L0P 1N0
Entrance is located at the south side of the building

The project team values the participation of all stakeholders and wishes to ensure that the community's interests and concerns are taken into consideration. Please contact the project team members listed below if you require a hard copy of the materials or for further information on this project.

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure Engineering Services
905.584.2272 X.4040
shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
416.497.8600 X.1471
winnie.wong@rvanderson.com

If you would like to be removed from the project mailing list, please let us know by responding to this email.

Regards,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



rvanderson.com



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Notice of Study Recommendations Comments



Connor MacIsaac

From: [REDACTED]
Sent: February 1, 2023 7:13 PM
To: Winnie Wong; Shun Cheung
Cc: Connor MacIsaac
Subject: Mill Street Reconstruction EA

Categories: Resident Comments

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

I have been following the discussions around the re-paving of Mill Street with interest. As a resident since 1989, I believe that I can add some perspective pertaining to the discussion.

I note with interest the stated intent to 'maintain rural roadway characteristics with curb and gutter reinstated only in existing locations'. This is a position that I am concerned about.

You should be aware that when I moved here the only curbing was on Creditview Road and terminated as the road edge blended into Mill Street. On our property, 1499 Mill Street, located at the south-west corner of Creditview and Mill, the curbing terminated at the box culvert where our east driveway is located. It did not extend on either side of the road between Creditview and the Mill Street bridge, as it currently does.

The addition of curbing was done with no prior consultation and, as evidenced by the outcome and continuing issues, little knowledge of surface water behaviour.

Prior to the installation of curbing, water flowing north and downhill on Creditview would evenly split on the crest of Mill Street with some water flowing north onto the property at the north-west corner and the balance flowing off the roadway into the ditch on the north of our lot, south side of Mill Street.

Water flowing east from the bridge would flow evenly off the road both north and south.

With curbing installed, all of the water is directed to catch-basins, located approximately 75 feet west of the centreline of Creditview Road. This water is now all focused into a ditch directly opposite the lowest part of our property. This redirection of surface water substantially increased the volume of water in the ditch to the extent that we have been flooded several times with water flowing across our lot and through our garage. In addition water flowing east from the bridge invariably flows down our west driveway toward the garage. Why? Apparently because there was insufficient attention paid to the grade of the road or the downward slope of the driveway. Since the catch basins are downhill from the west drive, during moderate to heavy rain the surface water overwhelms the shallow curb at the mouth of the driveway.

What is the solution? Revert back to how it was? Add additional catch-basins to intercept and redirect the water? I do not know the answer but I trust that your engineering staff will arrive at a workable solution.

Please confirm receipt of this message. If I can be of any assistance, please do not hesitate to contact me.

[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

Connor MacIsaac

From: [REDACTED]
Sent: January 17, 2023 4:56 PM
To: Connor MacIsaac
Cc: Shun Cheung; Winnie Wong
Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report

Categories: Resident Comments

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good Afternoon Folks

Thank you for the continued reach out and information sharing.

Its disappointing this is pushed, yet again, into 2025. Seems to me there is FAR too much energy into study and study and study..... not much energy into doing it. We tax payers keep paying, you guys keep getting paid.... and still the road is crap, and will be crap for at least 2-3 more years.

Again, I will voice my concerns in regards to details that thus far appear to be skimmed over or ignored; traffic calming, street lighting, guiderails,etc. When will the residents get specific and detailed information on exactly what is going to be done with these items. What say do we have in the specific installation details, or will this just be shuffled off to the contractor to source the easiest and cheapest? For example, over and over its talked about preserving the unique rural character while a few years back, some dope installed industrial LED street lights on Mill that floods stupid bright 6000K daylight into the people's homes, yards, bush, sky and in one small decision, poooof, the rural character on Mill St evaporated and its like an airport parking lot at night.

So, again, I urge that the devil is in the details and all that. We residents deserve to know ALL of what is going to be *actually* done on our street. And right now, the confidence that this can and will happen is, in my opinion, not there.

Thank you, and kind regards

[REDACTED]
[REDACTED]

On 2023-01-12 3:13 p.m., Connor MacIsaac wrote:

Dear Sir/Madam,

On behalf of the Town, please find attached the Notice of Study Recommendations for the Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA). The purpose of this notice is to garner feedback on the recommendations for Mill Street developed in consideration of the comments received during and following the second Public Information Centre (PIC), held on June 15, 2022. A detailed summary of the comments received at and following the second PIC, and how these comments were incorporated into the development of the study recommendations,

is presented in the PIC Summary Report on the Town's website at <https://www.caledon.ca/en/news/mill-street-ea.aspx>.

To provide comments, questions, or concerns on the study recommendations, or the PIC Summary Report, please contact the project team members listed on the attached notice by February 2, 2023.

Upon completion of the PIC Summary Report comment period, a Project File Report (PFR), containing details of the Municipal Class EA planning and decision-making process, public consultation process and study recommendations will be made available for a 30-day public review period, concluding the EA study. A Notice of Completion will be issued at that stage.

Thank you again for your continued input to the study.

Best Regards,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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APPENDIX 1-4

Technical Agency and Key Stakeholder Consultation



Connor MacIsaac

From: Koops, Krystina <Krystina.Koops@dpcdsb.org>
Sent: October 23, 2020 9:23 AM
To: Connor MacIsaac
Subject: RE: Mill Street EA - Notice of Commencement

Categories: Technical Agency Comments, Filed by Newforma

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Connor,

Thank you for circulating the Board with regards to the EA for Mill Street. While we have no comments at this time, please continue to keep the Board informed of the status of this project so that we may monitor its progress and provide comments as necessary.

Krystina Koops, MCIP, RPP

Planner - Planning Department
Dufferin-Peel Catholic District School Board
40 Matheson Boulevard West, Mississauga ON L5R 1C5
Tel: 905-890-0708 ext. 24407 | Email: krystina.koops@dpcdsb.org
Website: www.dpcdsb.org | Twitter: @DPCDSBSchools | YouTube: DPCDSBVideos

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From: Connor MacIsaac <cmaclsaac@rvanderson.com>
Sent: Thursday, October 22, 2020 3:09 PM
To: Connor MacIsaac <cmaclsaac@rvanderson.com>
Cc: Andrew McGregor <AMcGregor@rvanderson.com>; Shun Cheung <Shun.Cheung@caledon.ca>; John P. Does <jdoes@rvanderson.com>; Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: Mill Street EA - Notice of Commencement

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Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Notice of Study Commencement for the Mill Street Class Environmental Assessment Study (Schedule B MCEA). The study will review opportunities for improvements to the road between Mississauga Road and Creditview Road, including improvements to: road geometry and intersection safety; pedestrian and cyclist connectivity; and stormwater management.

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- Contact information and the identification of individuals that represent your agency or group that we should include as a primary contact throughout the study process;
- Description of the existing conditions or sensitivities within the study area as they relate to your interests; and
- Specific issues, concerns and/or expectations that your agency or group may have.

Please respond to this email with your comments, or contact directly the project team members listed on the attached notice for further information on this project.

Regards,



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Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

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43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

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Connor MacIsaac

From: Rebecca.Fu@HydroOne.com on behalf of hotosp@HydroOne.com
Sent: October 23, 2020 9:56 AM
To: Connor MacIsaac
Subject: RE: Mill Street EA - Notice of Commencement

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good morning Connor,

Please note that Hydro One Telecom does not have infrastructure (existing or planned) in the project work area.

Thank you.

Rebecca Fu
Operations Services
Hydro One Telecom Inc.
65 Kelfield Street, Rexdale
Ontario M9W 5A3
hotosp@hydroone.com
www.HydroOneTelecom.com

From: Connor MacIsaac <cmacIsaac@rvanderson.com>
Sent: Thursday, October 22, 2020 3:09 PM
To: Connor MacIsaac <cmacIsaac@rvanderson.com>
Cc: Andrew McGregor <AMcGregor@rvanderson.com>; Shun Cheung <Shun.Cheung@caledon.ca>; John P. Does <jdoes@rvanderson.com>; Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: Mill Street EA - Notice of Commencement

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Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Notice of Study Commencement for the Mill Street Class Environmental Assessment Study (Schedule B MCEA). The study will review opportunities for improvements to the road between Mississauga Road and Creditview Road, including improvements to: road geometry and intersection safety; pedestrian and cyclist connectivity; and stormwater management.

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Connor MacIsaac

From: SecondaryLandUse@HydroOne.com
Sent: October 29, 2020 2:21 PM
To: Winnie Wong
Cc: shun.cheung@caledon.ca
Subject: Hydro One Response: Mill Street Class EA Study
Attachments: 20201029-NoticeOfCommence-Mill Street Class EA Study.pdf

Categories: Filed by Newforma

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Please see the attached for Hydro One's Response.

Hydro One Networks Inc
SecondaryLandUse@HydroOne.com

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Hydro One Networks Inc
483 Bay St
Toronto, ON

October 29, 2020

Re: Mill Street Class EA Study

Attention:

Winnie Wong, P.Eng., PMP., M.Eng.
R.V. Anderson Associates Limited
Project Manager

Thank you for sending us notification regarding (Mill Street Class EA Study). In our preliminary assessment, we confirm there are no existing Hydro One Transmission assets in the subject area. Please be advised that this is only a preliminary assessment based on current information.

If plans for the undertaking change or the study area expands beyond that shown, please contact Hydro One to assess impacts of existing or future planned electricity infrastructure.

Any future communications are sent to Secondarylanduse@hydroone.com.

Be advised that any changes to lot grading and/or drainage within proximity to Hydro One transmission corridor lands must be controlled and directed away from the transmission corridor.

Sent on behalf of,

***Secondary Land Use
Asset Optimization
Strategy & Integrated Planning
Hydro One Networks Inc.***

Connor MacIsaac

From: Winnie Wong
Sent: June 24, 2022 10:24 AM
To: Connor MacIsaac
Subject: FW: Hydro One Response: 20220624-NoticeOfPIC2-Mill Street Class EA Study
Attachments: 20220624-NoticeOfPIC2-Mill Street Class EA Study.pdf

Categories: Technical Agency Comments

FYI - please see below.

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

-----Original Message-----

From: SUN Hongxia <Susan.SUN@HydroOne.com> On Behalf Of SECONDARY LAND USE Department
Sent: June 24, 2022 10:22 AM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: SECONDARY LAND USE Department <Department.SecondaryLandUse@hydroone.com>
Subject: Hydro One Response: 20220624-NoticeOfPIC2-Mill Street Class EA Study

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Please see the attached for Hydro One's Response.

Hydro One Networks Inc
SecondaryLandUse@HydroOne.com

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Hydro One Networks Inc.

483 Bay Street
8th Floor South Tower
Toronto, Ontario M5G 2P5

HydroOne.com

June 24, 2022

Re: Mill Street Class EA Study

Attention:

Winnie Wong, P.Eng., PMP., M.Eng.
R.V. Anderson Associates Limited
Project Manager

Thank you for sending us notification regarding (Mill Street Class EA Study). In our assessment, we confirm there are no existing Hydro One Transmission assets in the subject area.

If plans for the undertaking change or the study area expands beyond that shown, please contact Hydro One to assess impacts of existing or future planned electricity infrastructure.

Any future communications are sent to Secondarylanduse@hydroone.com.

Sent on behalf of,

***Secondary Land Use
Asset Optimization
Strategy & Integrated Planning
Hydro One Networks Inc.***

Connor MacIsaac

From: Sousa, Phillip <phillip.sousa@peelsb.com>
Sent: November 5, 2020 5:16 PM
To: Shun Cheung
Cc: Blakeman, Suzanne; Hanson, Nicole; Connor MacIsaac
Subject: RE: [EXTERNAL] Mill Street EA - Notice of Commencement
Attachments: Mill Street EA (Nov. 2020).docx

Categories: Technical Agency Comments, Filed by Newforma

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Good afternoon,

I am responding to the email from October 22, 2020. The Board would like to be kept informed of the project as it may impact school bussing.

Take care,
Phil

Phillip Sousa - Planning Officer
Planning & Accommodation Support Services
Peel District School Board
Tel: (905) 890-1010 ext: 2009
Fax: (905) 890-5295
phillip.sousa@peelsb.com

From: Blakeman, Suzanne
Sent: Thursday, October 22, 2020 4:02 PM
To: Sousa, Phillip
Subject: Fw: [EXTERNAL] Mill Street EA - Notice of Commencement

Suzanne Blakeman | Manager
Planning and Accommodation Support Services
Peel District School Board
905 890-1010 ext. 2216

From: Connor MacIsaac <cmacIsaac@rvanderson.com>
Sent: Thursday, October 22, 2020 3:08 PM
To: Connor MacIsaac <cmacIsaac@rvanderson.com>
Cc: Andrew McGregor <AMcGregor@rvanderson.com>; Shun Cheung <Shun.Cheung@caledon.ca>; John P. Does <jdoes@rvanderson.com>; Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: [EXTERNAL] Mill Street EA - Notice of Commencement

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Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Notice of Study Commencement for the Mill Street Class Environmental Assessment Study (Schedule B MCEA). The study will review opportunities for improvements to the road between Mississauga Road and Creditview Road, including improvements to: road geometry and intersection safety; pedestrian and cyclist connectivity; and stormwater management.

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Regards,



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Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

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C: (905) 933-5709

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November 5, 2020

Mr. Shun H. Cheung
Project Manager, Engineering Services
Finance and Infrastructure Services
Town of Caledon
6311 Old Church Road
Caledon, Ont. L7C 1J6

Dear Mr. Cheung:

**RE: Mill Street Class Environmental Assessment Study (Schedule BMCEA) –
Mississauga Road to Creditview Road**

Thank you for the notice the Board received October 22, 2020 informing us of the Mill Street Environmental Assessment. The Peel District School Board is interested in this project and needs to know what the haulage routes will be to determine possible impact, if any on school bus routes.

Please continue to keep us informed of the status of this project so that we may monitor its progress and provide comments as necessary.

If you require any further information please contact me at phillip.sousa@peelsb.com or 905-890-1010, ext. 2009.

Yours truly,

Phillip Sousa,
Planning Officer
Planning and Accommodation Support Services

- c. Nicole N. Hanson, Planning Officer (Development), PDSB
S. Blakeman, Manager of Planning & Accommodation Support Services, PDSB

Trustees

Carrie Andrews
Susan Benjamin
Stan Cameron
Robert Crocker
Nokha Dakroub
Will Davies

David Green
Sue Lawton
Brad MacDonald
John Marchant
Kathy McDonald
Balbir Sohi

Director of Education and Secretary

Colleen Russell-Rawlins

Associate Director, Instructional & Equity Support Services

Poleen Grewal

Associate Director, Operational Support Services

Jaspal Gill

Associate Director, School Support Services

Mark Haarmann

Connor MacIsaac

From: MOC (GT) <gt.moc@Telecon.ca>
Sent: November 9, 2020 5:16 PM
To: Connor MacIsaac
Subject: RE: Mill Street EA - Notice of Commencement/GT20-3229
Attachments: GT20-3229.doc

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi,

GT has no plant within 2m of proposed work-NO CONFLICT

Sincerely,

Mary Tina

Technicien CAO, Ingenierie-Centre du Canada
CAD Technician, Engineering-Central Canada

T 289-657-8072
7777 Weston Road, Woodbridge, ON L4L 0G9

telecon.ca



From: Connor MacIsaac <cmaclsaac@rvanderson.com>
Sent: Thursday, October 22, 2020 3:09 PM
To: Connor MacIsaac <cmaclsaac@rvanderson.com>
Cc: Andrew McGregor <AMcGregor@rvanderson.com>; Shun Cheung <Shun.Cheung@caledon.ca>; John P. Does <jdoes@rvanderson.com>; Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: Mill Street EA - Notice of Commencement

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Regards,



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Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

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GROUP TELECOM

C/O Telecon Design Inc
7777 Weston Road,
Vaughan, Ontario L4L 0G9
Ph: (905) 569 2882 Fax: (905) 460-8956
GT.MOC@telecon.ca

APPLICATION FOR PLANT LOCATION AND CONSENT

Applicant: R.V. Anderson Associates Limited
Mark Up #: GT20-3229
Applicant Ref #: Utility Record Request
Location: Mill Street
Date Received from Applicant: 10-22-2020
Marked By: Mary Tina

APPLICATION FOR PLANT LOCATION AND REQUEST

- ☐ Existing and/or proposed Group Telecom underground plant are indicated on the attached plan
- ☒ Our records show no existing and / or proposed underground plant within 2m of your proposed installation
- ☐ Conflict indicated
- ☒ Meets with our approval
- ☐ Not for PUCC approval - Mark up only
- ☐ If within 1 meter of GT plant, hand dig

REMARKS: Call for locates 1.800.400.2255. Maintain clearance of 0.6m. Hand dig when crossing Group Telecom plant. Call 877.865.6193 for locate if plant is in railway property.

PROCEDURES TO FOLLOW:

1. Request locates prior to construction 1-800-400-2255
2. If exact location and depth are critical - test pits are recommended
3. Group Telecom plant location information is approximate
4. If the location of your proposed design changes, it will be necessary to re-apply
5. Permits expire six (6) months from approval date

Signature:
Mary Tina

Date:
Nov 09, 2020

Connor MacIsaac

From: Nejadtaghi, Roya <Roya.Nejadtaghi@Telecon.ca>
Sent: November 12, 2020 5:50 PM
To: Connor MacIsaac
Cc: GTAW.Markups@rci.rogers.com; Rogers.MOC
Subject: M203848_Mill St and Mississauga Rd
Attachments: M203848_Mill St and Mississauga Rd_Rogers Markup Response Letter.pdf; M203848_Mill St and Mississauga Rd.dwg

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi,

Please find attached completed markup request.

*** Please use "REGEN" command for proper line style ***

Thank you.

Regards,

Roya Nejadtaghi

CAD Technician, Engineering - Central Canada
Technicien CAO, Ingénierie - Centre du Canada

T 289-657-8020
7777 Weston Rd, Woodbridge (Ontario) L4L 0G9

telecon.ca



From: Connor MacIsaac <cmacisaac@rvanderson.com>
Sent: Thursday, October 22, 2020 3:09 PM
To: Connor MacIsaac <cmacisaac@rvanderson.com>
Cc: Andrew McGregor <AMcGregor@rvanderson.com>; Shun Cheung <Shun.Cheung@caledon.ca>; John P. Does <jdoes@rvanderson.com>; Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: Mill Street EA - Notice of Commencement

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Notice of Study Commencement for the Mill Street Class Environmental Assessment Study (Schedule B MCEA). The study will review opportunities for improvements to the road between Mississauga Road and Creditview Road, including improvements to: road geometry and intersection safety; pedestrian and cyclist connectivity; and stormwater management.

At this point in the study, we are requesting any relevant background information from you as it relates to the study area. Information that would be of interest includes:

- Contact information and the identification of individuals that represent your agency or group that we should include as a primary contact throughout the study process;
- Description of the existing conditions or sensitivities within the study area as they relate to your interests; and
- Specific issues, concerns and/or expectations that your agency or group may have.

Please respond to this email with your comments, or contact directly the project team members listed on the attached notice for further information on this project.

Regards,



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offices are now open.*



Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

R.V. Anderson Associates Limited

43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

www.rvanderson.com



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Markup Response Form

Application Date	October 22, 2020	Applicant:	R.V. Anderson Associates Limited
Date Returned:	November 12, 2020		
Rogers Ref. No.:	M203848	Applicant Job No.:	N/A
Location:	Mill St and Mississauga Rd		

Rogers Communications has reviewed your drawing(s) as requested and returns one marked-up copy. Our comments follow below with an "X" indicating Rogers' stance on your proposed plan.

Comments:

- | | | |
|-------------------------------------|-------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <input type="checkbox"/> | No Conflict | Rogers Communications currently does not possess existing plant in the area indicated on your attached plans. |
| <input checked="" type="checkbox"/> | No Conflict
For your Reference | Rogers Communications currently has existing plant as marked on your drawing. Our standard depth in this municipality is: 1m .
Please ensure you maintain clearances of 0.3m vertically and 0.6m horizontally. |
| <input type="checkbox"/> | EXTREME
CAUTION | Use vactruck and expose ducts, maintain minimum of 0.6m clearance. |
| <input type="checkbox"/> | CONFLICT | Your proposed construction appears to encroach within existing Rogers Communications plant.
Please relocate your proposed construction to allow adequate clearance of 0.3 m vertically and 1 m horizontally. |
| <input checked="" type="checkbox"/> | CAUTION | Rogers Communications has aerial plant in this area, as it is indicated on the attached plans. |
| <input checked="" type="checkbox"/> | CAUTION | Fiber Optic Cable is present in the area of your proposed construction. |
| <input type="checkbox"/> | Note | Proposed Fiber Optic Cable in a joint use duct structure . |
| <input type="checkbox"/> | Note | Plant currently under construction. |
| <input checked="" type="checkbox"/> | Note | Please inform Rogers Communications well in advance of the proposed construction schedule in order to coordinate our plant relocation. |
| <input checked="" type="checkbox"/> | Note | Locates are still required. Call for locates at 1-800-400-2255 |
| <input checked="" type="checkbox"/> | Note | Hand dig when crossing, or within 1.0m of existing Rogers plant. |
| <input checked="" type="checkbox"/> | Note | Plant is to Approximation. |

Connor MacIsaac

From: Connor MacIsaac
Sent: November 16, 2020 9:59 AM
To: Harvey, Joseph (MHSTCI)
Cc: Winnie Wong; Eliza Brandy
Subject: RE: File 0006107: Mill Street EA - Notice of Commencement

Categories: Filed by Newforma

Hi Joseph,

The Project information Form number for the Mill Street EA is: P1066-0178-2020

Humber Station Road will be assigned this week.



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Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

R.V. Anderson Associates Limited

43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

www.rvanderson.com



From: Harvey, Joseph (MHSTCI) <Joseph.Harvey@ontario.ca>
Sent: November 13, 2020 11:11 AM
To: Connor MacIsaac <cmacIsaac@rvanderson.com>
Subject: RE: File 0006107: Mill Street EA - Notice of Commencement

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good Morning Connor,

Thanks for the response to our comments and clarifying the next steps for this project. Could you please send the Project Information Form number for the Stage 1 AA so we can link the files internally?

Kind Regards,

Joseph Harvey | Heritage Planner (A)

Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit
Ministry of Heritage, Sport, Tourism and Culture Industries
401 Bay Street
17th Floor, Suite 1700
Toronto, ON M7A 0A7
613.242.3743
Joseph.Harvey@ontario.ca

From: Connor MacIsaac <cmacisaac@rvanderson.com>
Sent: November 13, 2020 9:42 AM
To: Harvey, Joseph (MHSTCI) <Joseph.Harvey@ontario.ca>
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>; Barboza, Karla (MHSTCI) <Karla.Barboza@ontario.ca>; Lisa Merritt <lmerritt@asiheritage.ca>
Subject: RE: File 0006107: Mill Street EA - Notice of Commencement

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Hello Joseph,

Thank you for your response.

As part of this study, both a Stage 1 Archaeological Assessment and a Cultural Heritage Resource Assessment will be completed.

The Stage 1 Archaeological Report be submitted directly to the MHSTCI by the archaeologist (ASI) for review as required. Additionally, the Cultural Heritage Assessment Report will be provided to MHSTCI for review prior to issuing the Notice of Commencement for the study as requested.

Regards,



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Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

R.V. Anderson Associates Limited

43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

www.rvanderson.com



From: Harvey, Joseph (MHSTCI) <Joseph.Harvey@ontario.ca>
Sent: November 12, 2020 12:13 PM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: Barboza, Karla (MHSTCI) <Karla.Barboza@ontario.ca>; shunchung@caledon.ca; Andrew McGregor <AMcGregor@rvanderson.com>
Subject: File 0006107: Mill Street EA - Notice of Commencement

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Winnie Wong,

Please find attached MHSTCI's comments on the above referenced project update. Please do not hesitate to contact me if you have any questions.

Regards,

Joseph Harvey | Heritage Planner (A)

Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit

Ministry of Heritage, Sport, Tourism and Culture Industries

401 Bay Street

17th Floor, Suite 1700

Toronto, ON M7A 0A7

613.242.3743

Joseph.Harvey@ontario.ca

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**Ministry of Heritage, Sport,
Tourism and Culture Industries**

Programs and Services Branch
401 Bay Street, Suite 1700
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Tel: 613.242.3743

**Ministère des Industries du Patrimoine,
du Sport, du Tourisme et de la Culture**

Direction des programmes et des services
401, rue Bay, Bureau 1700
Toronto, ON M7A 0A7
Tél: 613.242.3743



November 12, 2020

EMAIL ONLY

Winnie Wong, P. Eng.
Project Manager
R.V. Anderson Associates Limited
43 Church Street, Suite 104
St. Catharines, ON L2R 7E1
winnie.wong@rvanderson.com

MHSTCI File : 0006107
Proponent : The Town of Caledon
Subject : Notice of Study Commencement
Project : Mill Street Class Environmental Study Assessment Study:
Mississauga Road to Credit view Road
Location : The Town of Caledon

Dear Winnie Wong:

Thank you for providing the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) with the Notice of Study Commencement for the above-referenced project. MHSTCI's interest in this Environmental Assessment (EA) project relates to its mandate of conserving Ontario's cultural heritage, which includes:

- Archaeological resources, including land and marine;
- Built heritage resources, including bridges and monuments; and,
- Cultural heritage landscapes.

Under the EA process, the proponent is required to determine a project's potential impact on cultural heritage resources.

Project Summary

The Town of Caledon has initiated a Municipal Class Environmental Assessment (Class EA) to review and identify road, intersection and drainage improvements to Mill Street between Mississauga Road and Creditview Road. The project is being completed as a Schedule "B" project in accordance with the Municipal Class Environmental Assessment (October 2000, amended in 2007, 2011 & 2015).

Identifying Cultural Heritage Resources

While some cultural heritage resources may have already been formally identified, others may be identified through screening and evaluation. Indigenous communities may have knowledge that can contribute to the identification of cultural heritage resources, and we suggest that any engagement with Indigenous communities includes a discussion about known or potential cultural heritage resources that are of value to these communities. Municipal Heritage Committees, historical societies and other local heritage organizations may also have knowledge that contributes to the identification of cultural heritage resources.

Archaeological Resources

This EA project may impact archaeological resources and should be screened using the MHSTCI [Criteria for Evaluating Archaeological Potential](#) to determine if an archaeological assessment is needed. MHSTCI archaeological sites data are available at archaeology@ontario.ca. If the EA project area exhibits archaeological potential, then an archaeological assessment (AA) should be undertaken by an archaeologist licenced under the OHA, who is responsible for submitting the report directly to MHSTCI for review.

Built Heritage and Cultural Heritage Landscapes

The MHSTCI [Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes](#) should be completed to help determine whether this EA project may impact cultural heritage resources. If potential or known heritage resources exist, MHSTCI recommends that a Heritage Impact Assessment (HIA), prepared by a qualified consultant, should be completed to assess potential project impacts. Our Ministry's [Info Sheet #5: Heritage Impact Assessments and Conservation Plans](#) outlines the scope of HIAs. Please send the HIA to MHSTCI for review and make it available to local organizations or individuals who have expressed interest in review.

Environmental Assessment Reporting

All technical cultural heritage studies and their recommendations are to be addressed and incorporated into EA projects. Please advise MHSTCI whether any technical cultural heritage studies will be completed for this EA project, and provide them to MHSTCI before issuing a Notice of Completion or commencing any work on the site. If screening has identified no known or potential cultural heritage resources, or no impacts to these resources, please include the completed checklists and supporting documentation in the EA report or file.

Thank you for consulting MHSTCI on this project and please continue to do so throughout the EA process. If you have any questions or require clarification, do not hesitate to contact me.

Sincerely,

Joseph Harvey
Heritage Planner
joseph.harvey@Ontario.ca

Copied to: Shun H. Cheung, Project Manager, Town of Caledon
Connor MacIsaac, Junior Environmental Planner, R.V. Anderson Associates Limited

It is the sole responsibility of proponents to ensure that any information and documentation submitted as part of their EA report or file is accurate. MHSTCI makes no representation or warranty as to the completeness, accuracy or quality of the any checklists, reports or supporting documentation submitted as part of the EA process, and in no way shall MHSTCI be liable for any harm, damages, costs, expenses, losses, claims or actions that may result if any checklists, reports or supporting documents are discovered to be inaccurate, incomplete, misleading or fraudulent.

Please notify MHSTCI if archaeological resources are impacted by EA project work. All activities impacting archaeological resources must cease immediately, and a licensed archaeologist is required to carry out an archaeological assessment in accordance with the *Ontario Heritage Act* and the *Standards and Guidelines for Consultant Archaeologists*.

If human remains are encountered, all activities must cease immediately and the local police as well as the Registrar, Burials of the Ministry of Government and Consumer Services (416-326-8800) must be contacted. In situations where human remains are associated with archaeological resources, MHSTCI should also be notified to ensure that the site is not subject to unlicensed alterations which would be a contravention of the *Ontario Heritage Act*.

From: Barboza, Karla (MHSTCI) <Karla.Barboza@ontario.ca> on behalf of Registrar (MHSTCI) <Registrar@ontario.ca>
Sent: December 2, 2020 5:00 PM
To: Meredith Stewart
Cc: Winnie Wong; 'jdoes@rvanderson.com' <jdoes@rvanderson.com>; Johanna Kelly; Registrar (MHSTCI)
Subject: MHSTCI Response: Information Request - Mill Street and Humber Station Road CHRAs - Town of Caledon
Attachments: 20CH127_Fig1.jpg; 20CH120_Fig1.jpg
Categories: Filed by Newforma

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File 0006107 – Mill Street EA from Mississauga Road to Creditview Road
File 0013276 – Humber Station Road EA from Castlederg Sideroad to Healey Road

Hi Meredith,

I hope this email finds you well.

As you may know, the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) developed screening checklists to assist property owners, developers, consultants and others to identify known and potential cultural heritage resources:

- [Criteria for Evaluating Archaeological Potential](#)
- [Criteria for Evaluating Marine Archaeological Potential](#)
- [Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes](#)

I have used the document above (Built Heritage and Cultural Heritage Landscapes) in order to respond to your question:

- Question 3a. i. Is the property (or project area) identified, designated or otherwise protected under the Ontario Heritage Act as being of cultural heritage value e.g. a property that is designated by order of the Minister of Heritage, Sport, Tourism and Culture Industries as being of cultural heritage value or interest of provincial significance [s.34.5]?
MHSTCI Response: To date, no properties have been designated by the Minister.
- Question 3a.v. Is the property (or project area) identified, designated or otherwise protected under the Ontario Heritage Act as being of cultural heritage value included in the Ministry of Heritage, Sport, Tourism and Culture Industries' list of provincial heritage properties?
MHSTCI Response: At this time, MHSTCI is not aware of any provincial heritage properties within or adjacent to the study area or in the Town of Caledon.

Please note that if the subject lands or parts of the subject lands are owned or controlled by an Ontario Ministry or Prescribed Public Body (PPB) on behalf of the Crown (the list of PPBs is available as O. Reg. 157/10), a

Ministry or PPB may have responsibilities under the [Standards and Guidelines for Conservation of Provincial Heritage Properties](#).

MHSTCI would appreciate if any technical cultural heritage studies (e.g. Cultural Heritage Assessment Report, Cultural Heritage Evaluation Report, Heritage Impact Assessment) be sent for MHSTCI review.

I hope this helps. Let me know if you have any questions.

Regards,
Karla

Karla Barboza MCIP, RPP, CAHP | (A) Team Lead, Heritage
Ministry of Heritage, Sport, Tourism and Culture Industries
Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit
T. 416.314.7120 | Email: karla.barboza@ontario.ca

From: Meredith Stewart <MStewart@asiheritage.ca>

Sent: December-01-20 1:25 PM

To: Registrar (MHSTCI) <Registrar@ontario.ca>

Cc: Johanna Kelly <jkelly@asiheritage.ca>; 'winnie.wong@rvanderson.com' <winnie.wong@rvanderson.com>; 'jdoes@rvanderson.com' <jdoes@rvanderson.com>

Subject: Mill Street and Humber Station Road CHRAs - Town of Caledon

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Good afternoon,

ASI has been retained by R.V. Anderson Associates Ltd. to complete two Cultural Heritage Resource Assessments as part of the Growth Related Roads program in the Town of Caledon; Mill Street (identified for construction in 2022) and a section of Humber Station Road (part of Phase II, identified for construction in 2023). I have attached two maps that outline the locations of the study areas where we will be conducting these assessments.

In addition to consulting municipal (Caledon Heritage Register, the Town of Caledon CHL Inventory, Heritage Caledon's Walking Tours, and the Village of Bolton Heritage Conservation District Plan), I have consulted the Ontario Heritage Trust's website to locate any conservation easements, plaques, places of worship, or properties on the Ontario Heritage Register within the study area. Resulting from this consultation, the following properties have been identified by the Town to have cultural heritage value within the study areas:

Previously identified cultural heritage properties within the Mill Street study area:

Designated under Part IV of the *Ontario Heritage Act*

- 14360 Creditview Road
- 14376 Creditview Road
- 14377 Creditview Road
- 14386 Creditview Road (also included on Canada's Register of Historic Places)
- 14396 Creditview Road
- 14404 Creditview Road
- 14409 Creditview Road
- 14411 Creditview Road
- 1402 Mill Street
- 1406 Mill Street
- 1499 Mill Street
- 14370 Mississauga Road

Listed on the Town of Caledon Heritage Register

- 14365 Creditview Road
- 14387 Creditview Road
- 14427 Creditview Road
- 1431 Mill Street
- 1443 Mill Street

The study area is also within the boundary of the Town of Caledon's Cheltenham and the Brickworks Cultural Heritage Landscape (CHL)

Previously identified cultural heritage properties within the Humber Station Road study area:

Listed on the Town of Caledon Heritage Register

- 13068 Humber Station Road
- 13297 Humber Station Road
- 13344 Humber Station Road
- 14436 Humber Station Road

I am reaching out today to confirm that I haven't missed any known cultural heritage resources in my search. Does the Ministry have any additional cultural heritage resources or concerns within the study area to bring to our attention for consideration in preparing this assessment?

Regards,
Meredith

Meredith Stewart, MA, MSc, CAHP Intern
Cultural Heritage Assistant • Cultural Heritage Division



ASI • Providing Archaeological & Cultural Heritage Services
MStewart@asiheritage.ca • 416 966 1069 x290 • Fax: 416 966 9723
528 Bathurst Street, Toronto, Ontario, M5S 2P9 • asiheritage.ca

Connor MacIsaac

From: Jessica Lytle <jlytle@asiheritage.ca>
Sent: March 28, 2022 4:11 PM
To: Catherine Kitchen
Subject: FW: ENTERED INTO REGISTER: Archaeological Report for P1066-0178-2020 / *
[20EA-135]
Attachments: ENTERED INTO REGISTER Archaeological Report for P1066-0178-2020.pdf

From: pastport <pastport@ontario.ca>
Sent: March 28, 2022 4:10:00 PM (UTC-05:00) Eastern Time (US & Canada)
To: Jessica Lytle <jlytle@asiheritage.ca>
Cc: shun.cheung@caledon.ca <shun.cheung@caledon.ca>; Winnie.Wong@rvanderson.com
<Winnie.Wong@rvanderson.com>; PastPort@ontario.ca <PastPort@ontario.ca>
Subject: ENTERED INTO REGISTER: Archaeological Report for P1066-0178-2020 / *

Dear Jessica Lytle,

The Original report for PIF P1066-0178-2020, submitted by you as a condition of your licence, has been entered into the *Ontario Public Register of Archaeological Reports* without technical review. Please refer to the attached letter.

Note: the ministry makes no representation or warrant as to the completeness, accuracy or quality of reports in the register.

Development proponents and approval authorities: the Ontario Ministry Heritage, Sport, Tourism, Culture Industries has copied you on this email as you have been identified by the consultant archaeologist as either the proponent or approval authority for this project.

Please do not reply to this e-mail. The message will be undeliverable and we are unable to respond from this address.

If you have any questions about this report email us at: Archaeology@ontario.ca

Thank you,

Jessica Marr

Jessica.Marr@ontario.ca

**Ministry of Heritage, Sport, Tourism, and
Culture Industries**

Archaeology Program Unit
Programs and Services Branch
Heritage, Tourism and Culture Division
5th Floor, 400 University Ave.
Toronto ON M7A 2R9
Tel.: (416) 414-7787
Email: Jessica.Marr@ontario.ca

**Ministère des Industries du patrimoine, du sport, du
tourisme et de la culture**

Unité des programme d'archéologie
Direction des programmes et des services
Division du patrimoine, du tourisme et de la culture
5e étage, 400 ave. University
Toronto ON M7A 2R9
Tél. : (416) 414-7787
Email: Jessica.Marr@ontario.ca



Mar 28, 2022

Jessica Lytle (P1066)
ASI Archaeological and Cultural Heritage Services
200 - 2321 Fairview Burlington ON L7R 2E3

RE: Entry into the Ontario Public Register of Archaeological Reports: Archaeological Assessment Report Entitled, "STAGE 1 ARCHAEOLOGICAL ASSESSMENT MILL STREET RECONSTRUCTION LOTS 29-30, CONS 3-5 WCR (FORMER TOWNSHIP OF CHINGUACOUSY, COUNTY OF PEEL) TOWN OF CALEDON, REGIONAL MUNICIPALITY OF PEEL, ONTARIO", Dated Mar 25, 2022, Filed with MHSTCI Toronto Office on N/A, MHSTCI Project Information Form Number P1066-0178-2020, MHSTCI File Number 0006107

Dear Ms. Lytle:

The above-mentioned report, which has been submitted to this ministry as a condition of licensing in accordance with Part VI of the *Ontario Heritage Act*, R.S.O. 1990, c 0.18, has been entered into the Ontario Public Register of Archaeological Reports without technical review.¹

Please note that the ministry makes no representation or warranty as to the completeness, accuracy or quality of reports in the register.

Should you require further information, please do not hesitate to send your inquiry to Archaeology@Ontario.ca

cc. Archaeology Licensing Officer
Winnie Wong, R.V. Anderson and Associates Ltd
Shun Cheung, Town of Caledon

¹In no way will the ministry be liable for any harm, damages, costs, expenses, losses, claims or actions that may result: (a) if the Report(s) or its recommendations are discovered to be inaccurate, incomplete, misleading or fraudulent; or (b) from the issuance of this letter. Further measures may need to be taken in the event that additional artifacts or archaeological sites are identified or the Report(s) is otherwise found to be inaccurate, incomplete, misleading or fraudulent.

Connor MacIsaac

From: Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>
Sent: March 6, 2023 12:36 PM
To: Connor MacIsaac
Cc: Shun Cheung; Winnie Wong; Meredith Stewart; Andrew McGregor
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report [MCM File 0006106]

Categories: Technical Agency Comments

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Hi Connor,

I have reviewed the report and am writing to confirm that we have no concerns with its content. All the best on the larger study.

Dan Minkin | Heritage Planner
Ministry of Citizenship and Multiculturalism
Citizenship, Inclusion and Heritage Division | Heritage Branch | Heritage Planning Unit
T. 416. 786.7553 | Email: dan.minkin@ontario.ca

Effective October 17, 2022, units responsible for cultural heritage matters have been transferred from the Ministry of Tourism, Culture and Sport (MTCSS) to the Ministry of Citizenship and Multiculturalism (MCM). Responsibility for the Ontario Heritage Act and associated Provincial functions is now held by MCM. Individual staff roles and contact information remain unchanged.

From: Connor MacIsaac <cmaclsaac@rvanderson.com>
Sent: January 13, 2023 2:46 PM
To: Barboza, Karla (MCM) <Karla.Barboza@ontario.ca>
Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>; Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>; Meredith Stewart <MStewart@asiheritage.ca>; Andrew McGregor <AMcGregor@rvanderson.com>
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report [MCM File 0006106]

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Hi Karla,

Looks like we can share the Draft report this week! Please find the draft Cultural Heritage Report: Existing Conditions Report and Preliminary Impact Assessment for MCM's review and input attached.

Best,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



From: Connor MacIsaac

Sent: January 13, 2023 11:27 AM

To: Barboza, Karla (MCM) <Karla.Barboza@ontario.ca>

Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>; Minkin, Dan (MHSTCI) <dan.minkin@ontario.ca>; Meredith Stewart <MStewart@asiheritage.ca>; Andrew McGregor <AMcGregor@rvanderson.com>

Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report [MCM File 0006106]

Hi Karla,

Thank you, and we have updated our study contact list appropriately. We are just finalizing the preliminary impact assessment portion of the Cultural Heritage Report based on the study recommendations and are expecting to be able to share the draft report with the MCM next week.

Best,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



From: Barboza, Karla (MCM) <Karla.Barboza@ontario.ca>

Sent: January 13, 2023 11:00 AM

To: Connor MacIsaac <cmaclsaac@rvanderson.com>

Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>; Minkin, Dan (MHSTCI) <dan.minkin@ontario.ca>

Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report [MCM File 0006106]

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Connor,

Thanks for sending the Notice of Study Recommendations and PIC No. 2 Summary Report for the above referenced project to the Ministry of Citizenship and Multiculturalism (MCM).

Please note that there has been some changes in our office. Rosi Zirger is no longer with our ministry and can be removed from your contact list. Please ensure that both Dan Minkin, MCM Heritage Planner, and I are included in this project's contact list.

Could you please advise what the status is of a Cultural Heritage Report (or related study)?

Let me know if you have any questions.

Thanks,
Karla

Karla Barboza, RPP, MCIP, CAHP

Team Lead, Heritage | Heritage Planning Unit | Ministry of Citizenship and Multiculturalism | 416-660-1027 | karla.barboza@ontario.ca

From: Connor MacIsaac <cmacisaac@rvanderson.com>

Sent: January-12-23 3:14 PM

Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>

Subject: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Dear Sir/Madam,

On behalf of the Town, please find attached the Notice of Study Recommendations for the Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA). The purpose of this notice is to garner feedback on the recommendations for Mill Street developed in consideration of the comments received during and following the second Public Information Centre (PIC), held on June 15, 2022. A detailed summary of the comments received at and following the second PIC, and how these comments were incorporated into the development of the study recommendations, is presented in the PIC Summary Report on the Town's website at <https://www.caledon.ca/en/news/mill-street-ea.aspx>.

To provide comments, questions, or concerns on the study recommendations, or the PIC Summary Report, please contact the project team members listed on the attached notice by February 2, 2023.

Upon completion of the PIC Summary Report comment period, a Project File Report (PFR), containing details of the Municipal Class EA planning and decision-making process, public consultation process and study recommendations will be made available for a 30-day public review period, concluding the EA study. A Notice of Completion will be issued at that stage.

Thank you again for your continued input to the study.

Best Regards,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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Connor MacIsaac

From: Connor MacIsaac
Sent: November 11, 2020 9:56 AM
To: Rizwan, Mishaal
Cc: Winnie Wong
Subject: FW: [External] FW: 205388 & 205421 Caledon Growth Related Roads

Categories: Technical Agency Comments, Filed by Newforma

Hi Mishaal,

- Project name: Caledon Growth Related Roads
- Proponent's name: Town of Caledon
- User's name: R.V. Anderson Associates Limited
- Intended use and publications: for stormwater management review and natural environmental inventory review

Thanks, Connor



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Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

R.V. Anderson Associates Limited

43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

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From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: November 11, 2020 9:55 AM
To: Connor MacIsaac <cmacisaac@rvanderson.com>
Subject: RE: [External] FW: 205388 & 205421 Caledon Growth Related Roads

Hi Connor,

I'm not sure if my email will go through or not – can you forward the following to Mishaal please:

- Project name: Caledon Growth Related Roads
- Proponent's name: Town of Caledon
- User's name: R.V. Anderson Associates Limited
- Intended use and publications: for stormwater management review and natural environmental inventory review

Thanks,
Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager

R.V. Anderson Associates Limited

From: Rizwan, Mishaal <mishaal.rizwan@cvc.ca>
Sent: November 11, 2020 9:32 AM
To: Connor MacIsaac <cmacisaac@rvanderson.com>
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: RE: [External] FW: 205388 & 205421 Caledon Growth Related Roads

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Hi Connor,

In order to process the data request, please confirm the following information:

- Project name: Caledon Growth Related Roads
- Proponent's name: Town of Caledon
- User's name: R.V. Anderson Associates Limited
- Intended use and publications:

Best regards,

Mishaal Rizwan
Technician, Planning | Planning and Development Services | Credit Valley Conservation
905-670-1615 ext 220 | 1-800-668-5557
mishaal.rizwan@cvc.ca | cvc.ca

From: Slaght, Tyler <Tyler.Slaght@cvc.ca>
Sent: Tuesday, November 10, 2020 11:40 AM
To: Rizwan, Mishaal <mishaal.rizwan@cvc.ca>
Subject: FW: [External] FW: 205388 & 205421 Caledon Growth Related Roads

From: Connor MacIsaac <cmacisaac@rvanderson.com>
Sent: Tuesday, November 10, 2020 11:16 AM
To: Slaght, Tyler <Tyler.Slaght@cvc.ca>
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: FW: [External] FW: 205388 & 205421 Caledon Growth Related Roads

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Tyler,

Not sure if Winnie's email went through...



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Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

R.V. Anderson Associates Limited

43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

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From: Winnie Wong <Winnie.Wong@rvanderson.com>

Sent: November 10, 2020 10:51 AM

To: Slaght, Tyler <Tyler.Slaght@cvc.ca>; Connor MacIsaac <cmaclsaac@rvanderson.com>

Subject: RE: [External] FW: 205388 & 205421 Caledon Growth Related Roads

Hi Tyler,

Hoping this goes through – please see attached maps and below for roads under CVC jurisdiction:

- Kennedy Road – from Beechgrove Sdrd to Highpoint Sdrd (CVC jurisdiction)
- Main Street – Highpoint Sdrd to Caledon East Garafraxa Townline (CVC jurisdiction)
- Mill Street – from Creditview Road to Mississauga Road (CVC jurisdiction)
- Humber Station Rd – from Healey Rd to Castlederg Sdrd (not under CVC jurisdiction)
- Mountainview Rd – from Granite Stones Dr to Charleston Sdrd (under CVC jurisdiction from Escarpment Sdrd to Charleston Sdrd)
- Willoughby Rd – from Charleston Sdrd to Beechgrove Sdrd (CVC jurisdiction)

Regards,

Winnie Wong, P. Eng., PMP, M.Eng.

Project Manager

R.V. Anderson Associates Limited

From: Slaght, Tyler <Tyler.Slaght@cvc.ca>

Sent: November 10, 2020 10:45 AM

To: Connor MacIsaac <cmaclsaac@rvanderson.com>

Cc: Winnie Wong <Winnie.Wong@rvanderson.com>

Subject: RE: [External] FW: 205388 & 205421 Caledon Growth Related Roads

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Connor,

I did get it, can you please provide a map of the roads in question?

Thanks,

Tyler Slaght, RPP
Regulations Officer | Credit Valley Conservation
905-670-1615 ext 406 | C: 647-286-7427 | 1-800-668-5557
tyler.slaght@cvc.ca | cvc.ca

Please be advised that I am currently working remotely and can be reached at 647-286-7427.

From: Connor MacIsaac <cmaclsaac@rvanderson.com>
Sent: Tuesday, November 10, 2020 10:31 AM
To: Slaght, Tyler <Tyler.Slaght@cvc.ca>
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: [External] FW: 205388 & 205421 Caledon Growth Related Roads

[CAUTION] This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe. If in doubt contact help211@cvc.ca

Hi Tyler,

Please see correspondence from Winnie regarding the two ongoing Class EAs in the Town of Caledon.

Please confirm that you have received this email.

Thanks,



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Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

R.V. Anderson Associates Limited

43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

www.rvanderson.com



From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: November 10, 2020 10:21 AM
To: Connor MacIsaac <cmaclsaac@rvanderson.com>
Subject: FW: 205388 & 205421 Caledon Growth Related Roads

Morning Connor,

Can I ask you to forward the email below to Tyler please? For some reason my emails aren't getting through to him and he mentioned the last RVA contact was from you back in August.

Please let me know when the email has been forwarded so I can follow up with him to make sure it has gone through.

Thanks!

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Winnie Wong
Sent: November 6, 2020 11:56 AM
To: tyler.slaght@cvc.ca
Subject: FW: 205388 & 205421 Caledon Growth Related Roads

Hi Tyler,

Thanks for chatting this morning. Please let us know when will be a good time for you and your team for a project initiation meeting.

Best,

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Winnie Wong
Sent: October 30, 2020 4:40 PM
To: tyler.slaght@cvc.ca
Cc: Oliver Olberg <OOlberg@rvanderson.com>; Naomi Vaset <NVaset@rvanderson.com>; John P. Does <jdoes@rvanderson.com>
Subject: RE: 205388 & 205421 Caledon Growth Related Roads

Hi Tyler,

Just following up with you re my email below. We would like to schedule a project initiation meeting with CVC soon to review the project scope of work and verify CVC's expectations for the permit application. Please let me know when will be a convenient time for you and your team and I will set up the meeting.

In addition, we would like to request the following information from CVC in advance of the meeting:

- Existing HEC-RAS model
- LIDAR information
- Drainage contour (in CAD shape files, preferably 1m intervals)
- Any existing drainage area plans, hydrologic, and hydraulic modelling files

Please let me know if we have to sign a data licensing agreement.

Thanks and have a great weekend!

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Winnie Wong
Sent: October 15, 2020 9:17 AM

To: tyler.slaght@cvc.ca

Cc: Oliver Olberg <OOlberg@rvanderson.com>; Naomi Vaset <NVaset@rvanderson.com>; John P. Does <jdoes@rvanderson.com>

Subject: 205388 & 205421 Caledon Growth Related Roads

Morning Tyler,

Hope this email finds you well! Thanks for your help on the 2019 Caledon Growth Related Roads – I am now starting their 2022 and 2023 projects of similar scope. Both projects, like the 2019, involves shoulder widening and overall drainage improvements through replacing road crossing culverts and ditch cleanout and regrading.

We would like to schedule an initiation meeting with you and your team in the next two weeks to go over the scope of work and CVC's requirements/ expectations re permit approvals. Please let me know when will be a good time for you and I'll set up the meeting.

Thanks,



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Winnie Wong, P.Eng., PMP, M.Eng.

Project Manager

P: (416) 497-8600 ext. 1471

C: (647) 390-5328

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2001 Sheppard Avenue East, Suite 300 Toronto, ON M2J 4Z8

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Transmittal

PROJECT: 2022 Roads Rehab 205388 DATE: 2021-06-07

SUBJECT: PCSWMM/ HEC-RAS modeling files TRANSMITTAL ID: 00001

PURPOSE: For your review and comment VIA: Info Exchange

FROM

NAME	COMPANY	EMAIL	PHONE
Winnie Wong		Winnie.Wong@rvanderson.com	

TO

NAME	COMPANY	EMAIL	PHONE
Tyler.Slaght@cvc.ca		Tyler.Slaght@cvc.ca	

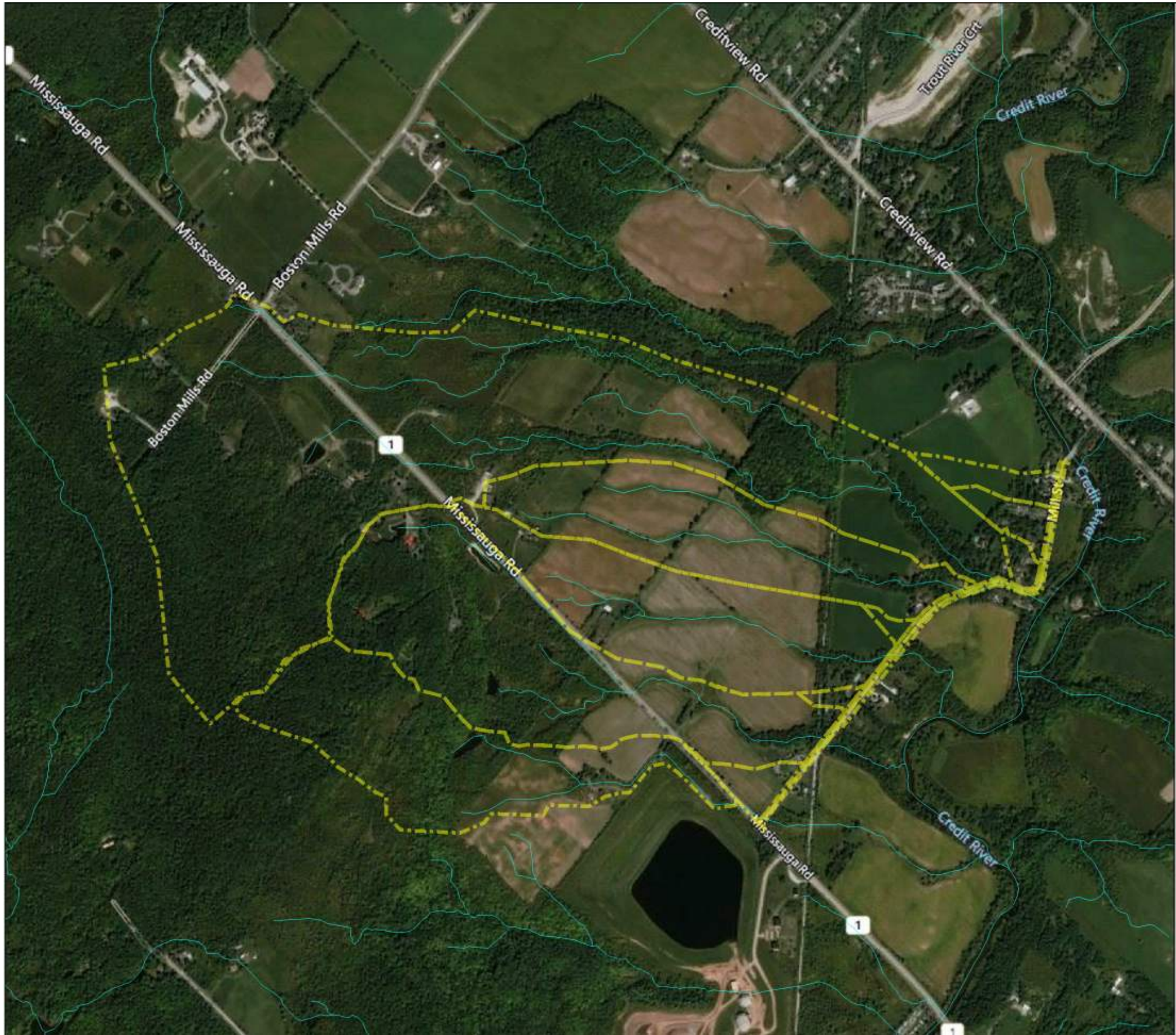
REMARKS:

DESCRIPTION OF CONTENTS


QTY	DATED	TITLE	NOTES
1	2021-06-04	Caledon PCSWMM and HEC-RAS Modelling Files- for CVC review	
1	2021-06-07	Mill St - two culverts proposed to be realigned	

COPIES:

Naomi Vaset (R.V. Anderson Associates Limited)
Matthew De Wit (R.V. Anderson Associates Limited)



Legend

 Subcatchments



0.5 km

NOTES

00	Preliminary Design Report – Mill Street	w.w.	2021.04.27	
No.	Description	By	YYYY.MM.DD	

Project Status



Town of Caledon



Growth Related Roads 2022- Mill Street

Project No:	205388.0	Designed:	N.V.
Date:	27/04/2021	Checked:	M.A.D
Scale:	AS SHOWN	Drawn:	N.V.

EXTERNAL DRAINAGE AREAS
OVERALL PLAN

DWG NO.
Fig 1
REV. 00



Legend

- EXT-1 ← Subcatchment ID
- Area=5 ← Area (ha)
- CN=74 ← Curve Number (CN)



150 m

File: 2023-09-25 11:18:36 AM

NOTES

00	Preliminary Design Report – Mill Street	w.w.	2021.04.27	
No.	Description	By	YYYY.MM.DD	

Project Status



Town of Caledon

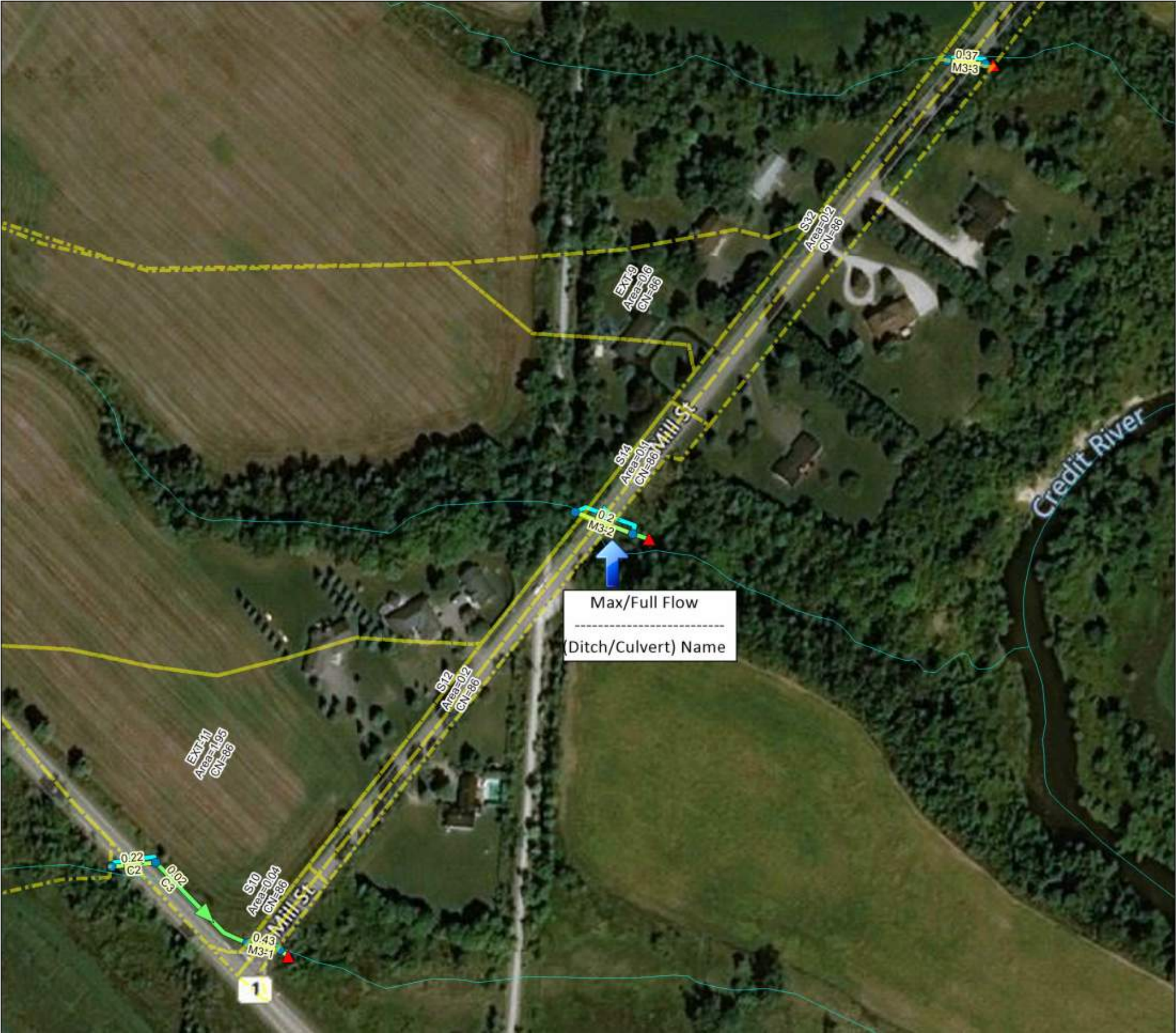


Growth Related Roads 2022- Mill Street

Project No:	205388.0	Designed:	N.V.
Date:	27/04/2021	Checked:	M.A.D
Scale:	AS SHOWN	Drawn:	N.V.

SUBCATCHMENT AREAS

DWG NO.
Fig 3
REV. 00



Legend

- Junctions
- ▲ Outfalls

Conduits - Max/Full Flow

0

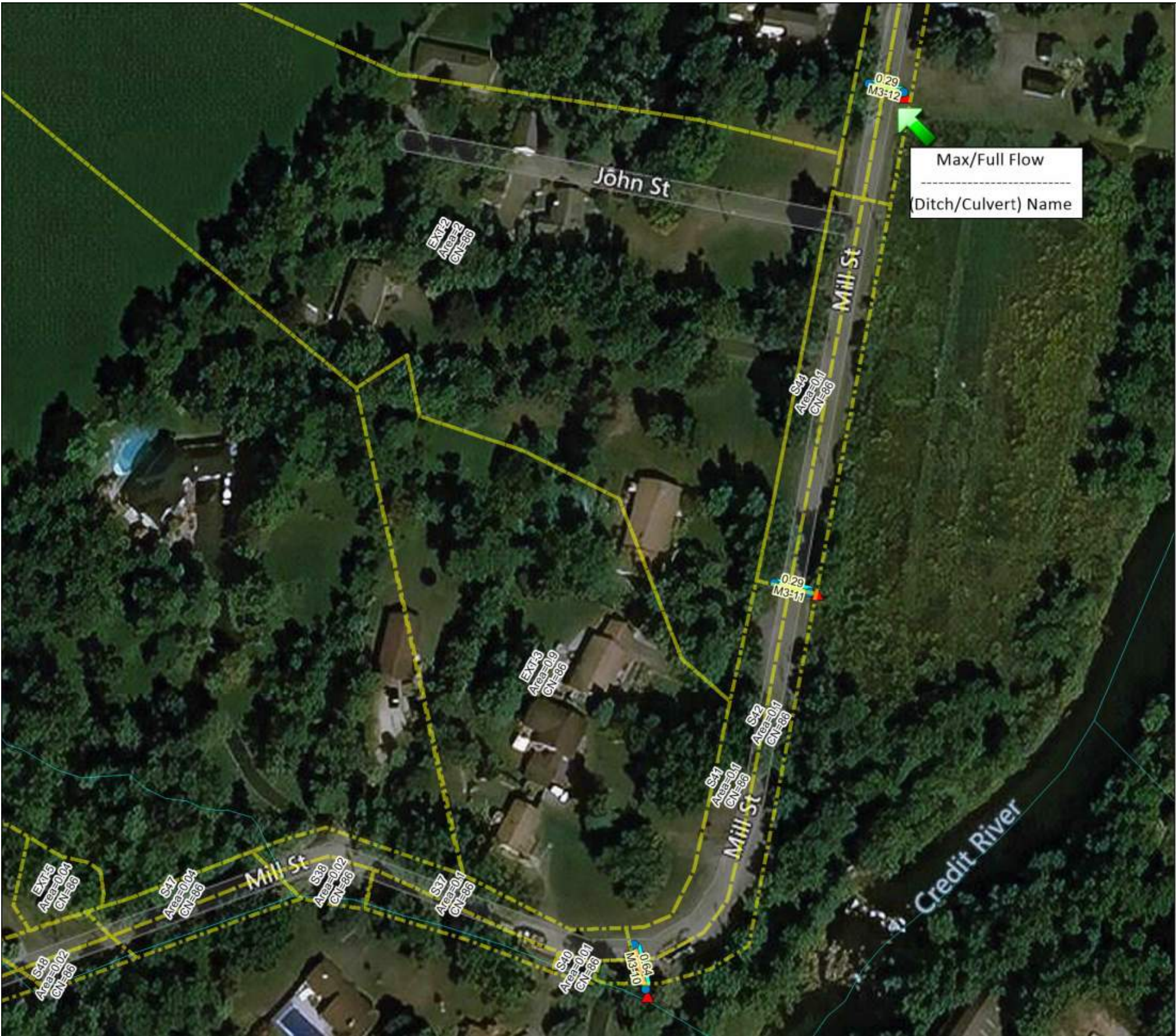
1

EXT-1
Area=5
CN=74

← Subcatchment ID
← Area (ha)
← Curve Number (CN)

N

50 m



Legend

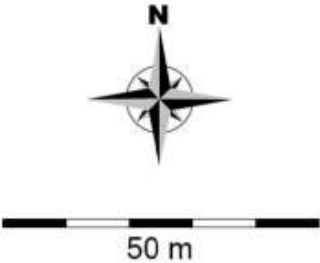
- Junctions
- Outfalls
- Conduits - Max/Full Flow

0

1

EXT-1
Area=5
CN=74

← Subcatchment ID
← Area (ha)
← Curve Number (CN)



NOTES

The "max/full flow" shown on the Figures represents Culvert/Ditch capacity utilization under 50-year storm event

00	Preliminary Design Report – Mill Street	w.w.	2021.04.27	
No.	Description	By	YYYY.MM.DD	

Project Status



Town of Caledon



Growth Related Roads 2022- Mill Street

Project No:	205388.0	Designed:	N.V.
Date:	27/04/2021	Checked:	M.A.D
Scale:	AS SHOWN	Drawn:	N.V.

CULVERTS AND DITCHES CAPACITY UTILIZATION

DWG NO.
Fig 5
REV. 00

Table 1 – Mill Street Proposed Minor Culverts

Culvert ID	Station	Length (m)	Size (mm)	Material	Slope (m/m)	Road Shoulder Elevation (m)	50-year			100-year		
							Max. Flow (Full Flow) (m³/s)	Max/Ful Flow	Max. U/S HGL (Freeboard) (m)	Max. Flow (Full Flow) (m³/s)	Max/Ful Flow	Max. U/S HGL (m)
M3-1	1+017	21.6	2x600	HDPE	0.057	277.95	1.28 (1.47)	43%	277.63 (0.32)	1.44 (1.47)	49%	277.63
M3-2	1+276	32.4	1600	CSP	0.024	273.51	1.394 (7.10)	20%	267.42 (6.09)	1.79 (7.10)	25%	267.54
M3-3	1+556	21.0	800	HDPE	0.046	267.14	1.036 (2.82)	37%	266.56 (0.58)	1.24 (2.82)	44%	266.9
M3-10	1+988	18.4	450	HDPE	0.010	256.76	0.18 (0.28)	64%	255.52 (1.24)	0.22 (0.28)	76%	255.55
M3-11	2+102	13.7	600	HDPE	0.017	257.90	0.189 (0.65)	29%	257.31 (0.59)	0.24 (0.65)	36%	257.38
M3-12	2+240	13.7	600	HDPE	0.012	257.70	0.163 (0.56)	29%	257.00 (0.70)	0.20 (0.56)	36%	257.06

Connor MacIsaac

From: Slaght, Tyler <Tyler.Slaght@cvc.ca>
Sent: May 11, 2021 10:10 AM
To: Winnie Wong
Cc: Shun Cheung; Marina Leonov; Matthew de Wit; Naomi Vaset; John P. Does; Mercedes Hincapie; Yevgenia Gubarev; Deborah Weeden
Subject: RE: [External] 2020-127 & 2020-97 - Caledon Growth Related Roads
Categories: Filed by Newforma

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Hi Winnie,

That guide is acceptable to use. Please also refer to [CVC's standard notes](#).

Please note that we are reviewing the 30% design and anticipate that will have comments to provide to you.

Regards,

Tyler Slaght, RPP
Senior Regulations Officer | Credit Valley Conservation
905-670-1615 ext 406 | C: 647-286-7427 | 1-800-668-5557
tyler.slaght@cvc.ca | cvc.ca

Please be advised that I am currently working remotely and can be reached at 647-286-7427.

From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: Monday, May 10, 2021 6:07 PM
To: Slaght, Tyler <Tyler.Slaght@cvc.ca>
Cc: Shun Cheung <shun.cheung@caledon.ca>; Marina Leonov <mleonov@rvanderson.com>; Matthew de Wit <MdeWit@rvanderson.com>; Naomi Vaset <nvaset@rvanderson.com>; John P. Does <jdoes@rvanderson.com>; Mercedes Hincapie <MHincapie@rvanderson.com>; Yevgenia Gubarev <YGubarev@rvanderson.com>
Subject: [External] 2020-127 & 2020-97 - Caledon Growth Related Roads

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Hi Tyler,

We are advancing our design for Caledon and looking into ESC measures. We are planning to following the guideline found here:

Is there another guideline that CVC prefer to see for both Caledon projects?

Thanks,



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Winnie Wong, P.Eng., PMP, M.Eng.

Project Manager

P: (416) 497-8600 ext. 1471

C: (647) 390-5328

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Connor MacIsaac

From: Slaght, Tyler <Tyler.Slaght@cvc.ca>
Sent: September 24, 2021 1:03 PM
To: Winnie Wong
Cc: Shun Cheung; Connor MacIsaac; Andrew McGregor; Naomi Vaset; Matthew de Wit; Marina Leonov
Subject: RE: [External] 2020-127 & 2020-97 Caledon 60% drawings

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Hi Winnie,

Thank you for the email. CVC staff had previously identified that additional technical studies would be required including an erosion assessment to delineate and confirm that the erosion hazard limits would not change a result of any proposed alternative. CVC also indicated that a natural heritage review would be required as well. These are typical aspects of a Schedule B EA. Additionally, CVC has not seen a summary of alternatives nor been able to comment on the evaluation of alternatives, again typical aspects of a Schedule B EA.

To date the focus seems to be on storm water management/flooding aspect the project and there are other important factors that seem to not be considered or supporting information has yet to be provided. This is concerning, particularly as that information may inform whether or not CVC can support particular design alternatives.

Regards,

I'm working remotely. The best way to reach me is by email, mobile phone or Microsoft Teams.

Tyler Slaght | RPP |
Senior Regulations Officer, Planning and Development Services | Credit Valley Conservation
905-670-1615 ext 406 | M: 647-286-7427
tyler.slaght@cvc.ca | cvc.ca



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From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: Thursday, September 23, 2021 3:49 PM
To: Slaght, Tyler <Tyler.Slaght@cvc.ca>
Cc: Shun Cheung <shun.cheung@caledon.ca>; Connor MacIsaac <cmaclsaac@rvanderson.com>; Andrew McGregor <AMcGregor@rvanderson.com>; Naomi Vaset <nvaset@rvanderson.com>; Matthew de Wit

<MdeWit@rvanderson.com>; Marina Leonov <mleonov@rvanderson.com>

Subject: RE: [External] 2020-127 & 2020-97 Caledon 60% drawings

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Hi Tyler,

We are waiting for the comments from the PIC before finalizing our drawings for Mill Street. We have prepared some roll plans and cross section to be presented at the PIC; the recommended alternative is to put curb and gutter on Mill Street and new stormsewer along the north side to direct surface runoff from the road away from private properties. Roll plan and cross section to be presented at PIC will be forwarded to you via Newforma Info Exchange shortly.

While the roadworks design may change depending on the outcome of the PIC, the following stormwater management improvements are proposed for all alternatives and we would like CVC comments:

- Proposed realignment of the two culverts at Sta. 1+880 and 1+882
- Shifting the ditch towards west at 1347 Mill Street, Kleinburg and upsizing the driveway culverts in this regulated ditch (1331 Mill Street, 1347 Mill Street, 1357 Mill Street)

The SWM files we forwarded back in June via Newforma Info Exchange contains the proposed culvert and ditch realignment details. Please let me know if you would like an updated link to the files.

Thanks,

Winnie Wong, P. Eng., PMP, M.Eng.

Project Manager

R.V. Anderson Associates Limited

From: Slaght, Tyler <Tyler.Slaght@cvc.ca>

Sent: September 20, 2021 2:32 PM

To: Winnie Wong <Winnie.Wong@rvanderson.com>

Cc: Shun Cheung <shun.cheung@caledon.ca>

Subject: RE: [External] 2020-127 & 2020-97 Caledon 60% drawings

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Hi Winnie,

Thank you. Will you be providing a submission for Mill Street. We are concerned that the Town is going to the PIC without having provided CVC the necessary supporting technical studies or the justification for the proposed alignment/culvert replacements.

Thanks,

I'm working remotely. The best way to reach me is by email, mobile phone or Microsoft Teams.

[Tyler Slaght](#) | RPP |

Senior Regulations Officer, Planning and Development Services | Credit Valley Conservation
905-670-1615 ext 406 | M: 647-286-7427
tyler.slaght@cvc.ca | cvc.ca



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From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: Monday, September 20, 2021 2:30 PM
To: Slaght, Tyler <Tyler.Slaght@cvc.ca>
Cc: Shun Cheung <shun.cheung@caledon.ca>
Subject: [External] 2020-127 & 2020-97 Caledon 60% drawings

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Hi Tyler,

I've forwarded you a copy of our 60% design drawings, along with our responses to CVC's previous comments via Newforma Info Exchange for the following four roads:

- Kennedy Road
- Main Street
- Mountainview Road
- Willoughby Road

I'll send you the completed application form shortly. Will need to request signatures from Town.

Thanks,



Winnie Wong, P.Eng., PMP, M.Eng. (she/her)

PROJECT MANAGER, TRANSPORTATION

t 416 497 8600 ext. 1471 | m 647 390 5328

a 2001 Sheppard Avenue East, Suite 300, Toronto, ON M2J 4Z8



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Transmittal

PROJECT: 2022 Roads Rehab 205388 DATE: 2021-09-23

SUBJECT: 2020-97 Mill Street PIC Alternatives TRANSMITTAL ID: 00014

PURPOSE: For your review and comment VIA: Info Exchange

FROM

NAME	COMPANY	EMAIL	PHONE
Winnie Wong		Winnie.Wong@rvanderson.com	

TO

NAME	COMPANY	EMAIL	PHONE
Tyler.Slaght@cvc.ca		Tyler.Slaght@cvc.ca	

REMARKS:

DESCRIPTION OF CONTENTS

QTY	DATED	TITLE	NOTES
1	2021-09-22	20210922	

COPIES:

Shun Cheung (Town of Caledon)



Transmittal

PROJECT: 2022 Roads Rehab 205388 DATE: 2021-10-07

SUBJECT: 2020-97 Mill Street PIC Slides TRANSMITTAL ID: 00020

PURPOSE: For your review and comment VIA: Info Exchange

FROM

NAME	COMPANY	EMAIL	PHONE
Winnie Wong		Winnie.Wong@rvanderson.com	

TO

NAME	COMPANY	EMAIL	PHONE
Tyler.Slaght@cvc.ca		Tyler.Slaght@cvc.ca	

REMARKS: Final Mill Street PIC presentation slides.

DESCRIPTION OF CONTENTS

QTY	DATED	TITLE	NOTES
1	2021-10-05	R205388-20210104-PICPresentation-vf-forAODA Compliant.pdf	

COPIES:

Shun Cheung (Town of Caledon)

Connor MacIsaac

From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: February 9, 2023 3:09 PM
To: Connor MacIsaac
Subject: 205388 - 2022 Roads Rehab - File Transfer: 205388 - Mill St drawings

Categories: Technical Agency Comments

Project: 205388 2022 Roads Rehab

Notification about File Transfer **205388 - Mill St drawings**

Note: You have been CC'd on this notification.

Remarks

Hi Tyler,

Thanks for chatting on the phone. Please use this link to download our Mill Street drawings that will be included in our draft Project File Report (PFR) which we will be forwarding to you end of February.

Since our last correspondence, we have revised our recommendations and the key proposed works included in these drawings are as follows:

- rehabilitate road pavement following the existing horizontal and vertical profile
- no pavement shoulder widening, paved platform width to match existing asphalt edges
- no ditch realignment
- replace culverts that do not meet drainage capacity or in poor physical condition
- cleaning out/ localize regrading of the ditches to restore positive drainage

We will be reviewing the following issues in further detail with CVC during our detailed design:

- flooding at 1499 Mill Street
- slope stability at culvert just east of Caledon Trailway
- steep embankment slopes on some culverts

Please note, as we have yet to file the PFR for public review there may be slight modifications to these drawings depending on comments/ feedback we receive. As such we kindly ask CVC to keep these drawings confidential until we are ready to file. We would also like to request a meeting with CVC anytime after end of February, as you've noted there is a backlog currently, so we can present our proposed works.

Regards,
Winnie

[View the Transmittal in Newforma Project Center](#)

Download all files

File Transfer Info

To: Tyler.Slaght@cvc.ca

CC: Shun Cheung (Town of Caledon); John Does (R.V. Anderson Associates Limited); Connor MacIsaac (R.V. Anderson Associates Limited); Andrew McGregor (R.V. Anderson Associates Limited)

Purpose: For your review and comment

Expiration Date: 2023-03-11

Transferred Files

205388-Mill St -P&P-DD-PP-30%.pdf	2022-12-20	5:05 PM	8,599 KB
205388-TS_Mill S-DD-TS.pdf	2022-11-30	5:44 PM	430 KB
205388-XSEC_Mill St-DD-Driveways.pdf	2022-12-01	12:08 PM	250 KB
205388-XSEC_Mill St-DD-x-section.pdf	2022-12-01	12:20 PM	486 KB
Transmittal - 00066.pdf	2023-02-09	3:08 PM	73 KB

Additional Links

[Reply to All](#)

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From: Winnie Wong
Sent: March 13, 2023 1:42 PM
To: Slaght, Tyler; Satgunarajah, Angelin; Shun Cheung
Cc: Golding, George; John P. Does; Matthew de Wit; Mercedes Hincapie
Subject: 2020-97 - Mill Street PFR and PTE requirements

All,

Thanks for meeting with us last Monday, March 6 re our proposed road improvements documented in the Project File Report, which was forwarded to you via Newforma Info Exchange. Please let us know if you have not received the files yet.

A quick recap on our discussion re Mill Street improvements:

- Rehabilitation will match back existing road alignment, profile, and existing pavement edges
- No ditch realignment are proposed
- Flooding issue at 1347 Mill Street and 1499 Mill Street is due to both properties being within the Credit River floodplain, and no proposed culvert/ ditch improvements will solve these flooding issues
- Town is undertaking slope stability analysis for the culvert east of the Caledon Trailway. Waiting for geotechnical recommendations before proceeding with detailed design.
- PFR documents the 30% design of this project. After the PFR is put on public record review for 30 days, our team will continue to work with CVC on the drainage improvements for Mill Street. At our 60% design submission, we will submit a permit application with a SWM memo documenting our hydraulic modeling results to support our proposed culvert/ drainage improvements

As the Town is also going through their property acquisition requirements, we may need PTE from CVC for the road improvements. Can you provide some details on the process for Town requesting PTE from CVC please? The property shows up as 0 Mississauga Road - [@Shun Cheung](#) can you provide the PIN number for this property please?

Thanks,
(205388)

Winnie Wong, P.Eng., PMP, M.Eng. (she / her)

Associate, Project Manager, Transportation



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1471 | m 416 390 5328

[LinkedIn](#) | [Facebook](#) | [Website](#)



Connor MacIsaac

From: Winnie Wong
Sent: March 30, 2023 3:17 PM
To: Slaght, Tyler
Cc: Connor MacIsaac; John P. Does; Shun Cheung; Ahmad, Iftekhar
Subject: RE: CVC File EA 21/003 - Mill Street EA

Thanks Tyler,

We'll continue working with CVC to address your comments below during detailed design stage.

Cheers,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: Slaght, Tyler <Tyler.Slaght@cvc.ca>
Sent: Tuesday, March 28, 2023 2:55 PM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: John P. Does <jdoes@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>; Ahmad, Iftekhar <Iftekhar.Ahmad@cvc.ca>
Subject: CVC File EA 21/003 - Mill Street EA

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Winnie,

Credit Valley Conservation (CVC) have reviewed the draft Project File Report (PFR). In general, there are no concerns with the PFR however there are a number of comments to be addressed at detailed design.

1. Please provide 60% general arrangement design drawings to show the individual crossings details with information such as existing & proposed invert elevation of the culverts, size, and length etc.
2. Section 3.4 of the SWM Design brief indicated that LID locations are to be confirmed by the soil study results at the vicinity of culverts M3-10 and M3-11. CVC will provide comments upon review of the LID design when submitted.
3. For culvert M3-2 (east of Caledon Trailway), slope stability issues are considered and a geotechnical study is proposed to be undertaken as per the draft PFR. CVC will provide comments upon receipt more detailed plans.
4. Appendix 4, Natural Environment Assessment Memo: Section 5.1.2 Excavation, Grading and Industrial Equipment identifies the nature of the work has potential to pollute the watercourse. Please provide a robust ESC plan to avoid polluted runoff reaching the watercourse. For further details, please follow the link below and refer to Erosion and

Sediment Control for Urban Construction (TRCA, 2019). <https://cvc.ca/document/erosion-sediment-control-guideline-for-urban-construction/>

5. Appendix 4, section 5.1.3 indicates the need for in-water works for culvert replacement. Please include a temporary dewatering plan and flow diversion plan on drawings.
6. ESC drawings should include (but not limited to) the following items:
 - Include all staging and stockpiling areas within the plan
 - Delineate the limit of disturbance on the plan
 - A note about the environmental monitoring should be added to the ESC drawing.
7. Please add CVC's standard ESC notes within the plan. Please refer to the following link for these notes: <https://cvc.ca/wp-content/uploads/2017/12/Standard-Notes-for-Drawings-Submitted-for-CVC-Review.pdf>
8. With the submission of detailed design of the proposed culverts, further comments will likely be provided.
9. Additional studies may be required at detailed design to evaluate and demonstrate no impacts to flooding and erosion or to provide appropriate mitigation measures.
10. CVC has previously completed research and have assessed several culverts within the project limits that are priorities for improving fish and wildlife passage. Enhancement of ecosystem connectivity is an objective in the Niagara Escarpment Natural Areas and as such the project should demonstrate improvements to these culverts to achieve enhancement of the NHS. Please have a qualified professional review the CVC Fish and Wildlife Crossing Guideline; a CVC Planning Ecologist can be contacted for further discussion.
 - a. Culvert 2 has been assessed as Very High Priority for enhancing fish passage given that the current culverts is perched by 20cm. The project scope should include improving fish passage and NHS connectivity in this location.
 - b. Culvert 5 has been assessed as High Priority for fish and moderate for wildlife passage given that the culvert is a complete barrier to fish passage (perched >100cm). The project scope should include improved fish and wildlife passage and NHS connectivity at the location.
 - c. Culvert 3 has been assessed as Moderate Priority for improve fish and wildlife passage given that the culvert is currently perched. The project scope should include improving fish and wildlife passage and NHS connectivity in this location.

Based on the draft PFR, it is unclear which culverts are to be replaced, the above comments are to be considered during detailed design if those culverts are proposed to be replaced.

11. It appears that the existing Right of Way will be increased as a result of the project. Please note that portions of this project are within the Regional NHS and any impacts to these features should be avoided, minimized, mitigated, or if required, offset according to the CVC Ecosystem Offsetting Guideline. Although the scale of the work appears to be less than previously anticipated, the comment remains relevant and the proponent should provide details on impact to vegetation within CVC's regulated areas, which should include quantities, species, DBH, and locations. This information should be used in conjunction with CVC's Ecosystem Offsetting Guidelines (Table D-1) to determine offsetting numbers for the loss of trees within regulated areas. Any forthcoming Restoration Plan should be guided by CVC's Plant Selection Guideline and CVC's Healthy Soils Guideline.

12. Given that works are proposed in or near water, it is the responsibility of the proponent to ensure that works, undertakings or activities do not cause the death of fish or cause the harmful alteration, disruption or destruction of fish habitat under the Fisheries Act. Please review the complete list of measures to avoid harm at <http://www.dfo-mpo.gc.ca/pnw-ppe/measures-mesures-eng.html> and implement those that are applicable to the proposed work. If it is not possible to avoid or mitigate impacts, proponents can submit a request for review form to their region's Fish and Fish Habitat Protection Program office (contact info: fisheriesprotection@dfo-mpo.gc.ca or 1 855 852-8320). Please refer to the Fisheries and Oceans Canada (DFO) website for additional information.
13. Roadside grading is proposed near 1+560 and 1+800 where the roadside ditch is also a section of the creeks.
- Please clarify why grading is necessary in this location. CVC does not typically support alteration of watercourses and all effort should be made to avoid and mitigate impact to natural features. The hydraulic benefits of altering the watercourse must be demonstrated to the satisfaction of CVC before staff can support such alterations.
 - Please provide clarification regarding the location of the proposed embankment protection 'Terraslope' and the rationale for using it as opposed to natural slope design. CVC recommends the use of natural material primarily through dense planting of woody vegetation to provide bank stabilization and erosion control.
14. Roadside grading is also proposed near 1+300 on the south side of the road.
- Please clarify why grading is necessary in this location and the extent of grading. CVC does not typically support alteration in watercourses and all effort should be made to avoid and mitigate impact to natural features.
15. Please clearly identify on the plans the locations of all tree hoarding measures.
16. Please clearly identify on the plans the locations of all staging and material stockpiling areas, which should all be outside the dripline of any trees and minimum 30m from a watercourse to the extent possible.
17. It is not clear what the areas of green overlay in Appendix 9 – Recommended Plan and Profile. Please clarify what those areas are and ensure drawings are labeled accordingly.
18. A permit from CVC will be required for the proposed works. In order to apply for a permit, please follow this [link](#). Details regarding how to pay for the permit will be provided once an application has been submitted and a file number has been assigned. A permit fee will be confirmed once detailed design drawings have been submitted.

Please do not hesitate to contact me if you have any questions or concerns.

Regards,

Tyler Slaght | RPP |
Senior Regulations Officer, Planning and Development Services | Credit Valley Conservation

905-670-1615 ext 406 | M: 647-286-7427

tyler.slaght@cvc.ca | cvc.ca



[View our privacy statement](#)

Connor MacIsaac

From: Bell, Trevor (MECP) <Trevor.Bell@ontario.ca>
Sent: October 23, 2020 3:46 PM
To: Connor MacIsaac
Cc: Andrew McGregor; Shun Cheung; John P. Does; Winnie Wong
Subject: RE: Humber Station Road EA - Notice of Commencement

Categories: Filed by Newforma

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good afternoon,

Thank you for your emails regarding Notices of Commencement for the Humber Station Road and Mill Street Class EAs. Please ensure that the Notices, along with the completed Project Information Forms, are submitted to our Central Region EA notifications email address (eanotification.cregion@ontario.ca) in accordance with our EA notifications procedures. I have included information regarding our EA notifications process for your reference below.

Thanks,
Trevor

Trevor Bell | Environmental Planner/Environmental Assessment Coordinator
Project Review Unit, Environmental Assessment and Permissions Branch
Ministry of the Environment, Conservation and Parks
5775 Yonge Street, 8th floor, Toronto ON, M2M 4J1
New Phone: 437-770-3731 | trevor.bell@ontario.ca

Dear Project Team,

Thank you for your email. I would like to inform you of a new process related to providing Class EA notifications to the Ministry of the Environment and Climate Change that is in effect as of May 1, 2018. The information is below. Please read carefully.

Please follow the new process and submit an electronic version of the Notice and completed Project Information Form to the Central Region email (eanotification.cregion@ontario.ca). All Notices of commencement and completion are to follow the new process. If you have any questions, please feel free to contact me. Please feel free to pass along this information to your colleagues. Thank you.

New Notification Procedure:

The Ministry of the Environment and Climate Change becomes aware of streamlined environmental assessments (e.g., class environmental assessment projects, electricity projects and waste management projects) through notifications by project owners. Notifying the ministry is an important step in the streamlined environmental assessment processes. As part of the ministry's ongoing efforts to improve processes and ensure the ministry has an opportunity to provide input on projects undergoing streamlined environmental assessments, the ministry has established dedicated email accounts in each regional office. These accounts will be used to receive notices

as required in your class environmental assessment process along with a new "Project Information Form". As of May 1, 2018, proponents must use this new process.

4 Step Process for Submitting Notices for Streamlined EAs

To submit your notice you need to do the following:

1. **Download and complete the Project Information Form.** (The Form can be found [here](#) under "Streamlined EAs". It is an excel spreadsheet with columns that need to be filled out by the proponent. The form has been developed for ease of use (i.e. drop down pick list for most fields). Instructions on filling out the form are contained in 2 tabs within the form itself).
2. **Create an email. The subject line of your email must include in this order: project location, type of streamlined EA and project name**

For example:

- York Region, MEA Class EA, Elgin Mills Rd East (Bayview to Woodbine)
- Durham Region, Electricity Screening Process, New Cogeneration Station
- City of Ottawa, Waste Management Screening Process, Landfill Expansion

3. **Attach the completed Project Information Form (in excel format) and a copy of your project notice (in PDF format) to the email.**

4. **Send by email to the appropriate ministry regional office:**

Central Region – eanotification.cregion@ontario.ca

Eastern Region – eanotification.eregion@ontario.ca

Northern Region – eanotification.nregion@ontario.ca

South West Region – eanotification.swregion@ontario.ca

West Central Region – eanotification.wcregion@ontario.ca

Notes:

- The hyperlink to the [MOECC District Officer Locator](#) website, can be used to assist with determining what ministry region your project is located.
- The minimum requirement is to send project initiation and completion notices (and where applicable, Revised Notice of Completion, Notice of Filing of Addendum, Statement of Completion). All other notices (e.g. Notice of PIC/OH) can be sent to the Regional email address but not required.
- If your project is located in more than one ministry region, you need to submit your notices to all appropriate regions.

From: Connor MacIsaac <cmaclsaac@rvanderson.com>

Sent: October 22, 2020 3:09 PM

Cc: Andrew McGregor <AMcGregor@rvanderson.com>; Shun Cheung <Shun.Cheung@caledon.ca>; John P. Does <jdoes@rvanderson.com>; Winnie Wong <Winnie.Wong@rvanderson.com>

Subject: Humber Station Road EA - Notice of Commencement

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Notice of Study Commencement for the Humber Station Road Class Environmental Assessment Study (Schedule B MCEA). The study will review opportunities for improvements to the road between Castlederg Sideroad and Healey Road, including improvements to: road geometry and intersection safety; pedestrian and cyclist connectivity; and stormwater management.

At this point in the study, we are requesting any relevant background information from you as it relates to the study area. Information that would be of interest includes:

- Contact information and the identification of individuals that represent your agency or group that we should include as a primary contact throughout the study process;
- Description of the existing conditions or sensitivities within the study area as they relate to your interests; and
- Specific issues, concerns and/or expectations that your agency or group may have.

Please respond to this email with your comments, or contact directly the project team members listed on the attached notice for further information on this project.

Regards,



RVA IS GROWING!

Our NEW Halton and Halifax offices are now open.



Connor MacIsaac, EPt

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

R.V. Anderson Associates Limited

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Connor MacIsaac

From: Winnie Wong
Sent: October 23, 2020 11:21 AM
To: eanotification.cregion@ontario.ca
Cc: Shun Cheung; Connor MacIsaac; Naomi Vaset; Andrew McGregor; John P. Does
Subject: Notice of Commencement - Humber Station Road and Mill Street
Attachments: 2020-127-HumberStationEA-NOC.pdf; 2020-97 -MillStreetEA-NOC-vf.pdf; 2020-97-streamlined_ea_project_information_form.xlsx; 2020-127-streamlined_ea_project_information_form.xlsx

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Notices of Study Commencement for the Class Environmental Assessment Study (Schedule B MCEA) for Humber Station Road and Mill Street. The study will review opportunities for improvements to the road including: road geometry and intersection safety; pedestrian and cyclist connectivity; and stormwater management.

At this point in the study, we are requesting any relevant background information from you as it relates to the study area. Information that would be of interest includes:

- Contact information and the identification of individuals that represent your agency or group that we should include as a primary contact throughout the study process;
- Description of the existing conditions or sensitivities within the study area as they relate to your interests; and
- Specific issues, concerns and/or expectations that your agency or group may have.

Please respond to this email with your comments, or contact directly the project team members listed on the attached notice for further information on this project.

Regards,



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Winnie Wong, P.Eng., PMP, M.Eng.
Project Manager

P: (416) 497-8600 ext. 1471
C: (647) 390-5328

R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300 Toronto, ON M2J 4Z8

rvanderson.com



What to do:
Step 1: Look for the type of EA project in column B that applies to you.
Step 2: Complete columns C to J for that project.
Step 3: Send this form in Excel format to the MECP regional office email address where the project is located.
MECP regional office email addresses are listed at
www.ontario.ca/page/preparing-environmental-assessments

	Class EA/Streamlined EA	Proponent Name	Proponent Contact	Project Name	Project Schedule	Project Type	Project Location	MOECC Region	Project Initiation Date
1	CO - Remedial flood and erosion control projects								
2	GO Transit - Class EA								
3	Hydro One - Minor transmission facilities								
4	MEA - Class EA for municipal infrastructure projects	Town of Caledon	Shun H. Cheung, P.Eng., PMP. (shun.cheung@caledon.ca) Winnie Wong, P.Eng., PMP., M.Eng. (winnie.wong@rvanderson.com)	Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road	Schedule B	Municipal road projects	Caledon, Town of	Central	2020-10-23
5	Ministry of Infrastructure - Public work								
6	MNDM - Activities of the Ministry of Northern Development and Mines under the Mining Act								
7	MNRF - Provincial parks and conservation reserves								
8	MNRF - Resource stewardship and facility development projects								
9	MTO - Provincial transportation facilities								
10	O. Reg. 101/07 - Waste management projects								
11	O. Reg. 116/01 - Electricity projects								
12	OWA - Waterpower projects								

Enter the proponent's name.

Enter the name and email address of the person who the MECP should contact about your project. This should be the same contact person who is listed on the notice.

Enter the project name as it appears on the notice.

Select the project schedule from the drop-down menu.

Select the project type from the drop-down menu.

Select the name of the municipality or unorganized/unsurveyed area where your project is located from the drop-down menu.

Select the MECP region from the drop-down menu. Read the "MECP regions" worksheet to find the MECP region where your project is located.

Enter the date that the streamlined EA process was initiated (e.g. notice of commencement). This date may be when the project notice was first published.

Connor MacIsaac

From: Bell, Trevor (MECP) <Trevor.Bell@ontario.ca>
Sent: March 22, 2023 10:27 AM
To: Connor MacIsaac
Cc: Winnie Wong; Andrew McGregor; Shun Cheung; John P. Does; Dufresne, Tina (MECP); Battarino, Gavin (MECP)
Subject: RE: Town of Caledon Mill Street _ Schedule B Municipal Class EA _ RVA 205388
Categories: Technical Agency Comments

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Connor,

Thanks for your email. We have no problem if you want to skip the draft review and issue the Notice of Completion. We will review during the public review period.

Thanks,
Trevor

From: Connor MacIsaac <cmacisaac@rvanderson.com>
Sent: March 21, 2023 5:19 PM
To: Bell, Trevor (MECP) <Trevor.Bell@ontario.ca>
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>; Andrew McGregor <AMcGregor@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>; John P. Does <jdoes@rvanderson.com>; Papageorgiou, Agni (MINES) <Agni.Papageorgiou@ontario.ca>; Dufresne, Tina (MECP) <Tina.Dufresne@ontario.ca>
Subject: RE: Town of Caledon Mill Street _ Schedule B Municipal Class EA _ RVA 205388

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hi Trevor,

I am reaching out to you regarding your request included in the MECP Acknowledgment Letter for the Mill Street Class EA, to provide a draft copy of the PFR to yourself, prior to the filing of the final for public review. Our understanding is that while it is the MECP's preference to review the draft Project File Report prior to the 30-day public review period, this is not a requirement under the EA Act.

Furthermore, as determined through the EA process the study recommendations are limited to reconstructing the roadway along the existing alignment and maintaining the existing road cross section, generally match the existing paved platform width. That being said, the Town would still like to complete the project under a Schedule B Class EA and issue a Notice of Completion.

Additionally, considering that this EA did not identify any impact on the Aboriginal Treaty rights and the Indigenous communities consulted did not express any concerns, we would like to provide the final PFR to MECP during the 30-day public review period.

Could you please confirm whether the MECP would have any strong opposition to this approach for this project?

Thanks,

Connor MacIsaac, ENV SP, EPT

Environmental Planner, EA & Approvals



R.V. Anderson Associates Limited
43 Church Street, Suite 104, St. Catharines ON L2R 7E1
t 905 685 5049 ext. 4218

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: Bell, Trevor (MECP) <Trevor.Bell@ontario.ca>

Sent: November 23, 2020 4:05 PM

To: Shun Cheung <shun.cheung@caledon.ca>

Cc: Winnie Wong <Winnie.Wong@rvanderson.com>; Papageorgiou, Agni (MECP) <Agni.Papageorgiou@ontario.ca>;

Dufresne, Tina (MECP) <Tina.Dufresne@ontario.ca>

Subject: Mill Street and Humber Station Road Improvements - Schedule B Municipal Class EAs

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good afternoon,

Please find letters attached from the Ministry of the Environment, Conservation and Parks, Environmental Approvals Branch, regarding the above mentioned projects. Feel free to contact me directly with any questions you may have.

Sincerely,

Trevor Bell | Environmental Planner/Environmental Assessment Coordinator

Project Review Unit, Environmental Assessment and Permissions Branch

Ministry of the Environment, Conservation and Parks

5775 Yonge Street, 8th floor, Toronto ON, M2M 4J1

New Phone: 437-770-3731 | trevor.bell@ontario.ca

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Ministry of the Environment,
Conservation and Parks

Environmental Assessment Branch

1st Floor
135 St. Clair Avenue W
Toronto ON M4V 1P5
Tel.: 416 314-8001
Fax.: 416 314-8452

Ministère de l'Environnement, de la
Protection de la nature et des Parcs

*Direction des évaluations
environnementales*

Rez-de-chaussée
135, avenue St. Clair Ouest
Toronto ON M4V 1P5
Tél. : 416 314-8001
Téléc. : 416 314-8452



November 23, 2020

Shun H. Cheung, P.Eng., PMP
Project Manager, Engineering Services
Finance and Infrastructure Services
Town of Caledon
shun.cheung@caledon.ca
BY EMAIL ONLY

Re: **Mill Street Improvements (Mississauga Road to Creditview Road)**
Town of Caledon
Schedule B Municipal Class Environmental Assessment
Notice of Study Commencement

Dear Shun Cheung,

This letter is in response to the Notice of Commencement for the above noted project. The Ministry of the Environment, Conservation and Parks (MECP) acknowledges that the Town of Caledon has indicated that the study is following the approved environmental planning process for a Schedule B project under the Municipal Engineers Association's Municipal Class Environmental Assessment (Class EA).

The attached "Areas of Interest" document provides guidance regarding the ministry's interests with respect to the Class EA process. Please identify the areas of interest which are applicable to the project and ensure they are addressed. Proponents who address all the applicable areas of interest can minimize potential delays to the project schedule.

The Crown has a legal duty to consult Aboriginal communities when it has knowledge, real or constructive, of the existence or potential existence of an Aboriginal or treaty right and contemplates conduct that may adversely impact that right. Before authorizing this project, the Crown must ensure that its duty to consult has been fulfilled, where such a duty is triggered. Although the duty to consult with Aboriginal peoples is a duty of the Crown, the Crown may delegate procedural aspects of this duty to project proponents while retaining oversight of the consultation process.

The proposed project may have the potential to affect Aboriginal or treaty rights protected under Section 35 of Canada's *Constitution Act* 1982. Where the Crown's duty to consult is triggered in relation to the proposed project, **the MECP is delegating the procedural aspects of rights-based consultation to the proponent through this letter.** The Crown intends to rely on the delegated consultation process in discharging its duty to consult and maintains the right to participate in the consultation process as it sees fit.

Based on information provided to date and the Crown's preliminary assessment the proponent is required to consult with the following communities who have been identified as potentially affected by the proposed project:

- Mississaugas of the Credit First Nation;
- Six Nations of the Grand River;
- Haudenosaunee Confederacy Chiefs Council; and
- Huron-Wendat Nation (if there are potential archeological impacts)

Steps that the proponent may need to take in relation to Aboriginal consultation for the proposed project are outlined in the "[Code of Practice for Consultation in Ontario's Environmental Assessment Process](#)".

Additional information related to Ontario's *Environmental Assessment Act* is available online at: www.ontario.ca/environmentalassessments

Please also refer to the attached document "A Proponent's Introduction to the Delegation of Procedural Aspects of consultation with Aboriginal Communities" for further information.

The proponent must contact the Director of Environmental Assessment Branch under the following circumstances subsequent to initial discussions with the communities identified by MECP:

- Aboriginal or treaty rights impacts are identified to you by the communities;
- You have reason to believe that your proposed project may adversely affect an Aboriginal or treaty right;
- Consultation with Indigenous communities or other stakeholders has reached an impasse; or
- A Part II Order request is expected based on impacts to Aboriginal or treaty rights.

The MECP will then assess the extent of any Crown duty to consult for the circumstances and will consider whether additional steps should be taken, including what role you will be asked to play should additional steps and activities be required.

Once the report is finalized, the proponent must issue a Notice of Completion providing a minimum 30-day period during which documentation may be reviewed and comment and input can be submitted to the Proponent.

Please ensure that the Notice of Completion advises that outstanding concerns are to be directed to the proponent for a response, and that in the event there are outstanding concerns regarding potential adverse impacts to constitutionally protected Aboriginal and treaty rights, Part II Order requests on those matters should be addressed in writing to:

Minister Jeff Yurek
Ministry of Environment, Conservation and Parks
777 Bay Street, 5th Floor
Toronto ON M7A 2J3
minister.mecp@ontario.ca

and

Director, Environmental Assessment Branch
Ministry of Environment, Conservation and Parks
135 St. Clair Ave. W, 1st Floor
Toronto ON, M4V 1P5
EABDirector@ontario.ca

Please note the project cannot proceed until at least 30 days after the end of the public review period provided for in the Notice of Completion.

Further, the project may not proceed after this time if:

- a Part II Order request has been submitted to the ministry regarding potential adverse impacts to constitutionally protected Aboriginal and treaty rights; or
- the Director has issued a Notice of Proposed Order regarding the project.

The public can request a higher level of assessment on a project if they are concerned about potential adverse impacts to constitutionally protected Aboriginal and treaty rights. In addition, the Minister may issue an order on his or her own initiative within a specified time period. The Director will issue a Notice of Proposed Order to the proponent if the Minister is considering an order for the project within 30 days after the conclusion of the comment period on the Notice of Completion. At this time, the Director may request additional information from the proponent.

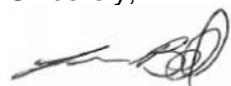
Once the requested information has been received, the Minister will have 30 days to make a decision or impose conditions on your project.

A draft copy of the report should be sent to me prior to the filing of the final report, allowing a minimum of 30 days for the ministry's technical reviewers to provide comments.

Please also ensure a copy of the final notice is sent to the ministry's Central Region EA notification email account (eanotification.cregion@ontario.ca) after the report is finalized.

Should you or your project team members have any questions regarding the material above, please contact me at trevor.bell@ontario.ca.

Sincerely,



Trevor Bell
Regional Environmental Assessment Coordinator

cc: Tina Dufresne, Manager, Halton Peel District Office, MECP
Agni Papageorgiou, Supervisor, Project Review Unit
Winnie Wong, Project Manager, R.V. Anderson Associates Limited

Attachments: Areas of Interest
A Proponent's Introduction to the Delegation of Procedural Aspects of
consultation with Aboriginal Communities

AREAS OF INTEREST

It is suggested that you check off each applicable area after you have considered / addressed it.

☐ Species at Risk

- The Ministry of the Environment, Conservation and Parks has now assumed responsibility of Ontario's Species at Risk program. For any questions related to subsequent permit requirements, please contact SAROntario@ontario.ca.

☐ Planning and Policy

- Ontario has released "A Place to Grow: Growth Plan for the Greater Golden Horseshoe (2019)" which replaces the "Growth Plan for the Greater Golden Horseshoe (2017)". More information, including the Plan, is found here: <https://www.placestogrow.ca>.
- Parts of the study area may be subject to the [A Place to Grow: Growth Plan for the Greater Golden Horseshoe](#) (2019), [Oak Ridges Moraine Conservation Plan](#) (2017), [Niagara Escarpment Plan](#) (2017), [Greenbelt Plan](#) (2017) or [Lake Simcoe Protection Plan](#) (2014). Applicable policies should be referenced in the report, and the proponent should describe how the proposed project adheres to the relevant policies in these plans.
- The [Provincial Policy Statement](#) (2020) contains policies that protect Ontario's natural heritage and water resources. Applicable policies should be referenced in the report, and the proponent should describe how the proposed project is consistent with these policies.

☐ Source Water Protection (all projects)

The *Clean Water Act*, 2006 (CWA) aims to protect existing and future sources of drinking water. To achieve this, several types of vulnerable areas have been delineated around surface water intakes and wellheads for every municipal residential drinking water system that is located in a source protection area. These vulnerable areas are known as a Wellhead Protection Areas (WHPAs) and surface water Intake Protection Zones (IPZs). Other vulnerable areas that have been delineated under the CWA include Highly Vulnerable Aquifers (HVAs), Significant Groundwater Recharge Areas (SGRAs), Event-based modelling areas (EBAs), and Issues Contributing Areas (ICAs). Source protection plans have been developed that include policies to address existing and future risks to sources of municipal drinking water within these vulnerable areas.

Projects that are subject to the Environmental Assessment Act that fall under a Class EA, or one of the Regulations, have the potential to impact sources of drinking water if they occur in designated vulnerable areas or in the vicinity of other at-risk drinking water systems (i.e. systems that are not municipal residential systems). MEA Class EA projects may include activities that, if located in a vulnerable area, could be a threat to sources of drinking water (i.e. have the potential to adversely affect the quality or quantity of drinking water sources) and the activity could therefore be subject to policies in a source protection plan. Where an activity poses a risk to drinking water, policies in the local source protection plan may impact how or where that activity is undertaken. Policies may prohibit certain activities, or they may require risk management measures for these activities. Municipal Official Plans, planning decisions, Class EA projects (where the project includes an activity that is a threat to drinking water) and prescribed instruments must conform with policies that address significant risks to drinking water and must have regard for policies that address moderate or low risks.

- In October 2015, the MEA Parent Class EA document was amended to include reference to the

Clean Water Act (Section A.2.10.6) and indicates that proponents undertaking a Municipal Class EA project must identify early in their process whether a project is or could potentially be occurring with a vulnerable area. **Given this requirement, please include a section in the report on source water protection.**

- The proponent should identify the source protection area and should clearly document how the proximity of the project to sources of drinking water (municipal or other) and any delineated vulnerable areas was considered and assessed. Specifically, the report should discuss whether or not the project is located in a vulnerable area and provide applicable details about the area.
- If located in a vulnerable area, proponents should document whether any project activities are prescribed drinking water threats and thus pose a risk to drinking water (this should be consulted on with the appropriate Source Protection Authority). Where an activity poses a risk to drinking water, the proponent must document and discuss in the report how the project adheres to or has regard to applicable policies in the local source protection plan. This section should then be used to inform and be reflected in other sections of the report, such as the identification of net positive/negative effects of alternatives, mitigation measures, evaluation of alternatives etc.
- While most source protection plans focused on including policies for significant drinking water threats in the WHPAs and IPZs it should be noted that even though source protection plan policies may not apply in HVAs, these are areas where aquifers are sensitive and at risk to impacts and within these areas, activities may impact the quality of sources of drinking water for systems other than municipal residential systems.
- In order to determine if this project is occurring within a vulnerable area, proponents can use this mapping tool: <http://www.applications.ene.gov.on.ca/swp/en/index.php>. The mapping tool will also provide a link to the appropriate source protection plan in order to identify what policies may be applicable in the vulnerable area.
- For further information on the maps or source protection plan policies which may relate to their project, proponents must contact the appropriate source protection authority. **Please consult with the local source protection authority to discuss potential impacts on drinking water. The contact for this project is Jennifer Stephens at (416) 661-6600 ext 5568 or jstephens@trca.on.ca. Please document the results of that consultation within the report and include all communication documents/correspondence.**

More Information

For more information on the *Clean Water Act*, source protection areas and plans, including specific information on the vulnerable areas and drinking water threats, please refer to Conservation Ontario's website where you will also find links to the local source protection plan/assessment report.

A list of the prescribed drinking water threats can be found in section 1.1 of Ontario Regulation 287/07 made under the *Clean Water Act*. In addition to prescribed drinking water threats, some source protection plans may include policies to address additional "local" threat activities, as approved by the MECP.

☐ **Climate Change**

Ontario is leading the fight against climate change through the [Climate Change Action Plan](#). Recently released, the plan lays out the specific actions Ontario will take in the next five years to meet its 2020 greenhouse gas reduction targets and establishes the framework necessary to meet its long-term

targets. As a commitment of the action plan, **the province has now finalized a guide, "[Considering Climate Change in the Environmental Assessment Process](#)" (Guide).**

The Guide is now a part of the Environmental Assessment program's Guides and Codes of Practice. The Guide sets out the MECP's expectation for considering climate change in the preparation, execution and documentation of environmental assessment studies and processes. The guide provides examples, approaches, resources, and references to assist proponents with consideration of climate change in EA. **Proponents should review this Guide in detail.**

- The MECP expects proponents to:
 1. Take into account during the assessment of alternative solutions and alternative designs, the following:
 - a. the project's expected production of greenhouse gas emissions and impacts on carbon sinks (climate change mitigation); and
 - b. resilience or vulnerability of the undertaking to changing climatic conditions (climate change adaptation).
 2. Include a discrete section in the report detailing how climate change was considered in the EA.

How climate change is considered can be qualitative or quantitative in nature, and should be scaled to the project's level of environmental effect. In all instances, both a project's impacts on climate change (mitigation) and impacts of climate change on a project (adaptation) should be considered.

- The MECP has also prepared another guide to support provincial land use planning direction related to the completion of energy and emission plans. The "[Community Emissions Reduction Planning: A Guide for Municipalities](#)" document is designed to educate stakeholders on the municipal opportunities to reduce energy and greenhouse gas emissions, and to provide guidance on methods and techniques to incorporate consideration of energy and greenhouse gas emissions into municipal activities of all types. We encourage you to review the Guide for information.

☐ **Air Quality, Dust and Noise**

- If there are sensitive receptors in the surrounding area of this project, an air quality/odour impact assessment will be useful to evaluate alternatives, determine impacts and identify appropriate mitigation measures. The scope of the assessment can be determined based on the potential effects of the proposed alternatives, and typically includes source and receptor characterization and a quantification of local air quality impacts on the sensitive receptors and the environment in the study area. The assessment will compare to all applicable standards or guidelines for all contaminants of concern. **Please contact this office for further consultation on the level of Air Quality Impact Assessment required for this project if not already advised.**
- **If a full Air Quality Impact Assessment is not required for the project, the report should still contain:**
 - A discussion of local air quality including existing activities/sources that significantly impact local air quality and how the project may impact existing conditions;
 - A discussion of the nearby sensitive receptors and the project's potential air quality impacts on present and future sensitive receptors;
 - A discussion of local air quality impacts that could arise from this project during both construction and operation; and

- A discussion of potential mitigation measures.
- As a common practice, “air quality” should be used as an evaluation criterion for all road projects.
- Dust and noise control measures should be addressed and included in the construction plans to ensure that nearby residential and other sensitive land uses within the study area are not adversely affected during construction activities.
- The MECP recommends that non-chloride dust-suppressants be applied. For a comprehensive list of fugitive dust prevention and control measures that could be applied, refer to [*Cheminfo Services Inc. Best Practices for the Reduction of Air Emissions from Construction and Demolition Activities*](#), report prepared for Environment Canada, March 2005.
- The report should consider the potential impacts of increased noise levels during the operation of the completed project. The proponent should explore all potential measures to mitigate significant noise impacts during the assessment of alternatives.

□ **Ecosystem Protection and Restoration**

- Any impacts to ecosystem form and function must be avoided where possible. The report should describe any proposed mitigation measures and how project planning will protect and enhance the local ecosystem.
- All natural heritage features should be identified and described in detail to assess potential impacts and to develop appropriate mitigation measures. The following sensitive environmental features may be located within or adjacent to the study area:
 - Areas of Natural and Scientific Interest (ANSIs)
 - Rare Species of flora or fauna
 - Watercourses
 - Wetlands
 - Woodlots

We recommend consulting with the Ministry of Natural Resources and Forestry (MNR), Fisheries and Oceans Canada (DFO) and your local conservation authority to determine if special measures or additional studies will be necessary to preserve and protect these sensitive features. In addition, you may consider the provisions of the Rouge Park Management Plan if applicable.

□ **Surface Water**

- The report must include enough information to demonstrate that there will be no negative impacts on the natural features or ecological functions of any watercourses within the study area. Measures should be included in the planning and design process to ensure that any impacts to watercourses from construction or operational activities (e.g. spills, erosion, pollution) are mitigated as part of the proposed undertaking.
- Additional stormwater runoff from new pavement can impact receiving watercourses and flood conditions. Quality and quantity control measures to treat stormwater runoff should be considered for all new impervious areas and, where possible, existing surfaces. The ministry’s [*Stormwater Management Planning and Design Manual \(2003\)*](#) should be referenced in the report and utilized when designing stormwater control methods. **A Stormwater Management Plan should be prepared as part of the Class EA process** that includes:
 - Strategies to address potential water quantity and erosion impacts related to stormwater draining into streams or other sensitive environmental features, and to ensure that

- adequate (enhanced) water quality is maintained
 - Watershed information, drainage conditions, and other relevant background information
 - Future drainage conditions, stormwater management options, information on erosion and sediment control during construction, and other details of the proposed works
 - Information on maintenance and monitoring commitments.
- Ontario Regulation 60/08 under the *Ontario Water Resources Act* (OWRA) applies to the Lake Simcoe Basin, which encompasses Lake Simcoe and the lands from which surface water drains into Lake Simcoe. If the proposed sewage treatment plant is listed in Table 1 of the regulation, the report should describe how the proposed project and its mitigation measures are consistent with the requirements of this regulation and the OWRA.
 - Any potential approval requirements for surface water taking or discharge should be identified in the report. A Permit to Take Water (PTTW) under the OWRA will be required for any water takings that exceed 50,000 L/day, except for certain water taking activities that have been prescribed by the Water Taking EASR Regulation – *O. Reg. 63/16*. These prescribed water-taking activities require registration in the EASR instead of a PTTW. Please review the [Water Taking User Guide for EASR](#) for more information. Additionally, an Environmental Compliance Approval under the OWRA is required for municipal stormwater management works.

☐ **Groundwater**

- The status of, and potential impacts to any well water supplies should be addressed. If the project involves groundwater takings or changes to drainage patterns, the quantity and quality of groundwater may be affected due to drawdown effects or the redirection of existing contamination flows. In addition, project activities may infringe on existing wells such that they must be reconstructed or sealed and abandoned. Appropriate information to define existing groundwater conditions should be included in the report.
- If the potential construction or decommissioning of water wells is identified as an issue, the report should refer to Ontario Regulation 903, Wells, under the OWRA.
- Potential impacts to groundwater-dependent natural features should be addressed. Any changes to groundwater flow or quality from groundwater taking may interfere with the ecological processes of streams, wetlands or other surficial features. In addition, discharging contaminated or high volumes of groundwater to these features may have direct impacts on their function. Any potential effects should be identified, and appropriate mitigation measures should be recommended. The level of detail required will be dependent on the significance of the potential impacts.
- Any potential approval requirements for groundwater taking or discharge should be identified in the report. A Permit to Take Water (PTTW) under the OWRA will be required for any water takings that exceed 50,000 L/day, with the exception of certain water taking activities that have been prescribed by the Water Taking EASR Regulation – *O. Reg. 63/16*. These prescribed water-taking activities require registration in the EASR instead of a PTTW. Please review the [Water Taking User Guide for EASR](#) for more information.

☐ **Contaminated Soils**

- Since the removal or movement of soils may be required, appropriate tests to determine contaminant levels from previous land uses or dumping should be undertaken. If the soils are contaminated, you must determine how and where they are to be disposed of, consistent with *Part XV.1 of the Environmental Protection Act* (EPA) and Ontario Regulation 153/04, Records of

Site Condition, which details the new requirements related to site assessment and clean up. Please contact the appropriate MECP District Office for further consultation if contaminated sites are present.

- Any current or historical waste disposal sites should be identified in the report. The status of these sites should be determined to confirm whether approval pursuant to Section 46 of the EPA may be required for land uses on former disposal sites.
- The location of any underground storage tanks should be investigated in the report. Measures should be identified to ensure the integrity of these tanks and to ensure an appropriate response in the event of a spill. The ministry's Spills Action Centre must be contacted in such an event.
- The report should identify any underground transmission lines in the study area. The owners should be consulted to avoid impacts to this infrastructure, including potential spills.

☐ **Excess Materials Management**

- Activities involving the management of excess soil should be completed in accordance with the MECP's current guidance document titled "[Management of Excess Soil – A Guide for Best Management Practices](#)" (2014).
- All waste generated during construction must be disposed of in accordance with ministry requirements

☐ **Servicing and Facilities**

- Any facility that releases emissions to the atmosphere, discharges contaminants to ground or surface water, provides potable water supplies, or stores, transports or disposes of waste must have an Environmental Compliance Approval (ECA) before it can operate lawfully. Please consult with the Environmental Approvals Access and Service Integration Branch (EAASIB) to determine whether a new or amended ECA will be required for any proposed infrastructure.
- We recommend referring to the ministry's [environmental land use planning guides](#) to ensure that any potential land use conflicts are considered when planning for any infrastructure or facilities related to wastewater, pipelines, landfills or industrial uses.

☐ **Mitigation and Monitoring**

- Contractors must be made aware of all environmental considerations so that all environmental standards and commitments for both construction and operation are met. Mitigation measures should be clearly referenced in the report and regularly monitored during the construction stage of the project. In addition, we encourage proponents to conduct post-construction monitoring to ensure all mitigation measures have been effective and are functioning properly.
- Design and construction reports and plans should be based on a best management approach that centres on the prevention of impacts, protection of the existing environment, and opportunities for rehabilitation and enhancement of any impacted areas.
- The proponent's construction and post-construction monitoring plans must be documented in the report, as outlined in Section A.2.5 and A.4.1 of the MEA Class EA parent document.

☐ **Consultation**

- The report must demonstrate how the consultation provisions of the Class EA have been fulfilled, including documentation of all stakeholder consultation efforts undertaken during the planning process. This includes a discussion in the SR that identifies concerns that were raised and **describes how they have been addressed by the proponent** throughout the planning process. The Class EA also directs proponents to include copies of comments submitted on the project by interested stakeholders, and the proponent's responses to these comments.

□ Class EA Process

- The report should provide clear and complete documentation of the planning process in order to allow for transparency in decision-making.
- If this project is a Master Plan: there are several different approaches that can be used to conduct a Master Plan, examples of which are outlined in Appendix 4 of the Class EA. The Master Plan should clearly indicate the selected approach for conducting the plan, by identifying whether the levels of assessment, consultation and documentation are sufficient to fulfill the requirements for Schedule B or C projects. Please note that any Schedule B or C projects identified in the plan would be subject to Part II Order Requests under the *Environmental Assessment Act*, although the plan itself would not be.
- The report must demonstrate how the consultation provisions of the Class EA have been fulfilled, including documentation of all stakeholder consultation efforts undertaken during the planning process. This includes a discussion in the report that identifies concerns that were raised and **describes how they have been addressed by the proponent** throughout the planning process. The Class EA also directs proponents to include copies of comments submitted on the project by interested stakeholders, and the proponent's responses to these comments.
- The Class EA requires the consideration of the effects of each alternative on all aspects of the environment. The report should include a level of detail (e.g. hydrogeological investigations, terrestrial and aquatic assessments) such that all potential impacts can be identified, and appropriate mitigation measures can be developed. Any supporting studies conducted during the Class EA process should be referenced and included as part of the report.
- Please include in the report a list of all subsequent permits or approvals that may be required for the implementation of the preferred alternative, including but not limited to, MECP's PTTW, EASR Registrations and ECAs, conservation authority permits, species at risk permits, and approvals under the *Impact Assessment Act*, 2019.
- Ministry guidelines and other information related to the issues above are available at <http://www.ontario.ca/environment-and-energy/environment-and-energy>. We encourage you to review all the available guides and to reference any relevant information in the report.

A PROPONENT'S INTRODUCTION TO THE DELEGATION OF PROCEDURAL ASPECTS OF CONSULTATION WITH ABORIGINAL COMMUNITIES

Definitions

The following definitions are specific to this document and may not apply in other contexts:

Aboriginal communities – the First Nation or Métis communities identified by the Crown for the purpose of consultation.

Consultation – the Crown's legal obligation to consult when the Crown has knowledge of an established or asserted Aboriginal or treaty right and contemplates conduct that might adversely impact that right. This is the type of consultation required pursuant to s. 35 of the *Constitution Act, 1982*. Note that this definition does not include consultation with Aboriginal communities for other reasons, such as regulatory requirements.

Crown – the Ontario Crown, acting through a particular ministry or ministries.

Procedural aspects of consultation – those portions of consultation related to the process of consultation, such as notifying an Aboriginal community about a project, providing information about the potential impacts of a project, responding to concerns raised by an Aboriginal community and proposing changes to the project to avoid negative impacts.

Proponent – the person or entity that wants to undertake a project and requires an Ontario Crown decision or approval for the project.

I. Purpose

The Crown has a legal duty to consult Aboriginal communities when it has knowledge of an existing or asserted Aboriginal or treaty right and contemplates conduct that may adversely impact that right. In outlining a framework for the duty to consult, the Supreme Court of Canada has stated that the Crown may delegate procedural aspects of consultation to third parties. This document provides general information about the Ontario Crown's approach to delegation of the procedural aspects of consultation to proponents.

This document is not intended to instruct a proponent about an individual project, and it does not constitute legal advice.

II. Why is it Necessary to Consult with Aboriginal Communities?

The objective of the modern law of Aboriginal and treaty rights is the *reconciliation* of Aboriginal peoples and non-Aboriginal peoples and their respective rights, claims and interests. Consultation is an important component of the reconciliation process.

The Crown has a legal duty to consult Aboriginal communities when it has knowledge of an existing or asserted Aboriginal or treaty right and contemplates conduct that might adversely impact that right. For example, the Crown's duty to consult is triggered when it considers issuing a permit, authorization or approval for a project which has the potential to adversely impact an Aboriginal right, such as the right to hunt, fish, or trap in a particular area.

The scope of consultation required in particular circumstances ranges across a spectrum depending on both the nature of the asserted or established right and the seriousness of the potential adverse impacts on that right.

Depending on the particular circumstances, the Crown may also need to take steps to accommodate the potentially impacted Aboriginal or treaty right. For example, the Crown may be required to avoid or minimize the potential adverse impacts of the project.

III. The Crown's Role and Responsibilities in the Delegated Consultation Process

The Crown has the responsibility for ensuring that the duty to consult, and accommodate where appropriate, is met. However, the Crown may delegate the procedural aspects of consultation to a proponent.

There are different ways in which the Crown may delegate the procedural aspects of consultation to a proponent, including through a letter, a memorandum of understanding, legislation, regulation, policy and codes of practice.

If the Crown decides to delegate procedural aspects of consultation, the Crown will generally:

- Ensure that the delegation of procedural aspects of consultation and the responsibilities of the proponent are clearly communicated to the proponent;
- Identify which Aboriginal communities must be consulted;
- Provide contact information for the Aboriginal communities;
- Revise, as necessary, the list of Aboriginal communities to be consulted as new information becomes available and is assessed by the Crown;
- Assess the scope of consultation owed to the Aboriginal communities;
- Maintain appropriate oversight of the actions taken by the proponent in fulfilling the procedural aspects of consultation;
- Assess the adequacy of consultation that is undertaken and any accommodation that may be required;
- Provide a contact within any responsible ministry in case issues arise that require direction from the Crown; and
- Participate in the consultation process as necessary and as determined by the Crown.

IV. The Proponent's Role and Responsibilities in the Delegated Consultation Process

Where aspects of the consultation process have been delegated to a proponent, the Crown, in meeting its duty to consult, will rely on the proponent's consultation activities and documentation of those activities. The consultation process informs the Crown's decision of whether or not to approve a proposed project or activity.

A proponent's role and responsibilities will vary depending on a variety of factors including the extent of consultation required in the circumstance and the procedural aspects of consultation the Crown has delegated to it. Proponents are often in a better position than the Crown to discuss a project and its potential impacts with Aboriginal communities and to determine ways to avoid or minimize the adverse impacts of a project.

A proponent can raise issues or questions with the Crown at any time during the consultation process. If issues or concerns arise during the consultation that cannot be addressed by the proponent, the proponent should contact the Crown.

a) What might a proponent be required to do in carrying out the procedural aspects of consultation?

Where the Crown delegates procedural aspects of consultation, it is often the proponent's responsibility to provide notice of the proposed project to the identified Aboriginal communities. The notice should indicate that the Crown has delegated the procedural aspects of consultation to the proponent and should include the following information:

- a description of the proposed project or activity;
- mapping;
- proposed timelines;
- details regarding anticipated environmental and other impacts;
- details regarding opportunities to comment; and
- any changes to the proposed project that have been made for seasonal conditions or other factors, where relevant.

Proponents should provide enough information and time to allow Aboriginal communities to provide meaningful feedback regarding the potential impacts of the project. Depending on the nature of consultation required for a project, a proponent also may be required to:

- provide the Crown with copies of any consultation plans prepared and an opportunity to review and comment;
- ensure that any necessary follow-up discussions with Aboriginal communities take place in a timely manner, including to confirm receipt of information, share and update information and to address questions or concerns that may arise;
- as appropriate, discuss with Aboriginal communities potential mitigation measures and/or changes to the project in response to concerns raised by Aboriginal communities;
- use language that is accessible and not overly technical, and translate material into Aboriginal languages where requested or appropriate;
- bear the reasonable costs associated with the consultation process such as, but not limited to, meeting hall rental, meal costs, document translation(s), or to address technical & capacity issues;
- provide the Crown with all the details about potential impacts on established or asserted Aboriginal or treaty rights, how these concerns have been considered and addressed by the proponent and the Aboriginal communities and any steps taken to mitigate the potential impacts;
- provide the Crown with complete and accurate documentation from these meetings and communications; and
- notify the Crown immediately if an Aboriginal community not identified by the Crown approaches the proponent seeking consultation opportunities.

b) What documentation and reporting does the Crown need from the proponent?

Proponents should keep records of all communications with the Aboriginal communities involved in the consultation process and any information provided to these Aboriginal communities.

As the Crown is required to assess the adequacy of consultation, it needs documentation to satisfy itself that the proponent has fulfilled the procedural aspects of consultation delegated to it. The documentation required would typically include:

- the date of meetings, the agendas, any materials distributed, those in attendance and copies of any minutes prepared;
- the description of the proposed project that was shared at the meeting;
- any and all concerns or other feedback provided by the communities;

- any information that was shared by a community in relation to its asserted or established Aboriginal or treaty rights and any potential adverse impacts of the proposed activity, approval or disposition on such rights;
- any proposed project changes or mitigation measures that were discussed, and feedback from Aboriginal communities about the proposed changes and measures;
- any commitments made by the proponent in response to any concerns raised, and feedback from Aboriginal communities on those commitments;
- copies of correspondence to or from Aboriginal communities, and any materials distributed electronically or by mail;
- information regarding any financial assistance provided by the proponent to enable participation by Aboriginal communities in the consultation;
- periodic consultation progress reports or copies of meeting notes if requested by the Crown;
- a summary of how the delegated aspects of consultation were carried out and the results; and
- a summary of issues raised by the Aboriginal communities, how the issues were addressed and any outstanding issues.

In certain circumstances, the Crown may share and discuss the proponent's consultation record with an Aboriginal community to ensure that it is an accurate reflection of the consultation process.

c) Will the Crown require a proponent to provide information about its commercial arrangements with Aboriginal communities?

The Crown may require a proponent to share information about aspects of commercial arrangements between the proponent and Aboriginal communities where the arrangements:

- include elements that are directed at mitigating or otherwise addressing impacts of the project;
- include securing an Aboriginal community's support for the project; or
- may potentially affect the obligations of the Crown to the Aboriginal communities.

The proponent should make every reasonable effort to exempt the Crown from confidentiality provisions in commercial arrangements with Aboriginal communities to the extent necessary to allow this information to be shared with the Crown.

The Crown cannot guarantee that information shared with the Crown will remain confidential. Confidential commercial information should not be provided to the Crown as part of the consultation record if it is not relevant to the duty to consult or otherwise required to be submitted to the Crown as part of the regulatory process.

V. What are the Roles and Responsibilities of Aboriginal Communities' in the Consultation Process?

Like the Crown, Aboriginal communities are expected to engage in consultation in good faith. This includes:

- responding to the consultation notice;
- engaging in the proposed consultation process;
- providing relevant documentation;
- clearly articulating the potential impacts of the proposed project on Aboriginal or treaty rights; and
- discussing ways to mitigate any adverse impacts.

Some Aboriginal communities have developed tools, such as consultation protocols, policies or processes that provide guidance on how they would prefer to be consulted. Although not legally binding, proponents are encouraged to respect these community processes where it is reasonable to do so. Please note that there is no obligation for a proponent to pay a fee to an Aboriginal community in order to enter into a consultation process.

To ensure that the Crown is aware of existing community consultation protocols, proponents should contact the relevant Crown ministry when presented with a consultation protocol by an Aboriginal community or anyone purporting to be a representative of an Aboriginal community.

VI. What if More Than One Provincial Crown Ministry is Involved in Approving a Proponent's Project?

Depending on the project and the required permits or approvals, one or more ministries may delegate procedural aspects of the Crown's duty to consult to the proponent. The proponent may contact individual ministries for guidance related to the delegation of procedural aspects of consultation for ministry-specific permits/approvals required for the project in question. Proponents are encouraged to seek input from all involved Crown ministries sooner rather than later.

From: Saddi, Asha <asha.saddi@peelregion.ca>
Sent: June 4, 2021 12:27 PM
To: Winnie Wong
Cc: Shun Cheung; Yevgenia Gubarev; Mercedes Hincapie; Connor MacIsaac; Marina Leonov; Avsec, Joe; Carrick, Sean; Crawford, Heather; Lovisotto, Sandy; Tatla, Manvir
Subject: RE: 2020-97 & 2020-127 - Caledon Growth Related Roads

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Winnie,

Attached are additional comments from our teams, including a response on the two intersections you raised questions about:

Roads Design & Construction – Sandy Lovisotto

- Mill Street and Mississauga Road (Regional Road 1) intersection - From the 30% drawings, RVA is not really including this intersection except for the noted culvert extension on the north side, I support them not including the actual intersection, as it hasn't been long since we've reconstructed it. There should be no cost to the Region since the improvements are being made within the Town's jurisdiction.

Intersection improvement works will generally include new asphalt pavement through the intersection, new mountable curb and gutter completed with 1m asphalt strip behind the curb and subdrains under the curb. We note the intersection at Humber Station Road and King Street is newly constructed and we will not be altering it. I support this as it has been newly constructed.

Infrastructure Programming and Studies - Heather Crawford

- Willoughby Road and Charleston Sideroad (Regional Road 24) intersection – EA to begin Fall 2021 – this is a co-ordination opportunity and the Town of Caledon will be able to provide comments through the EA process for the works required at this intersection.
- Humber Station Road and King Street (Regional Road 9) intersection – EA to begin 2026 – work can go ahead. This is advising of future works Peel has in the vicinity should co-ordination be required. It is a distant future project and probably won't affect the Town of Caledon works.

Sustainable Transportation – Manvir Tatla

Mill Street and Mississauga Road - consider adding a crossing treatment at the intersection of Mill St and the Caledon Trailway path.

- Humber Station Road and King Street - please provide drawings for this intersection, including a plan view.
- Mountainview Road and Charleston Side Road - Sustainable Transportation has plans for paved shoulders in the connecting section of Charleston Side Rd with a project horizon of 2023-2031. Please advise how future paved shoulders on Charleston Side Road would be carried through the intersection considering the current proposed placement of the curb and paved shoulder at the intersection.
- Willoughby Road and Charleston Side Road - please provide a plan view drawing of this intersection.

We look forward to receiving the requested information.

Regards,

*Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca*



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From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: June 3, 2021 2:35 PM
To: Saddi, Asha <asha.saddi@peelregion.ca>
Cc: Shun Cheung <shun.cheung@caledon.ca>; Yevgenia Gubarev <YGubarev@rvanderson.com>; Mercedes Hincapie <MHincapie@rvanderson.com>; Connor MacIsaac <cmaclsaac@rvanderson.com>; Marina Leonov <mleonov@rvanderson.com>; Avsec, Joe <joe.avsec@peelregion.ca>; Carrick, Sean <sean.carrick@peelregion.ca>; Crawford, Heather <heather.crawford@peelregion.ca>
Subject: RE: 2020-97 & 2020-127 - Caledon Growth Related Roads

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Thanks Asha,

Based on below, does that mean the following two intersections we should not touch because of upcoming work:

- Willoughby Road and Charleston Sideroad (Regional Road 24) intersection – EA – Integrated Corridor Charleston – Kennedy to McLaren (to begin Fall 2021)
- Humber Station Road and King Street (Regional Road 9) intersection – King St Grade Separation – just east of Humber Station – EA to begin 2026

If yes then we will leave the above intersections out of our design. Please let us know.

Thanks,

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Saddi, Asha <asha.saddi@peelregion.ca>
Sent: June 3, 2021 2:21 PM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: Shun Cheung <shun.cheung@caledon.ca>; Yevgenia Gubarev <YGubarev@rvanderson.com>; Mercedes Hincapie <MHincapie@rvanderson.com>; Connor MacIsaac <cmaclsaac@rvanderson.com>; Avsec, Joe <joe.avsec@peelregion.ca>; Carrick, Sean <sean.carrick@peelregion.ca>; Crawford, Heather

<heather.crawford@peelregion.ca>

Subject: RE: 2020-97 & 2020-127 - Caledon Growth Related Roads

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Hi Winnie,

I hope you are well. Attached below are comments from our Infrastructure Programming and Studies team:

Infrastructure Programming and Studies - Heather Crawford

- Mill Street and Mississauga Road (Regional Road 1) intersection – **Currently there are no future works programmed at this location.**
- Main Street and Caledon East Garafraxa (Regional Road 23) intersection – can the Region confirm if this section of Caledon East Garafraxa west of A Line is under the Region's jurisdiction? **Dufferin County has jurisdiction on this segment.**
- Humber Station Road and King Street (Regional Road 9) intersection – **King St Grade Separation – just east of Humber Station – EA to begin 2026**
- Mountainview Road and Charleston Sideroad (Regional Road 24) intersection - **Currently there are no future works programmed at this location**
- Willoughby Road and Charleston Sideroad (Regional Road 24) intersection – **EA – Integrated Corridor Charleston – Kennedy to McLaren (to begin Fall 2021)**

I will be in touch again as I receive additional comments from our teams.

Kind regards,

*Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca*



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From: Saddi, Asha

Sent: May 18, 2021 4:08 PM

To: Winnie Wong <Winnie.Wong@rvanderson.com>; Avsec, Joe <Joe.Avsec@peelregion.ca>; Carrick, Sean <Sean.Carrick@peelregion.ca>

Cc: Shun Cheung <shun.cheung@caledon.ca>; Yevgenia Gubarev <YGubarev@rvanderson.com>; Mercedes Hincapie <MHincapie@rvanderson.com>; Connor MacIsaac <cmacisaac@rvanderson.com>

Subject: RE: 2020-97 & 2020-127 - Caledon Growth Related Roads

Good Afternoon Winnie,

Thank you for your email. I will get back to you with comments from our teams by early June.

Kind regards,

*Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca*



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From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: May 17, 2021 3:10 PM
To: Saddi, Asha <asha.saddi@peelregion.ca>; Avsec, Joe <joe.avsec@peelregion.ca>; Carrick, Sean <sean.carrick@peelregion.ca>
Cc: Shun Cheung <shun.cheung@caledon.ca>; Yevgenia Gubarev <YGubarev@rvanderson.com>; Mercedes Hincapie <MHincapie@rvanderson.com>; Connor MacIsaac <cmaclsaac@rvanderson.com>
Subject: 2020-97 & 2020-127 - Caledon Growth Related Roads

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Asha, Joe, and Sean,

RVA was retained by the Town of Caledon to complete the detailed design of their 2022 and 2023 (Phase 2) Growth Related Road projects. The scope of work primarily consists of pavement rehabilitation, intersection improvements, shoulder widening, and overall drainage improvements. The project includes segments of roads within Caledon, some of which intersects with Regional Roads:

- Mill Street and Mississauga Road (Regional Road 1) intersection
- Main Street and Caledon East Garafraxa (Regional Road 23) intersection – can the Region confirm if this section of Caledon East Garafraxa west of A Line is under the Region's jurisdiction?
- Humber Station Road and King Street (Regional Road 9) intersection
- Mountainview Road and Charleston Sideroad (Regional Road 24) intersection
- Willoughby Road and Charleston Sideroad (Regional Road 24) intersection

Intersection improvement works will generally include new asphalt pavement through the intersection, new mountable curb and gutter completed with 1m asphalt strip behind the curb and subdrains under the curb. We note the intersection at Humber Station Road and King Street is newly constructed, and we will not be altering it. Can the Region confirm if our design should include the above intersections?

30% design drawings are available below:

-  [Peel 2020-127](#)
-  [Peel 2020-97](#)

Please let me know if you require more information, or would like a quick meeting to go through the above. We appreciate your feedback.

Thanks,



RVA IS GROWING!

Our NEW Halton and Halifax offices are now open.



Winnie Wong, P.Eng., PMP, M.Eng.

Project Manager

P: (416) 497-8600 ext. 1471

C: (647) 390-5328

R.V. Anderson Associates Limited

2001 Sheppard Avenue East, Suite 300 Toronto, ON M2J 4Z8

rvanderson.com



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Connor MacIsaac

From: Winnie Wong
Sent: August 31, 2022 11:01 AM
To: Saddi, Asha; Shun Cheung
Cc: Connor MacIsaac; Mannie, Sharon
Subject: RE: Town of Caledon: Mill Street Class EA

Hi Asha,

Thank you for your comments. Please see below responses in [blue](#).

Thanks,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: Saddi, Asha <asha.saddi@peelregion.ca>
Sent: July 15, 2022 8:49 AM
To: Shun Cheung <shun.cheung@caledon.ca>
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>; Connor MacIsaac <cmacIsaac@rvanderson.com>; Mannie, Sharon <sharon.mannie@peelregion.ca>
Subject: RE: Town of Caledon: Mill Street Class EA

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Shun,

Attached are comments from our Traffic Engineering teams:

Traffic Operations - Sharon Mannie

- Does this EA cover the entire intersection of Mississauga Road and Mill Street? <<RVA_20220831: EA does not include the intersection of Mississauga Road.>>
- Please provide soft copies of the materials for reference after the PIC Meeting #2 <<RVA_20220831: Town forwarded the link to PIC #2 presentation to the Region on June 14.>>

Traffic Signals - Rebecca Caughey

- Will Town of Caledon be conducting a Trail Crossing Treatment Warrant for the Trail Crossing at Mill Street? <<RVA_20220831: Pedestrian crossing improvements including new traffic signs and pavement markings are proposed and details to be confirmed during detailed design.>>
- Would the Town of Caledon consider a connection between Mill Street and the Trail crossing as well along Mississauga Road? <<RVA_20220831: Trail currently intersects both Mill Street and Mississauga Road.>>

I will be in touch again if we have additional comments from other teams and in the meantime we look forward to receiving your response comments.

Thanks,

Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca



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From: Saddi, Asha
Sent: July 12, 2022 11:26 AM
To: Shun Cheung <Shun.Cheung@caledon.ca>
Cc: winnie.wong@rvanderson.com; Connor MacIsaac <cmaclsaac@rvanderson.com>
Subject: RE: Town of Caledon: Mill Street Class EA

Hi Shun,
This is to let you know that due to vacations some of our teams are still reviewing materials and I will be able to get back to you by the end of the week with any additional comments.

Thanks,

Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca



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From: Saddi, Asha
Sent: June 23, 2022 11:37 AM
To: Shun Cheung <Shun.Cheung@caledon.ca>
Cc: winnie.wong@rvanderson.com; Connor MacIsaac <cmaclsaac@rvanderson.com>; Khan, Sabrina <Sabrina.Khan@peelregion.ca>
Subject: RE: Town of Caledon: Mill Street Class EA

Hi Shun,
Our Transportation System Planning team have the following comment/question:

Transportation System Planning – Sabrina Khan

- Option 2 – Sidewalk is our preferred alternative as it provides the safest option for pedestrians
- Why is the focus only on pedestrians and not active transportation (cyclists)?

I will be in touch if other teams have comments and in the meantime we look forward to receiving your response comments.

Thanks,

Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca



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From: Saddi, Asha
Sent: June 20, 2022 3:27 PM
To: 'Shun Cheung' <Shun.Cheung@caledon.ca>
Cc: winnie.wong@rvanderson.com; 'Connor MacIsaac' <cmacisaac@rvanderson.com>; ZZG-PWI <pw@peelregion.ca>; Ponce Vanelli, Italia <Italia.Ponce@peelregion.ca>
Subject: FW: Town of Caledon: Mill Street Class EA

Hi Shun,
 Our Water and Wastewater team have the following comments:

The Region has a 250mm and 300mm watermain that runs predominantly on the north side of Mill Street. This is the only source that provides water to the residents in the area. Impact to our underground infrastructure, Valve, chambers and fire hydrants will need to be taken into consideration during this project. Furthermore, in order to comment on the solutions presented we would like the project material to be available for viewing digitally as many of us could not attend the in person meeting and the Town's website does not contain the material online.

I will be in touch again if other teams have comments and in the meantime, we look forward to your response comments.

Thanks,

Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca

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From: Connor MacIsaac <cmacisaac@rvanderson.com>

Sent: May 30, 2022 10:57 AM

Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>

Subject: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

CAUTION: EXTERNAL MAIL. DO NOT CLICK ON LINKS OR OPEN ATTACHMENTS YOU DO NOT TRUST.

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Public Information Centre (PIC) #2 for the **Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA)**. The purpose of this meeting is garner feedback on the options for pedestrian accommodations developed for Mill Street in consideration of the feedback received during and following PIC #1, held online October 5, 2021.

The PIC will be held as an informal "Open House" with materials pertaining to the study on display, with members of the project team on hand to answer questions and discuss issues related to the project. Attendees will be asked to review the materials on display and provide their preferences for pedestrian accommodations. A final recommendation for Mill Street will be developed following the PIC, in consideration of the input received.

The details of the PIC are below:

Date: Wednesday June 15, 2022
Time: 5:30 to 7:00 pm
Location: Cheltenham Community Centre - 14190 Creditview Rd, Caledon, ON L0P 1N0
Entrance is located at the south side of the building

The project team values the participation of all stakeholders and wishes to ensure that the community's interests and concerns are taken into consideration. Please contact the project team members listed below if you require a hard copy of the materials or for further information on this project.

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure Engineering Services
905.584.2272 X.4040
shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
416.497.8600 X.1471
winnie.wong@rvanderson.com

If you would like to be removed from the project mailing list, please let us know by responding to this email.

Regards,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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Connor MacIsaac

From: Shun Cheung <Shun.Cheung@caledon.ca>
Sent: February 10, 2023 10:15 AM
To: Winnie Wong
Cc: John P. Does; Connor MacIsaac; Andrew McGregor
Subject: FW: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report

Categories: Technical Agency Comments

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Morning Winnie,

Please see Region's Transportation Engineering's comments.

Shun Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure
Engineering Services

Office: 905.584.2272 x.4040

Cell: 416.436.0910

Email: shun.cheung@caledon.ca

Town of Caledon | www.caledon.ca | www.visitcaledon.ca | Follow us @YourCaledon

From: Saddi, Asha <asha.saddi@peelregion.ca>
Sent: Friday, February 10, 2023 9:59 AM
To: Shun Cheung <Shun.Cheung@caledon.ca>
Cc: Al-Hemiry, Salam <salam.alhemiry@peelregion.ca>; Mulcahy, Grace <grace.mulcahy@peelregion.ca>
Subject: FW: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Hi Shun,

Our Traffic Engineering team have the following comments:

Traffic Signals and Streetlighting – Salam Al-Hemiry
Will look into streetlighting requirements when this project moves forward to design

We look forward to receiving your response comments.

Thanks,

*Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division*



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From: Saddi, Asha
Sent: February 6, 2023 1:35 PM
To: Shun Cheung <Shun.Cheung@caledon.ca>
Cc: ZZG-PWI <pw@peelregion.ca>; Briatico, Jonathan <Jonathan.Briatico@peelregion.ca>
Subject: FW: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report

Hi Shun,
Our Water and Wastewater team has reviewed the materials and have the following comments:

General comments for all Options:

- Potential final grade adjustment of watermain valve boxes
- Potential final grade adjustment of hydrant valve boxes
- Potential final grade adjustment of hydrants themselves
- Concept designs had identified proposed retaining walls and steel beam guide rails. Please be aware of existing watermain in close proximity. The Region of Peel requests to see a detailed design of proposed infrastructure in relation to existing watermain.
 - Things to consider are the zone of influence of proposed infrastructure in relation to existing watermain.
 - Furthermore, the design shall consider structural integrity of proposed infrastructure should the watermain break and liquify the soil. (Existing watermain is not within a steel linear)

Comments pertaining to Options 2 and 3:

- Current hydrant locations may be in conflict of proposed concrete/asphalt sidewalk. Minimum clearance of hydrant body to backside of sidewalk must be a minimum of 1.0m clearance.

We look forward to receiving the response comments.

Thanks,

*Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca*



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From: Connor MacIsaac <cmacisaac@rvanderson.com>

Sent: January 12, 2023 3:14 PM

Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>

Subject: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report

CAUTION: EXTERNAL MAIL. DO NOT CLICK ON LINKS OR OPEN ATTACHMENTS YOU DO NOT TRUST.

Dear Sir/Madam,

On behalf of the Town, please find attached the Notice of Study Recommendations for the Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA). The purpose of this notice is to garner feedback on the recommendations for Mill Street developed in consideration of the comments received during and following the second Public Information Centre (PIC), held on June 15, 2022. A detailed summary of the comments received at and following the second PIC, and how these comments were incorporated into the development of the study recommendations, is presented in the PIC Summary Report on the Town's website at <https://www.caledon.ca/en/news/mill-street-ea.aspx>.

To provide comments, questions, or concerns on the study recommendations, or the PIC Summary Report, please contact the project team members listed on the attached notice by February 2, 2023.

Upon completion of the PIC Summary Report comment period, a Project File Report (PFR), containing details of the Municipal Class EA planning and decision-making process, public consultation process and study recommendations will be made available for a 30-day public review period, concluding the EA study. A Notice of Completion will be issued at that stage.

Thank you again for your continued input to the study.

Best Regards,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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Connor MacIsaac

From: Henderson, Brandon (MNRF) <Brandon.Henderson@ontario.ca>
Sent: December 2, 2020 9:59 AM
To: Winnie Wong
Subject: RE: 2020-127 & 2020-97 Caledon Growth Related Roads

Categories: Filed by Newforma

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Winnie,

I would be available the following days:

December 4th am
December 7th am
December 8th pm
December 11th pm.

Thanks,

Brandon

From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: Monday, November 30, 2020 2:32 PM
To: Henderson, Brandon (MNRF) <Brandon.Henderson@ontario.ca>
Subject: RE: 2020-127 & 2020-97 Caledon Growth Related Roads

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Thanks Brandon,

Please let me know if any of the following slots work with you and I'll set up the meeting:

Dec 2 – PM
Dec 3 – AM
Dec 4 – AM
Dec 7 – AM
Dec 8 – PM
Dec 9 – AM
Dec 10 – PM
Dec 11 – PM

Thanks,

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Henderson, Brandon (MNRF) <Brandon.Henderson@ontario.ca>
Sent: November 26, 2020 3:36 PM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: RE: 2020-127 & 2020-97 Caledon Growth Related Roads

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Winnie,

I would definitely have some availability open in the next two weeks. I am fairly open so let me know what days would work best for you and I will do my best to be available.

Thanks,

Brandon

From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: Thursday, November 26, 2020 3:34 PM
To: Henderson, Brandon (MNRF) <Brandon.Henderson@ontario.ca>
Subject: RE: 2020-127 & 2020-97 Caledon Growth Related Roads

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hi Brandon,

Hope this email finds you well! I missed your Monday/Tuesday afternoon opening this week – would you be free in the next two weeks? I am trying to get my environ team and drainage team on the meeting as well.

Best regards,

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Henderson, Brandon (MNRF) <Brandon.Henderson@ontario.ca>
Sent: November 17, 2020 9:20 PM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: RE: 2020-127 & 2020-97 Caledon Growth Related Roads

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Winnie,

My current week is looking pretty busy at the moment, would you be available sometime Monday or Tuesday afternoon next week?

Best regards,

Brandon

From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: Thursday, November 12, 2020 2:03 PM
To: Henderson, Brandon (MNRF) <Brandon.Henderson@ontario.ca>
Cc: John P. Does <jdoes@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>; Connor MacIsaac <cmaclsaac@rvanderson.com>
Subject: FW: 2020-127 & 2020-97 Caledon Growth Related Roads

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hi Brandon,

Please to meet you! I was referred to you by Cheryl Tansony re our two projects with the Town of Caledon and we would like to set up a project initiation meeting with you and your team in the next two weeks to go over the scope of work and NEC's requirements/ expectations re permit approvals. Please let me know when will be a good time for you and I'll set up the meeting.

Thanks,

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Winnie Wong
Sent: November 9, 2020 8:58 PM
To: necowensound@ontario.ca
Cc: John P. Does <jdoes@rvanderson.com>; Shun Cheung <Shun.Cheung@caledon.ca>
Subject: FW: 2020-127 & 2020-97 Caledon Growth Related Roads

Dear NEC,

I have been instructed by Rick Watt's email to direct my inquiries to this email. I am trying to coordinate a project initiation meeting for the Town's 2022 and 2023 Growth Related Road projects – please see my email below and advise.

Thanks,

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Winnie Wong
Sent: November 9, 2020 8:54 PM
To: rick.watt@ontario.ca
Cc: Shun Cheung <Shun.Cheung@caledon.ca>; John P. Does <jdoes@rvanderson.com>
Subject: 2020-127 & 2020-97 Caledon Growth Related Roads

Hi Rick,

Please to meet you! I got your contact information from the Town of Caledon – Shun Cheung. RVA have been retained by the Town to undertake the detailed design of their 2022 and 2023 Growth Related Road projects. The scope of work generally involves shoulder widening and overall drainage improvements through replacing road crossing culverts and ditch cleanout/ regrading.

We would like to schedule an initiation meeting with you and your team in the next two weeks to go over the scope of work and NEC's requirements/ expectations re permit approvals. Please let me know when will be a good time for you and I'll set up the meeting.

Best regards,
(R205388 & R205421)



RVA IS GROWING!

Our NEW Halton and Halifax
offices are now open.



Winnie Wong, P.Eng., PMP, M.Eng.

Project Manager

P: (416) 497-8600 ext. 1471

C: (647) 390-5328

R.V. Anderson Associates Limited

2001 Sheppard Avenue East, Suite 300 Toronto, ON M2J 4Z8

[rvanderson.com](http://www.rvanderson.com)



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MEETING NOTES

THE CORPORATION OF THE TOWN OF CALEDON GROWTH RELATED ROADS DETAILED DESIGN 2022 & 2023 (PHASE 2) Town File No. 2020-97 & 2020-127

NEC Project Initiation Meeting

DATE: December 8, 2020 at 1:30pm

PLACE: Microsoft Teams Meeting

PROJECT NO.: R205388 & R205421

PRESENT:	Brandon Henderson Tisha Doucette Andrew McGregor Naomi Vaset Winnie Wong	Niagara Escarpment Commission (NEC) R.V. Anderson Associates Limited R.V. Anderson Associates Limited R.V. Anderson Associates Limited R.V. Anderson Associates Limited
-----------------	--------------------------------------------------------------------------------------	-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

DISTRIBUTED:	All present, and: Shun Cheung John Does Oliver Olberg	Town of Caledon (Town) R.V. Anderson Associates Limited R.V. Anderson Associates Limited
---------------------	----------------------------------------------------------------	------------------------------------------------------------------------------------------------

DISCUSSION:

ACTION BY:

1.0 Introduction

Info

- RVA is undertaking two Caledon projects providing detailed design for six roads. Of the six roads, only Mountainview Rd and a section of Mill St (from Mississauga Rd to west of John St) are under NEC's jurisdiction
- Scope of work for both projects include pavement rehabilitation, shoulder pavement widening, and overall drainage improvements (culvert replacement, ditch cleanout/ regrading)
- NEC noted those areas not under development control and some classes of development are exempt from obtaining a Development Permit from the NEC
- The area west of the trail on Mill St will most likely require a Development Permit

2.0 Communication

Info

- All communication to NEC will be addressed to Brandon Henderson
- NEC noted the current review timeline for Development Permit is 6 to 8 months, and advises to submit the application for review as early as possible

3.0 Information required

DISCUSSION:

ACTION BY:

- | | | |
|-----|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 3.1 | Permit application to be submitted at 90% and should include details on the scope of work, grading limits and impacts. NEC noted there is no formal application guideline and a site plan will be required on the application | Info |
| 3.2 | NEC noted their Development Permit is required before other conservation authorities will issue their permit. NEC to forward RVA their exemption criteria (O.Reg 828/90, Section 4.3 and 4.4) | NEC |
| 3.3 | NEC advised RVA to work closely with the conservation authorities on drainage and environmental impact reviews, and cc NEC on the correspondence. | Info |
| 3.4 | NEC noted the focus of their review will be on tree removals and scenic/ visual impacts, and the associated mitigation measures. NEC will retain a landscape architect to evaluate visual impacts if swaths of trees are removed in the project. | Info |
| 3.5 | NEC noted the Development Permit does not allow work to be done on private property. Town is to be responsible for securing PTE/ easements for work outside of the road ROW. | Info |

4.0 NEC expectation

- | | | |
|-----|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 4.1 | NEC noted the application should demonstrate the project is in compliance with the Endangered Species Act and that it is necessary and inevitable. The application needs to include identification of the ELC communities and screen SAR, similar to TRCA/ CVC application requirements. | Info |
| 4.2 | NEC noted the timing windows, ESC, mitigation measures will follow CVC/ TRCA's review. There is an OPSS standard for ESC fence that should be used in this project. | Info |
| 4.3 | RVA to contact Brandon for environmental impact statement (EIS) if required. | Info |

5.0 Project schedule

- | | | |
|-----|-----------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 5.1 | Based on the project schedule and required review periods, the NEC application will have to be submitted by September 2021 to be in time for 2022 construction. | Info |
|-----|-----------------------------------------------------------------------------------------------------------------------------------------------------------------|------|

6.0 Other Discussion

- | | | |
|-----|--------------------------------|------|
| 6.1 | Nothing to report at this time | Info |
|-----|--------------------------------|------|

Next review meeting TBD.

Notes prepared by: Winnie Wong

R:\2020\205388 - 2022 Roads Rehab\D Meetings\01 External\D. NEC\20201208 - Initiation\2020-97_2020-127-NEC Initiation Minutes.docx

PLEASE ADVISE THE WRITER OF ANY ERRORS OR OMISSIONS WITHIN 1 WEEK OF RECEIPT OF THESE NOTES

Connor MacIsaac

From: Winnie Wong
Sent: February 22, 2023 4:45 PM
To: Sarwar, Haleema (MNRF)
Cc: Muller, Joe (MNRF); Shun Cheung; John P. Does; Connor MacIsaac
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

Hi Haleema/ Joe,

Hope this email finds you well!

I've forwarded a copy of our preliminary design for Mill St to you via Newforma Info Exchange. Please let me know if it doesn't appear in your inbox. The same drawings have been sent to CVC for their review.

Mill Street detailed design is being completed under Schedule 'B' of the Municipal Class EA, and we are aiming to forward you a copy of our draft Project File Report (PFR) by next week. We are sending the drawings in advance for your review and comment.

Regards,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: Sarwar, Haleema (MNRF) <Haleema.Sarwar@ontario.ca>
Sent: October 26, 2022 11:02 AM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: Muller, Joe (MNRF) <Joe.Muller@ontario.ca>
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good morning,

Would it be possible to re-schedule our meeting on October 27 at 10:00 to another day? Below I have provided the days/times that Joe and I are both available:

- Monday Oct 31 at 11:00
- Tuesday Nov 1 at 2:30
- Thursday Nov 3 at 10:00

Please let me know if these dates are not workable and I can provide you with a different set of dates.

Kind regards,

Haleema Sarwar
Planner



Niagara Escarpment Commission
232 Guelph Street | Georgetown, ON | L7G 4B1
Tel: (289) 924-1275 | Website: www.escarpment.org

As part of providing [accessible customer service](#), please let me know if you have any accommodation needs or require communication supports or alternate formats.

The NEC offices are now open to the public in a limited capacity. In order to ensure a safe and secure environment for staff and clients, the NEC **requires** that you make an appointment to meet with staff in person. Alternatively, the NEC will continue to provide services via telephone and email. Updates can be found on our website: <https://escarpment.org/covid-19-update/>

From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: October 17, 2022 2:12 PM
To: Muller, Joe (MNRF) <Joe.Muller@ontario.ca>
Cc: Sarwar, Haleema (MNRF) <Haleema.Sarwar@ontario.ca>; John P. Does <jdoes@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.
Hi Joe/ Haleema,

Hope you had a good weekend. Just following up with my email below. If you can advise your availability, or direct me to the representative that is overseeing this project, that will be greatly appreciated!

Best,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: Winnie Wong
Sent: October 11, 2022 6:12 PM
To: Muller, Joe (NDMNRF) <Joe.Muller@ontario.ca>
Cc: Sarwar, Haleema (NDMNRF) <Haleema.Sarwar@ontario.ca>; John P. Does <jdoes@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

Hi Joe and Haleema,

Following up on my previous email re Mountainview Road and Mill St with the Town of Caledon, we would like to request a meeting with you and your team to review permitting requirements and timeline.

We are also liaising with CVC/ TRCA re both roads, and we intend to acquire their permits and approvals as well.

Please advise your next availability and we look forward to speaking with you.

Regards,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: Winnie Wong
Sent: September 9, 2022 12:32 AM
To: Muller, Joe (NDMNRF) <Joe.Muller@ontario.ca>; Shun Cheung <shun.cheung@caledon.ca>
Cc: Sarwar, Haleema (NDMNRF) <Haleema.Sarwar@ontario.ca>
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

Hi Joe & Haleema,

Hope this email finds you both well. R.V. Anderson is currently undertaking detailed design for Mountainview Road and Mill Street for the Town of Caledon, both of which falls under NEC jurisdiction.

Since our last correspondence, there have been updates to the design of both roads. We have completed the 60% design drawings for Mountainview Road as well as the Natural Environmental Assessment and Arborist reports. These are being forwarded to you via Newforma Info Exchange. We are currently updating our design drawings to address CVC/ TRCA/ Town's comments, and will forward you a copy of our 90% design drawings once complete in early/mid October.

We have recently completed PIC #2 for Mill Street and working on the 60% design drawings. When those are ready in mid/ late October we will circulate the drawings via Newforma Info Exchange.

In the meantime if you need additional information please feel free to contact me.

Best regards,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: Muller, Joe (NDMNRF) <Joe.Muller@ontario.ca>
Sent: June 17, 2022 2:35 PM
To: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: Sarwar, Haleema (NDMNRF) <Haleema.Sarwar@ontario.ca>
Subject: FW: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Please add me and Haleema Sarwar to your circulation list: Brandon is no longer with the NEC.
Thanks, & take care,

Joe

Joe Muller, RPP, MCIP

Senior Strategic Advisor Niagara Escarpment Commission
232 Guelph Street | Georgetown, ON | L7G 4B1
Tel: 905-703-5837 | Website: www.escarpment.org



As part of providing [accessible customer service](#), please let me know if you have any accommodation needs or require communication supports or alternate formats.

The NEC offices are now open to the public in a limited capacity. In order to ensure a safe and secure environment for staff and clients, the NEC **requires** that you make an appointment to meet with staff in person. Alternatively, the NEC will continue to provide services via telephone and email. Updates can be found on our website: <https://escarpment.org/covid-19-update/>

From: Henderson, Brandon (NDMNRF) <Brandon.Henderson@ontario.ca>
Sent: May 30, 2022 11:09 AM
To: Muller, Joe (NDMNRF) <Joe.Muller@ontario.ca>
Subject: FW: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

FYI – here you go!

From: Connor MacIsaac <cmacisaac@rvanderson.com>
Sent: Monday, May 30, 2022 10:57 AM
Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Public Information Centre (PIC) #2 for the Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA). The purpose of this meeting is garner feedback on the options for pedestrian accommodations developed for Mill Street in consideration of the feedback received during and following PIC #1, held online October 5, 2021.

The PIC will be held as an informal "Open House" with materials pertaining to the study on display, with members of the project team on hand to answer questions and discuss issues related to the project. Attendees will be asked to review the materials on display and provide their preferences for pedestrian accommodations. A final recommendation for Mill Street will be developed following the PIC, in consideration of the input received.

The details of the PIC are below:

Date: Wednesday June 15, 2022
Time: 5:30 to 7:00 pm

Location: Cheltenham Community Centre - 14190 Creditview Rd, Caledon, ON L0P 1N0
Entrance is located at the south side of the building

The project team values the participation of all stakeholders and wishes to ensure that the community's interests and concerns are taken into consideration. Please contact the project team members listed below if you require a hard copy of the materials or for further information on this project.

Town of Caledon
Shun H. Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure Engineering Services
905.584.2272 X.4040
shun.cheung@caledon.ca

R.V. Anderson Associates Limited
Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
416.497.8600 X.1471
winnie.wong@rvanderson.com

If you would like to be removed from the project mailing list, please let us know by responding to this email.

Regards,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



rvanderson.com



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MEETING NOTES

THE CORPORATION OF THE TOWN OF CALEDON GROWTH RELATED ROADS DETAILED DESIGN 2022 & 2023 (PHASE 2) Town File No. 2020-97 & 2020-127

NEC Meeting #2

DATE: October 31, 2022 at 11:00am

PLACE: Microsoft Teams Meeting

PROJECT NO.: R205388 & R205421

PRESENT:	Joe Muller Haleema Sarwar Shun Cheung Ben Pressman John Does Winnie Wong	Niagara Escarpment Commission (NEC) Niagara Escarpment Commission Town of Caledon (Town) Town of Caledon R.V. Anderson Associates Limited (RVA) R.V. Anderson Associates Limited
-----------------	-----------------------------------------------------------------------------------------	-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

DISTRIBUTED: All present

DISCUSSION:

ACTION BY:

1.0 Introduction

- | | | |
|-----|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 1.1 | The purpose of this meeting is to review three ongoing projects in Town of Caledon that are under NEC's jurisdiction: | Info |
| | <ul style="list-style-type: none"> 2020-87 – Mountainview Road from Grange Sideroad to Granite Stones Drive 2020-97 – Mill Street from Caledon Trailway to Mississauga Road 2020-127 – Mountainview Road from Granite Stones Drive to Charleston Sideroad | |

- | | | |
|-----|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 1.2 | 2020-127 Mountainview Road is a detailed design project to implement the recommendations in the Town's completed 2019 ESR Study. During the EA Study in 2019, NEC confirmed the proposed scope of work does not require a development permit but request a set of IFC drawings be circulated to them. RVA to forward IFC drawings to NEC when ready. | Info |
|-----|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|

2.0 Progress Update

- | | | |
|-----|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 2.1 | RVA has been in consultation with MECP, CVC, and TRCA for all three roads regarding stormwater management and environmental inventory. | Info |
| 2.2 | Mill Street detailed design is being completed under Schedule 'B' of the Municipal Class Environmental Assessment. To date, the Town has completed two PICs and RVA is working towards the first draft of the Project File Report. When the Project File Report has been filed and closed, RVA will proceed to the 60% detailed design. | Info |

DISCUSSION:

ACTION BY:

- | | | |
|-----|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------|
| 2.3 | RVA to forward the completed 90% design drawings for 2020-127 Mountainview Road. NEC will review and provide comment. <i>[Post meeting note: 60% design drawings, arborist report, and Natural Environmental Assessment memo for Mountainview Road was forwarded to NEC on September 8, 2022 via Newforma Info Exchange. 90% design drawings for Mountainview Road was forwarded to NEC via Newforma Info Exchange on November 21, 2022.]</i> | NEC/
RVA |
|-----|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------|

3.0 Permit Requirements

- | | | |
|-----|-----------------------------------------------------------------------------------------------------------------|------|
| 3.1 | NEC confirmed there is only one permit application form for both developments and road rehabilitation projects. | Info |
|-----|-----------------------------------------------------------------------------------------------------------------|------|

6.0 Other Discussion

Nothing to report at this time	Info
--------------------------------	------

Next review meeting TBD.

Notes prepared by: Winnie Wong

R:\2020\205388 - 2022 Roads Rehab\Meetings\01 External\D. NEC\20221031 - Meeting 2\2020-97_2020-127-NEC Meeting #2.docx

PLEASE ADVISE THE WRITER OF ANY ERRORS OR OMISSIONS WITHIN 1 WEEK OF RECEIPT OF THESE NOTES



Transmittal

PROJECT: 2022 Roads Rehab 205388 DATE: 2023-02-22

SUBJECT: 205388 - Mill Street preliminary drawings TRANSMITTAL ID: 00067

PURPOSE: For your review and comment VIA: Info Exchange

FROM

NAME	COMPANY	EMAIL	PHONE
Winnie Wong		Winnie.Wong@rvanderson.com	

TO

NAME	COMPANY	EMAIL	PHONE
Haleema.Sarwar@ontario.ca		Haleema.Sarwar@ontario.ca	
Joe.Muller@ontario.ca		Joe.Muller@ontario.ca	

REMARKS: Hi Haleema/ Joe,

Please use this link to download our Mill Street drawings that will be included in our draft Project File Report (PFR) which we will be forwarding to you end of February.

The key proposed works included in these drawings are as follows:

- rehabilitate road pavement following the existing horizontal and vertical profile
- no pavement shoulder widening, paved platform width to match existing asphalt edges
- no ditch realignment
- replace culverts that do not meet drainage capacity or in poor physical condition
- cleaning out/ localize regrading of the ditches to restore positive drainage

We will be reviewing the following issues in further detail with CVC during our detailed design:

- flooding at 1499 Mill Street
- slope stability at culvert just east of Caledon Trailway
- steep embankment slopes on some culverts

Please note, as we have yet to file the PFR for public review there may be slight modifications to these drawings depending on comments/ feedback we receive. As such we kindly ask NEC to keep these drawings confidential until we are ready to file. We would also like to request a meeting with NEC anytime after end of February. The same drawings have been forwarded to CVC for their review.

Regards,
Winnie

Transmittal

DATE: 2023-02-22
TRANSMITTAL ID: 00067

DESCRIPTION OF CONTENTS

QTY	DATED	TITLE	NOTES
1	2022-12-20	205388-Mill St -P&P-DD-PP-30%.pdf	
1	2022-11-30	205388-TS_Mill S-DD-TS.pdf	
1	2022-12-01	205388-XSEC_Mill St-DD-Driveways.pdf	
1	2022-12-01	205388-XSEC_Mill St-DD-x-section.pdf	

COPIES:

Shun Cheung	(Town of Caledon)
John Does	(R.V. Anderson Associates Limited)
Connor MacIsaac	(R.V. Anderson Associates Limited)

*Intentionally left blank. Will be updated based on CVC comments on
Draft PFR.*

Connor MacIsaac

From: Henderson, Brandon (MNRF) <Brandon.Henderson@ontario.ca>
Sent: December 2, 2020 9:59 AM
To: Winnie Wong
Subject: RE: 2020-127 & 2020-97 Caledon Growth Related Roads

Categories: Filed by Newforma

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Winnie,

I would be available the following days:

December 4th am
December 7th am
December 8th pm
December 11th pm.

Thanks,

Brandon

From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: Monday, November 30, 2020 2:32 PM
To: Henderson, Brandon (MNRF) <Brandon.Henderson@ontario.ca>
Subject: RE: 2020-127 & 2020-97 Caledon Growth Related Roads

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Thanks Brandon,

Please let me know if any of the following slots work with you and I'll set up the meeting:

Dec 2 – PM
Dec 3 – AM
Dec 4 – AM
Dec 7 – AM
Dec 8 – PM
Dec 9 – AM
Dec 10 – PM
Dec 11 – PM

Thanks,

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Henderson, Brandon (MNRF) <Brandon.Henderson@ontario.ca>
Sent: November 26, 2020 3:36 PM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: RE: 2020-127 & 2020-97 Caledon Growth Related Roads

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Winnie,

I would definitely have some availability open in the next two weeks. I am fairly open so let me know what days would work best for you and I will do my best to be available.

Thanks,

Brandon

From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: Thursday, November 26, 2020 3:34 PM
To: Henderson, Brandon (MNRF) <Brandon.Henderson@ontario.ca>
Subject: RE: 2020-127 & 2020-97 Caledon Growth Related Roads

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hi Brandon,

Hope this email finds you well! I missed your Monday/Tuesday afternoon opening this week – would you be free in the next two weeks? I am trying to get my environ team and drainage team on the meeting as well.

Best regards,

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Henderson, Brandon (MNRF) <Brandon.Henderson@ontario.ca>
Sent: November 17, 2020 9:20 PM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: RE: 2020-127 & 2020-97 Caledon Growth Related Roads

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Winnie,

My current week is looking pretty busy at the moment, would you be available sometime Monday or Tuesday afternoon next week?

Best regards,

Brandon

From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: Thursday, November 12, 2020 2:03 PM
To: Henderson, Brandon (MNRF) <Brandon.Henderson@ontario.ca>
Cc: John P. Does <jdoes@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>; Connor MacIsaac <cmaclsaac@rvanderson.com>
Subject: FW: 2020-127 & 2020-97 Caledon Growth Related Roads

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hi Brandon,

Please to meet you! I was referred to you by Cheryl Tansony re our two projects with the Town of Caledon and we would like to set up a project initiation meeting with you and your team in the next two weeks to go over the scope of work and NEC's requirements/ expectations re permit approvals. Please let me know when will be a good time for you and I'll set up the meeting.

Thanks,

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Winnie Wong
Sent: November 9, 2020 8:58 PM
To: necowensound@ontario.ca
Cc: John P. Does <jdoes@rvanderson.com>; Shun Cheung <Shun.Cheung@caledon.ca>
Subject: FW: 2020-127 & 2020-97 Caledon Growth Related Roads

Dear NEC,

I have been instructed by Rick Watt's email to direct my inquiries to this email. I am trying to coordinate a project initiation meeting for the Town's 2022 and 2023 Growth Related Road projects – please see my email below and advise.

Thanks,

Winnie Wong, P. Eng., PMP, M.Eng.
Project Manager
R.V. Anderson Associates Limited

From: Winnie Wong
Sent: November 9, 2020 8:54 PM
To: rick.watt@ontario.ca
Cc: Shun Cheung <Shun.Cheung@caledon.ca>; John P. Does <jdoes@rvanderson.com>
Subject: 2020-127 & 2020-97 Caledon Growth Related Roads

Hi Rick,

Please to meet you! I got your contact information from the Town of Caledon – Shun Cheung. RVA have been retained by the Town to undertake the detailed design of their 2022 and 2023 Growth Related Road projects. The scope of work generally involves shoulder widening and overall drainage improvements through replacing road crossing culverts and ditch cleanout/ regrading.

We would like to schedule an initiation meeting with you and your team in the next two weeks to go over the scope of work and NEC's requirements/ expectations re permit approvals. Please let me know when will be a good time for you and I'll set up the meeting.

Best regards,
(R205388 & R205421)



RVA IS GROWING!

Our NEW Halton and Halifax
offices are now open.



Winnie Wong, P.Eng., PMP, M.Eng.

Project Manager

P: (416) 497-8600 ext. 1471

C: (647) 390-5328

R.V. Anderson Associates Limited

2001 Sheppard Avenue East, Suite 300 Toronto, ON M2J 4Z8

[rvanderson.com](http://www.rvanderson.com)



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MEETING NOTES

THE CORPORATION OF THE TOWN OF CALEDON GROWTH RELATED ROADS DETAILED DESIGN 2022 & 2023 (PHASE 2) Town File No. 2020-97 & 2020-127

NEC Project Initiation Meeting

DATE: December 8, 2020 at 1:30pm

PLACE: Microsoft Teams Meeting

PROJECT NO.: R205388 & R205421

PRESENT:	Brandon Henderson Tisha Doucette Andrew McGregor Naomi Vaset Winnie Wong	Niagara Escarpment Commission (NEC) R.V. Anderson Associates Limited R.V. Anderson Associates Limited R.V. Anderson Associates Limited R.V. Anderson Associates Limited
-----------------	--------------------------------------------------------------------------------------	-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

DISTRIBUTED:	All present, and: Shun Cheung John Does Oliver Olberg	Town of Caledon (Town) R.V. Anderson Associates Limited R.V. Anderson Associates Limited
---------------------	----------------------------------------------------------------	------------------------------------------------------------------------------------------------

DISCUSSION:

ACTION BY:

1.0 Introduction

Info

- RVA is undertaking two Caledon projects providing detailed design for six roads. Of the six roads, only Mountainview Rd and a section of Mill St (from Mississauga Rd to west of John St) are under NEC's jurisdiction
- Scope of work for both projects include pavement rehabilitation, shoulder pavement widening, and overall drainage improvements (culvert replacement, ditch cleanout/ regrading)
- NEC noted those areas not under development control and some classes of development are exempt from obtaining a Development Permit from the NEC
- The area west of the trail on Mill St will most likely require a Development Permit

2.0 Communication

Info

- All communication to NEC will be addressed to Brandon Henderson
- NEC noted the current review timeline for Development Permit is 6 to 8 months, and advises to submit the application for review as early as possible

3.0 Information required

DISCUSSION:

ACTION BY:

- | | | |
|-----|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 3.1 | Permit application to be submitted at 90% and should include details on the scope of work, grading limits and impacts. NEC noted there is no formal application guideline and a site plan will be required on the application | Info |
| 3.2 | NEC noted their Development Permit is required before other conservation authorities will issue their permit. NEC to forward RVA their exemption criteria (O.Reg 828/90, Section 4.3 and 4.4) | NEC |
| 3.3 | NEC advised RVA to work closely with the conservation authorities on drainage and environmental impact reviews, and cc NEC on the correspondence. | Info |
| 3.4 | NEC noted the focus of their review will be on tree removals and scenic/ visual impacts, and the associated mitigation measures. NEC will retain a landscape architect to evaluate visual impacts if swaths of trees are removed in the project. | Info |
| 3.5 | NEC noted the Development Permit does not allow work to be done on private property. Town is to be responsible for securing PTE/ easements for work outside of the road ROW. | Info |

4.0 NEC expectation

- | | | |
|-----|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 4.1 | NEC noted the application should demonstrate the project is in compliance with the Endangered Species Act and that it is necessary and inevitable. The application needs to include identification of the ELC communities and screen SAR, similar to TRCA/ CVC application requirements. | Info |
| 4.2 | NEC noted the timing windows, ESC, mitigation measures will follow CVC/ TRCA's review. There is an OPSS standard for ESC fence that should be used in this project. | Info |
| 4.3 | RVA to contact Brandon for environmental impact statement (EIS) if required. | Info |

5.0 Project schedule

- | | | |
|-----|-----------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 5.1 | Based on the project schedule and required review periods, the NEC application will have to be submitted by September 2021 to be in time for 2022 construction. | Info |
|-----|-----------------------------------------------------------------------------------------------------------------------------------------------------------------|------|

6.0 Other Discussion

- | | | |
|-----|--------------------------------|------|
| 6.1 | Nothing to report at this time | Info |
|-----|--------------------------------|------|

Next review meeting TBD.

Notes prepared by: Winnie Wong

R:\2020\205388 - 2022 Roads Rehab\Meetings\01 External\D. NEC\20201208 - Initiation\2020-97_2020-127-NEC Initiation Minutes.docx

PLEASE ADVISE THE WRITER OF ANY ERRORS OR OMISSIONS WITHIN 1 WEEK OF RECEIPT OF THESE NOTES

Connor MacIsaac

From: Winnie Wong
Sent: February 22, 2023 4:45 PM
To: Sarwar, Haleema (MNRF)
Cc: Muller, Joe (MNRF); Shun Cheung; John P. Does; Connor MacIsaac
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

Hi Haleema/ Joe,

Hope this email finds you well!

I've forwarded a copy of our preliminary design for Mill St to you via Newforma Info Exchange. Please let me know if it doesn't appear in your inbox. The same drawings have been sent to CVC for their review.

Mill Street detailed design is being completed under Schedule 'B' of the Municipal Class EA, and we are aiming to forward you a copy of our draft Project File Report (PFR) by next week. We are sending the drawings in advance for your review and comment.

Regards,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: Sarwar, Haleema (MNRF) <Haleema.Sarwar@ontario.ca>
Sent: October 26, 2022 11:02 AM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: Muller, Joe (MNRF) <Joe.Muller@ontario.ca>
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good morning,

Would it be possible to re-schedule our meeting on October 27 at 10:00 to another day? Below I have provided the days/times that Joe and I are both available:

- Monday Oct 31 at 11:00
- Tuesday Nov 1 at 2:30
- Thursday Nov 3 at 10:00

Please let me know if these dates are not workable and I can provide you with a different set of dates.

Kind regards,

Haleema Sarwar
Planner



Niagara Escarpment Commission
232 Guelph Street | Georgetown, ON | L7G 4B1
Tel: (289) 924-1275 | Website: www.escarpment.org

As part of providing [accessible customer service](#), please let me know if you have any accommodation needs or require communication supports or alternate formats.

The NEC offices are now open to the public in a limited capacity. In order to ensure a safe and secure environment for staff and clients, the NEC **requires** that you make an appointment to meet with staff in person. Alternatively, the NEC will continue to provide services via telephone and email. Updates can be found on our website: <https://escarpment.org/covid-19-update/>

From: Winnie Wong <Winnie.Wong@rvanderson.com>
Sent: October 17, 2022 2:12 PM
To: Muller, Joe (MNRF) <Joe.Muller@ontario.ca>
Cc: Sarwar, Haleema (MNRF) <Haleema.Sarwar@ontario.ca>; John P. Does <jdoes@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.
Hi Joe/ Haleema,

Hope you had a good weekend. Just following up with my email below. If you can advise your availability, or direct me to the representative that is overseeing this project, that will be greatly appreciated!

Best,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: Winnie Wong
Sent: October 11, 2022 6:12 PM
To: Muller, Joe (NDMNRF) <Joe.Muller@ontario.ca>
Cc: Sarwar, Haleema (NDMNRF) <Haleema.Sarwar@ontario.ca>; John P. Does <jdoes@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

Hi Joe and Haleema,

Following up on my previous email re Mountainview Road and Mill St with the Town of Caledon, we would like to request a meeting with you and your team to review permitting requirements and timeline.

We are also liaising with CVC/ TRCA re both roads, and we intend to acquire their permits and approvals as well.

Please advise your next availability and we look forward to speaking with you.

Regards,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: Winnie Wong
Sent: September 9, 2022 12:32 AM
To: Muller, Joe (NDMNRF) <Joe.Muller@ontario.ca>; Shun Cheung <shun.cheung@caledon.ca>
Cc: Sarwar, Haleema (NDMNRF) <Haleema.Sarwar@ontario.ca>
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

Hi Joe & Haleema,

Hope this email finds you both well. R.V. Anderson is currently undertaking detailed design for Mountainview Road and Mill Street for the Town of Caledon, both of which falls under NEC jurisdiction.

Since our last correspondence, there have been updates to the design of both roads. We have completed the 60% design drawings for Mountainview Road as well as the Natural Environmental Assessment and Arborist reports. These are being forwarded to you via Newforma Info Exchange. We are currently updating our design drawings to address CVC/ TRCA/ Town's comments, and will forward you a copy of our 90% design drawings once complete in early/mid October.

We have recently completed PIC #2 for Mill Street and working on the 60% design drawings. When those are ready in mid/ late October we will circulate the drawings via Newforma Info Exchange.

In the meantime if you need additional information please feel free to contact me.

Best regards,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: Muller, Joe (NDMNRF) <Joe.Muller@ontario.ca>
Sent: June 17, 2022 2:35 PM
To: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: Sarwar, Haleema (NDMNRF) <Haleema.Sarwar@ontario.ca>
Subject: FW: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Please add me and Haleema Sarwar to your circulation list: Brandon is no longer with the NEC.
Thanks, & take care,

Joe

Joe Muller, RPP, MCIP

Senior Strategic Advisor Niagara Escarpment Commission
232 Guelph Street | Georgetown, ON | L7G 4B1
Tel: 905-703-5837 | Website: www.escarpment.org



As part of providing [accessible customer service](#), please let me know if you have any accommodation needs or require communication supports or alternate formats.

The NEC offices are now open to the public in a limited capacity. In order to ensure a safe and secure environment for staff and clients, the NEC **requires** that you make an appointment to meet with staff in person. Alternatively, the NEC will continue to provide services via telephone and email. Updates can be found on our website: <https://escarpment.org/covid-19-update/>

From: Henderson, Brandon (NDMNRF) <Brandon.Henderson@ontario.ca>
Sent: May 30, 2022 11:09 AM
To: Muller, Joe (NDMNRF) <Joe.Muller@ontario.ca>
Subject: FW: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

FYI – here you go!

From: Connor MacIsaac <cmacisaac@rvanderson.com>
Sent: Monday, May 30, 2022 10:57 AM
Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>
Subject: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Public Information Centre (PIC) #2 for the Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA). The purpose of this meeting is garner feedback on the options for pedestrian accommodations developed for Mill Street in consideration of the feedback received during and following PIC #1, held online October 5, 2021.

The PIC will be held as an informal "Open House" with materials pertaining to the study on display, with members of the project team on hand to answer questions and discuss issues related to the project. Attendees will be asked to review the materials on display and provide their preferences for pedestrian accommodations. A final recommendation for Mill Street will be developed following the PIC, in consideration of the input received.

The details of the PIC are below:

Date: Wednesday June 15, 2022
Time: 5:30 to 7:00 pm

Location: Cheltenham Community Centre - 14190 Creditview Rd, Caledon, ON L0P 1N0
Entrance is located at the south side of the building

The project team values the participation of all stakeholders and wishes to ensure that the community's interests and concerns are taken into consideration. Please contact the project team members listed below if you require a hard copy of the materials or for further information on this project.

Town of Caledon
Shun H. Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure Engineering Services
905.584.2272 X.4040
shun.cheung@caledon.ca

R.V. Anderson Associates Limited
Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
416.497.8600 X.1471
winnie.wong@rvanderson.com

If you would like to be removed from the project mailing list, please let us know by responding to this email.

Regards,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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MEETING NOTES

THE CORPORATION OF THE TOWN OF CALEDON GROWTH RELATED ROADS DETAILED DESIGN 2022 & 2023 (PHASE 2) Town File No. 2020-97 & 2020-127

NEC Meeting #2

DATE: October 31, 2022 at 11:00am

PLACE: Microsoft Teams Meeting

PROJECT NO.: R205388 & R205421

PRESENT:	Joe Muller Haleema Sarwar Shun Cheung Ben Pressman John Does Winnie Wong	Niagara Escarpment Commission (NEC) Niagara Escarpment Commission Town of Caledon (Town) Town of Caledon R.V. Anderson Associates Limited (RVA) R.V. Anderson Associates Limited
-----------------	-----------------------------------------------------------------------------------------	-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

DISTRIBUTED: All present

DISCUSSION:

ACTION BY:

1.0 Introduction

- | | | |
|-----|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 1.1 | The purpose of this meeting is to review three ongoing projects in Town of Caledon that are under NEC's jurisdiction: | Info |
| | <ul style="list-style-type: none"> 2020-87 – Mountainview Road from Grange Sideroad to Granite Stones Drive 2020-97 – Mill Street from Caledon Trailway to Mississauga Road 2020-127 – Mountainview Road from Granite Stones Drive to Charleston Sideroad | |

- | | | |
|-----|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 1.2 | 2020-127 Mountainview Road is a detailed design project to implement the recommendations in the Town's completed 2019 ESR Study. During the EA Study in 2019, NEC confirmed the proposed scope of work does not require a development permit but request a set of IFC drawings be circulated to them. RVA to forward IFC drawings to NEC when ready. | Info |
|-----|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|

2.0 Progress Update

- | | | |
|-----|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 2.1 | RVA has been in consultation with MECP, CVC, and TRCA for all three roads regarding stormwater management and environmental inventory. | Info |
| 2.2 | Mill Street detailed design is being completed under Schedule 'B' of the Municipal Class Environmental Assessment. To date, the Town has completed two PICs and RVA is working towards the first draft of the Project File Report. When the Project File Report has been filed and closed, RVA will proceed to the 60% detailed design. | Info |

DISCUSSION:

ACTION BY:

- | | | |
|-----|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------|
| 2.3 | RVA to forward the completed 90% design drawings for 2020-127 Mountainview Road. NEC will review and provide comment. <i>[Post meeting note: 60% design drawings, arborist report, and Natural Environmental Assessment memo for Mountainview Road was forwarded to NEC on September 8, 2022 via Newforma Info Exchange. 90% design drawings for Mountainview Road was forwarded to NEC via Newforma Info Exchange on November 21, 2022.]</i> | NEC/
RVA |
|-----|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------|

3.0 Permit Requirements

- | | | |
|-----|-----------------------------------------------------------------------------------------------------------------|------|
| 3.1 | NEC confirmed there is only one permit application form for both developments and road rehabilitation projects. | Info |
|-----|-----------------------------------------------------------------------------------------------------------------|------|

6.0 Other Discussion

Nothing to report at this time	Info
--------------------------------	------

Next review meeting TBD.

Notes prepared by: Winnie Wong

R:\2020\205388 - 2022 Roads Rehab\D Meetings\01 External\D. NEC\20221031 - Meeting 2\2020-97_2020-127-NEC Meeting #2.docx

PLEASE ADVISE THE WRITER OF ANY ERRORS OR OMISSIONS WITHIN 1 WEEK OF RECEIPT OF THESE NOTES



Transmittal

PROJECT: 2022 Roads Rehab 205388 DATE: 2023-02-22

SUBJECT: 205388 - Mill Street preliminary drawings TRANSMITTAL ID: 00067

PURPOSE: For your review and comment VIA: Info Exchange

FROM

NAME	COMPANY	EMAIL	PHONE
Winnie Wong		Winnie.Wong@rvanderson.com	

TO

NAME	COMPANY	EMAIL	PHONE
Haleema.Sarwar@ontario.ca		Haleema.Sarwar@ontario.ca	
Joe.Muller@ontario.ca		Joe.Muller@ontario.ca	

REMARKS: Hi Haleema/ Joe,

Please use this link to download our Mill Street drawings that will be included in our draft Project File Report (PFR) which we will be forwarding to you end of February.

The key proposed works included in these drawings are as follows:

- rehabilitate road pavement following the existing horizontal and vertical profile
- no pavement shoulder widening, paved platform width to match existing asphalt edges
- no ditch realignment
- replace culverts that do not meet drainage capacity or in poor physical condition
- cleaning out/ localize regrading of the ditches to restore positive drainage

We will be reviewing the following issues in further detail with CVC during our detailed design:

- flooding at 1499 Mill Street
- slope stability at culvert just east of Caledon Trailway
- steep embankment slopes on some culverts

Please note, as we have yet to file the PFR for public review there may be slight modifications to these drawings depending on comments/ feedback we receive. As such we kindly ask NEC to keep these drawings confidential until we are ready to file. We would also like to request a meeting with NEC anytime after end of February. The same drawings have been forwarded to CVC for their review.

Regards,
Winnie

Transmittal

DATE: 2023-02-22
TRANSMITTAL ID: 00067

DESCRIPTION OF CONTENTS

QTY	DATED	TITLE	NOTES
1	2022-12-20	205388-Mill St -P&P-DD-PP-30%.pdf	
1	2022-11-30	205388-TS_Mill S-DD-TS.pdf	
1	2022-12-01	205388-XSEC_Mill St-DD-Driveways.pdf	
1	2022-12-01	205388-XSEC_Mill St-DD-x-section.pdf	

COPIES:

Shun Cheung	(Town of Caledon)
John Does	(R.V. Anderson Associates Limited)
Connor MacIsaac	(R.V. Anderson Associates Limited)

Connor MacIsaac

From: Winnie Wong
Sent: March 3, 2023 2:55 PM
To: Sarwar, Haleema (MNRF); Henderson, Brandon (MNRF)
Cc: John P. Does; Connor MacIsaac
Subject: 205388 - Mill Street NEC review

Hi Brandon/ Haleema,

Thanks again for calling in today re our Mill Street. Below is a quick summary of what we discussed:

- Mill Street is a detailed design project completed under Schedule 'B' Municipal Class Environmental Assessment. RVA is currently wrapping up the Project File Report (PFR) and will be forwarding a copy of that to the NEC, CVC, and the Town
- After the public consultation process, the Town opted to maintain the existing road alignment, profile, and match back the existing asphalt pavement edges. Ditches will be regraded (localized sections) and cleaned out to restore positive drainage
- The section of Mill Street under NEC's jurisdiction runs between Mississauga Road and the Caledon Trailway Path. RVA confirmed there are no culvert replacement or new culvert additions in this section
- Based on the scope of work, Mill Street may be granted an exemption from NEC. RVA to submit application form together with the PFR for NEC to review. Review period is approximately 3 weeks
- If scope of work changes during detailed design, then RVA will notify NEC and forward a copy of drawings for review again

Have a good weekend,

Winnie Wong, P.Eng., PMP, M.Eng. (she / her)

Associate, Project Manager, Transportation



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1471 | m 416 390 5328

[LinkedIn](#) | [Facebook](#) | [Website](#)



APPENDIX 1-5

Indigenous Communities Communication



Connor MacIsaac

From: Connor MacIsaac
Sent: January 23, 2023 3:40 PM
To: Abby LaForme
Cc: Shun Cheung; Winnie Wong; Mark LaForme
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report (DOCA Project #2020-0598) .
Attachments: RE Mill Street EA MCFN Stage 1 AA Archaeological Review (DOCA Project # 2020-0598)-20211019.msg
Categories: Filed by Newforma

Hi Abby,

On behalf of the Town of Caledon, thank you for MCFN DOCA's continued interest and input to the Mill Street Class EA. The Stage 1 Archaeological Assessment Report was submitted to the Ministry of Heritage, Sport, Tourism, and Culture Industries (now MCM), and entered into the Ontario Public Register of Archaeological Reports.

As previously requested by Erika in the attached, MCFN DOCA will be invited to participate in the recommended Stage 2 Archaeological Assessment during detailed design.

We will also ensure that MCFN DOCA will receive notice of the completion of the EA as well as a digital copy of EA Project File Report as requested.

Best,



Connor MacIsaac, ENV SP, EPt

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



From: Abby LaForme <Abby.LaForme@mncfn.ca>
Sent: January 13, 2023 9:19 AM
To: Connor MacIsaac <cmaclsaac@rvanderson.com>
Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>; Mark LaForme <Mark.LaForme@mncfn.ca>
Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good Morning Connor,

Thank you for reaching out to MCFN DOCA regarding the Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report.

MCFN DOCA would like a digital copy of the EA for review when completed.

MCFN DOCA would also like to request a follow-up on the Archaeological studies that were done for this project, if possible.

Thank you and be safe out on those roads today!

Abby (LaForme) Lee
Acting Consultation Coordinator



Mississaugas of the Credit First Nation (MCFN)
Department of Consultation & Accommodation (DOCA)
4065 Highway 6, Hagersville, ON N0A 1H0
Ph: (905) 768 – 4260
Email: Abby.LaForme@mncfn.ca

From: Connor MacIsaac <cmaclsaac@rvanderson.com>

Sent: Thursday, January 12, 2023 3:14 PM

Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>

Subject: Town of Caledon: Mill Street Class EA - Notice of Study Recommendations and PIC No.2 Summary Report

Dear Sir/Madam,

On behalf of the Town, please find attached the Notice of Study Recommendations for the Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA). The purpose of this notice is to garner feedback on the recommendations for Mill Street developed in consideration of the comments received during and following the second Public Information Centre (PIC), held on June 15, 2022. A detailed summary of the comments received at and following the second PIC, and how these comments were incorporated into the development of the study recommendations, is presented in the PIC Summary Report on the Town's website at <https://www.caledon.ca/en/news/mill-street-ea.aspx>.

To provide comments, questions, or concerns on the study recommendations, or the PIC Summary Report, please contact the project team members listed on the attached notice by February 2, 2023.

Upon completion of the PIC Summary Report comment period, a Project File Report (PFR), containing details of the Municipal Class EA planning and decision-making process, public consultation process and study recommendations will be made available for a 30-day public review period, concluding the EA study. A Notice of Completion will be issued at that stage.

Thank you again for your continued input to the study.

Best Regards,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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Connor MacIsaac

From: Erika Johannsen <Erika.Johannsen@mncfn.ca>
Sent: October 19, 2021 2:03 PM
To: Connor MacIsaac; Shun Cheung
Cc: Megan DeVries; Adrian Blake; Fawn Sault
Subject: RE: Mill Street EA MCFN Stage 1 AA Archaeological Review (DOCA Project #2020-0598)

Categories: Indigenous

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good afternoon Connor,

Thank you for your patience. Regrettably MCFN-DOCA is currently at capacity regarding our ability to review reports. We have forgone a full review of the Stage 1 Archaeological Assessment Report for the Mill Street Class EA in anticipation that MCFN Field Liaison Representatives will be invited to participate in the recommended Stage 2 archaeological assessment.

Our records show that MCFN-DOCA's agreements with The Town of Caledon for MCFN FLR participation and archaeological report review require renewal. When Stage 2 work is anticipated, please follow up with Field Archaeologist Adrian Blake at Adrian.Blake@mncfn.ca to execute the necessary agreements, and contact Acting Field Coordinator Marlene Morton at Field.coordinator@mncfn.ca regarding FLR scheduling.

Thank you for consulting with MCFN-DOCA on this project.

Kind regards,

Erika Johannsen (she/they)
Field Archaeologist



Department of Consultation and Accommodation (DOCA)
Mississaugas of the Credit First Nation (MCFN)
4065 Highway 6 North, Hagersville, ON N0A 1H0
Mobile: 905-870-5844
<http://www.mncfn.ca>

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From: Connor MacIsaac <cmacisaac@rvanderson.com>
Sent: Tuesday, October 19, 2021 11:16 AM
To: Megan DeVries <Megan.DeVries@mncfn.ca>
Cc: Mark LaForme <Mark.LaForme@mncfn.ca>; Shun Cheung <shun.cheung@caledon.ca>; Eliza Brandy

<ebrandy@asiheritage.ca>; Fawn Sault <Fawn.Sault@mncfn.ca>

Subject: RE: Mill Street EA MCFN Stage 1 AA Archaeological Review (DOCA Project #2020-0598)

Hi Megan,

Have you had a chance to review the report for the Stage 1 Archaeological Assessment Report for the Mill Street Class EA, DOCA Project #2020-0598? We were hoping that your team would be provide any input for incorporation into the final report by Monday October 18th, however if more time is needed, we will need to request an extension for our project registered with the Ministry this week.

If MCFN requires additional time to review and provide comment, kindly provide a timeline for when we can expect the comments. On behalf of the Town of Caledon, thank you again for your interest and participation in the Mill Street Class EA Study

Kind Regards,



Connor MacIsaac, EPt

JUNIOR ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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From: Connor MacIsaac

Sent: September 17, 2021 2:58 PM

To: Megan DeVries <Megan.DeVries@mncfn.ca>

Cc: Mark LaForme <Mark.LaForme@mncfn.ca>; Shun Cheung <shun.cheung@caledon.ca>; Eliza Brandy <ebrandy@asiheritage.ca>; Fawn Sault <Fawn.Sault@mncfn.ca>

Subject: RE: Mill Street EA MCFN Stage 1 AA Archaeological Review & Notice of PIC

Hi Megan,

In accordance with the Archaeological Review Agreement executed for the ongoing Mill Street Class EA, please find attached the Draft Stage 1 archaeological assessment report for your review and comment. Please review the report and provide any input for incorporation into the final report by Monday October 18th.

I would also like to take this opportunity to share the attached Notice of Online Public Information Centre (PIC) for the Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA). The purpose of this meeting is to share information and receive public input on the to study date, including the problem / opportunity, existing conditions, alternative solutions, and the preliminary preferred solution. The meeting will be held on **October 5, 2021 at 6 p.m.**, and will consist of a presentation by the Town followed by a Question and Answer period concluding at 7 p.m.

Due to continuing efforts to contain the spread of COVID-19 and to protect individuals, this meeting will be held electronically. Refer to the attached notice, or visit the Town of Caledon's project website, <https://www.caledon.ca/en/news/mill-street-ea.aspx>, for further information on how to participate. The meeting link will also be posted to the project website closer to the meeting date. Questions can be submitted prior to the event to the contacts below.

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Engineering Services
905.584.2272 X.4040
shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
416.497.8600 X.1471
winnie.wong@rvanderson.com

On behalf of the Town, I would like to again thank you for your input in this Class EA study.

Kind Regards,

**Connor MacIsaac, EPt**

JUNIOR ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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From: Megan DeVries <Megan.DeVries@mncfn.ca>

Sent: January 4, 2021 4:03 PM

To: Connor MacIsaac <cmacisaac@rvanderson.com>; Fawn Sault <Fawn.Sault@mncfn.ca>

Cc: Mark LaForme <Mark.LaForme@mncfn.ca>; Shun Cheung <shun.cheung@caledon.ca>; Eliza Brandy <ebrandy@asiheritage.ca>

Subject: RE: Humber Station Rd EA & Mill Street EA MCFN Archaeological Review Agreements

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Connor,

Please find attached the fully executed review agreements for Humber Station Road (DOCA Project #2020-0599) and Mill Street (DOCA Project #2020-0598).

All the best,
Megan.

Megan DeVries, M.A.
Archaeological Operations Supervisor



Department of Consultation and Accommodation (DOCA)

Mississaugas of the Credit First Nation (MCFN)

4065 Highway 6 North, Hagersville, ON N0A 1H0

P: 905-768-4260 | M: 289-527-2763

<http://www.mncfn.ca>

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From: Connor MacIsaac <cmacisaac@rvanderson.com>

Sent: Thursday, December 24, 2020 9:29 AM

To: Megan DeVries <Megan.DeVries@mncfn.ca>; Fawn Sault <Fawn.Sault@mncfn.ca>

Cc: Mark LaForme <Mark.LaForme@mncfn.ca>; Shun Cheung <shun.cheung@caledon.ca>; Eliza Brandy <ebrandy@asiheritage.ca>

Subject: Humber Station Rd EA & Mill Street EA MCFN Archaeological Review Agreements

Hi Megan,

On behalf of the Town of Caledon, please find attached the provided MCFN Archaeological Review Agreements signed by the Town of Caledon for both the Humber Station Road, and Mill Street Environmental Assessments.

From all of us at RVA, we hope you have a safe and happy holidays, and look forward to working with the MCFN in the new year.

Regards,



RVA IS GROWING!

Our NEW Halton and Halifax
offices are now open.



Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

R.V. Anderson Associates Limited

43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

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From: Megan DeVries <Megan.DeVries@mncfn.ca>

Sent: December 15, 2020 1:40 PM

To: Connor MacIsaac <cmacisaac@rvanderson.com>; Fawn Sault <Fawn.Sault@mncfn.ca>

Cc: Mark LaForme <Mark.LaForme@mncfn.ca>; Shun Cheung <shun.cheung@caledon.ca>; Eliza Brandy <ebrandy@asiheritage.ca>; Tisha Doucette <TDoucette@rvanderson.com>

Subject: RE: MCFN Response to Humber Station Rd EA

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hello Connor,

Thank you kindly for the update and information you have provided. We look forward to receiving the signed agreements from you and arranging for our participation and review of the upcoming assessments.

Sincerely,
Megan.

Megan DeVries, M.A.
Archaeological Operations Supervisor



Department of Consultation and Accommodation (DOCA)
Mississaugas of the Credit First Nation (MCFN)
4065 Highway 6 North, Hagersville, ON N0A 1H0
P: 905-768-4260 | M: 289-527-2763
<http://www.mncfn.ca>

HOLIDAY ALERT: Please note that MCFN-DOCA will be closed from December 19th until January 3rd.

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From: Connor MacIsaac <cmacisaac@rvanderson.com>
Sent: Friday, December 11, 2020 2:06 PM
To: Megan DeVries <Megan.DeVries@mncfn.ca>; Fawn Sault <Fawn.Sault@mncfn.ca>
Cc: Mark LaForme <Mark.LaForme@mncfn.ca>; Shun Cheung <shun.cheung@caledon.ca>; Eliza Brandy <ebrandy@asiheritage.ca>; Tisha Doucette <TDoucette@rvanderson.com>
Subject: RE: MCFN Response to Humber Station Rd EA

Hello Fawn and Megan,

On behalf of the Town of Caledon, thank you for your interest in the Mill Street Class EA Study. I have cc'd key members of the Project team including RVA's project manager Winnie Wong, Town of Caledon project manager Shun Cheung, ASI's Archaeologist Eliza Brandy, and RVA's natural environmental lead Tisha Doucette.

As part of the EA, ASI will be complete a Stage 1 Archaeological Assessment. While the property inspection has been completed for the Stage 1 AA, we'd be happy to provide a copy of the draft report to you for your review and comment, once it's completed in the new year. The Town of Caledon will review and execute the Archaeological Review Agreement provided prior to sending back to you. In the event a Stage 2 archaeological Assessment is required as part of the EA or during detailed design, we will notify MCFN and arrange for FLRs to participate prior to undertaking any fieldwork.

Our natural environmental team will be conducting a tree screening to support the Class EA in February. We would like to welcome MCFN FLR representatives to attend this field work if interested. Additional natural environmental field work (ELC, tree inventory, Fish and Fish Habitat, incidental wildlife) will be conducted during detailed design, likely mid May 2021. Again, we will notify MCFN and arrange for FLRs to participate prior to undertaking this fieldwork during detailed design.

With regards to the information requested in your letter, here is some additional background information regarding the Class EA. The Mill Street Class EA is being undertaken by RVA on behalf of the Town of Caledon (proponent).

The study will be completed as a Schedule "B" project in accordance with the Municipal Class Environmental Assessment (October 2000, amended in 2007, 2011 & 2015). The final deliverable will be a Project File Report, which is anticipated to be completed and available for public review in March 2021. Subject to comments received on the Project File Report, the Town intends to proceed with the detailed design of this project throughout 2021, with construction in 2022. Please see the Notice of Commencement attached for further details regarding the goals of the study and subsequent road reconstruction.

The contact information for the Project Managers undertaking the Class EA are found below:

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Engineering Services
Finance and Infrastructure Services
Tel: 905.584.2272 x 4040
E-mail: shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
Tel: 416.497.8600 x 1471
E-mail: winnie.wong@rvanderson.com

Once again, thank you for your comments and your interest in the Class EA study. Please let us know if you have any additional comments or concerns.

Kind Regards,



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Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

R.V. Anderson Associates Limited

43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

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From: Megan DeVries <Megan.DeVries@mncfn.ca>

Sent: November 25, 2020 9:36 AM

To: Fawn Sault <Fawn.Sault@mncfn.ca>; Connor MacIsaac <cmacisaac@rvanderson.com>; Shun Cheung

<shun.cheung@caledon.ca>

Cc: Mark LaForme <Mark.LaForme@mncfn.ca>

Subject: RE: MCFN Response to Humber Station Rd EA

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good morning,

Please find attached a letter from the Mississaugas of the Credit First Nation ("MCFN") regarding the upcoming assessment for Humber Station Road EA, as identified below.

Please note that this year, in order to continue maintaining DOCA capacity for fulsome project participation, DOCA will be introducing charges for technical review of project information. In the exercise of its stewardship responsibility, DOCA seeks to work together with project proponents and their archaeological consultants to ensure that archaeological work is done properly and respectfully. DOCA has retained technical advisers with expertise in the field of archaeology. These experts will review the technical aspects and cultural appropriateness of the archaeological assessments and strategies associated with your project. Upon completion of these reviews, MCFN will identify, if necessary, mitigation measures to address any project impacts upon MCFN rights. For cultural materials and human remains, DOCA may advise that this includes ceremonies required by Anishinaabe law, as well as request adjustments to the proposed fieldwork strategy.

The proponent is expected to pay the costs for MCFN to engage in a technical review of the project. DOCA anticipates at this time that all archaeological review will be undertaken by in-house technical experts, but will advise the proponent if an outside peer-review is required. Please find attached the agreement that covers MCFN's inhouse technical review of the archaeological assessments and strategies associated with your project(s). If you could please fill in the additional required information, highlighted in yellow, and return to us a signed copy, that would be greatly appreciated. After we have received it, we can execute the contract on our end and return the completed contract to you. Afterwards, I can arrange scheduling and other related matters directly with the consultant if you prefer.

Sincerely,
Megan.

Megan DeVries, M.A.
Archaeological Operations Supervisor



Department of Consultation and Accommodation (DOCA)
Mississaugas of the Credit First Nation (MCFN)

4065 Highway 6 North, Hagersville, ON NOA 1H0

P: 905-768-4260 | M: 289-527-2763

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From: Fawn Sault
Sent: Monday, November 23, 2020 4:16 PM
To: Connor MacIsaac <cmacisaac@rvanderson.com>; Shun.Cheung@caledon.ca
Cc: Mark LaForme <Mark.LaForme@mncfn.ca>; Megan DeVries <Megan.DeVries@mncfn.ca>
Subject: MCFN Response to Humber Station Rd EA

Good Afternoon Connor,

Please see the attached letter regarding the Humber Station Rd. EA commencement.

Miigwech,

Fawn Sault
Consultation Coordinator
Mississaugas of the Credit First Nation
4065 Hwy. 6, Hagersville, N0A 1H0
Website: <http://mncfn.ca/>
Ph: 905-768-4260
Cell:289-527-6580

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November 23,2020

VIA EMAIL

Connor MacIsaac, EPt
Junior Environmental Planner, EA & Approvals
cmacisaac@rvanderson.com

Dear Connor,

RE: MCFN Response to Mill Street Class Environmental Assessment Study
(Schedule B MCEA)

Confirmation of Receipt


I am writing on behalf of the Mississaugas of the Credit First Nation ("MCFN") to acknowledge that we have received your above named communication, dated October 22,2020 .

Outline of MCFN Rights and Territory

In 1818 , the Crown and MCFN entered into Ajetance Treaty, No. 19 (1818) regarding the lands in which your project is situated.

The Mississaugas of the Credit First Nation are the descendants of the "River Credit" Mississaugas. The undisputed Territory of the MCFN is defined as a Territory commencing at Long Point on Lake Erie thence eastward along the shore of the Lake to the Niagara River. Then down the River to Lake Ontario, northward along the shore of the Lake to the River Rouge east of Toronto then up that river to the dividing ridges to the head waters of the River Thames then southward to Long Point, the place of the beginning. Our Territory encompasses the lands and waters that were used and occupied by our Ancestors. Territories are usually large tracts of land that reflect the breadth required for seasonal activities and habitation and changes in those movement patterns through time. Through Treaties with the Crown, MCFN agreed to share our Territory with newcomers. However, not all of MCFN's Territory has been dealt with through a Treaty.





With the exception of a small part of the Credit River, our Treaties with the Crown did not deal with the water parts of our Territory. We have not agreed to share any part of our waters with settlers. We formally gave notice to the Crown of this claim in 2016. We note that any lands that have been artificially created on our waters have also not been dealt with by any Treaty.

Like our ancestors before us, we continue to use the lands, waters, and watershed ecosystems within our Territory for a variety of livelihood, harvesting, ceremonial and spiritual purposes. We have always exercised governance functions and stewardship in order to protect our Territory, conserve the fish and wildlife that depend upon it, and ensure its ongoing ability to sustain our people. We assert that our Aboriginal and treaty rights fundamentally entitle us to continue to act as stewards of our Territory, to be involved in decisions that affect it, and to participate in the ongoing, responsible management of the resources it provides.

Duty to Consult and Accommodate

As you will know, the Crown has a constitutional duty to consult and accommodate MCFN in respect of any decisions that might affect its asserted or proven Aboriginal and/or Treaty Rights. We expect that, consistent with the Crown's constitutional duty, no approval should be issued to this project until MCFN has been sufficiently consulted and accommodated. Nothing in this letter shall be construed as to affect our Aboriginal and/or Treaty Rights and hence shall not limit any consultation and accommodation owed to MCFN by the Crown or any proponent, as recognized by section 35 of the Constitution Act, 1982.

MCFN has the right to free and informed consent prior to the approval of any project or any planning decision adversely impacting its Territory and to benefit economically from resource development within its Territory.

MCFN has formed the Department of Consultation and Accommodation ("DOCA") to represent its interests in consultation and accommodation matters. It is DOCA's mandate to ensure that we are directly involved in all planning and development that impacts the integrity of our Territory. In this regard, DOCA will assess and help alleviate impacts on our rights, land claims, and ways of life by building relationships with governments and private sector proponents. We share a mutual interest in ensuring that projects in the Territory are planned, reviewed, and developed in a manner which ensures healthy communities, ecological protection, and sustainable development for present and future generations in the Territory.



DEPARTMENT OF CONSULTATION AND ACCOMMODATION

Mississaugas of the Credit First Nation
4065 Hwy #6, Hagersville, Ontario N0A 1H0



Phone: (905) 768-4260



MCFN is not opposed to development, but MCFN must to be involved in development decision making. MCFN has a deep connection to its Territory and we have a stewardship responsibility for our land. By engaging with us, a project proponent can learn our perspective on how to care for this land and we can work together to shape the project to mitigate damaging effects to our land and perhaps even work to improve our environment. MCFN is the only party who shall determine whether there are impacts to our Aboriginal and treaty rights.

One of the ways we require proponents to engage with us is in providing transparency during the environmental survey and archaeological assessment process. The best way to accomplish this is by having Field Liaison Representatives (“FLRs”) on location while fieldwork is occurring, who can ensure that the Nation’s special interests and concerns are respected and considered during fieldwork. The cultural and natural resources in question are part of MCFN’s territory and heritage and it is our responsibility to ensure their protection, on behalf of the Nation. MCFN’s stewardship of its territory extends through the life of any development project and beyond.

DOCA Project Registration

DOCA has completed an initial intake review of the project communication you have provided. This file has been assigned DOCA Project 2020-0598 ; please use this number in all future communications.

We respectfully ask you to immediately notify us if there are any changes to the project.

Referral to DOCA Units

Following DOCA’s initial intake review of the project communication, the file has been referred to the following DOCA Units for additional follow-up.

	Unit Identification	Primary Contact	Email Address
✓	Archaeology	Megan DeVries	megan.devries@mncfn.ca
✓	Cultural/Historical	Darin Wybenga	darin.wybenga@mncfn.ca
✓	Environment	Fawn Sault (Temp)	fawn.sault@mncfn.ca
✓	FLR Participation	Megan DeVries	megan.devries@mncfn.ca
	Governance	Mark LaForme	mark.laforme@mncfn.ca
	Economic Development	Director	SED.Director@mncfn.ca

If you have not been contacted by the indicated DOCA Units within fourteen days following receipt of this letter, please let me know.



DEPARTMENT OF CONSULTATION AND ACCOMMODATION

Mississaugas of the Credit First Nation
4065 Hwy #6, Hagersville, Ontario N0A 1H0



Phone: (905) 768-4260



Request for Missing Information

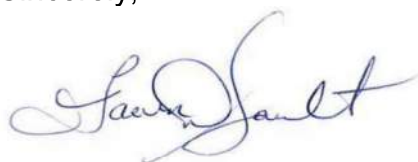
In order to proceed with our follow-up review, we ask you to ensure that all available information relating to the project has been transmitted to us. We have identified the following general information as missing from your initial project communication:

Outstanding Project Information	
✓	Name of person or body undertaking the action or decision.
✓	Contact information for the person or body undertaking the action or decision.
✓	List of documents pertaining to the proposed action/decision that are available for MCFN to review.
✓	Description of what other information is expected to become available before the proposed action/decision is undertaken.
✓	Deadlines or filing dates pertaining to the action/decision.
✓	The Crown or Municipal review/ approval that is required for the project.
✓	How the proposed action or decision may affect and/or benefit MCFN, its rights and territory.

Closing

We ask that you respond with the above requested information within fourteen days following receipt of this letter. We thank you in advance for your attention to our requirements and we look forward to working with you further to shape the planning for development in our Territory.

Sincerely,



Fawn Sault
Consultation Coordinator
fawn.sault@mncfn.ca



DEPARTMENT OF CONSULTATION AND ACCOMMODATION

Mississaugas of the Credit First Nation
4065 Hwy #6, Hagersville, Ontario N0A 1H0



Phone: (905) 768-4260



November 25,2020

VIA EMAIL

Connor MacIsaac, EPt
Junior Environmental Planner, EA & Approvals
cmacisaac@rvanderson.com

Dear Connor MacIsaac,

RE: MCFN FLR Participation for Mill Street Class Environmental Assessment Study.

Confirmation of Receipt

I am writing in follow up to the letter of response dated November 23,2020 sent by Fawn Sault, Consultation Coordinator, from the Department of Consultation and Accommodation ("DOCA") on behalf of the Mississaugas of the Credit First Nation ("MCFN") to acknowledge that we have received your above named communication, dated October 22,2020 .

Outline of MCFN Rights and Territory


In 1818 , the Crown and MCFN entered into Ajetance Treaty, No. 19 (1818) regarding the lands in which your project is situated.

MCFN has formed the Department of Consultation and Accommodation ("DOCA") to represent its interests in consultation and accommodation matters. In this regard, it is DOCA's mandate to ensure that we are directly involved in all planning and development that impacts the integrity of our Territory. DOCA will assess and help alleviate impacts on our rights, land claims, and ways of life by building relationships with governments and private sector proponents. We share a mutual interest in ensuring that projects in the Territory are planned, reviewed, and developed in a manner which ensures healthy communities, ecological protection, and sustainable development for present and future generations in the Territory.


One of the ways we require proponents to engage with us is in providing transparency during the environmental survey and archaeological assessment process. The best way

DEPARTMENT OF CONSULTATION AND ACCOMMODATION

Mississaugas of the Credit First Nation
4065 Hwy #6, Hagersville, Ontario NOA 1H0

 Phone: (905) 768-4260





to accomplish this is by having Field Liaison Representatives (“FLRs”) on location while fieldwork is occurring, who can ensure that the Nation’s special interests and concerns are respected and considered during fieldwork. The cultural and natural resources in question are part of MCFN’s territory and heritage and it is our responsibility to ensure their protection, on behalf of the Nation. MCFN’s stewardship of its territory extends through the life of any development project and beyond.

It is our expectation that no project-related fieldwork will take place without the participation of our FLRs. MCFN considers it disrespectful of our rights as Indigenous peoples if our natural and cultural heritage is interfered with without our involvement.

FLR Participation

DOCA deploys FLRs to be boots on the ground so that fieldwork by a proponent and their consultants/contractors is carried out with appropriate care, thoroughness, and respect. In the context of MCFN’s Territory, where so much natural and cultural heritage has already been lost or destroyed, MCFN’s monitoring of fieldwork is of utmost importance to ensure that the trail of desecration stops. FLRs are deployed to observe fieldwork, provide cultural advice, act as a direct link back to DOCA and MCFN, and assist with compliance.

FLRs are MCFN band members who have received training in environmental and archaeological assessments, traditional medicine identification and use, Anishinaabe burial practices, and more throughout their employment with DOCA.

DOCA requires, at minimum, FLR participation during the following project-related studies and/or activities:

- ecological and natural heritage technical studies
- archaeological assessments (Stages 2 through 4) and site visits
- monitoring of activities within 50m of areas of special concern (e.g. waterways and wetlands, archaeological sites, species at risk)
- post-construction remediation activities and follow-up impact monitoring

Agreement for FLR Participation

The cost for the participation of our FLRs is covered by the proponent, not the consultant, whom we view as having the ultimate responsibility to consult with, and accommodate, the Nation. Therefore, please find attached the agreement that covers MCFN’s participation in the upcoming fieldwork. The costs associated with this involvement reflect a number of expenses not visible at first glance: payment for the FLRs themselves, operational costs for DOCA, and efforts to engage the community to garner feedback on development projects. If you could please fill in the additional required information, highlighted in yellow, and return to us a signed copy so that we may arrange for FLR participation on your project, that would be greatly appreciated.



Once a signed agreement is in place, DOCA generally arranges scheduling and other related matters directly with the consultant conducting the fieldwork, unless you prefer otherwise.

Please note that MCFN requires two of its FLRs to be on location whenever fieldwork is taking place within its territory. The reason for this is so that FLRs can provide support and security for each other in the field. This has become a requirement in light of uncommon, but unfortunate, occurrences when FLRs have felt pressured or intimidated from external persons while at work locations. We ask that you would respect this request.

Request for Missing Information

In order to complete our project record, we ask that you provide the following information:

1. Please provide a list of all completed technical studies for the project, their date of completion, and the contact information of the consultant who completed each study.
2. Please provide a list of all incomplete and/or upcoming technical studies for the project, the anticipated date of fieldwork for each, and the contact information for the consultant who will complete them.
3. Are there any short-term and/or long-term avoidance and protection strategies currently in place for the natural and/or cultural resources in the study area for this project? If yes, what are they?

Closing

The participation of FLRs in project fieldwork is only one part of the consultation process that may be required for your development. Please contact DOCA's Consultation Coordinator, Fawn Sault, if you have any questions about the process.

We ask that you respond with the above requested information and executed agreement within fourteen days following receipt of this letter. We thank you in advance for your attention to our requirements and we look forward to working with you further to shape the planning for development in our Territory.

Sincerely,



Megan DeVries
Archaeological Operations Supervisor
megan.devries@mncfn.ca



Attachment(s) MCFN *Standards and Guidelines for Archaeology* [2018]
FLR Participation Agreement [2020]



DEPARTMENT OF CONSULTATION AND ACCOMMODATION

Mississaugas of the Credit First Nation
4065 Hwy #6, Hagersville, Ontario N0A 1H0



Phone: (905) 768-4260



November 25, 2020

VIA EMAIL

Connor MacIsaac, EPt
Junior Environmental Planner, EA & Approvals
cmacisaac@rvanderson.com

Dear Connor MacIsaac,

RE: MCFN Archaeological Review for Mill Street Class Environmental Assessment

Confirmation of Receipt

I am writing in follow up to the letter of response dated November 23, 2020 by Fawn Sault, Consultation Coordinator, from the Department of Consultation and Accommodation ("DOCA") on behalf of the Mississaugas of the Credit First Nation ("MCFN") to acknowledge that we have received your above named communication, dated October 22, 2020.

Outline of MCFN Rights and Territory

In 1818, the Crown and MCFN entered into Ajetance Treaty, No. 19 (1818) regarding the lands in which your project is situated.


MCFN has formed the Department of Consultation and Accommodation ("DOCA") to represent its interests in consultation and accommodation matters. In this regard, it is DOCA's mandate to ensure that we are directly involved in all planning and development that impacts the integrity of our Territory. DOCA will assess and help alleviate impacts on our rights, land claims, and ways of life by building relationships with governments and private sector proponents. We share a mutual interest in ensuring that projects in the Territory are planned, reviewed, and developed in a manner which ensures healthy communities, ecological protection, and sustainable development for present and future generations in the Territory.

MCFN has a stewardship responsibility over its Territory and asserts that our Aboriginal and treaty rights fundamentally entitle us to preserve our culture and heritage, including

DEPARTMENT OF CONSULTATION AND ACCOMMODATION

Mississaugas of the Credit First Nation
4065 Hwy #6, Hagersville, Ontario N0A 1H0

Phone: (905) 768-4260



archaeological materials and human burials. Our Territory is the source of our identity as a First Nation and the basis for many cultural activities and spiritual ceremonies. It is home to sacred sites, burial grounds, traditional teachings and meeting places, and sites of profound archaeological and historical significance. We assert that our Aboriginal and treaty rights fundamentally entitle us to preserve our cultural and heritage.

Too much of our cultural objects and the remains of our ancestors have been lost already through development of the most intensely urbanized lands in Canada and we have a strong interest in ensuring that no more of it becomes bulldozed and desecrated.

MCFN Standards and Guidelines for Archaeology

In April 2018, MCFN Chief and Council adopted the *MCFN Standards and Guidelines for Archaeology*, a document aimed to provide guidance to consultant archaeologists, proponents, governments, etc. who are conducting archaeological assessment activities within MCFN's Territory. It sets out, in MCFN's own words, what engagement with our Nation should entail for archaeology as well as technical expectations for fieldwork, in relation to the provincial regulations which were created without our input and feedback. It is important to note that MCFN holds all archaeological resources present within its Territory as of interest to the Nation as part of their cultural patrimony. Resources, regardless of size, frequency, condition, etc., should not be interpreted by non-MCFN representatives in such a way as to remove the requirement for engagement with our Nation.

We are attaching a copy for your reference. We expect compliance with these *Standards and Guidelines* as any fieldwork you will be conducting will have the potential of disturbing MCFN's cultural artifacts or its ancestors' remains.

MCFN Expectations Regarding Ancestors' Remains

MCFN has obligations under Anishinaabe law to protect burials within its Territory and MCFN maintains its right to do so. Our ancestors buried their loved ones in our Territory with the understanding that they would not be disturbed.

We would also like to draw your attention to our expectation that at any time that ancestral remains are encountered during fieldwork, we expect all activities on site to stop and that MCFN be contacted immediately to determine a proper course of action.

Technical Review

In the exercise of its stewardship responsibility, DOCA seeks to work together with project proponents and their archaeological consultants to ensure that archaeological work is done properly and respectfully. DOCA has retained technical advisers with expertise in the field of archaeology. These experts will review the technical aspects and cultural appropriateness of the archaeological assessments and strategies associated with your project. Upon completion of these reviews, MCFN will identify, if



necessary, mitigation measures to address any project impacts upon MCFN rights. For cultural materials and human remains, DOCA may advise that this includes ceremonies required by Anishinaabe law, as well as request adjustments to the proposed fieldwork strategy.

The proponent is expected to pay the costs for MCFN to engage in a technical review of the project. DOCA anticipates at this time that all archaeological review will be undertaken by in-house technical experts, but will advise the proponent if an outside peer-review is required. Please find attached the agreement that covers MCFN's in-house technical review of the archaeological assessments and strategies associated with your project. Please fill in the additional required information, highlighted in yellow, and return to us a signed copy.

Please note that capacity at DOCA is limited. We maintain the right to review all material that comes to our office as part of our consultation process. If you have specific filing deadlines, please advise us as soon as possible. However, it is MCFN's assertion that part of the process of meaningful engagement is allowing our Nation a reasonable amount of time to review, reflect upon, and respond to reports and recommendations. On average, this process can be accomplished in 4-6 weeks. It is our position that no archaeological assessment – but especially Stage 4 mitigation – should begin until DOCA has completed our review and is in agreement that with the proposed strategy for fieldwork.

Request for Missing Information

In order to complete our project record, we ask that you provide the following information:

1. Is an archaeological assessment required for this project? If no, why not?
2. Have any archaeological assessments already been completed for this project and/or its study area? If yes, please provide all documentation including reports, supplementary documentation, etc.
3. Has the MHSTCI issued a letter of entry into register for some or all of the study area? If yes, please provide all documentation, including letter, communications to and from MHSTCI, etc.
4. Is there any archaeological activity (e.g. assessment, excavation, monitoring) that has not yet been completed for the project?
5. If the answer to #4 is yes, please provide the following:
 - a. A description of the outstanding archaeological activity/activities.
 - b. Anticipated date of the activity/activities.
 - c. The appropriate contact person overseeing the archaeological activity/activities.



Closing

The review of project-related archaeological assessments is only one part of the consultation process that may be required for your development. Please contact DOCA's Consultation Coordinator, Fawn Sault, if you have any questions about the process.

We ask that you respond with the above requested information and executed agreement within fourteen days following receipt of this letter. We thank you in advance for your attention to our requirements and we look forward to working with you further to shape the planning for development in our Territory.

Sincerely,



Megan DeVries
Archaeological Operations Supervisor
megan.devries@mncfn.ca

Attachment(s)

MCFN Standards and Guidelines for Archaeology [2018]
DOCA Archaeological Review Agreement [2020]



DEPARTMENT OF CONSULTATION AND ACCOMMODATION

Mississaugas of the Credit First Nation
4065 Hwy #6, Hagersville, Ontario N0A 1H0



Phone: (905) 768-4260



Connor MacIsaac

From: Marie-Sophie Gendron <Marie-Sophie.Gendron@wendake.ca>
Sent: October 22, 2021 1:42 PM
To: Connor MacIsaac; Lori-Jeanne Bolduc
Cc: Dominic Ste-Marie; Isabelle Lechasseur; Jean-Francois Richard; Andrew McGregor; Shun Cheung; Winnie Wong; Eliza Brandy
Subject: RE: Mill Street Class EA - Stage 1 AA Archaeological Review & Notice of PIC
Attachments: Mill Street Class EA - Stage 2.pdf

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good afternoon Connor,

I understand that we are already late to send our comment. We would kindly ask if it is possible for us to send them Friday, Oct. 29th ?

On another note, I also attached a quote for our participation on the stage 2 archaeological assessment. If you could provide me with the estimated duration of the project, I could send you a more detailed and accurate quote. If you have any questions, please let me know.

Best,

Marie-Sophie



NATION HURONNE-WENDAT
Bureau du Nionwentsïo

Marie-Sophie Gendron, B.A.
Analyste archéologue

255, Place Chef Michel-Laveau
Wendake (Qc) G0A 4V0
Téléphone : 418-843-3767
Courriel : marie-sophie.gendron@wendake.ca



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De : Connor MacIsaac <cmaclsaac@rvanderson.com>

Envoyé : jeudi 21 octobre 2021 16:49

À : Marie-Sophie Gendron <Marie-Sophie.Gendron@wendake.ca>; Lori-Jeanne Bolduc <Lori-Jeanne.Bolduc@wendake.ca>

Cc : Dominic Ste-Marie <Dominic.Sainte-Marie@wendake.ca>; Isabelle Lechasseur <Isabelle.Lechasseur@wendake.ca>; Jean-Francois Richard <Jean-Francois.Richard@wendake.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>; Eliza Brandy <ebrandy@asiheritage.ca>

Objet : RE: Mill Street Class EA - Stage 1 AA Archaeological Review & Notice of PIC

Hello,

Marie-Sophie, we were hoping that your team would be provide any input for incorporation into the final report by Monday October 18th, however if more time is needed, we will need to request an extension for our project registered with the Ministry this week.

Lori-Jeanne, as for participation in the Stage 2 Archaeological Assessment, for the Town's budgeting purposes would you be able to provide am approximate cost to send a monitor on site for stage 2 archaeological assessment for approximately 19 properties?

Thank you and Kind Regards,



Connor MacIsaac, EPt

JUNIOR ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



rvanderson.com



From: Marie-Sophie Gendron <Marie-Sophie.Gendron@wendake.ca>

Sent: October 5, 2021 11:43 AM

To: Connor MacIsaac <cmacisaac@rvanderson.com>

Cc: Dominic Ste-Marie <Dominic.Sainte-Marie@wendake.ca>; Isabelle Lechasseur <Isabelle.Lechasseur@wendake.ca>; Jean-Francois Richard <Jean-Francois.Richard@wendake.ca>; Lori-Jeanne Bolduc <Lori-Jeanne.Bolduc@wendake.ca>

Subject: RE: Mill Street Class EA - Stage 1 AA Archaeological Review & Notice of PIC

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good morning Connor,

When is the deadline for submitting our comments?

Thank you,
Marie-Sophie



NATION HURONNE-WENDAT
Bureau du Nionwentsïo

Marie-Sophie Gendron, B.A.

Analyste archéologue

255, Place Chef Michel-Laveau

Wendake (Qc) G0A 4V0

Téléphone : 418-843-3767

Courriel : marie-sophie.gendron@wendake.ca



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De : Lori-Jeanne Bolduc <Lori-Jeanne.Bolduc@wendake.ca>

Envoyé : vendredi 1 octobre 2021 14:26

À : cmacIsaac@rvanderson.com <cmacIsaac@rvanderson.com>

Cc : Dominic Ste-Marie <Dominic.Sainte-Marie@wendake.ca>; Marie-Sophie Gendron <Marie-Sophie.Gendron@wendake.ca>; Isabelle Lechasseur <Isabelle.Lechasseur@wendake.ca>; Jean-Francois Richard <Jean-Francois.Richard@wendake.ca>

Objet : TR: Mill Street Class EA - Stage 1 AA Archaeological Review & Notice of PIC

Hi Connor,

Thank you for sending the documents. My colleague Marie-Sophie (cc'd) will follow-up if we have any comments on the report.

Will it be possible for us to send a monitor on site for stage 2 archaeology? Is there funding available for the Huron-Wendat Nation to be involved?

Thank you,

Lori-Jeanne

ATTENTION: Please note that Maxime Picard has a new position at the Huron-Wendat Nation Council and is no longer in charge of Ontario consultations. Any new consultation from Ontario must be sent to Mario Gros-Louis (mario.gros-louis@wendake.ca) and Lori-Jeanne Bolduc (lori-jeanne.bolduc@wendake.ca).

For inquiries relating specifically to archaeology (fieldwork planning, monitoring, reports review, etc.), please contact Marie-Sophie Gendron (marie-sophie.gendron@wendake.ca), Isabelle Lechasseur (isabelle.lechasseur@wendake.ca) and Jean-François Richard (jean-francois.richard@wendake.ca).



NATION HURONNE-WENDAT
Bureau du Nionwentsïo

Lori-Jeanne Bolduc, M. ATDR

Conseillère en aménagement du territoire

255, Place Chef Michel-Laveau

Wendake (Qc) G0A 4V0

Téléphone : 418-843-3767 # 2211

Courriel : lori-jeanne.bolduc@wendake.ca



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De : Connor MacIsaac <cmacisaac@rvanderson.com>

Envoyé : 17 septembre 2021 14:59

À : Maxime Picard <Maxime.Picard@wendake.ca>

Cc : Andrew McGregor <AMcGregor@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>; John P. Does <jdoes@rvanderson.com>

Objet : RE: Mill Street Class EA - Stage 1 AA Archaeological Review & Notice of PIC

Hi Maxime,

In accordance with your request for the ongoing Mill Street Class EA, please find attached the Draft Stage 1 archaeological assessment report for your review and comment. Please review the report and provide any input for incorporation into the final report by Monday October 18th.

I would also like to take this opportunity to share the attached Notice of Online Public Information Centre (PIC) for the Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA). The purpose of this meeting is to share information and receive public input on the to study date, including the problem / opportunity, existing conditions, alternative solutions, and the preliminary preferred solution. The meeting will be held on **October 5, 2021 at 6 p.m.**, and will consist of a presentation by the Town followed by a Question and Answer period concluding at 7 p.m.

Due to continuing efforts to contain the spread of COVID-19 and to protect individuals, this meeting will be held electronically. Refer to the attached notice, or visit the Town of Caledon's project website, <https://www.caledon.ca/en/news/mill-street-ea.aspx>, for further information on how to participate. The meeting link will also be posted to the project website closer to the meeting date. Questions can be submitted prior to the event to the contacts below.

Town of Caledon

Shun H. Cheung, P.Eng., PMP

Project Manager, Engineering Services

905.584.2272 X.4040

shun.cheung@caledon.ca

R.V. Anderson Associates Limited
Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
416.497.8600 X.1471
winnie.wong@rvanderson.com

On behalf of the Town, I would like to again thank you for your input in this Class EA study.

Kind Regards,



Connor MacIsaac, EPt

JUNIOR ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



rvanderson.com



From: Maxime Picard <maxime.picard@cnhw.qc.ca>

Sent: October 26, 2020 8:50 AM

To: Connor MacIsaac <cmaclsaac@rvanderson.com>

Cc: Andrew McGregor <AMcGregor@rvanderson.com>; Shun Cheung <Shun.Cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>; John P. Does <jdoes@rvanderson.com>

Subject: Re: Mill Street Class EA - Notice of Commencement

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Good morning Connor,

Thanks for following-up quickly.

We would effectively appreciate receiving copy of the Stage 1 report.

We will wait to hear back from you.

Best regards,

Maxime Picard

De: "Connor MacIsaac" <cmaclsaac@rvanderson.com>

À: "Maxime Picard" <maxime.picard@cnhw.qc.ca>

Cc: "Andrew McGregor" <AMcGregor@rvanderson.com>, "Shun Cheung" <Shun.Cheung@caledon.ca>, "Winnie Wong" <Winnie.Wong@rvanderson.com>, "John P. Does" <jdoes@rvanderson.com>

Envoyé: Vendredi 23 Octobre 2020 13:21:44
Objet: RE: Mill Street Class EA - Notice of Commencement

Hi Maxime,

Yes, we are planning to complete a Stage 1 Archaeological Assessment as part of the study. If you'd like, we'd be happy to provide a copy of the draft report to you for your comments, once it's completed. In the event a Stage 2 archaeological Assessment is required we will notify Huron-Wendat First Nation prior to undertaking any fieldwork.

On behalf of the Town, thank you for your comments and your interest in the Class EA study. Please let us know if you have any additional comments or concerns.

Regards,



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Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

R.V. Anderson Associates Limited

43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

www.rvanderson.com



From: Maxime Picard <maxime.picard@cnhw.qc.ca>

Sent: October 23, 2020 8:11 AM

To: Connor MacIsaac <cmaclsaac@rvanderson.com>

Cc: Andrew McGregor <AMcGregor@rvanderson.com>; Shun Cheung <Shun.Cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>; John P. Does <jdoes@rvanderson.com>

Subject: Re: Mill Street Class EA - Notice of Commencement

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good morning Connor,

First of all let me thank you for your email on the upcoming Mill Street Class EA.

Could you please let us know if any archaeological studies are anticipated as part of the EA process ?

Thanks and best regards,

Maxime Picard

De: "Connor MacIsaac" <cmacisaac@rvanderson.com>
À: "Connor MacIsaac" <cmacisaac@rvanderson.com>
Cc: "Andrew McGregor" <AMcGregor@rvanderson.com>, "Shun Cheung" <Shun.Cheung@caledon.ca>, "Winnie Wong" <Winnie.Wong@rvanderson.com>, "John P. Does" <jdoes@rvanderson.com>

Envoyé: Jeudi 22 Octobre 2020 15:08:25

Objet: Mill Street Class EA - Notice of Commencement

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Notice of Study Commencement for the **Mill Street Class Environmental Assessment Study (Schedule B MCEA)**. The study will review opportunities for improvements to the road between Mississauga Road and Creditview Road, including improvements to: road geometry and intersections; pedestrian and cyclist amenities; and, stormwater management.

The Town of Caledon values the participation of all stakeholders and wishes to ensure that your community's interests and concerns are taken into consideration and addressed in a timely manner. At this point in the study, we respectfully request your response pertaining to the following information:

- Contact information and the identification of individuals that represent your community that we should include as a primary contact throughout the study process;
- Description of any existing conditions or sensitivities within the study area as they relate to your interests; and
- Specific issues, concerns and/or expectations that your community may have.

The project team values the participation of all stakeholders and wishes to ensure that the community's interests and concerns are taken into consideration. Kindly respond to this email with your comments, or contact directly the project team members listed on the attached notice.

Regards,



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Connor MacIsaac, EPt

Junior Environmental Planner, EA & Approvals

P: (905) 685-5049

C: (905) 933-5709

R.V. Anderson Associates Limited

43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

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Connor MacIsaac

From: Alexandra Daigle <Alexandra.Daigle@wendake.ca>
Sent: October 25, 2021 11:47 AM
To: Connor MacIsaac
Cc: Louis Lesage; Jean-Francois Richard; Marie-Sophie Gendron; Raphaelle Gaudreau-Couture; Isabelle Lechasseur
Subject: Mill Street Reconstruction Stage 1 AA - HWN Comments
Attachments: HWN Comments - Stage 1 AA Mill Street.pdf
Categories: Indigenous

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Mr. MacIsaac,

Please find attached a letter of comments from the Huron-Wendat Nation regarding the Stage 1 AA Report for the Mill Street Reconstruction.

Best regards,

Alexandra Bédard-Daigle
Agent de recherche - Projets archéologiques
Bureau du Nionwentsio

Wendake, October 25, 2021

Mr. Connor MacIsaac
Junior Environmental Planner, EA & Approvals
R.V. Anderson

By email: cmacIsaac@rvanderson.com

RE: Mill Street Class EA – Stage 1 AA Archaeological Review & Notice of PIC

Dear Mr. MacIsaac,

Further to your email addressed to the Huron-Wendat Nation dated September 17, 2021, on the Stage 1 Archaeological Assessment for the Mill Street Reconstruction, which is part of the Caledon Growth Related Roads Detailed Design in the Town of Caledon, the Huron-Wendat Nation (HWN) would like to thank you for giving us this opportunity to provide comments and feedback.

The project consists of the urban reconstruction of Mill Street from Mississauga Road to Creditview Road, historically situated on Lots 29-30, Concessions 2-5 West of Center Road, Former Township of Chinguacousy, County of Peel, in the Town of Caledon, Regional Municipality of Peel, Ontario.

The report adequately exposes the context of the study area from a historical point of view, summarizing the Indigenous land use and settlement, the treaties including the area, and the Euro-Canadian land use and development of Chinguacousy Township, the village of Cheltenham and the railway built and then abandoned in the vicinity.

Appropriate historical documents are used in order to have a more precise view of the evolution of the study area, and the limits of their use is noted. Tremaine's 1859 *Map of the County of Peel* and Walker and Miles' 1877 *Illustrated Historical Atlas of the County of Peel* both show Cheltenham, built along the Credit River, with rural land use around it. In 1859, Mississauga Road, Mill Street, John Street and Creditview Road are already surveyed. The Hamilton and North Western Railway is shown on the 1877 map. Areas of village density are visible, mostly around Creditview Street and near it on Mill Street. More recently, the National Topographic System 1918-1919 Brampton and Bolton Sheets show the Cheltenham Brickworks west of the study area, and notes various other structures present in the study area. Also, two tributaries of the credit river intersect the study area, and there are seven bridges illustrated within the study area. An aerial photograph from 1954 shows that the western part of the

study area is still mostly agricultural, while there is an increased density of buildings in the east side, in the village core of Cheltenham. A map from 1994 shows additional structures on both sides of Mill Street in the western part of the study area, and that the surroundings are still rural. Google satellite imagery shows that not much has changed since 2004, except earth moving activities at the Brickworks in 2009.

Regarding the environmental context of the study area, the report indicates that it is located within the spillways and escarpments of the Niagara Escarpment physiographic region, that the soils “consist of Oneida clay loam and Lockport clay, grey-brown podzolics with good drainage; Chinguacousy clay loam, a grey-brown podzolic with imperfect drainage; and Bottom Land, an alluvial with variable drainage (Figure 8).”, and that the Credit River and tributaries cross the study area.

Not much is known of the archaeological context of the study area, because there are no previous reports detailing fieldwork within 50m, or any known archaeological site within 1km of the study area.

A property inspection was conducted, and the observations are indicated on Figures 9-10 of the report, with associated photographs (Plates 1-10).

The report demonstrates that the study area meets multiple criteria indicative of archaeological potential: proximity to water sources (Credit River and its tributaries), to early historic transportation routes (Mississauga Road, Mill Street, Creditview Road, John Street), to early settlements (Cheltenham), and well-drained soils (Oneida clay loam and Lockport clay). Additionally, 14 properties within the Study Area are Listed or Designated under the Ontario Heritage Act, all part of the Cheltenham Village Core. These properties all have archaeological potential and require test pit survey, although for some the frontage is disturbed (from ROW construction and utilities). Thus, the majority of the study area has archaeological potential, which is also the conclusion of the Town of Caledon Archaeological Management Plan Draft Report (ASI 2020).

However, there are some areas that have been determined to have no or low archaeological potential, due to deep and extensive soil disturbance events (at the Cheltenham Brickworks, and associated with the construction of Mill Street and Creditview Road, deeply buried utility installation, and residential and commercial building construction) or excessive slope (around tributaries of the Credit River and on

the west side of Creditview Road at the south end of the Study Area). This is illustrated on Figures 9 and 10 and documented with photographs.

The report concludes that the study area includes many zones of archaeological potential, which will need to be subject to a Stage 2 AA in the form of test pit survey or pedestrian survey where applicable, and that the rest of the study area, which does not retain archaeological potential, does not require further survey.

In conclusion, the report adequately contextualizes the study area from a historical, environmental, and archaeological point of view, and the conclusions are logical. The report is satisfactory for the purposes of the Stage 1 assessment of the Mill Street Reconstruction Project study area. Regarding future archaeological work, the Huron-Wendat Nation is requesting to be consulted at every stage and of course provide liaisons for all field work. Please do not hesitate to contact our team should you have questions and to follow up with the next steps.

Best regards,

Alexandra Bédard-Daigle

Agent de recherche – Projets archéologiques

Bureau du Nionwentsio

Connor MacIsaac

From: Connor MacIsaac
Sent: January 22, 2021 10:02 AM
To: Justin Hunt
Cc: Shun Cheung; Andrew McGregor; Winnie Wong; John P. Does; Tisha Doucette
Subject: RE: Mill Street Class EA - Notice of Commencement

Hi Justin,

On behalf of the Town of Caledon, thank you for your comments and your interest in the Class EA study. I have included our Senior Ecologist Tisha Doucette in this email, as her team will be completing the natural environmental investigation for this study. Please see below the response that Tisha has prepared to your question regarding the study's consideration of potential impacts on the local ecology such as turtle nesting and/or beaver lodging.

Field work has not yet been initiated and is planned for the spring 2021, so your email and inquiry are quite timely. We have, however, completed a review of available natural heritage background information.

With regard to turtles, two species came up in our background review: Midland Painted Turtle and Common Snapping Turtle. These are our most common native turtles and both have been recorded within the 10km survey square as a part of the Ontario Nature Ontario Reptile and Amphibian Atlas. Midland Painted Turtle was also noted within the more detailed (1km squares) Nature Heritage Information Center (NHIC) database. Both species have a high-probability of being present within the Study Area and we will assume their presence within the permanent water features. Nesting habitat is a very important and limited habitat for these species and they are known to use roadways and other disturbed areas for nesting. As such, we will be looking for evidence of nesting (scavenged nests, old shells, test pits) within the Study Area as a component of our field work. We will also note their likely presence as a part of mitigation recommendations for construction.

The iNaturalist database does indicate some records of Beaver in the local area, mainly in parks and protected areas, such as the Heart Lake Conservation Area. During field investigations, we will note any beaver activity and include these observations into the natural heritage report.

Thank you for your interest in this project, and please let us know if you have any additional comments or concerns.

Regards,



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Connor MacIsaac, EPT

Junior Environmental Planner, EA & Approvals

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From: Justin Hunt <justinh@metisnation.org>
Sent: January 21, 2021 3:48 PM
To: Connor MacIsaac <cmaclsaac@rvanderson.com>

Cc: Shun Cheung <shun.cheung@caledon.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; Winnie Wong <Winnie.Wong@rvanderson.com>; John P. Does <jdoes@rvanderson.com>

Subject: Mill Street Class EA - Notice of Commencement

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Connor,

Sorry for the late response on this topic but I received a question from one of our representatives I was hoping you could answer. Our representative was concerned about the two creeks flowing through the site and was wondering if you have considered potential impacts on the local ecology such as turtle nesting or beaver lodging?

We appreciate your communication,

Justin Hunt
Consultation Advisor
Métis Nation of Ontario
355 Cranston Crescent PO Box 4
Midland, Ont. L4R-4K6
PH: 705-526-6335 Ext. 201 (Not Currently Monitored)
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Please note that as part of the effort to reduce the likelihood of workplace or community transmission of COVID-19, Métis Nation of Ontario employees will begin to offer limited client facing services in office beginning September 14, 2020 by appointment only. Other services that are being offered remotely will continue that way. Please check <http://www.metisnation.org/news-media/news/covid-19-support-programs/> for updates.

If you have COVID-19 related concerns or need help accessing support, please contact us by phone at 1-800-263-4889 or by email at covidhelp@metisnation.org.

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