



Mill Street Municipal Class Environmental Assessment

Public Information Centre #2 Summary Report FINAL

Town of Caledon

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RVA 205388

January 12, 2023

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1.0 INTRODUCTION

The Town of Caledon is currently undertaking a Schedule 'B' Municipal Class Environmental Assessment for improvements to Mill Street between Creditview Road and Mississauga Road. The purpose of the study is to identify a recommended design for Mill Street that enhances road safety and improves traffic operations and connectivity for all users. In identifying improvements to the corridor, the study is considering technical requirements; as well as socio-economic, cultural heritage, and natural environment factors.

R.V. Anderson Associates Limited (RVA) was retained by the Town to complete the Class Environmental Assessment (EA) for this project. This is a Schedule "B" project that covers phases 1 and 2 of the planning process, in accordance with the *Municipal Class Environmental Assessment (October 2000, amended in 2007, 2011 & 2015)*. As part of the Class EA planning and design process, extensive public and technical agency consultation is required in developing the study recommendations. Two (2) Public Information Centre (PIC) were held as part of the study.

1.1 Public Information Centre #2

The second Public Information Centre (PIC) was held on June 15, 2022, at the Cheltenham Community Centre (14190 Creditview Rd, Caledon, ON) from 5:30pm to 7:00pm. The format of the PIC was an informal "Open House" with materials pertaining to the study on display for the public to review. Members of the project team (Town of Caledon and RV Anderson Associates staff) were on hand to discuss the project and concerns brought forward by the attendees.

2.0 STAKEHOLDERS AND NOTIFICATION

Notification of the PIC was provided through various methods and media, as outlined below.

- **General Public:**
 - Notice posted on the Town's website on May 26, 2022
 - Notice published in *Caledon Enterprise* (local newspaper) on May 26, 2022, and in the Town's e-newsletter May 27, 2022, and June 22, 2022
- **Local Residents & Businesses:**
 - Notice mailed to all property owners within the study area
- **Technical Agencies, Local Interest Groups and First Nations Communities:**
 - Emails containing the notice and details of the PIC were sent on May 30, 2022

- **Project Mailing List (other parties who previously submitted comments or indicated interest in the project)**
 - Email containing the notice and details of the PIC was sent on May 30, 2022

Refer to **Appendix 1** for copies of the published notifications and stakeholder contact list.

3.0 CONSULTATION MATERIAL

The purpose of the second PIC was to share and receive input from the public on the options for pedestrian accommodations developed for Mill Street in consideration of the feedback received during and following PIC #1, held online October 5, 2021.

The display materials presented information on the study to date including the study area and purpose of the study, public and technical agency consultation completed to date, preliminary recommendations for the roadway, and alternative solutions for pedestrian accommodation. Display materials were posted to the project website in advance of the PIC, and were available in an alternate format (i.e., hard copy) upon request.

The PIC presented three pedestrian accommodation alternatives for Mill Street. These were developed in response to the team's earlier preliminary recommendation for pedestrians to be accommodated via paved shoulders throughout the corridor.

Refer to **Appendix 2** for copies of the materials made available for comment during the PIC.

4.0 PARTICIPATION AND COMMENT SUMMARY

Attendees were requested to sign-in upon arriving at the Public Information Centre. Based on the sign-in sheet, there were 31 attendees, including representation from Town Council, Cheltenham Area Residents Association, and residents of the Town.

All attendees were encouraged to provide comments and feedback on the material presented and the study in general. Comment forms were provided for attendees to complete. Attendees were also encouraged to submit additional comments by email or mail following the open house. Comments received are summarized below and included in **Appendix 3**.

4.1 Pre-PIC Comments and Questions Received

A total of eleven (11) comments / questions were received from the public in advance of the PIC, from 4 residents, regarding the format of PIC, participation and documentation of the PIC, availability of the PIC Slides, content of the PIC, and the design of the roadway.

Comments / questions received are summarized in the table below by these categories, in no particular order.

Table 4.1 – Summary of Pre-PIC Comments Received

TOPIC OF COMMENT RECEIVED	COMMENT SUMMARY	CONSIDERATION OF COMMENTS IN CLASS EA
Format of PIC	<ul style="list-style-type: none"> - Residents requested that the PIC include a formal presentation. 	<ul style="list-style-type: none"> - The format of the PIC remained as an open house, to allow for one-on-one discussions. - Residents were advised that based on previous experience, the requested presentation format tends to result in the conversation dominated by a few vocal participants, while some individuals are reluctant to express their opinions in front of others.
Participation and Documentation of the PIC	<ul style="list-style-type: none"> - Resident inquired how the results of the PIC and input provided will be documented. - Resident inquired how those who are not able to attend can participate in the PIC. - Resident inquired how the feedback provided by residents of Mill Street will be prioritized. 	<ul style="list-style-type: none"> - Resident was advised that all attendees will be invited to provide their comments within a comment form, which will be documented in the Project File Report. - Resident was advised that the PIC display material and comment sheet would be available online following the PIC. - Residents within the study area were directly mailed the PIC notice, however all members of the general public were welcome to provide input to the study.
Availability of PIC Slides	<ul style="list-style-type: none"> - Resident requested that the PIC display boards be made available for review prior to the PIC. 	<ul style="list-style-type: none"> - Presentation slides were posted to the project website and distributed via email to the residents who requested the information prior to the PIC (June 14th).
Content of PIC	<ul style="list-style-type: none"> - Resident inquired whether the Cheltenham Area Residents Association proposal will be included as an option at the PIC. 	<ul style="list-style-type: none"> - The resident's proposal was received last Fall and was considered in the development of the pedestrian accommodation options presented at the PIC. - Details of the resident's proposal, and how it was incorporated into the study, will be included in the Project File Report, available for

TOPIC OF COMMENT RECEIVED	COMMENT SUMMARY	CONSIDERATION OF COMMENTS IN CLASS EA
		public review at the end of the study.
Design of the Roadway	- Residents expressed desire that any recommended improvements to the roadway should retain and compliment the existing rural heritage of the Village, and not include widening the roadway.	- The desire to retain a rural roadway, and maintain narrow travel lanes, was considered in the development of the additional pedestrian accommodation options presented at the PIC.

4.2 PIC Comments and Questions Received – Residents and General Public

A total of twenty (20) completed comment sheets questions were submitted, in addition to three comments provided via email. The comment sheet included six questions, requesting respondents to identify their mode and frequency of travel along Mill Street, their opinion of how well the presented alternatives accommodate pedestrians, as well as any other comments and questions they may have.

A summary of the comments received is provided below.

4.2.1 Question 1 Comment Sheet Results – Demographics

Figure 4.1 demonstrates the responses received to Question 1 on the comment sheet, which asked respondents to identify whether they are a resident of Mill Street. Of the 20 comment sheets received, 16 responses were from residents on Mill Street and 4 responses were from residents within Cheltenham community

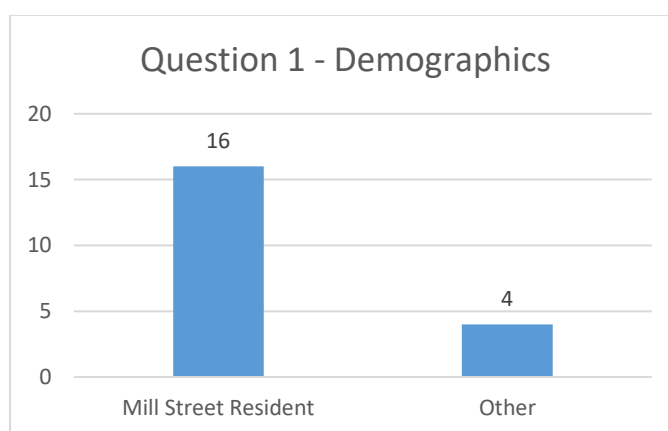


Figure 4.1 – Question 1 Results

4.2.2 Question 2 Comment Sheet Results – Primary Mode of Travel

Figure 4.2 demonstrates the responses received to Question 2 on the comment sheet, which asked respondents to identify their primary mode of travelling along Mill Street. Of the 20 comment sheets received, 14 respondents use more than one mode of transportation (walking, cycling, car, other) to travel on Mill Street.

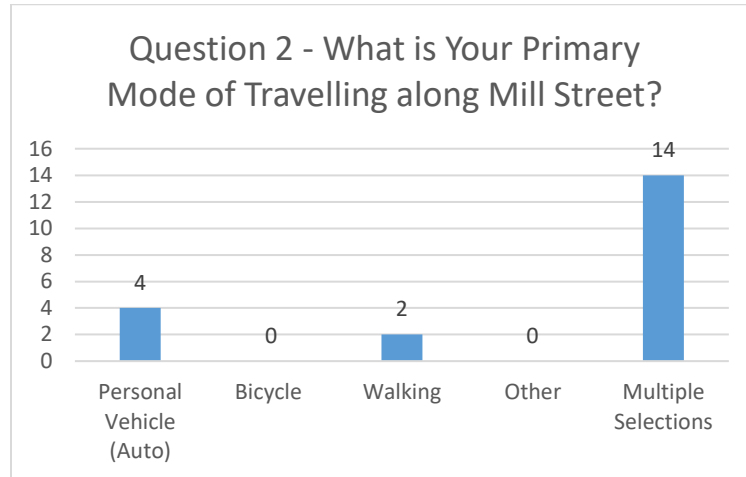


Figure 4.2 – Question 2 Results

4.2.3 Question 3 Comment Sheet Results – Primary Mode of Travel

Figure 4.3 demonstrates the responses received to Question 3 on the comment sheet, which asked respondents to identify the approximate frequency that they walk along Mill Street. Of the 20 comment sheets received, the majority of respondents (15), indicated they walk along the street daily.

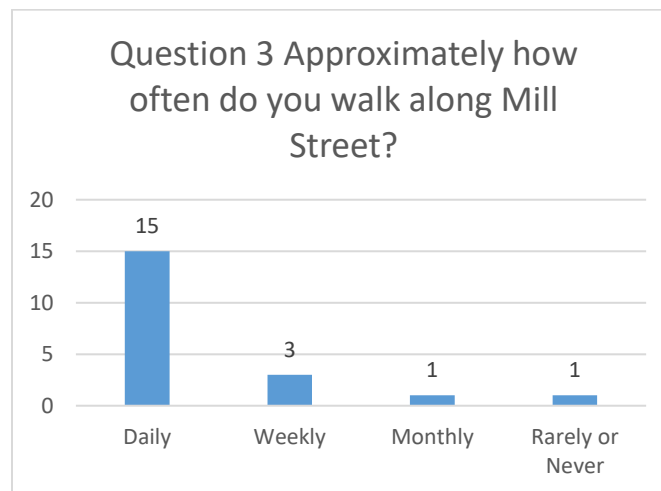


Figure 4.3 – Question 3 Results

4.2.4 Question 4 Comment Sheet Results – Likelihood of Increased Walking

Figure 4.4 demonstrates the responses received to Question 4 of the comment sheet, which asked respondents whether they would be more likely to walk along Mill Street if there was a designated pedestrian facility. Of the 20 comment sheets received, the majority of respondents (15), indicated that they would not be more likely to walk on Mill Street if there was a dedicated pedestrian facility.

Supplementary comments provided in response to the question largely indicated that residents feel safe walking along the roadway. 3 residents identified that they would be more likely to walk along Mill Street if there was a designated pedestrian facility, due to feeling safer with a designated facility than walking along the road. Of the 3 respondents who expressed that they would be more likely to walk along Mill Street if there was a designated pedestrian facility, 1 was a resident of Mill Street, while the 2 others were residents within the Cheltenham community, not residing directly on Mill Street.



Figure 4.4 – Question 3 Results

4.2.5 Question 5 Comment Sheet Results – Rank of Pedestrian Accommodation

Figures, Figure 4.5, Figure 4.6, and Figure 4.7 demonstrate the responses received to Question 5 on the comment sheet, which asked respondents to rank how they feel each of the alternatives presented accommodate pedestrians. Of the 20 comment sheets received, 4 respondents expressed support for sidewalk, however, the majority of respondents expressed a strong preference for not introducing pedestrian accommodations. Of the 4 respondents who expressed support for sidewalk, 1 was a resident of Mill Street, while the 3 others were residents within the Cheltenham community, not residing directly on Mill Street.

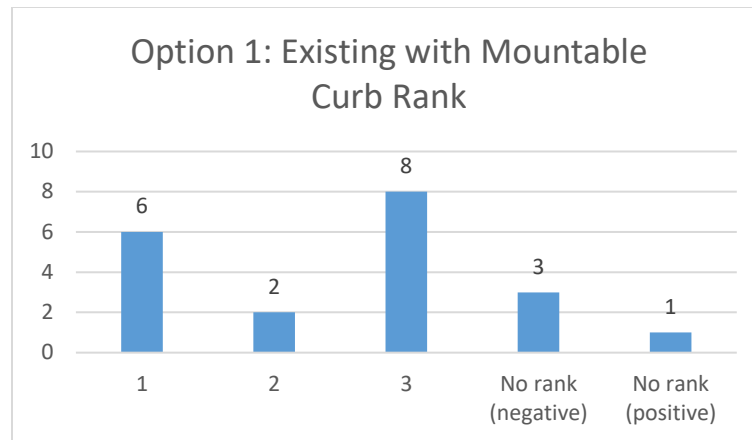


Figure 4.5 – Question 5: Option 1 Rank

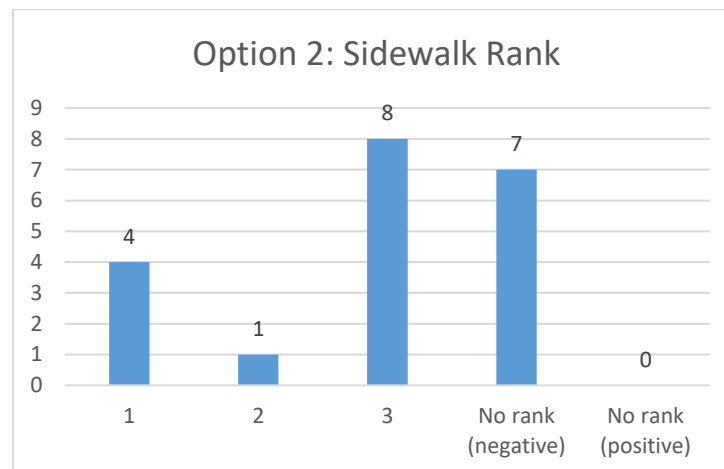


Figure 4.6 – Question 5: Option 2 Rank

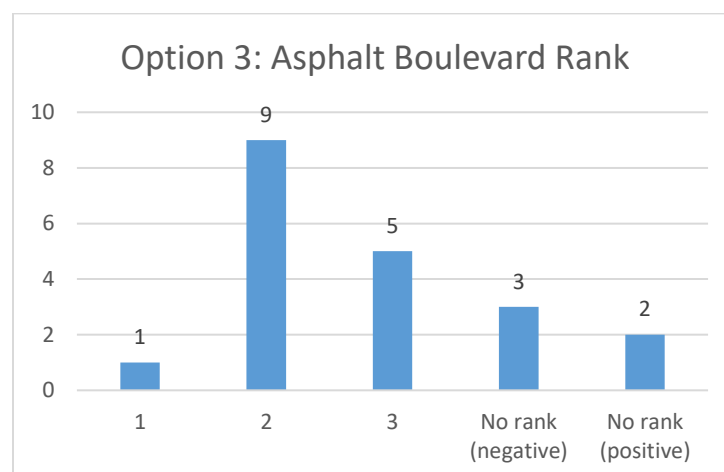


Figure 4.7 – Question 5: Option 3 Rank

4.2.6 Question 6 Comment Sheet Results – General Comments and Questions

Several common categories were identified within the comments provided, including concerns related retaining the existing cultural heritage, reducing speeding along the corridor, pedestrian accommodation, roadway drainage and stormwater management

Illumination on Mill Street utility services, property impacts & driveway access, natural environmental impacts including drainage, and the Class EA planning process & construction timeline. Comments / questions received are summarized in the table below by these categories, in no particular order.

Table 4.2 – Summary of Residents and General Public Comments Received

TOPIC OF COMMENT RECEIVED	COMMENT SUMMARY	CONSIDERATION OF COMMENTS IN CLASS EA
Retaining the Heritage of the Street	<ul style="list-style-type: none"> - Residents made clear that any recommended improvements to the roadway should retain and compliment the existing rural heritage of the Village. Residents generally defined rural heritage as no pedestrian facility, preserve trees, no guiderail, no pavement widening, no curb and gutter or semi-mountable curb and gutter and streetlighting appropriate for a rural road. 	<ul style="list-style-type: none"> - The preferred alternative maintains the existing road alignment, pavement width, ditches, curb and gutter location, and introduces new guiderail only where required for safety. Detailed design to review existing lighting levels and provide recommendation
Reducing Speeding along the corridor	<ul style="list-style-type: none"> - Residents stated concerns related to speeding within the study area, expressing support for traffic calming features. 	<ul style="list-style-type: none"> - Maintain existing lane widths and maintaining the existing road alignment to not increase speeds. - Additional traffic calming measures to be recommended during detailed design.
Pedestrian Accommodation	<ul style="list-style-type: none"> - Many residents stated a strong preference for not introducing pedestrian accommodations. - Some residents expressed support for a sidewalk. 	<ul style="list-style-type: none"> - Due to significant push back from residents on Mill Street, designated pedestrian facilities were not incorporated into the study recommendations.
Roadway Drainage and Stormwater Management	<ul style="list-style-type: none"> - Support for improved stormwater management and drainage improvements were expressed. 	<ul style="list-style-type: none"> - Recommendations for improvement stormwater management and drainage include:

TOPIC OF COMMENT RECEIVED	COMMENT SUMMARY	CONSIDERATION OF COMMENTS IN CLASS EA
		<ul style="list-style-type: none"> ○ Maintain existing ditches with new subdrain, soak away pits to improve drainage; ○ Replace and upsizing culverts that do not meet drainage capacity; and ○ Ditch clean out to improve ditch capacity.

4.3 PIC Comments and Meetings – Technical Agencies and Stakeholders

An email containing the notice and details of the PIC was sent to relevant technical agencies and stakeholders on May 30, 2022. The complete list of Technical Agencies and Stakeholders who received the PIC Notification is provided in **Appendix 2**.

Comments on the PIC display materials were received from the Region of Peel. Table 4.3 - Summary of Technical Agency Comments Received summarizes the comments received from various technical agencies during the PIC.

Table 4.3 - Summary of Technical Agency Comments Received

AGENCY/ GROUP	COMMENT SUMMARY	CONSIDERATION OF COMMENTS IN CLASS EA
Region of Peel	<ul style="list-style-type: none"> - The Region noted the presence and location of watermains along Mill Street, noting that any impact to Regional underground infrastructure, Valve, chambers and fire hydrants will need to be taken into consideration during this project. - Region requested PIC display boards. 	<ul style="list-style-type: none"> - Study recommendations were developed in consideration of Regional input and infrastructure. - The project team provided the Region with the PIC display boards June 14th.
	<ul style="list-style-type: none"> - In review of the PIC displays, the Region identified Option 2 – Sidewalk, as the Region's preferred alternative, noting that it provides the safest option for pedestrians. The Region also inquired why the focus of the alternatives presented at the PIC are only on pedestrians and not active transportation (cyclists)? 	<ul style="list-style-type: none"> - Designated pedestrian and cyclist facilities were not incorporated into the study recommendations in consideration of comments received from the public.
	Region of Peel staff made the following inquiries:	<ul style="list-style-type: none"> - The Mill Street Class EA does not include the intersection of Mississauga Road.

AGENCY/ GROUP	COMMENT SUMMARY	CONSIDERATION OF COMMENTS IN CLASS EA
	<ul style="list-style-type: none"> - Does this EA cover the entire intersection of Mississauga Road and Mill Street? - Please provide soft copies of the materials for reference after the PIC Meeting #2 - Will Town of Caledon be conducting a Trail Crossing Treatment Warrant for the Trail Crossing at Mill Street? - Would the Town of Caledon consider a connection between Mill Street and the Trail crossing as well along Mississauga Road? 	<ul style="list-style-type: none"> - The PIC display slides were previously provided to the Region on June 14, 2022. - Pedestrian crossing improvements including new traffic signs and pavement markings are proposed at the Caledon Trailway Crossing on Mill Street, with details to be confirmed during detailed design. - The Caledon Trailway currently intersects both Mill Street and Mississauga Road.

All comments received, meeting agendas and follow-up discussions involving Technical Agencies and Stakeholder Groups are included in **Appendix 3**.

5.0 CONCLUSIONS, STUDY RECOMMENDATIONS AND NEXT STEPS

The PIC provided an opportunity for residents to provide input to the development of the study recommendations, with a focus on pedestrian accommodations. Comments on various topics were received, with a number of trends becoming evident:

- Little support for road safety improvements at intersections and the “S” bend.
- Pedestrian and cyclist accommodation throughout the corridor is not supported by Mill Street residents.
- General support for rehabilitating the road pavement only, with no other changes.
- Residents expressed a desire to maintain the rural character of the roadway.
- Concern over property impacts and tree removals.
- Support for drainage improvements.
- Concern over road improvements will potentially increase traffic volumes and attract other road users to Mill Street.
- Support for additional traffic calming measures.

5.1 Study Recommendations

Preliminary recommendations were presented at the first Public Information Centre (PIC), October 5, 2021, with additional alternatives for pedestrian accommodations presented at the second PIC on June 15, 2022. In consideration of the feedback received during and following the second PIC, the following recommendations have been developed for the Mill Street corridor:

- Maintain existing roadway pavement width and slightly widen at curves to accommodate truck and snow plow turning movements;
- Maintain the existing road alignment, including the S-curve;
- Maintain rural roadway characteristic with curb and gutter reinstated only in existing locations;
- Maintain existing ditches with new subdrain, soak away pits to improve drainage;
- Cross culvert improvement;
- No designated pedestrian facility (Option #1 presented at PIC #2 and shown below);
- Pedestrian crossing improvements at the Caledon Trailway;
- New guiderail where required by standards;
- Minimize property/tree impact during the road enhancement where possible;
- Streetlighting improvements (types and locations to be confirmed); and
- Additional traffic calming measures to be recommended during detailed design.

An example typical cross-section of the study recommendations is provided below.

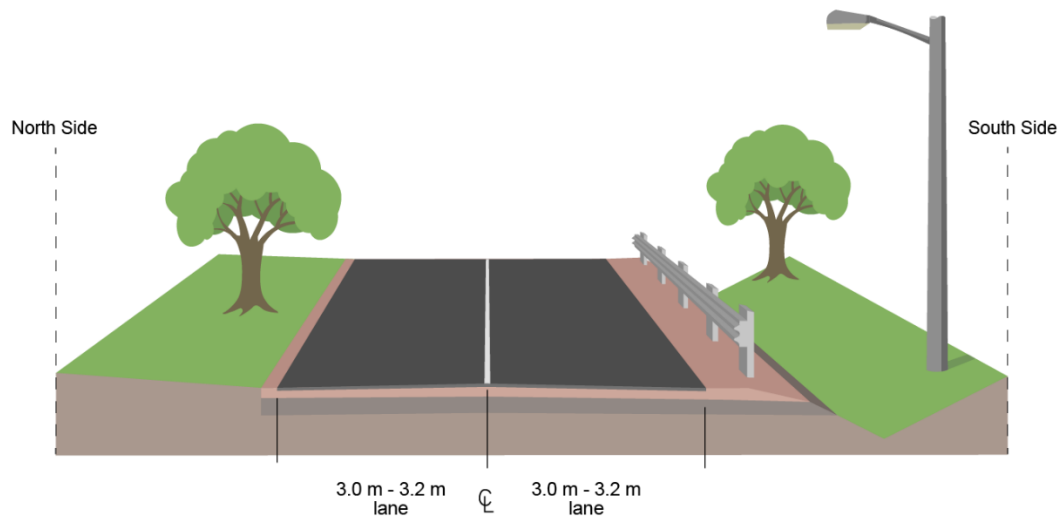


Figure 5.1 – Example Typical Cross-Section

5.2 Next Steps

In accordance with the requirements of the Municipal Class Environmental Assessment, the public is to be contacted following the identification of the recommended solution(s). As such, a Notice of Study Recommendations has been prepared, along with this summary report, to notify the public of the study recommendations described above.

Upon completion of the PIC Summary Report comment period (February 2, 2023), a Project File Report (PFR) documenting the planning and public consultation process undertaken throughout the EA, as well as details regarding the study recommendations will be made available to the public for a 30-day review period, through issuance of a Notice of Study Completion.

The Notice of Study Completion will also advise the public that a request may be made to the Ministry of the Environment, Conservation and Parks (MECP) for an order requiring a higher level of study (i.e. requiring an individual/comprehensive EA approval before being able to proceed), or that conditions be imposed (e.g. require further studies), **on the grounds that the requested order may prevent, mitigate or remedy adverse impacts on constitutionally protected Aboriginal and treaty rights.**

Following the public review period, the MECP has an additional 30 days to consider the project and review any potential Section 16 Order requests submitted. Following MECP review period, the project may proceed to detailed design and construction, provided the ministry is not reviewing Section 16 Order requests related to the project, and subject to any other permits and approvals that may be required.

Subject to comments received on the Project File Report, the Town intends to proceed with the detailed design of this project in 2023. Due to utility relocation and permitting consideration, the anticipated road construction is expected in 2025, subject to budget approval. A summary of the next steps as anticipated timeline is described in the table below.

Table 5.1 – Anticipated Timeline of Next Steps

Activity	Timing
PIC Summary Report Review Period	January 12, 2023 – February 2, 2023
Project File Report and Notice of Study Completion	Winter 2023
Project File Report Public Review Period	30-days following Notice of Study Completion
Additional MECP Review Period	30-days following close of public review period
Detailed Design	2023 - 2024
Road Construction (Subject to Budget Approval)	2025

APPENDIX 1

NOTIFICATION MATERIAL

APPENDIX 1-1

Notices

**MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD**

The Study

The Town of Caledon is considering options to enhance safety, improve traffic operations and better accommodate pedestrians and cyclists on Mill Street, to serve the community for years to come. The project is being completed as a **Schedule “B”** project in accordance with the *Municipal Class Environmental Assessment (October 2000, as amended in 2007, 2011 & 2015)*. As such, public and technical agency consultation will play a key role in developing the study recommendations.



Public Input

Preliminary recommendations were presented to the public for input at the first Public Information Centre (PIC), held online October 5, 2021. In consideration of feedback received during and following PIC #1, the project team has developed new alternatives for pedestrian accommodations along Mill Street.

A second PIC is being held to garner feedback on the options for pedestrian accommodations. The PIC will be held as an informal “Open House” with materials pertaining to the study on display, with members of the project team on hand to answer questions and discuss issues related to the project. Attendees will be asked to review the materials on display and provide their preferences for pedestrian accommodations. A final recommendation for Mill Street will be developed following the PIC, in consideration of the input received.

Public Information Centre #2:

Date: Wednesday June 15, 2022

Time: 5:30 pm to 7:00 pm

Location: Cheltenham Community Centre (14190 Creditview Rd, Caledon, ON L0P 1N0)
Entrance is located at the south side of the building

If you would like more information prior to the PIC, or to be added to the study mailing list, please contact one of the following members of the Project Team:

Town of Caledon

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Project Manager, Capital Infrastructure
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This notice was first issued May 26, 2022.

*Information collected will be used in accordance with the Freedom of Information and Protection Privacy Act. With the exception of personal information, all comments will become part of the public record



MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY MISSISSAUGA ROAD TO CREDITVIEW ROAD

Posted on Tuesday, June 07, 2022

MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY

MISSISSAUGA ROAD TO CREDITVIEW ROAD

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Town of Caledon

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Project Manager

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This notice was first issued May 26, 2022.

How can we help?

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Have your say

Contact Council or visit our new public engagement site.

Our residents make Caledon a vibrant place to live, work and play.

Contact Us

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Affordability, climate: Future Majority

respond." "In the Federal election last year, Future Majority was able to kind of attract local youth events to remind them that the election is coming up and they should register to vote and get out to vote. They did things like pledge drives, reminding people of the date they have to vote, the steps they have to take to vote, and also making clear the importance of voting for young people.

"I think a lot of people think their vote doesn't matter, they are uninterested or apathetic to the political process, but young people make up 30 per cent of Ontario's electorate and they are concentrated in key ridings that party leaders need to speak to [in order] to have a majority government, particularly in Mississauga, Brampton and Newmarket-Aurora."

To challenge the idea that the youth vote might not "matter," Santana says it isn't hard to find examples of action when "young people come together to push for change."

"It does actually influence policy platforms in subsequent elections," he says. "For example, in the last Federal election, young Canadians raised the issue of climate change over and over and now we're seeing all parties include climate action on their platforms. It is clear that when we show up to the poll in that regard we do hold some power to push for better and young people participating can influence elections [and] they can really make themselves heard among political leaders."

"I think for quite a while politics has been dominated by the middle-aged to older crowd who tend to show up to vote more, but politicians look for votes. If young people are voting, that is where their policies will shift to."

For more information on Future Majority, including how to get involved, visit futuremajority.ca.

Councillor Kiernan wants to see a community pool built in Ward 1

BY ZACHARY ROMAN

A Caledon councillor would like municipal staff to dive into research on the feasibility of building a community and therapeutic pool in Caledon's Ward 1.

Councillor Lynn Kiernan brought forward a motion at Council's May 17 General Committee meeting, asking that "staff be directed to investigate the feasibility of constructing and operating a community/therapeutic pool in Ward 1 and report back on potential locations, proposed budget and projected usership."

In her motion, she explains that the Caledon Central Pool was demolished in 2014 due to structural issues and mold growth. She added that a Northwest Caledon Indoor Recreation Facility Reserve Fund was created to meet community needs following the closure of the pool, and that \$1,038,350.16 is currently in that fund.

Kiernan's motion goes on to note Caledon's 2017 Facility Needs Assessment Study recommends one indoor aquatic centre per 25,000 population and that Caledon is expected to grow to 160,000 residents by 2041. It also notes that the Town's Age Friendly Caledon Plan says the Town will build, support and advocate for local programs that promote social participation with an emphasis on health and friendship.

Kiernan said her constituents have expressed a need and desire to have an indoor aquatic centre such as can be found in other areas of Caledon. She said Caledon community service staff have been seeing the same thing, reporting an increase in requests for therapeutic programs such as therapy classes in an aquatic setting.

Speaking to her motion at the meeting, Kiernan said she was very happy to have her motion come forward. She said even when she was campaigning to be Ward 1 area councillor in 2018, she heard the need for a pool from residents in her ward.

"It's been a big issue for the residents in Northwest Caledon, we have families that go to Orangeville and Georgetown to take their children to swimming lessons, the seniors have definitely lost their pool and really did miss some of that aquatherapy or aquafit," said Kiernan.

She said she put together a business plan in November of 2021 and met with Town Staff to discuss making the pool financially viable. She said she's explored potential partnerships with parties such as Hills of Headwaters Collaborative (an Ontario Health Team), sports clubs, or private partners. Kiernan said she learned it's not about the cost of building the pool, rather the cost to maintain it. She explained she's not seeking a high-level athletic pool, rather a warm, accessible community pool that all Ward 1 residents can enjoy.

Councillor Ian Sinclair supported Kiernan's motion and said it was well-researched. He suggested a friendly amendment to it: he proposed a concrete deadline for staff to report back by being added.

An amendment to Kiernan's motion to give Town staff a deadline of the third quarter of 2023 was passed unanimously. The motion itself was also passed unanimously.



Mayor Allan Thompson and Vaughan Mayor Maurizio Bevilacqua are pictured touting the GO plan earlier this year.

FILE PHOTO

Advisory firm to continue working for Caledon's GO line

Continued from FRONT

Councillor Nick deBoer put forward an amendment to the motion before it passed, a requirement to ask York Region and the City of Vaughan to share proportionately in the costs of the business case work. The motion was passed as amended.

Councillor Ian Sinclair said he had a number of questions about the Town's involvement in funding the business case for GO Train service.

"I don't understand the role of Metrolinx, do they not do anything? What do they do?" he asked.

"Caledon seems to be undertaking the technical work to justify the business plan for it... why on earth are we paying for Metrolinx's work? Surely that's their job to work on the business plan," said Sinclair.

He suggested getting in touch with Metrolinx before spending any money on the project.

Andrew Pearce, Caledon's Director of Engineering services, said as the GO Rail is a strategic priority for the Town, staff have been doing all they can to advance it. Pearce said staff have been in touch with Metrolinx and that Metrolinx staff have a full workload; thus, the Caledon-Vaughan GO Rail is not an immediate priority for them at this time.

Pearce said Metrolinx's next regional transportation plan is expected in 2023 and the Town is trying to advance its case to be a priority in that plan.

He said everything he knows about York Region and City of Vaughan staff leads him to believe they will support Caledon in cost-sharing business case work.

"What we're suggesting is investing a bit now, to advance that delivery of the GO service," said Pearce.

He explained York Region and the City of Vaughan have expressed verbally their intent to support Caledon in the cost of required studies

for advancing the business case, such as a capacity analysis.

Councillor Tony Rosa said he'd like to see more from verbal intent, as he said the work that needs to be done will benefit both regions and municipalities.

Councillor Lynn Kiernan echoed his concerns.

A motion to refer the item back to staff until they could receive confirmation from York Region and the City of Vaughan that they'd be willing to cost-share was put forward and supported by councillors Annette Groves, Tony Rosa, Sinclair, and Christina Early.

However, the motion was lost as Mayor Allan Thompson and the rest of Caledon Council opposed it.

Caledon's Chief Administrative Officer, Carrey Herd, said Town staff believe the work on the business case is critical for Caledon's future and needs to be done as expeditiously as possible; and that's why Caledon is taking the lead.

Mayor Allan Thompson said he believes in the experts from StrategyCorp as they have experience in rail planning. "If we want to change the way... communities thrive, we're going to have to do a bit of investment here and these are the experts that know how to do this," he said.

Councillor Groves said she agreed with Sinclair, and that Metrolinx should be doing the planning work. "Why are we doing Metrolinx's work? We have an MPP here, she should really be advocating for the advancement of our GO train service," said Groves.

She also said she didn't like that the staff report requesting funding for the business case was added as an addendum to the May 17 meeting's agenda, and that residents who check committee meeting agendas when they are posted on Fridays may not have had a chance to read it and be involved in the decision-making process.



Development Charges Study

the municipality of the Town of Caledon of the Development Charges Act, 1997, as a municipality's proposed area-specific ground study. The area under the Bolton Parkway. It will be imposed on the Bolton Parkway.

Members of Council participating in the public are invited to view the meeting by joining the Webex.

ate in person, via phone or Webex Meeting for email finance@caledon.ca to facilitate

TS: @caledon.ca

available to the public, the background study is

ground study are available from the Municipal

nts at the public meeting or in writing, address prior to June 9, 2022.

6311 Old Church Road
Caledon, ON L7C 1J6
caledon.ca
T. 905.584.2272 | 1.888.225.3366 | F. 905.584.4325

PUBLIC NOTICE

Notice of Intention to pass amending bylaw to remove Holding Symbol (H)

FILE NUMBER: R2 2021-0016

Please be advised that the Town of Caledon is now offering a Hybrid Committee and Council Meeting model offering many ways for you to engage with Council, including in-person or virtual opportunities.

Earliest Planning and Development Committee Meeting Date: June 21, 2022
Earliest Town Council Meeting Date: June 28, 2022

HOW TO PARTICIPATE

- Attend In-Person: Members of the public may register to delegate in-person and speak to an item listed on the agenda or sit in the gallery to watch the meeting. To do so, please complete the Participation Request form on the Town's website.
- Virtually: Members of the public are welcome to participate via phone or WebEx Meeting upon request. For more information on how to participate, please visit caledon.ca/agenda or contact the Planning Department. Please note that there are deadlines by which the request to participate need to be made.

APPLICANT

The Alton Development Inc.

PROPOSED CHANGES

The applicant has applied for a Zoning By-law Amendment (Lifting of Holding Symbol ("H") to remove the Holding Symbol ("H") and "H10") from the subject lands zoned Village Commercial Exception 35 - Floodplain (CV-35-F-H) by Zoning By-law 87-250, as amended, and Village Commercial Exception 601 (CV-601-H10) by Zoning By-law 2006-50, as amended. These site-specific zones and Holding Symbol restrictions were approved by Council in 2008 and the removal of the Holding Symbol ("H" and "H10") will permit the place of assembly, place of entertainment and restaurant uses on the subject lands.

LEAD PLANNER

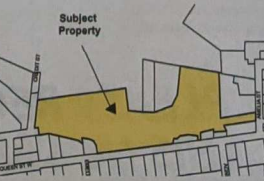
Adam Wardlaw, MCP, PMP
Community Planning, Development and Design,
Planning Department
905-584-2272 ext. 4034
adam.wardlaw@caledon.ca

ADDITIONAL INFORMATION

For more information about this matter, including a copy of the proposed Zoning By-law Amendment applications, additional information and material, please visit caledon.ca/notices or contact the Lead Planner at 905-584-2272 ext. 4024 from Monday to Friday, 9:00 a.m. to 4:00 p.m.

WARD 1 LOCATION:

1402 Queen Street West
Part of Lot 23, Concession 4 WMS (Caledon); Part of Mill Privilege No. 4 in Block 5 on CALS (Village of Alton); Part of Mill Privilege No. 5 in Block 5 on CALS (Village of Alton); West side of Amelia Street, north of Queen Street West and east of Credit Street



ACCESSIBILITY

If you require an accessibility accommodation to attend or participate in these meetings, or to access any materials related to this item in an alternate format please contact Legislative Services by phone at 905-584-2272 x2066 or via email at legislative.services@caledon.ca. Requests should be submitted at least 10 days before the meeting.

Multi-Tenant Landowners: Please post this notice in a visible location to all residents of the land.

Notice Date: May 26, 2022



6311 Old Church Road
Caledon, ON L7C 1J6
caledon.ca
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PUBLIC NOTICE

NOTICE OF 2ND PUBLIC INFORMATION CENTRE

Mill street class environmental assessment study Mississauga road to Creditview road

THE STUDY

The Town of Caledon is considering options to enhance safety, improve traffic operations and better accommodate pedestrians and cyclists on Mill Street, to serve the community for years to come. The project is being completed as a Schedule "B" project in accordance with the Municipal Class Environmental Assessment (October 2000, as amended in 2007, 2011 & 2015). Public and technical agency consultation will play a key role in developing the study recommendations.



PUBLIC INPUT

Preliminary recommendations were presented to the public for input at the first Public Information Centre (PIC), held online October 5, 2021. In consideration of feedback received during and following PIC #1, the project team has developed new alternatives for pedestrian accommodations along Mill Street.

A second PIC is being held to gather feedback on the options for pedestrian accommodations. The PIC will be held as an informal "Open House" with materials pertaining to the study on display, with members of the project team on hand to answer questions and discuss issues related to the project. Attendees will be asked to review the materials on display and provide their preferences for pedestrian accommodations. A final recommendation for Mill Street will be developed following the PIC, in consideration of the input received.

PUBLIC INFORMATION CENTRE #2

Date: Wednesday June 15, 2022
Time: 5:30 to 7 p.m.
Location: Cheltenham Community Centre
(14190 Creditview Rd, Caledon, ON L0P 1N0)
Entrance is located at the south side of the building

If you would like more information prior to the PIC, or to be added to the study mailing list, please contact one of the following members of the project team:

TOWN OF CALEDON
Shun H. Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure
Engineering Services
Tel: 905.584.2272 x 4040
E-mail: shun.cheung@caledon.ca

R.V. ANDERSON ASSOCIATES LIMITED
Winnie Wong, P.Eng., PMP, M.Eng.
Project Manager
Tel: 416.497.8800 x 1471
E-mail: winnie.wong@rvanderson.com

This notice was first issued May 26, 2022.



6311 Old Church Road
Caledon, ON L7C 1J6
caledon.ca
T. 905.584.2272 | 1.888.225.3366 | F. 905.584.4325

Town of Caledon e-News (May
27, 2022)

No images? [Click here](#)



Celebrate Caledon

This free community event brings together the best of the beloved Caledon Day and TRCA's Caledon Canada Day events. Newly named 'Celebrate Caledon' organizers say it will provide an opportunity to celebrate as a community again. Visit [Celebrate Caledon](#) for details.



Bike Month

June is Bike Month and the opportunity to explore the many benefits of biking using the network of bike routes throughout Caledon.

Caledon is a well-known destination for cyclists, with [both recreational trails and challenging road routes](#). Explore our many trails during Bike Month and get active while cycling in Caledon.

Learn more about [Bike Month](#)



Mill Street

Class Environmental Assessment Study

Mississauga Road to Creditview Road

Public Information Centre #2:

Date: Wednesday June 15, 2022
Time: 5:30 p.m. to 7 p.m.
Location: Cheltenham Community Centre
(14190 Creditview Rd, Caledon, ON L0P 1N0)

Entrance is located at the south side
of the building

[Details](#)



Tea and Talk

In recognition of World Elder Abuse Awareness day

June 15, 2022: 12-3 p.m.

Albion Bolton Community Centre, Auditorium

12 p.m. Welcome

12:30 p.m. Presentations by local community services organizations

1 p.m. Webinar with Elder Abuse Prevention Ontario

2:30 p.m. Tea & Talk Giveaway

Registration is open at caledon.ca/adult55 or by calling 311

caledon.ca/adult55



Mill Street

Class Environmental Assessment Study

Mississauga Road to Creditview Road

Public Information Centre #2:

Date: Wednesday June 15, 2022
Time: 5:30 to 7 p.m.
Location: Cheltenham Community Centre
(14190 Creditview Rd, Caledon, ON L0P 1N0)

Entrance is located at the south side
of the building

[Details](#)



Forward

Town of Caledon
6311 Old Church Road | Caledon ON L7C 1J6
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APPENDIX 1-2

Technical Agency Stakeholder List

Mill Street Class EA
Technical Agency Stakeholder Contact List

Agency	Contact	Title	Email	Address	Phone	Notes	Date Added to List	Removed from List	Notice of Study (date sent)	Notice of PIC1 (date sent)
Provincial Ministries, Agencies and Departments										
Ministry of the Environment, Conservation and Parks (MECP)	Central Region		eanotification.cregion@ontario.ca	Suite 300, 4145 North Service Road, Burlington ON L7L 6A3	1-800-335-5906	Complete the project information form and send copy of notice + form by email NOTICE OF COMMENCEMENT ONLY				
Ministry of the Environment, Conservation and Parks (MECP)	General (Notices)		MEA.Notices.EAAB@ontario.ca			NOTICE OF COMPLETION ONLY				
Ministry of the Environment, Conservation and Parks (MECP)	Trevor Bell	Environmental Planner/Environmental Assessment Coordinator	trevor.bell@ontario.ca							2021-09-17
Ministry of Heritage, Sport, Tourism, Culture Industries (MHSTCI)	Karla Barboza	Team Lead(A), Heritage Heritage Planning Unit Programs and Services Branch	karla.barboza@ontario.ca	Ministry of Tourism, Culture and Sport 401 Bay Street, Suite 1700 Toronto ON M7A 0A7	416-314 7120				2020-10-10	2021-09-17
Ministry of Heritage, Sport, Tourism, Culture Industries (MHSTCI)	Dan Minkin	Heritage Planner (Culture Services Unit)	Dan.Minkin@ontario.ca	Suite 1700, 401 Bay Street, Toronto, ON M7A 0A7	416-314-7147				22-Oct-20	2021-09-17
Ministry of Heritage, Sport, Tourism, Culture Industries (MHSTCI)	Rosi Zirger	Heritage Planner (Culture Services Unit)	rosi.zirger@ontario.ca	Suite 1700, 401 Bay Street, Toronto, ON M7A 0A7	416-314-7159				22-Oct-20	2021-09-17
Ministry of Heritage, Sport, Tourism, Culture Industries (MHSTCI)	Joseph Harvey	Heritage Planner	Joseph.Harvey@ontario.ca	Suite 1700, 401 Bay Street, Toronto, ON M7A 0A7	613.242.3743				22-Oct-20	2021-09-17
Infrastructure Ontario	Frank Dieterman		Frank.Dieterman@infrastructureontario.ca	1 Dundas Street West, Suite 2000, Toronto, Ontario M5G 2L5	(416) 212-3768				22-Oct-20	2021-09-17
Ministry of Transportation	Jason White	Manager	jason.white@ontario.ca	1201 Wilson Avenue, 5th Floor Downsview, ON					22-Oct-20	2021-09-17
Ministry of Natural Resources and Forestry (MNRF)	Steven Strong	District Planner - Aurora District	steven.strong@ontario.ca	50 Bloomington Road Aurora, ON	905-713-7366				22-Oct-20	2021-09-17
Ministry of Indigenous Affairs		Consultation Unit	moeccpermissions@ontario.ca	160 Bloor Street East, 4th Floor Toronto, ON	416-326-4740				22-Oct-20	2021-09-17
Ministry of Aboriginal Affairs	Camille Assam	Executive Assistant to ADM	camille.assam@ontario.ca	160 Bloor Street East, 4th Floor, Toronto ON M7A 2E6	416-26-4772				22-Oct-20	2021-09-17
Environmental Assessment and Permissions Branch		Director	enviroperrmissions@ontario.ca	135 St. Clair Avenue West, 1st Floor, Toronto ON M4V 1P5					22-Oct-20	2021-09-17
Federal Agencies										
Fisheries and Oceans Canada			FisheriesProtection@dfo-mpo.gc.ca	867 Lakeshore Road Burlington, Ontario					22-Oct-20	2021-09-17
Environment Canada	Rob Dobos	Manager, Environmental Assessment Section	rob.dobos@ec.gc.ca	PO Box 5050, 867 Lakeshore Road, Burlington, ON L7S 1A1	905-336-4953				22-Oct-20	2021-09-17
GO Transit	Elise Croll	Director, Environmental Programs and Assessments	elise.croll@gotransit.com	20 Bay Street, Suite 600, Toronto ON M5J 2W3	416-869-3600 ext. 5520				22-Oct-20	2021-09-17
Municipal Departments, Committees and Services										
Region of Peel	Joe Avsec	Manager, Engineering and Infrastructure Planning	joe.avsec@peelregion.ca	10 Peel Centre Drive, Brampton, ON	905-791-7800 x7910				22-Oct-20	2021-09-17
Region of Peel	Andrew Farr	Commisioner of Public Works	andrew.farr@peelregion.ca	10 Peel Centre Drive, Brampton, ON	905-791-7800 x4395				22-Oct-20	2021-09-17
Region of Peel	Sean Carrick	Manager - Transportation	sean.carrick@peelregion.ca	10 Peel Centre Drive, Brampton, ON	905-791-7800 x4100				22-Oct-20	2021-09-17

Mill Street Class EA
Technical Agency Stakeholder Contact List

Agency	Contact	Title	Email	Address	Phone	Notes	Date Added to List	Removed from List	Notice of Study (date sent)	Notice of PIC1 (date sent)
Region of Peel	Neal Smith	PM - Sustainable Transportation	neal.smith@peelregion.ca	10 Peel Centre Drive, Brampton, ON	905-791-7800 x5638				22-Oct-20	2021-09-17
Region of Peel	Sally Rok	Project Manager, Transportation Division	sally.rook@peelregion.ca	10 Peel Centre Drive, Brampton, ON					22-Oct-20	2021-09-17
Region of Peel	Asha Saddi	Technical Analyst, Infrastructure Programming & Studies Transportation Division	asha.saddi@peelregion.ca	10 Peel Centre Drive, Brampton, ON		Contact person for all communications on this project for the Region	2022-05-30			
Caledon OPP	Mike Garant		Mike.Garant@opp.ca						22-Oct-20	2021-09-17
Caledon Fire & Emergency	Dave Peyalo	Fire Chief	Dave.Pelayo@caledon.ca		905-584-2272, 4340				22-Oct-20	2021-09-17
Town of Caledon	Fuwing Wong	General Manager, Finance and Infrastructure Services / Chief Financial Officer	fuwing.wong@caledon.ca	6311 Old Church Road Caledon, ON	905.584.2272 x4257				22-Oct-20	2021-09-17
Town of Caledon	Ryan Grodecki	Manager of Engineering	Ryan.Grodecki@caledon.ca	6311 Old Church Road Caledon, ON	905.584.2272 x4101				22-Oct-20	2021-09-17
Town of Caledon	Kant Chawla	Senior Transportation Planner, Development Approval and Planning Policy Department	kant.chawla@caledon.ca	6311 Old Church Road Caledon, ON	905-584-2272 ext. 4293				22-Oct-20	2021-09-17
Town of Caledon	Arash Olia	Manager, Transportation Engineering, Finance & Infrastructure Services	arash.olia@caledon.ca	6312 Old Church Road Caledon, ON	905-584-2272 x 4073				22-Oct-20	2021-09-17
Town of Caledon, Heritage Resoures Office	Douglas McGlynn	Heritage Urban Design Planner	douglas.mcglynn@caledon.ca		905.584.2272 x. 4232				22-Oct-20	2021-09-17
Town of Caledon - Heritage Committee	Sally Drummond	Heritage Resource Officer	sally.drummond@caledon.ca	6311 Old Church Road Caledon, ON	905.584.2272 x4243				22-Oct-20	2021-09-17
School Boards										
Peel District School Board	Stan Cameron	Trustee - Caledon	stan.cameron@peelsb.com	5650 Hurontario Street Mississauga, ON					22-Oct-20	2021-09-17
Peel District School Board	Suzanne Blakeman	Manager, Planning & Enrolement / Planning & Accomodation Support Services	suzanne.blakeman@peelsb.com	5650 Hurontario Street Mississauga, ON	905-890-1010 ext. 2216				22-Oct-20	2021-09-17
Peel District School Board	Bianca MV Bielski	Manager, Planning & Enrolement	bianca.bielski@peelb.com	5650 Hurontario Street, Mississauga, ON L5R 1C6	905-890-1010 ext. 2221				22-Oct-20	2021-09-17
Dufferin-Peel Catholic District School Board	Frank Di Cosola	Trustee - Caledon/Dufferin	frank.dicosola@dpcdsb.org	40 Matheson Boulevard West Mississauga, ON					22-Oct-20	2021-09-17
Dufferin-Peel Catholic District School Board	Krystina Koops	Planner	krystina.koops@dpcdsb.org	40 Matheson Boulevard West Mississauga, ON					22-Oct-20	2021-09-17
Conservation Authority										
Credit Valley Conservation Authority (CVC)	Tyler Slaght	Regulations Officer	tyler.slaght@cvc.ca	1225 Derry Road Mississauga, ON	905-670-1615 x406					2021-09-17
Niagara Escarpment Commission	Brandon Henderson		brandon.henderson@ontario.ca	232 Guelph St, Georgetown, ON L7G 4B1		Planner Responsible for Caledon Area				2021-09-17
Utilities										
TransCanada Pipelines Inc.	Darlene Quilty	Project & Planning Coordinator	dquilty@mhbcplan.com	86 Healey Road, Bolton ON L7E 5A7	705-727-0663 x21				22-Oct-20	2021-09-17
Hydro One Networks			secondarylanduse@hydroone.com	483 Bay Street, North Tower, 15th Floor Toronto, ON					22-Oct-20	2021-09-17
Hydro One Networks	Matt Casey		matthew.casey@hydroone.com		519-939-8729				22-Oct-20	2021-09-17

Mill Street Class EA
Technical Agency Stakeholder Contact List

Agency	Contact	Title	Email	Address	Phone	Notes	Date Added to List	Removed from List	Notice of Study (date sent)	Notice of PIC1 (date sent)
Hydro One Caledon		Planning and Scheduling Department	westcentralzonescheduling@HydroOne.com OR Zone2Scheduling@HydroOne.com	185 Clegg Rd, Markham, ON L6G 1B7 or 40 Olympic Drive, Dundas, ON L9H 7P5	905-627-6050				22-Oct-20	2021-09-17
Hydro One Caledon			tpumarkup@hydroOne.com	185 Clegg Rd Markham, ON L6G 1B7					22-Oct-20	2021-09-17
Bell Canada	Ahmed Saeed	Bell Implementation Mgr.	ahmed.saeed2@bell.ca		416-215-1076				22-Oct-20	2021-09-17
Bell Canada	Diego Tobias	GT Fiber Services Inc.	PUCC.circulations.GT@bell.ca	5025 Creekbank Road Mississauga, ON L4W 0B6	905-614-6820				22-Oct-20	2021-09-17
Bell Canada Municipal Operations Centre	Diana Velez	c/o Netricom	bell.moc@netricom.com	200 Town Centre Bvd, Suite 300 Markham, ON L3R 8G5	905-470-2112 ext. 40309				22-Oct-20	2021-09-17
Cogeco Cable Systems Inc.	Craig Krueger	System Planner	cdkrueger@cogeco.ca						22-Oct-20	2021-09-17
Rogers	Roy Holland		Roy.Holland@rci.rogers.com						22-Oct-20	2021-09-17
Rogers	Graham McPherson	System Planner (Bloton Area)	Graham.McPherson@rci.rogers.com	244 Newkirk Road, Richmond Hill, ON L4C 3S5	(705) 896-9608				22-Oct-20	2021-09-17
Rogers Telecon			Rogers.MOC@telecon.ca						22-Oct-20	2021-09-17
Rogers Cable TV Limited	Marian Wright	Planning Coordinator	marian.wright@rci.rogers.com	3573 Wolfdale Road Mississauga, ON	647-771-9327				22-Oct-20	2021-09-17
Rogers Cable TV Limited	Travis Graham	Municipal & Utility Relations GTA West	travis.gragam@rci.rogers.com	3573 Woldedale Road	416-913-1243				22-Oct-20	2021-09-17
Rogers Cable	Richard Humpage	EA Coordinator - Region Representative East of Centreville Creek Road	richard.humpage@rci.rogers.com	244 Newkirk Road, Richmond Hill, ON L4C 3S5	905-780-7014				22-Oct-20	2021-09-17
Rogers Cable			GTAW.Markups@rci.rogers.com	3573 Wolfdale Road Mississauga, ON					22-Oct-20	2021-09-17
Enbridge Gas Distribution Incorporated	Emilio Labra	Advisor Planning – Central Region West	Emilio.labra@enbridge.com	500 Consumers Road North York, ON	905-458-3811				22-Oct-20	2021-09-17
Enbridge Gas Distribution Incorporated	Meetpal Chhina	Planning Technical Specialist	meetpal.chhina@enbridge.com	6 Colony Court, Brampton ON L6T 4E4	905-458-2159 905-867-9329				22-Oct-20	2021-09-17
Enbridge Gas Distribution Incorporated			markups@enbridge.com						22-Oct-20	2021-09-17
Alectra	Paul Sidhu	Senior Manager, Systems Planning	paul.sidhu@alecrautilities.com	3240 Mavis Road Mississauga, ON	905-566-2738				22-Oct-20	2021-09-17
Student Transportation of Peel Region	Marcy Macina	Operations Officer, East Area	marcy.macina@dpcdsb.org	Keaton Centre, 5685 Keaton Crescent Mississauga, ON L5R 3H5	905-890-6000				22-Oct-20	2021-09-17
MTS Allstream	Ian Fleming	EA Coordinator	utility.circulations@zayo.com	50 Worcester Road Toronto, ON M9W 5X2	416-345-3406				22-Oct-20	2021-09-17
Indigenous Groups										
Metis Nation of Ontario			consultations@metisnation.org	Métis Consultation Unit Métis Nation of Ontario Head Office Suite 1100 – 66 Slater Street Ottawa, ON K1P 5H1	613-798-1488				22-Oct-20	2021-09-17
Metis Nation of Ontario	Justin Hunt	Consultation Advisor	JustinH@metisnation.org	355 Cranston Crescent PO Box 4 Midland, Ont. L4R-4K6	PH: 705-526-6335 Ext. 201 (Not Currently Monitored) Cell: 705-527-3612		21-Jan-21			2021-09-17

Mill Street Class EA
Technical Agency Stakeholder Contact List

Agency	Contact	Title	Email	Address	Phone	Notes	Date Added to List	Removed from List	Notice of Study (date sent)	Notice of PIC1 (date sent)
Mississaugas of the Credit First Nation	Mark LaForme	Director, Department of Consultation and Accommodation	Mark.LaForme@mncfn.ca	Mississaugas of the Credit First Nation 2789 Mississauga Road R.R. #6 Hagersville, Ontario, N0A 1H0	Office: 905-768-4260 Mobile: 289-527-6577	Replaced Fawn Sault as Consultaiton Director	2022-05-27			
Mississaugas of the Credit First Nation	Adam LaForme	Archaeological Operations Supervisor	adam.laforme@mncfn.ca	Mississaugas of the Credit First Nation 2789 Mississauga Road R.R. #6 Hagersville, Ontario, N0A 1H0	Cell 289-527-2763		2022-05-27			
Mississaugas of the Credit First Nation	Abby LaForme	Acting Consultation Coordinator	abby.laforme@mncfn.ca	Mississaugas of the Credit First Nation 2789 Mississauga Road R.R. #6 Hagersville, Ontario, N0A 1H0	Ph: (905) 768 – 4260		2022-05-27			
Six Nations of the Grand River	Lonny Bomberry	Lands & Resource Director	lonnybomberry@sixnations.ca	Six Nations of the Grand River Consultation and Accommodations Team 2498 Chiefswood Rd. P.O. Box 5000 Ohsweken, ON N0A 1M0					22-Oct-20	2021-09-17
Six Nations of the Grand River	Robbin Vanstone	Land Use Office, Lands and Research	rvanstone@sixnations.ca							2021-09-17
The Haudenosaunee Confederacy	Haudenosaunee Development Institute	General Contact	info@hdi.land	The Haudenosaunee Confederacy c/o Haudenosaunee Development Institute P.O. Box 714 Ohsweken, Ontario, N0A 1M0N0A 1M0	519-445-4222					
Huron-Wendat Nation			administration@cnhw.qc.ca	Nation Huronne-Wendat 255, place Chef Michel Laveau Wendake (Québec) G0A 4V0 Canada					22-Oct-20	2021-09-17
Huron-Wendat Nation	Marie-Sophie Gendron	Archealogy Analyst	Marie-Sophie.Gendron@wendake.ca							
Huron-Wendat Nation	Maxime Picard	Coordinateur de projets - Ontario	maxime.picard@cnhw.qc.ca	Nation Huronne-Wendat 255, place Chef Michel Laveau Wendake (Québec) G0A 4V0 Canada	418-843-3767 x2105				22-Oct-20	2021-09-17
Elected Officials										
Ontario Government	Sylvia Jones	MPP - Dufferin Caledon	sylvia.jonesco@pc.ola.org	180 Broadway, 3rd Floor, Suite A, Organville ON L9W 1K3	519-941-7751				22-Oct-20	2021-09-17
Federal Government	Kyle Seeback	MP - Dufferin Caledon	kyle.seeback@parl.gc.ca	229 Broadway, Unit #2, Orangville ON L9W 1K4					22-Oct-20	2021-09-17
Town of Caledon	Allan Thompson	Mayor	allan.thompson@caledon.ca	6311 Old Church Road Caledon, ON	416-319-6543				22-Oct-20	2021-09-17
Town of Caledon	Ian Sinclair	Ward 1 Regional Councillor	ian.sinclair@caledon.ca	6311 Old Church Road Caledon, ON	647-542-0261				22-Oct-20	2021-09-17
Town of Caledon	Lynn Kiernan	Ward 1 Area Councillor	lynn.kiernan@caledon.ca	6311 Old Church Road Caledon, ON	416-578-9156				22-Oct-20	2021-09-17
Town of Caledon	Johanna Downey	Ward 2 Regional Councillor	johanna.downey@caledon.ca	6311 Old Church Road Caledon, ON	416-434-4102				22-Oct-20	2021-09-17

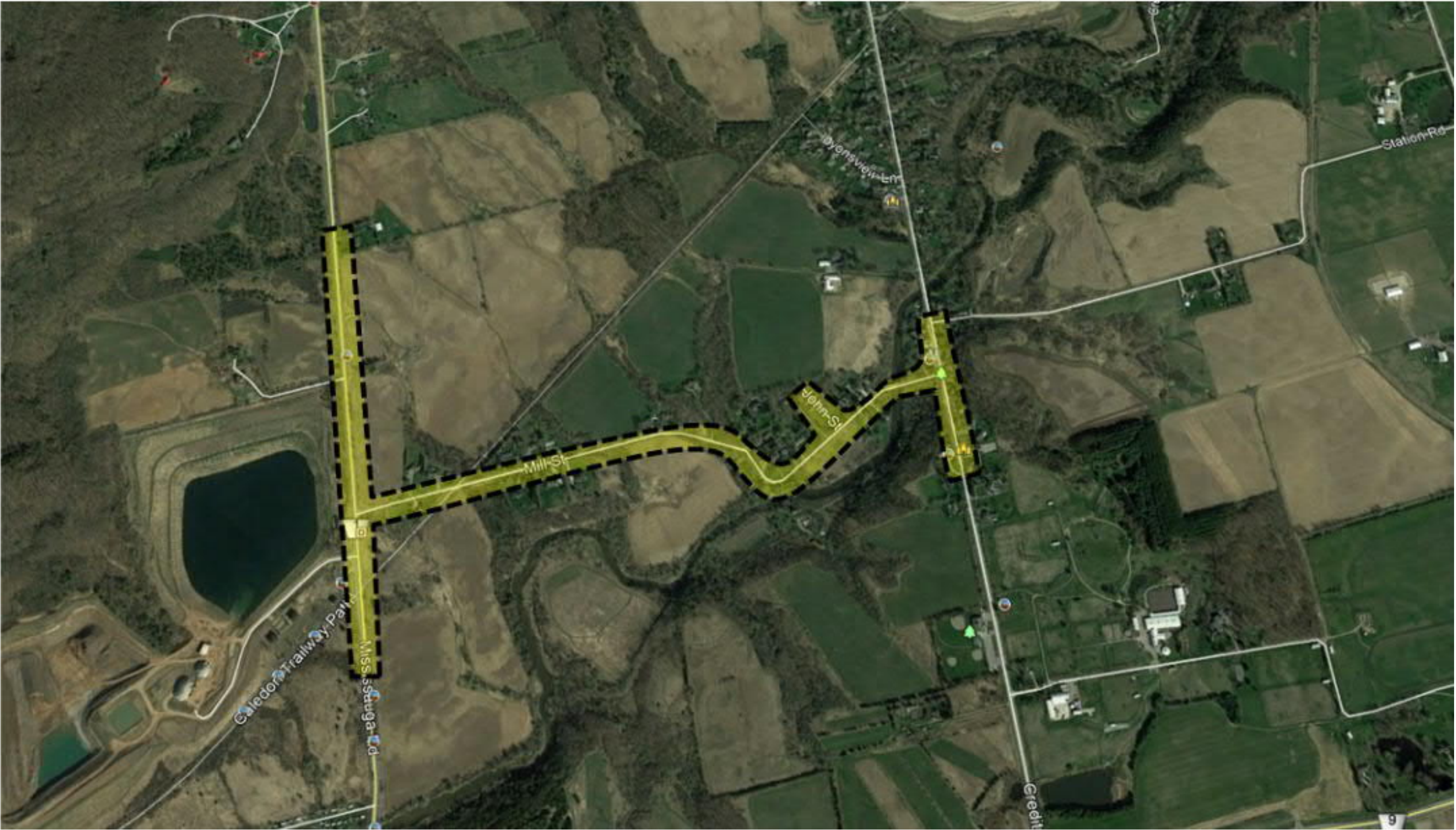
Mill Street Class EA
Technical Agency Stakeholder Contact List

Updated: 2022-08-25

Agency	Contact	Title	Email	Address	Phone	Notes	Date Added to List	Removed from List	Notice of Study (date sent)	Notice of PIC1 (date sent)
Town of Caledon	Christina Early	Ward 2 Area Councillor	christina.early@caledon.ca	6311 Old Church Road Caledon, ON	416-576-9366				22-Oct-20	2021-09-17
Other										

APPENDIX 1-3


Notice Distribution Area




LEGEND:

PIC NOTIFICATION DISTRIBUTION AREA





TOWN OF CALEDON



R.V. Anderson Associates Limited
engineering • environment • infrastructure

MILL STREET MUNICIPAL CLASS
ENVIRONMENTAL ASSESSMENT

PIC NOTIFICATION DISTRIBUTION AREA

APPENDIX 2

PIC MATERIALS

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT



MILL STREET (MISSISSAUGA ROAD TO CREDITVIEW ROAD)

WELCOME PUBLIC INFORMATION CENTRE #2

June 15, 2022
5:30pm to 7:00pm

PLEASE SIGN IN

**Please review the materials and provide your comments using the sheets provided.
Your feedback is important to us.**

Staff are available to answer your questions.

WHY ARE WE HERE?



In consideration of the comments received to date, the Town has developed additional pedestrian accommodation options for Mill Street. The Town is holding this PIC to allow residents and the public to review these options and provide input.

No recommended option has been selected.

The following preliminary recommendations presented at PIC #1 will be carried forward:

- Additional trees will be provided per Town's replanting policy
- Curb and gutter with catch basins to capture stormwater runoff
- Streetlighting improvements to enhance community safety
- Guiderail where required
- Slope stability (to be confirmed)
- Implement additional traffic calming measures (type and location to be confirmed)

Please provide your input on the pedestrian accommodation options by completing the comment sheet provided.

Please complete and submit the comment sheet by July 6th

STUDY AREA AND PURPOSE OF STUDY



The Study is being completed to review opportunities within the study limits to address:

- Increasing wear and tear of the roadway
- Enhanced road safety for all users
- Improved accessible active transportation (walking, cycling) including Caledon Trail crossing improvements
- Roadway drainage and stormwater management needs.

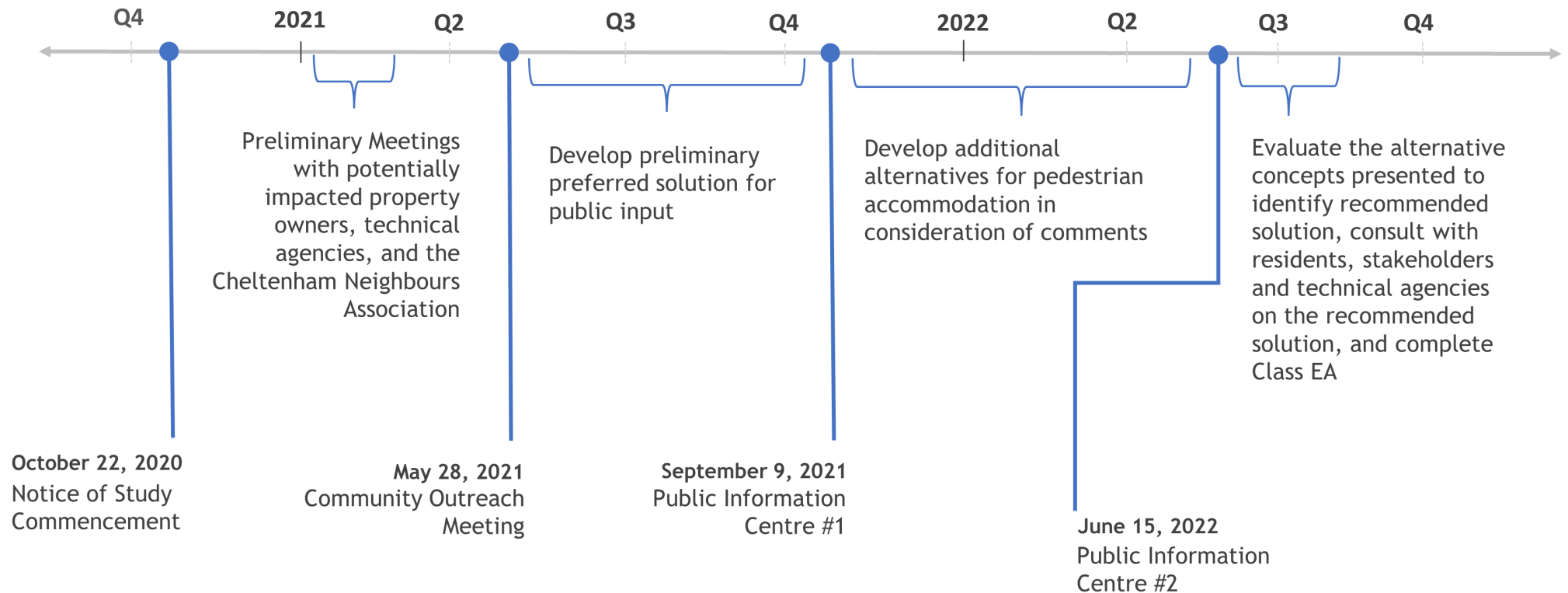


The project is being completed as a Schedule “B” project in accordance with the *Municipal Class Environmental Assessment* (October 2000, amended in 2007, 2011 & 2015).

PUBLIC AND TECHNICAL AGENCY CONSULTATION



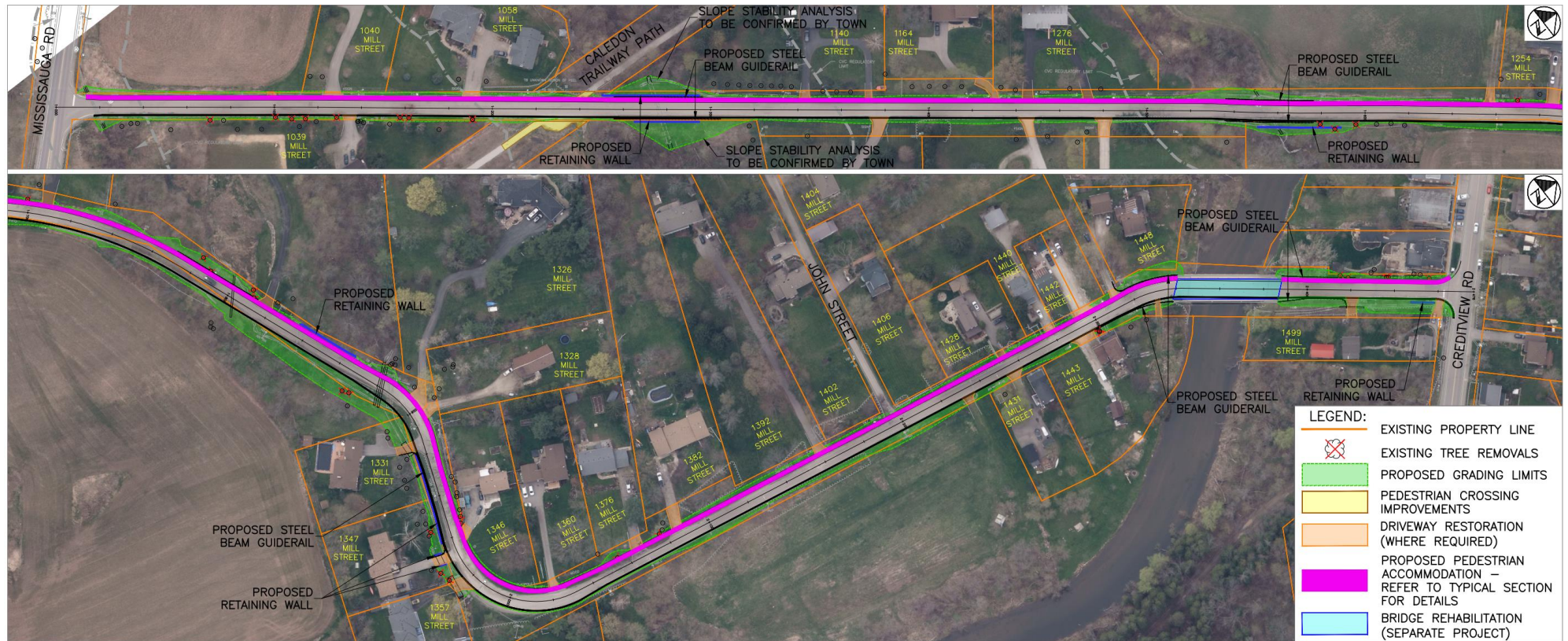
Significant public and technical agency consultation has been completed so far, but the Town wants additional input before identifying the recommended pedestrian amenities.



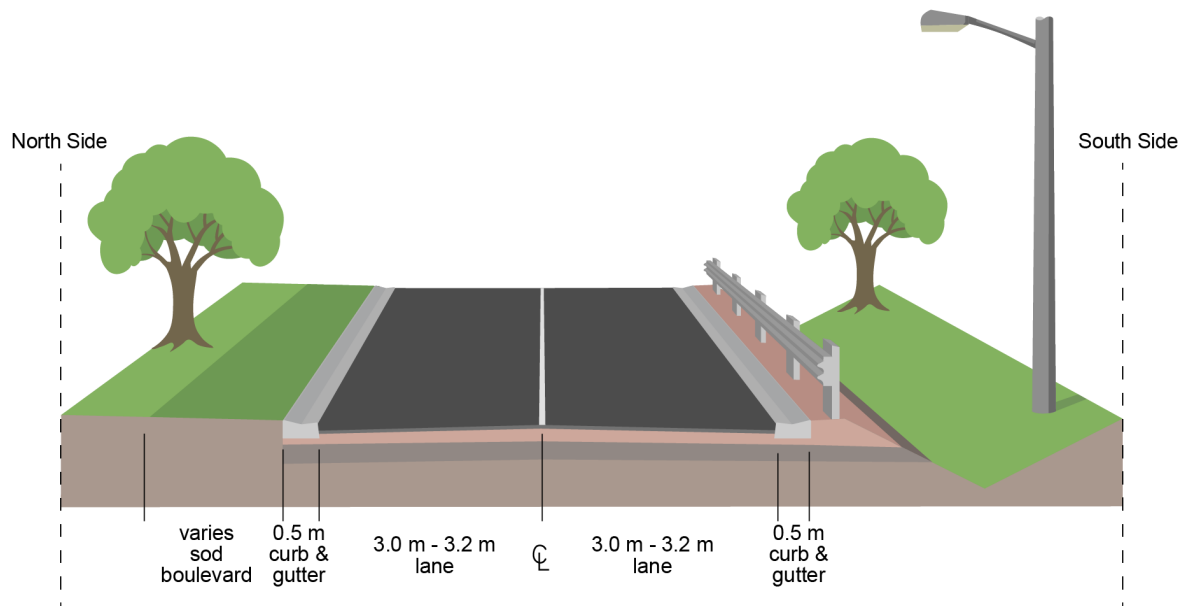
PEDESTRIAN ACCOMMODATION FOR MILL STREET



New pedestrian accommodation options have been developed for input from residents and the public (highlighted in **purple** below). All options have similar property and tree impacts.



OPTION 1 – EXISTING WITH MOUNTABLE CURB



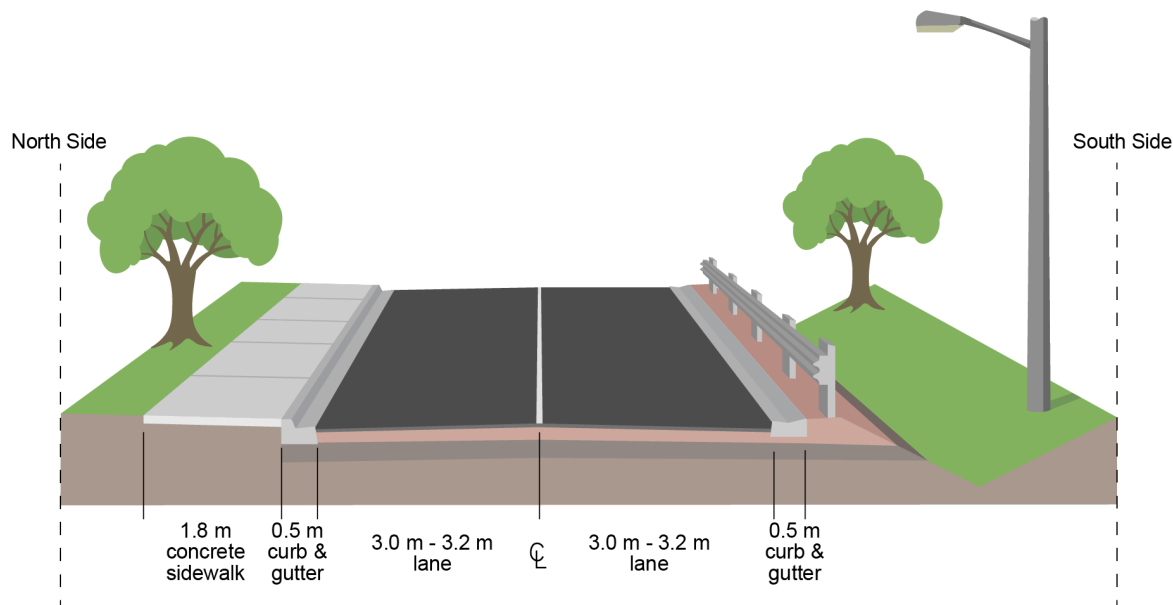
Advantages

- ✓ Maintains general rural characteristic of road (mountable curbs and no sidewalk)
- ✓ Less impervious area (less stormwater runoff)

Disadvantages

- ✗ Does not address study objectives in enhancing road safety for all users and improving accessible active transportation
- ✗ Does not accommodate pedestrians
- ✗ No separation between pedestrians, cyclists and vehicular traffic
- ✗ Boulevard becomes snow storage area in winter

OPTION 2 – SIDEWALK



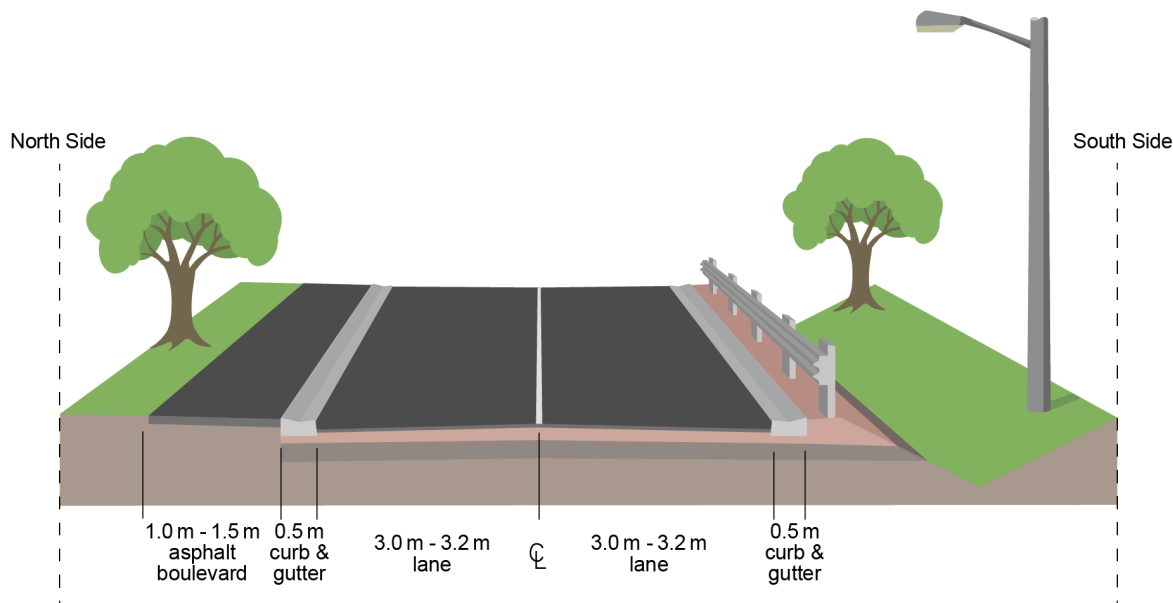
Advantages

- ✓ Provides dedicated pedestrian facility - Accessibility for Ontarians with Disabilities Act (AODA) compliant, accommodates mobility aids (ex. wheelchairs, wheeled walkers)
- ✓ Provides even surface to walk on
- ✓ Full barrier curb separates pedestrians from roadway and provides traffic calming benefits
- ✓ Will be maintained by the Town in the winter

Disadvantages

- ✗ Does not maintain general rural characteristic of road (barrier curbs and sidewalk)
- ✗ No separation between cyclists and vehicular traffic

OPTION 3 – ASPHALT BOULEVARD



Advantages

- ✓ Maintains general rural characteristic of road (mountable curbs and no sidewalk)
- ✓ Provides even surface to walk on (seasonal)

Disadvantages

- ✗ Not a dedicated pedestrian facility – not AODA compliant
- ✗ No separation between cyclists and vehicular traffic
- ✗ Boulevard becomes snow storage area in winter
- ✗ Mountable curb allows vehicles to drive onto boulevard, creating potential conflict with pedestrians

NEXT STEPS



- Review comment sheets received from this PIC
- Evaluate the alternative concepts presented and identify the preferred solution – check website for updates
- Consult with residents, stakeholders and technical agencies on the preferred solution
- Prepare and submit a Project File Report for 30 Day public review
- Proceed to detailed design (2022) and construction (2023) pending Council approval and budget

WE WANT YOUR INPUT!



Please submit your input on the pedestrian accommodation options in the comment sheet provided and place it in the comment box or submit via email by **July 6, 2022**.

**THANK YOU FOR ATTENDING!
PLEASE FILL IN A COMMENT SHEET**

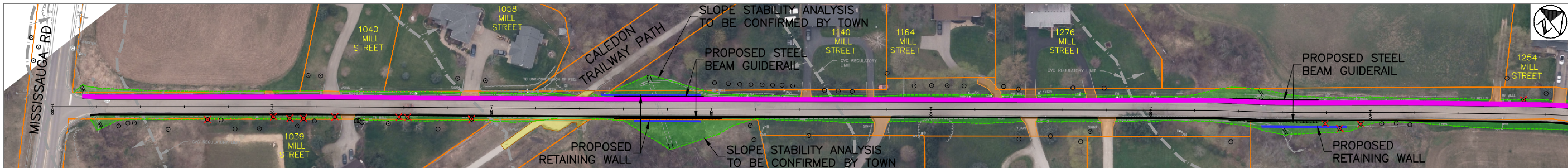
Project Team Contacts:

Town of Caledon

Shun Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure Engineering
Services
Tel: (905) 584-2272 x 4040
E-mail: shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P. Eng., PMP., M.Eng.
Project Manager, Transportation
Tel: (416) 497-8600 x1471
E-mail: winnie.wong@rvanderson.com



APPENDIX 3

COMMENTS RECEIVED

APPENDIX 3-1

Comments Received: General Public and Residents

Comments Received: Prior to PIC

Connor MacIsaac

From: [REDACTED]
Sent: June 14, 2022 2:52 PM
To: [REDACTED]
Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2
Attachments: IMG_5150.MOV

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Thank you [REDACTED]. We all want to retain the rural feel of this neighborhood. We bought here because we love it.

[REDACTED]
Sent from my iPhone

On Jun 14, 2022, at 2:42 PM, [REDACTED] wrote:

Seeing as I'm still unsure how this meeting will be on record, I will send this through here as most of the residents have been included.

I was in Prince Edward County this weekend and they have figured out how to keep the countryside, the countryside and yet still accommodate the hundreds of tourists that visit every year. The pictures attached so a road, VERY similar to Mill st in width, usage, hills and bends with blind spots etc and this road was the main road that takes pedestrians, cyclists, ebikes, cars, buses, limos, taxis etc to all the wineries and breweries that are scattered along the way. There was no asphalt (just chip & tar and NO pot holes!), no dedicated pedestrian/cyclist lane, no guide rails, just appropriate and beneficial signage, tall native grasses and hundreds of trees! As you can see, there are parts of our province that strive to maintain a rural landscape, but also provide safety to all who use the roads and visit the "countryside". If parts of Ontario can figure out a way to do this, why can't we be a little bit more inventive?





On Tue, Jun 14, 2022 at 1:01 PM [REDACTED] wrote:

Thank you for sharing the slides with us.

As far as the community feeling like they are being heard and that the Town and the engineers are eager to receive comments, we are very skeptical, as my email above was never answered and after briefly looking at the slides, it is clear that the Town of Caledon is choosing to urbanize our rural areas regardless of resident support.

See you all tomorrow.

[REDACTED]

On Tue, Jun 14, 2022 at 12:11 PM Winnie Wong <Winnie.Wong@rvanderson.com> wrote:

All,

The PIC slides are posted on the Town's website and can be accessed here:

<https://www.caledon.ca/en/news/mill-street-ea.aspx>

We look forward to meeting everyone tomorrow.

Regards,

Winnie Wong, P. Eng., PMP, M.Eng.

Associate, Project Manager

R.V. Anderson Associates Limited

From: [REDACTED]
Sent: June 8, 2022 2:31 PM
To: Shun Cheung <shun.cheung@caledon.ca>
Cc: [REDACTED]

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hello,

I didn't mean if someone wants to write something down. Obviously that would be recorded. I meant if someone has a verbal question. How is that being documented and recorded?

[REDACTED]

On Wed, Jun 8, 2022 at 2:13 PM Shun Cheung <Shun.Cheung@caledon.ca> wrote:

Hi [REDACTED]

Comment sheets will be available at the Public Information Centre (PIC) meeting and on the project website to submit your questions. It would be appreciated to submit any project related questions by July 6th.

All the questions and answers submitted from the comment sheets will be documented and filed in the Project File Report.

Shun Cheung, P.Eng., PMP

Project Manager, Capital Infrastructure

Engineering Services

Office: 905.584.2272 x.4040

Cell: 416.436.0910

Email: shun.cheung@caledon.ca

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From: [REDACTED]
Sent: Wednesday, June 08, 2022 1:55 PM
To: Andrew Pearce <Andrew.Pearce@caledon.ca>
Cc: [REDACTED]

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

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Hello,

Please advise on the suggestion and question below regarding how all the questions and answers, asked and answered personally to each resident at the open house, will be recorded and documented.

Thank you,

[REDACTED]

On Mon, Jun 6, 2022 at 5:41 PM [REDACTED] wrote:

Perhaps there could be a sit down question/answer/comment after the "open house" for those residents who prefer and believe the more formal format is much more appropriate.

On Mon, Jun 6, 2022 at 5:37 PM [REDACTED] wrote:

How will it be documented?

On Mon, Jun 6, 2022 at 5:36 PM Andrew Pearce <Andrew.Pearce@caledon.ca> wrote:

[REDACTED]

Very good question, thanks.

All information presented at the PIC including any questions and answers will be documented and included in the Environmental Study Report for the public record.

Thanks,

Andrew Pearce,

Director, Engineering Services

Engineering Services Department

Office: 905.584.2272, Ext 4003

Cell: 416.347.4881

Email: andrew.pearce@caledon.ca

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Please consider the environment before printing.

From: [REDACTED]
Sent: Monday, June 6, 2022 5:31 PM
To: Andrew Pearce <Andrew.Pearce@caledon.ca>
Cc: [REDACTED]

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Thank you for your email.

So will there be a presentation describing and explaining the rationale behind the proposals that will be on public record? Or will the presentations be displayed and people have to ask questions individually in order to get answers? If so, how is this being recorded then? The questions being asked and the answers being given?

[REDACTED]

On Mon, Jun 6, 2022 at 5:24 PM Andrew Pearce <Andrew.Pearce@caledon.ca> wrote:

Good afternoon [REDACTED]

Thank you for your inquiry regarding the upcoming public information centre on June 15. I look forward to meeting you there.

The project team has arranged an extra PIC for this road project for the purpose of illustrating to the residents how all the input to date has been addressed and to present the recommended road and drainage design. In addition, the project team is seeking feedback from the local residents on three new design options to accommodate pedestrians on the road. The three options include; 1. curb side concrete sidewalk, 2. paved shoulder, and 3. Grassed boulevard - no dedicated pedestrian facility.

The presentation slides are being finalized with the objective of posting them on the project website before the PIC. You will be notified by email when they are available. Although the slides will be available before the meeting, it's still important to attend the PIC to hear the description and rationale behind each of the design alternatives. I appreciate this is an extra PIC but we want to get it right.

In the interim, if you have any questions, please feel free to contact me.

Regards,

Andrew Pearce,

Director, Engineering Services

Engineering Services Department

Office: 905.584.2272, Ext 4003

Cell: 416.347.4881

Email: andrew.pearce@caledon.ca

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Please consider the environment before printing.

From: [REDACTED]
Sent: Thursday, June 2, 2022 10:35 AM
To: Christina Early <Christina.Early@caledon.ca>

Connor MacIsaac

From: [REDACTED]
Sent: June 3, 2022 9:29 AM
To: Winnie Wong
Cc: [REDACTED]
Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2
Categories: Resident Comments

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Good morning Ms. Wong,
I read your email to [REDACTED] and Councillor Early. In response to your comment about the public meeting process, both the process and format of the past sessions were determined by the presenters, not the community or the “vocal participants”. As you may recall, the process was that community members were required to submit their questions online or “wave” online to make a request to speak. The questions that were, or were not answered by the panel were determined entirely by the panel. The opportunity for any individual to speak was also entirely at the discretion of the panel as they controlled both the format and the technology. As a result, there is really no way for the community to know if anyone was *unable to provide any feedback* as you state in your email below. Based on your comments, I have to wonder, do you consider a vocal participant someone that spoke once; someone that spoke more than once; or perhaps someone that just spoke passionately about their concerns? Really, your comment that the public participation was *dominated by a few vocal participants* is simply “unhelpful”.

Further to [REDACTED] comment about the format, is this format mandated by the process, or simply at the discretion of the presenters?

It is my sincere hope that the next iteration of plans for Mill St. demonstrates more creativity for the genuine betterment of the community, and not some slavish adherence to standards where the project is really nothing more than an exercise in sewers, sidewalks and setbacks. We all understand that changing various factors can change the applicable standards. For example, and if I understood the explanations provided in previous presentations, doing something as simple as changing the speed limit in a section of the road can change the applicable standards for the curve radius on the road. We also all understand that widening the road **will** increase the speed of traffic. Unless meaningful design measures are included to address traffic calming for a widened road, the net result of the project will be a degradation of the community environment resulting from speeding and increased traffic volume. Simply adding more concrete to the project will not improve Cheltenham. Unless the Town simply wants to urbanize the area without consideration for the community environment and its residents, in my view, more creativity is required from the planners than was demonstrated in the past plans.

I look forward to reviewing and commenting on the next set of plans for the Mill St. project.
Sincerely,

[REDACTED]

From: Winnie Wong [mailto:Winnie.Wong@rvanderson.com]

Sent: Thursday, June 02, 2022 5:23 PM

To: [REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

Subject: RE: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

Councillor Early and [REDACTED],

The open house PIC format provides us with an one on one opportunity to talk to the participants. In our previous PIC experience a presentation format tends to result in the conversation dominated by a few vocal participants, while the others stay silent/ not able to provide any feedback.

The intent of this upcoming PIC is to gather feedback from all participants regarding the pedestrian accommodation alternatives on Mill Street. We want to listen to participants individually and answer questions they may have regarding the alternatives presented, hence we proposed the open house format.

Please let me know if you have other questions. I hope this helps.

Regards,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: [REDACTED]

Sent: June 2, 2022 10:35 AM

To: Christina Early <Christina.Early@caledon.ca>

Cc: [REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]
[REDACTED]

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

Cc:

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

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Good morning,

Hoping you can respond today, Christina.

Thank you,



On Tue, May 31, 2022 at 11:45 AM Christina Early <Christina.Early@caledon.ca> wrote:

Hi everyone, I am currently out of town returning late this evening. I will respond tomorrow.

Thank you

Christina

Christina Early

Area Councillor, Ward 2

Office: 905.584.2272 x. 5130

Cell: 416.576.9366

Email: christina.early@caledon.ca

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From: [REDACTED]
Sent: Tuesday, May 31, 2022 11:23:01 AM
To: Winnie Wong <Winnie.Wong@rvanderson.com>
Cc: [REDACTED]

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

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Thank you.

Christina, can you please step in here and explain to RV Anderson the importance of a formal meeting for your residents.

[REDACTED]

On Tue, May 31, 2022 at 10:46 AM Winnie Wong <Winnie.Wong@rvanderson.com> wrote:

Morning everyone,

At this time our team is working to pull together the presentation slides and if they are ready prior to the PIC, they will be posted on the project website and you will be notified by email when they are available. The intent of this PIC is to gather feedback from the public regarding options for pedestrian accommodation. No recommendation has been made. Our team will be at the open house to answer any questions attendees may have related on the presented options.

The PIC slides and comment sheet will be made available on the project website after the June 15. All attendees, including residents who could not attend in person will have the opportunity to review the material and submit their comments till July 6.

The resident's proposal was received last Fall and will be included in the Project File for this EA study, which will be available for public's review at the end of the study.

Regards,

Winnie Wong, P. Eng., PMP, M.Eng.

Associate, Project Manager

R.V. Anderson Associates Limited

From: [REDACTED]
Sent: May 30, 2022 10:19 PM
To: Connor MacIsaac <cmacisaac@rvanderson.com>; Andrew Pearce <andrew.pearce@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>; Shun Cheung <shun.cheung@caledon.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; John P. Does <jdoes@rvanderson.com>
Cc: [REDACTED]
Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Connor and All

Further to the concerns raised below, we are again asking that the proposal(s) that will be on display at the open house on June 15 be made available to our community and the residents in advance of the open house. This will allow all the residents who will be impacted by any alternative proposal a fair opportunity to review the latest

proposals and therefore attend the open house better informed. There has been no response yet to this request that was made last week.

Also, you have mentioned below that "all attendees" will be invited to provide their comments and preferences. If there are residents who cannot attend the open house that evening, what plans have been made for those residents to have their questions and comments heard?

Finally, can you please confirm that the residents' proposal that was submitted last fall (which also included questions, comments and a supporting letter from the Cheltenham Area Residents Association) is included as part of the public record for this project?

Thank you,

[REDACTED]

From: [REDACTED]
Sent: May 30, 2022 12:49 PM
To: Christina Early <Christina.Early@caledon.ca>; Connor MacIsaac <cmacisaac@rvanderson.com>

Cc: [REDACTED]

Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

Hi Connor,

Considering how much feedback you have had from our community regarding the proposals, I find it infuriating that this is the chosen forum.

A few months ago, we came together as a community and signed a petition with a counter proposal, however this was never even acknowledged by either the Town or your engineering firm.

As I said before, as this project will solely affect the residents, I am asking our councillor, Christina Early, who has been included in this thread, to step in and amend the format of this "PIC".

Thank you,

[REDACTED]

On Mon, May 30, 2022 at 11:24 AM Connor MacIsaac <cmaclsaac@rvanderson.com> wrote:

[REDACTED]

Thank you for your continued input to this study. All attendees will be invited to provide their comments and preferences for the alternatives presented, after reviewing the display boards at their own pace. All of the comment forms will be documented in the Project File Report, in accordance with the *Municipal Class Environmental Assessment Act (October 2000, amended in 2007, 2011 & 2015)*.

Thank you and kind regards,

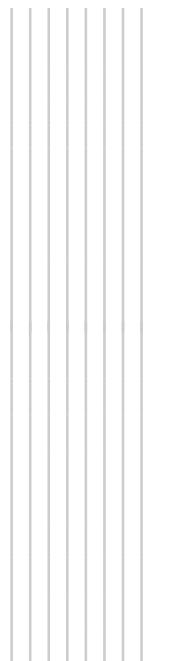


Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a [43 Church Street, Suite 104, St. Catharines, ON L2R 7E1](#)



From: [REDACTED]

To: Connor MacIsaac <cmacisaac@rvanderson.com>

Cc: [REDACTED]

[REDACTED]
Subject: Re: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Connor,

I'd like to know how this meeting is being recorded as public record since it is in an "open house" format.

As the small community that this project affects, we request a more formal meeting with the consultants and the town, that will record the questions asked and responses given.

Please advise.

Thank you,

[REDACTED]
[REDACTED]
[REDACTED]

On Mon, May 30, 2022 at 10:57 AM Connor MacIsaac <cmacIsaac@rvanderson.com> wrote:

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Public Information Centre (PIC) #2 for the **Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA)**. The purpose of this meeting is garner feedback on the options for pedestrian accommodations developed for Mill Street in consideration of the feedback received during and following PIC #1, held online October 5, 2021.

The PIC will be held as an informal “Open House” with materials pertaining to the study on display, with members of the project team on hand to answer questions and discuss issues related to the project. Attendees will be asked to review the materials on display and provide their preferences for pedestrian accommodations. A final recommendation for Mill Street will be developed following the PIC, in consideration of the input received.

The details of the PIC are below:

Date: Wednesday June 15, 2022
Time: 5:30 to 7:00 pm
Location: Cheltenham Community Centre - [14190 Creditview Rd, Caledon, ON L0P 1N0](#)

Entrance is located at the south side of the building

The project team values the participation of all stakeholders and wishes to ensure that the community’s interests and concerns are taken into consideration. Please contact the project team members listed below if you require a hard copy of the materials or for further information on this project.

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure Engineering Services
905.584.2272 X.4040
shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
416.497.8600 X.1471
winnie.wong@rvanderson.com

If you would like to be removed from the project mailing list, please let us know by responding to this email.

Summary of Comment Sheets Received

Reference Number	Reason for Interest in Study	Description of Other	What is your primary mode of travelling along Mill Street?		Approximately how often do you walk along Mill Street		Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?		Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 4 the least) and provide comments						Please provide any additional comments on your preferred option for pedestrian accommodation.	Please include other questions you have below
						If you answered "Rarely or Never", please identify why		If you answered "No", please provide your reason(s)	Existing with mountable curb	How do you feel this option accommodates pedestrians?	Sidewalk	How do you feel this option accommodates pedestrians?	Asphalt Boulevard	How do you feel this option accommodates pedestrians?		
1	A Mill Street Resident		Multiple Selections	Selected personal vehicle and walking	Daily		No	Have been walking since 1965	1		No rank (negative)		2			
2	A Mill Street Resident		Multiple Selections	Selected personal vehicle, bicycle and walking	Daily		No	I feel comfortable walking on the current road. Have never felt unsafe.	1	Accommodating pedestrians is not in my opinion necessary. The road should be created such that it maintains its rural character.	3		2		Curbs + asphalt + sidewalks create an urban appearance which I think detracts from the rural character of the village. Cheltenham has the most heritage structures in Caledon + this character should be preserved.	Why is a retaining wall needed on the ditch by 1499 Mill Street?
3	A Mill Street Resident		Multiple Selections	Selected personal vehicle and walking	Daily	"Several days / wk"	No	the road is better for walking dogs - very little traffic - can currently walk most of the way down the street + back without seeing any vehicles	2	If we must pick an option this is the least intolerable	No rank (negative)	Not a consideration	3		Option 1 for me is just fix the road + leave everything else as is. Most of us walk on the road, even in areas where there are sidewalks. Sidewalks are too hard on body when walking distances. Pls, pls, pls, do <u>not</u> widen the road	Why the push for sidewalks - if because there is money in the budget, why not upgrade the horrible sidewalks that were put on Creditview. Pls avoid making this road any kind of major thoroughway.
4	A Mill Street Resident		Multiple Selections	Selected personal vehicle, bicycle and walking	Daily		No	Having been a resident for ten years and walking daily I have never experienced any safety issues with vehicle traffic while walking.	1	This gives pedestrians an option to walk on the boulevard or the road whichever they feel is safest. It also gives the option for cyclist or pedestrians to move off the road if need be without an obstacle to mount or step over.	3	Poorly. During winter months snow from the road will be pushed up on to sidewalk. Then the sidewalk plow will in turn plow the side walk and return the snow back on to the road. This is happening on the new sidewalk that was installed on Creditview.	2	Poorly. If you think that this type of curb is going to stop a vehicle from leaving the paved surface and crossing the sidewalk there is no sense discussing this any further. This also is applicable to the sidewalk and curb option	The list of disadvantages is very inconsistent between options they appear to be just points used to lobby rather than truly looking at the disadvantages equally between options. Lighting - When looking at streetlighting enhancements will it be a separate street light system with proper illumination but not light pollution.	Because we own property on both sides of Mill Street there is an existing 2" duct structure under Mill Street from property on north side to property on south side. The integrity of these two ducts need to remain in tact and usable once the construction has been completed.
5	A Mill Street Resident		Personal Vehicle (Auto)		Daily	Daily but only very short	No	Know how to walk at side of road. This is not the city.	3	What pedestrians	3	Not needed. Not the City. This Town just got sidewalk that does not work. Built next to road when road plow comes sidewalk is gone when sidewalk plowed one half lane on road closes.	3	Not needed	Not wanted not needed. Curbs are not the answer and catch basins equal salt in river	How much money spent so far? Who comes up with these ideas? Why spend money on things residents don't want!!
6	A Mill Street Resident		Multiple Selections	Selected personal vehicle and walking	Daily	Many times a day	No	It is nice as is, quiet safe	3	No curb is better	No rank (negative)	ridiculous	No rank (positive)	Pedestrians are already happy. Added an option "as is" ranked as 1	I'd prefer repaving. - Trail very close to for bikes alternative - Please redo traffic study as 1st one was biased due to construction - We are very fearful that the quality of any changes to Mill Street will be similar to Creditview - sidewalk ends before community centre, looks poorly and is an eye sore.	Have there been pedestrian injuries? - Have there been car accidents? - Will the same people be doing the sidewalk install? - Upgrade sidewalks on Creditview since they are unfriendly and stop before Community Centre.
7	A Mill Street Resident		Personal Vehicle (Auto)		Daily		No	I walk daily from my lane to the trail entrance (approx. 200 feet). I use the Caledon Trailway to walk my dog. The trail is better for dogs to walk than a sidewalk.	No rank (negative)	I do not want an urban road for this neighbourhood. This is a rural setting and therefore the road should continue as a rural road. I do not want mountable curbs because these are used in urban areas, not rural areas. We live in the Greenbelt, adding more concrete to the green spaces on Mill St. seems contradictory to the purpose of a Greenbelt. This option would cause an exorbitant use of funding and would lead to property disruption for many Mill St residents, such as: mature tree removal, unsightly guardrails & excessive grading. I do not want an urban. Mountable Curb because it will increase the volume & speed of vehicular traffic on Mill St, thereby increasing noise pollution and reducing road safety and quality of rural living.	No rank (negative)	I do not want an urban road for this neighbourhood. This is a rural setting and therefore the road should continue as a rural road. I do not want a sidewalk on Mill St. This urban option would lead to an increase in other urban road features such as street lights, increased road signage. Increase in street lights-an increase in light pollution. We need to protect the night scape, for the sake of our residents and for the birds/animals that dwell here. Additional road signage is unsightly. There is an excessive use of road signs and these detract from the natural beauty of the properties on this street and the surrounding area. A sidewalk option would cause an exorbitant use of funding and would lead to property disruption for many Mill St residents, such as: mature tree removal, unsightly guardrails & excessive grading. I do not want a sidewalk because these are used in urban areas, not rural areas. We live in the Greenbelt, adding more concrete to the green spaces on Mill St. seems contradictory to the purpose of a Greenbelt. I do not want the extra pedestrian traffic from non-residents spilling from the Caledon trail to the roadway. More pedestrians = more noise, more garbage and pollution and more annoyance for the residents who live here.	No rank (negative)	I do not want an asphalt boulevard, because these are used in urban areas, not rural areas. We live in the Greenbelt, adding more asphalt to the green spaces on Mill St. seems contradictory to the purpose of a Greenbelt. I do not want an asphalt boulevard on Mill St. This urban option would lead to an increase in other urban road features such as street lights, increased road signage. Increase in street lights-an increase in light pollution. Asphalt boulevards are unsightly. What happens when the asphalt breaks down from the weather conditions, or is broken up due to snow removal? These asphalt boulevards would eventually become hazardous for pedestrians, much like the road is now in its current state.	I prefer to keep this a rural road. No urban features like mountable curbs, sidewalks or asphalt boulevards. I say no to street lights, additional road signage, and other urban road features. I say no to cutting down mature trees to make way for concrete walkways. I say no to disrupting this charming neighbourhood, a place admired by many for its rural features and charm.	
8	A Mill Street Resident		Multiple Selections	Selected personal vehicle and walking	Daily		No	We, however would feel more safe with traffic calming features for cars.	No rank (negative)	Does not offer safety for pedestrians and cyclists.	No rank (negative)	It urbanizes our country setting which is not what we want.	No rank (positive)	We like this option as it accommodates pedestrians and cyclists and supports their safety. It also is more in keeping with our rural setting. Traffic calming features would help with slowing traffic to ensure public safety.	Our primary concern is with the improvement of the road, traffic volume and speed will increase significantly. Safety measures need to be put in place for pedestrians and cyclists, especially traffic calming features. E.g. like they have in Terra Cotta and Belfountain Villages.	
9	Other	CARA - Treasurer	Multiple Selections	Bicycle, Walking	Daily				3	I feel like this is the same as what we currently have on Mill Street. Can we get rid of the guard rails? Would these be all the way along Mill Street?	1	I feel this is a safer option than what we currently have on Mill Street, looks nicer than the asphalt.	2	I feel this is a safer option for pedestrians.		Where would the guard rails be located? Really wouldn't look nice to have them all the way along.
10	A Mill Street Resident		Multiple Selections	Selected personal vehicle and walking	Daily		No	There is not enough traffic to warrant a sidewalk.	1	Maybe it wont but may stop the snow plow from destroying the lawn. Not really interested in any other options as they just create more issues.	3	Multi-use path should follow St. David Road south side to St David Street south side connecting at Front Street at the flag.	2		By the way the sidewalk snow plow is not a good option. On Creditview there is not enough room for the snow. It went on to the road last year and blocked one half of the live traffic lane.	
11	A Mill Street Resident		Walking		Daily		Yes		3	Need somewhere to walk this street	2	It could be good, but don't want to take up too much space	3	The road is in terrible condition and need asphalt.		
12	A Mill Street Resident		Personal Vehicle (Auto)		Weekly			I am indifferent usually on the shoulder off to the side.	3		1	Safest option for pedestrians	1	Enough to be off to the side of road and not walk in the middle of the road - cars can avoid.	I appreciate and value the safety respect. I also value the tranquil and rural aspect of Mill Street. I moved from the city with the deliberate intention to be on a quiet country street. Trees are sacred. I want to preserve the heritage of Cheltenham as reasonably as possible.	
13	A Mill Street Resident		Multiple Selections	Selected personal vehicle, bicycle and walking	Daily		No	Walk, ride daily as is.	No rank (negative)	Not in favor of this option.	No rank (negative)	Not in favor of this option.	No rank (negative)	Not in favor of this option.	I am not in favor of any options listed. All I want is for the road to be repaired to a safe condition, install proper guide railing near trail if road repair can not resolve the current issue. Have lived in Cheltenham for 36 years and purchased this property because my wife and I wanted a nice country home with a quiet rural setting. All of a sudden the Town of Caledon wants to urbanize our community. I am in total disagreement of any curbs, sidewalks, walkways or street lighting. I don't see any of this on other streets, (Kennedy Rd., Station Rd., Grange sideroad etc.). The sidewalk on Creditview Rd. looks like hell and really took away from the original rural setting. 95% of people using the road are local residents. Just Repair the Road !!!	I continue to hear about safety but it was not until this year that signing for pedestrians at the trail were installed. There has never been any Curve Warning / Speed tabs for the curve in the 26 years I have lived here. The guide rail at the trail is useless. The signing for Pedestrians crossing the trail at Old Base Line Rd. is a disaster waiting to happen, very rarely cars stop or make any attempt to stop. Very poor design.

Reference Number	Reason for Interest in Study	Description of Other	What is your primary mode of travelling along Mill Street?		Approximately how often do you walk along Mill Street		Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?		Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 4 the least) and provide comments						Please provide any additional comments on your preferred option for pedestrian accommodation.	Please include other questions you have below
						If you answered "Rarely or Never", please identify why		If you answered "No", please provide your reason(s)	Existing with mountable curb	How do you feel this option accommodates pedestrians?	Sidewalk	How do you feel this option accommodates pedestrians?	Asphalt Boulevard	How do you feel this option accommodates pedestrians?		
14	Other	Creditview Road resident	Walking		Weekly		No	I don't feel walking on Mill Street is an unsafe activity.	1	Any option that leaves the street in the most "natural" state would be, in my opinion, the most attractive solution to repairing Mill Street. Mountable curbs help prevent breakage at the edges of the asphalt which is the only reason I see them as acceptable inside this option.	3	This option would, without doubt, ruin Mill Street. The removal of the majestic 100 year old trees to Widen Mill Street flies in the face of Caledon's claim of being an eco-friendly place.	3	All I can think about this is why oh why was this not an option for Creditview Road.	I decided to address the guardrails, NOT REQUIRED!!! There hasn't been a need for guardrails on Mill Street in 200 years. There is no need for them now. Truly there should be a legislative exemption for the small villages in Caledon in order to allow them to be upgraded without the threat of them being bastardized beyond recognition.	I am a member of CABA. People may say "not my street, not my problem" but I disagree. Chipping away at the uniqueness of Cheltenham has already begun without input from the residents there will be more unpleasant changes. Like the abomination that now spans the west side of Creditview Road. I tried, to no avail, to convince the powers that be to install a asphalt curb instead of sidewalks (UBC has this all around its ring road). Thank you for listening hopefully this time...
15	Other	Creditview Road resident	Multiple Selections	Selected personal vehicle and walking	Monthly	I love walking around my neighbourhood but find it challenging and unsafe as there is no sidewalk and road is deteriorated.	Yes	Sidewalk would be beneficial and safer for everyone	3		1	I think it is time to make neighbourhood safe for pedestrians. Creditview Road just had sidewalk built. Great idea.	2	Cyclists tend to be out of (illegible) who come to the area to ride their bikes. I believe sidewalks for pedestrians who live in the area, is best solution.	Mill Street is a winding and hilly road and is unsafe for pedestrians, wheelchairs, walkers and strollers. I understand that current residents of Mill Street may be opposed to the encroachment on their property, however the safety of the community is paramount.	I believe sidewalks are the solution for a safer community. Now that Creditview has a sidewalk, my family walks on it more often as it is finally safe.
16	A Mill Street Resident		Personal Vehicle (Auto)		Weekly		No		No rank (positive)	I feel this is the best. Just a rural road repair upgrade.	No rank (negative)	Pedestrians walk on the road daily. Never any problems. Don't need to be like the suburbs. We are a small country community and wish to stay that way. No no no	No rank (negative)	no no no	Pedestrians can walk on the road. Never been a problem. Don't want the road fixed to make cars drive faster. People on Mill walk their dogs and kids and we all know speed kills.	Do the residents on Mill get to vote on final decision.
17	A Mill Street Resident	Address isn't Mill Street	Multiple Selections	Selected personal vehicle and walking	Daily		No	The traffic is generally slow, respectful and infrequent. There is already plenty of road width and adequate sightlines (other than the crest of the hill at the S-bend. The crest level could be adjusted without installing a specific pedestrian route) to be safe for pedestrians. A dedicated pedestrian route gives motorists a sense that they are entitled to drive faster...and they do, especially more so since the installment of sidewalk on Creditview Rd.	3	This does not accommodate pedestrians. A mountable curb restricts pedestrian entry and egress. Mountable curbs funnel water and given the current topography, the discharge will likely add to flooding which will restrict access as well as damage to property. There are no proposals to deal with flooding in the current study.	3	A sidewalk categorizes pedestrians. A sidewalk instructs pedestrians that they must only walk where designated and it projects that viewpoint on motorists it speeds up traffic which is detrimental to pedestrians and is antithetical to accommodating pedestrians. Sidewalks undermine the quiet rural nature of the community which makes it it less appealing to pedestrians, thereby failing to accommodate pedestrian. To summarize, a sidewalk requires road widening, which introduces a risk to the road that does not currently exist.	2	An asphalt blvd widens the road for motorists, which increase speeds. Increased speeds do not accommodate pedestrians This removes trees and significantly alters the streetscape. Urbanizing the streetscape does not accommodate pedestrians. The ranking does not reflect my view of these proposals in the community. I originally ranked them 3-3-2	The basic premise of this form and the presentation appears to be accommodating pedestrians. No one has asked, or stated, what accommodation means for this study. What it appears to mean from the proposals, is to get pedestrians off the road in order to accommodate motorists. This is NOT what Mill St. residents want. Given the quiet rural character of the road, I suggest that the Town and its consultants are asking the question incorrectly or at least placing the emphasis in the wrong place. Perhaps it is the behaviour of the motorists that the design needs to alter in favour of the pedestrians. Perhaps such an approach may also help in maintaining the character of the street and the sense of community. The work done over the years on Creditview is evidence of how to damage a rural village. Urbanizing everything is not the answer. Maintain a rural street environment.	The EA has not communicated a need for reconstruction/realignment based on quantifiable information and the analysis is based on flawed data. The comments form is poorly drafted with leading questions. The comment form is very poorly formatted, making it less user friendly and is not a saveable pdf. The less user-friendly, the less likely people are to complete it. It restricts community engagement. Ranking poor proposals against each other creates a false ranking of the proposal, generally. Again, this can create a misrepresentation that may be used to justify a particular proposal. A ranking of "1" does not mean it is something the community likes or WANTS.
17	A Mill Street Resident	Address isn't Mill Street	Multiple Selections	Selected personal vehicle and walking	Daily		No	The traffic is generally slow, respectful and infrequent. There is already plenty of road width and adequate sightlines (other than the crest of the hill at the S-bend. The crest level could be adjusted without installing a specific pedestrian route) to be safe for pedestrians. A dedicated pedestrian route gives motorists a sense that they are entitled to drive faster...and they do, especially more so since the installment of sidewalk on Creditview Rd.	3	This does not accommodate pedestrians. A mountable curb restricts pedestrian entry and egress. Mountable curbs funnel water and given the current topography, the discharge will likely add to flooding which will restrict access as well as damage to property. There are no proposals to deal with flooding in the current study.	3	A sidewalk categorizes pedestrians. A sidewalk instructs pedestrians that they must only walk where designated and it projects that viewpoint on motorists it speeds up traffic which is detrimental to pedestrians and is antithetical to accommodating pedestrians. Sidewalks undermine the quiet rural nature of the community which makes it it less appealing to pedestrians, thereby failing to accommodate pedestrian. To summarize, a sidewalk requires road widening, which introduces a risk to the road that does not currently exist.	2	An asphalt blvd widens the road for motorists, which increase speeds. Increased speeds do not accommodate pedestrians This removes trees and significantly alters the streetscape. Urbanizing the streetscape does not accommodate pedestrians. The ranking does not reflect my view of these proposals in the community. I originally ranked them 3-3-2	The basic premise of this form and the presentation appears to be accommodating pedestrians. No one has asked, or stated, what accommodation means for this study. What it appears to mean from the proposals, is to get pedestrians off the road in order to accommodate motorists. This is NOT what Mill St. residents want. Given the quiet rural character of the road, I suggest that the Town and its consultants are asking the question incorrectly or at least placing the emphasis in the wrong place. Perhaps it is the behaviour of the motorists that the design needs to alter in favour of the pedestrians. Perhaps such an approach may also help in maintaining the character of the street and the sense of community. The work done over the years on Creditview is evidence of how to damage a rural village. Urbanizing everything is not the answer. Maintain a rural street environment.	The EA has not communicated a need for reconstruction/realignment based on quantifiable information and the analysis is based on flawed data. The comments form is poorly drafted with leading questions. The comment form is very poorly formatted, making it less user friendly and is not a saveable pdf. The less user-friendly, the less likely people are to complete it. It restricts community engagement. Ranking poor proposals against each other creates a false ranking of the proposal, generally. Again, this can create a misrepresentation that may be used to justify a particular proposal. A ranking of "1" does not mean it is something the community likes or WANTS.
19	Other	Cheltenham Resident	Multiple Selections	Walking and Golf Cart	Rarely or Never	Because there isn't anywhere really to walk. We walk to the bridge and look at the river. My kids go to the park instead.	Yes		2	I think if we aren't going to put in a sidewalk then just do the simple one. I also don't think the road needs all the city things like road paintings and it definitely doesn't need guard rails and things like that. It will look terrible for Cheltenham.	1	I personally believe if we don't put the sidewalk in now it will never get done. We waited like 20 years to get a sidewalk on Creditview. I think it will make it safer for people to walk. I do understand the people that live on Mill streets concerns though don't live on Mill street but we might walk to more if there was somewhere to walk. But no need for all the extra road stuff like guard rails and city looking things.	3	I say either go for it with the sidewalk or just don't bother.	I am also noticing this is all just focussed on pedestrian accommodation. Why is that? Shouldn't it also be about road safety? About keeping the Cheltenham Charm? About saving as many trees and much more. Why is this comment sheet only addressing somewhere for pedestrians?	Will we be notified once a decision is made? With our Creditview sidewalk we weren't told of the final decision (that I am aware of) When will we find out this answer? I hope we can come to some agreement as a community because we really just need something done with the road. Thanks for all the extra time working with the community and providing extra meetings etc.
20	A Mill Street Resident		Multiple Selections	Selected personal vehicle, bicycle and walking	Daily		No	I walk the road at least everyday, sometimes twice...doubt any "walkway" would up that!	1	The question is biased. There are many accommodations requirements. If the road had traffic calming measures, lower than 40km/h posted speeds, narrowed lanes that don't invite cars to speed, then pedestrians would be very much accommodated to walk on the road edge and sod/ grass edge.	3	A concrete sidewalk is a no brainer for accommodating pedestrians - of course it's the option if this is the only consideration. But its not just pedestrians to accommodate.	2	Again, a paved shoulder is the same as a concrete sidewalk - so it's a no brainer accommodation for pedestrians, of course! So again the question is biased.	Again this is a biased "comment" request. To suggest that pedestrian accommodation is the only issue to comment on is again slanted, biased and rather insulting and diminishing to all the concerns and needs to accommodate. I believe pedestrians need better / safer accommodations, but is the cars that need less accommodations: narrow lanes, tighter turns, slower speeds, limited sight lines, traffic calming, etc etc - slow the cars down, and the pedestrian issue takes care of itself.	A.) List specific traffic calming measures that will be implemented. B.) Explain in detail why the posted speed limit can't be lower than 40km/h. C.) What is required to allow the Town to drop this minimum 2 lane width mantra? D.) Can someone give detailed cross section dimension of the "S" bend retaining wall and guard rail?

Copy of Comments Received

Winnie Wong

From: Shun Cheung <Shun.Cheung@caledon.ca>
Sent: July 5, 2022 8:09 AM
To: [REDACTED]
Cc: Winnie Wong; Connor MacIsaac
Subject: Re: Public Information Centre #2 Comment Form

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Thank you [REDACTED] for your comments.

Get [Outlook for iOS](#)

From: [REDACTED]
Sent: Tuesday, July 5, 2022 7:35:37 AM
To: Shun Cheung <Shun.Cheung@caledon.ca>; Christina Early <Christina.Early@caledon.ca>
Subject: Public Information Centre #2 Comment Form

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Please consider this email as a formal response to the comment form. I found the questionnaire misleading and biased towards option 3 and therefore I am not responding to it .

[REDACTED]
[REDACTED]
[REDACTED]

"This message (and any associated files) is intended only for the use of the individual or entity to which it is addressed. The content of the message is the property of the Corporation of the Town of Caledon. The message may contain information that is privileged, confidential, subject to copyright and exempt from disclosure under applicable law. If the reader of this message is not the intended recipient, you are notified that any dissemination, distribution, copying, or modification of this message is strictly prohibited. If you have received this message in error, please notify the sender immediately, advising of the error and delete this message without making a copy. (Information related to this email is automatically monitored and recorded and the content may be required to be disclosed by the Town to a third party in certain circumstances). Thank you."

Winnie Wong

From: [REDACTED]
Sent: June 17, 2022 11:25 AM
To: Shun Cheung
Cc: Winnie Wong; Connor MacIsaac
Subject: RE: Mill Street, Cheltenham

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Thank you for forwarding my email . I appreciate it.

Everyone should have equal and safe access to Mill Street which is a public (not private) road.

From: Shun Cheung <Shun.Cheung@caledon.ca>
Sent: Friday, June 17, 2022 11:19 AM
To: [REDACTED]
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>; Connor MacIsaac <cmaclsaac@rvanderson.com>
Subject: RE: Mill Street, Cheltenham

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Thanks [REDACTED] The Town is appreciated for your input.

Shun Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure
Engineering Services
Office: 905.584.2272 x.4040
Cell: 416.436.0910
Email: shun.cheung@caledon.ca

Town of Caledon | www.caledon.ca | www.visitcaledon.ca | Follow us @YourCaledon

From: [REDACTED]
Sent: Friday, June 17, 2022 11:17 AM
To: Shun Cheung <Shun.Cheung@caledon.ca>
Subject: re: Mill Street, Cheltenham

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Dear Shun

I attended the meeting on Wednesday June 15, 2022 with regards to the upgrade to Mill Street. I really am confused as why the residents of Mill Street are trying to prevent any changes to the street. I believe that installing a sidewalk is a prudent idea. Having a sidewalk would make it safer and more accessible for wheel chairs, strollers, walkers and to accommodate those with mobility issues. The addition of the sidewalk on Creditview Rd in Cheltenham has made the street safer for all pedestrians. The addition of sidewalk on Mill Street will do the same. Walking on the street should not even be the only option.

During the meeting, many residence expressed their distain for the idea of having a sidewalk or asphalt boulevard. These residence do not own the street and have sense of entitlement as they do not want non residence to use the street. They were complaining that vehicular traffic will increase, cycles will use the road and people will walk in the neighbourhood if a sidewalk would be added. The more I listened the more upset I was becoming at their belief that the street belong to them and no one should have access to it. Mill Street is a public street and everyone should have safe access to the street. It is not just for the residence of Mill street.

Thanks for listening to my concerns.

[REDACTED]

[REDACTED]

[REDACTED]

[REDACTED]

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MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

PUBLIC INFORMATION CENTRE (PIC) #2 COMMENT FORM

The Town is presenting three options for the north side of Mill Street. Option 1 is existing with mountable curb, Option 2 is dedicated sidewalk with barrier curb, and Option 3 is asphalt boulevard with mountable curb. All three options allow for grass boulevard and mountable curb on the south side of Mill Street. Please provide your input on the options presented by completing this comment form.

NAME (REQUIRED): _____

MAILING ADDRESS (REQUIRED): _____

EMAIL (OPTIONAL): _____

☐ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

- ☒ A Mill Street Resident
☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

- ☒ Personal Vehicle (Auto)
☐ Bicycle
☒ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

HAVE BEEN WALKING SINCE 1965

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 1
How do you feel this option accommodates pedestrians?	

Sidewalk	Rank:
How do you feel this option accommodates pedestrians?	

Asphalt Boulevard	Rank: 2
How do you feel this option accommodates pedestrians?	

Please provide any additional comments on your preferred option for pedestrian accommodation.

6. Please include other questions you have below:

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

With the exception of personal information, all comments will become part of the public record of study. The study is being conducted according to the requirements of the Municipal Class Environmental Assessment, which is a planning process approved under Ontario's Environmental Assessment Act.

THANK YOU FOR YOUR TIME AND EFFORT!

**MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD**

PUBLIC INFORMATION CENTRE (PIC) #2 COMMENT FORM

The Town is presenting three options for the north side of Mill Street. Option 1 is existing with mountable curb, Option 2 is dedicated sidewalk with barrier curb, and Option 3 is asphalt boulevard with mountable curb. All three options allow for grass boulevard and mountable curb on the south side of Mill Street. Please provide your input on the options presented by completing this comment form.

NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

☒ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

☒ A Mill Street Resident

☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

☒ Personal Vehicle (Auto)

☒ Bicycle

☒ Walking

☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

☒ Daily

☐ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

1. You are:

- ☒ A Mill Street Resident
☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

- ☒ Personal Vehicle (Auto)
☒ Bicycle
☒ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

I feel comfortable walking on the
current road. Have never felt
unsafe.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 1
How do you feel this option accommodates pedestrians?	
Accommodating pedestrians is not in my opinion necessary. The road should be created such that it maintains its rural character.	

Sidewalk	Rank: 3
How do you feel this option accommodates pedestrians?	

Asphalt Boulevard	Rank: 2
How do you feel this option accommodates pedestrians?	

road
That it maintains its rural
character.

Sidewalk

Rank: 3

How do you feel this option accommodates pedestrians?

Asphalt Boulevard

Rank: 2

How do you feel this option accommodates pedestrians?

Please provide any additional comments on your preferred option for pedestrian accommodation.

Curbs + asphalt + sidewalks
create an urban appearance
which I think detracts from
the village rural character of
the village. Cheltenham has the
most heritage structures in
Caledon + this character should
be preserved.

6. Please include other questions you have below:

Why is a retaining wall needed on
the ditch by 1499 Mill Street?

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

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MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

PUBLIC INFORMATION CENTRE (PIC) #2 COMMENT FORM

The Town is presenting three options for the north side of Mill Street. Option 1 is existing with mountable curb, Option 2 is dedicated sidewalk with barrier curb, and Option 3 is asphalt boulevard with mountable curb. All three options allow for grass boulevard and mountable curb on the south side of Mill Street. Please provide your input on the options presented by completing this comment form.

NAME (REQUIRED): _____

MAILING ADDRESS (REQUIRED): _____

EMAIL (OPTIONAL): _____

☐ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

- ☒ A Mill Street Resident
☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

- ☒ Personal Vehicle (Auto)
☐ Bicycle
☒ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☒ Weekly *several days / wk.*
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

the road is better for walking dogs - very little traffic - can currently walk most of the way down the street & back without seeing any vehicles

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 2
How do you feel this option accommodates pedestrians?	

Sidewalk	Rank: X
How do you feel this option accommodates pedestrians?	

Asphalt Boulevard	Rank: 3
How do you feel this option accommodates pedestrians?	

option 1 for me is just fix the road & leave everything else as is

Please provide any additional comments on your preferred option for pedestrian accommodation.

most of us walk on the road, even in
areas where there are sidewalks.
Sidewalks are too hard on body when
walking distance.

Pls, pls, pls, do not widen the road

6. Please include other questions you have below:

- why the push for sidewalks - if because
there is money in the budget, why not upgrade
the horrible sidewalks that were put on
Creditview.

Pls avoid making this road any kind
of major throughway.

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THANK YOU FOR YOUR TIME AND EFFORT!



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MAILING ADDRESS (REQUIRED): _____

EMAIL (OPTIONAL): _____

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1. You are:

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2. What is your primary mode of travelling along Mill Street?

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☒ Bicycle
☒ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

[Faint horizontal lines across the page]

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

HAVING BEEN A RESIDENT FOR TEN
AND WALKING DAILY I HAVE NEVER
EXPERIENCED ANY SAFETY ISSUES
WITH VEHICLE TRAFFIC WHILE
WALKING. ~~BT~~

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 1
How do you feel this option accommodates pedestrians?	
THIS GIVES PEDESTRIANS AN OPTION TO WALK ON THE BOULEVARD OR THE ROAD WHICHEVER THEY FEEL IS SAFEST. IT ALSO GIVES THE OPTION FOR CYCLIST OR PEDESTRIANS TO MOVE OFF THE ROAD IF NEED BE WITHOUT AN OBSTACLE TO MOUNT OR STEP OVER.	

Sidewalk	Rank: 3
How do you feel this option accommodates pedestrians?	
POORLY. DURING WINTER MONTHS SNOW FROM THE ROAD WILL BE PUSHED UP ON TO SIDEWALK. THEN THE SIDEWALK PLOW WILL IN TURN PLOW THE SIDE WALK AND RETURN THE SNOW BACK ON TO THE ROAD.	
* THIS IS NOW HAPPENING ON THE NEW SIDEWALK THAT WAS INSTALLED ON CREDIT VIEW.	

Asphalt Boulevard	Rank: 2
How do you feel this option accommodates pedestrians?	
POOR 4. IF YOU THINK THAT THIS TYPE OF CURB IS NOT GOING TO STOP A VEHICLE FROM LEAVING THE PAVED SURFACE AND ^{CROSSING} ON TO THE SIDEWALK THERE IS NO SENSE DISCUSSING THIS ANY FURTHER.	
* THIS ALSO IS APPLICABLE TO	

THE SIDEWALK AND CURB OPTION.

Please provide any additional comments on your preferred option for pedestrian accommodation.

THE LIST OF DISADVANTAGES IS VERY
INCONSISTANT BETWEEN OPTIONS
THEY SEEM TO BE JUST POINTS
USED TO LOBBY RATHER THAN
TRULY LOOKING AT THE DISADVANTAGES
EQUALLY BETWEEN OPTIONS.

LIGHTING - WHEN LOOKING AT STREET LIGHT
ENHANCEMENTS WILL IT BE A SEPARATE
STREET LIGHT SYSTEM WITH PROPER
ILLUMINATION BUT NOT LIGHT POLLUTION

6. Please include other questions you have below:

BECAUSE WE OWN PROPERTY ON BOTH
SIDES OF MILL ST. THERE IS AN EXISTING
2 DUCT STRUCTURE UNDER MILL ST,
FROM PROPERTY ON NORTH SIDE TO
PROPERTY ON SOUTH SIDE. THE INTEGRITY
OF THESE TWO DUCTS NEED TO
REMAIN IN TACT AND USABLE
ONCE THE CONSTRUCTION HAS BEEN
COMPLETED.

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By Mail: Shun Cheung
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MISSISSAUGA ROAD TO CREDITVIEW ROAD

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NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

☒ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

☒ A Mill Street Resident

☐ Other (Please Identify -

2. What is your primary mode of travelling along Mill Street?

☒ Personal Vehicle (Auto)

☐ Bicycle

☐ Walking

☐ Other (Please Identify -

3. Approximately how often do you walk along Mill Street?

☒ Daily

☐ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

Daily but only very short

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

If you answered "No", please provide your reason(s).

Know how to walk at side of Road.
This is NOT the City

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 3
How do you feel this option accommodates pedestrians?	
?	
What Pedestrians	

Sidewalk	Rank: 3
How do you feel this option accommodates pedestrians?	
Not Needed	
Not The City	
This Town Just got Sidewalk That	
Does Not work Built Next To Rd when	
Road Plow Comes Sidewalk is gone when	
Sidewalk plowed one half Lane on Road closes	

Asphalt Boulevard	Rank: 3
How do you feel this option accommodates pedestrians?	
Not Needed	

Please provide any additional comments on your preferred option for pedestrian accommodation.

Not Wanted Not Needed
Curbs Are Not The Answer.
" AND CATCH BASINS EQUAL SALT IN RIVER

6. Please include other questions you have below:

How Much Money Spent So Far?
Who Comes Up With These Ideas?
Why Spend money ON Things Residents
DON'T WANT!!

Why Not Leave Road width? Just Repair!

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2. What is your primary mode of travelling along Mill Street?

☒ Personal Vehicle (Auto)

☐ Bicycle

☒ Walking

☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

☒ Daily

Many times a day

☐ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

If you answered "No", please provide your reason(s).

- it is nice as is , quiet safe .

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: NA	(3)
How do you feel this option accommodates pedestrians?		
no curb is better		

Sidewalk	Rank: No
How do you feel this option accommodates pedestrians?	
rediculous	

Asphalt Boulevard	Rank: No
How do you feel this option accommodates pedestrians?	
pedestrians are already happy	
As is. please	Rank

Please provide any additional comments on your preferred option for pedestrian accommodation.

- I'd prefer repaving
- Trail very close to pikes alternative
- Please redo traffic study as 1st one was biased due to construction.
- we are very fearful that the quality of any changes to Mill St will be similar to Credit View - side walk ends before Community Centre, looks poorly and is an eye sore.

6. Please include other questions you have below:

- Have there been pedestrian injuries?
- Have there been car accidents?
- Will the same people be doing the side walk install?
- up grade side walks on Credit View since they are unfriendly and stop before Community Centre!

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NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

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1. You are:

- ☒ A Mill Street Resident
☐ Other (Please Identify -)

2. What is your primary mode of travelling along Mill Street?

- ☒ Personal Vehicle (Auto)
☐ Bicycle
☐ Walking
☐ Other (Please Identify -)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

I walk daily from my lane to the trail entrance (approx 200 feet). I use the Caledon Trailway to walk my dog.

The trail is better for dogs to walk than a sidewalk.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank:
How do you feel this option accommodates pedestrians?	
I do not want an urban road for this neighbourhood. This is a rural setting and therefore the road should continue as a rural road.	
I do not want mountable curbs because these are used in urban areas, not rural areas. We live in the Greenbelt, adding more concrete to the green spaces on Mill St. seems contradictory to the purpose of a Greenbelt.	
This option would cause an exorbitant use of funding and would lead to property disruption for many Mill St residents, such as; mature tree removal, unsightly guardrails & excessive grading.	
I do not want an urban Mountable Curb because it will increase the volume & speed of vehicular traffic on Mill St, thereby increasing noise pollution and reducing road safety and quality of rural living.	

Sidewalk	Rank:
How do you feel this option accommodates pedestrians?	
I do not want an urban road for this neighbourhood. This is a rural setting and therefore the road should continue as a rural road.	
I do not want a sidewalk on Mill St. This urban option would lead to an increase in other urban road features such as street lights, increased road signage. Increase in street lights=an increase in light pollution.	
We need to protect the night scape, for the sake of our residents and for the birds/animals that dwell here.	
Additional road signage is unsightly. There is an excessive use of road signs and these detract from the natural beauty of the properties on this street and the surrounding area.	
A sidewalk option would cause an exorbitant use of funding and would lead to property disruption for many Mill St residents, such as; mature tree removal, unsightly guardrails & excessive grading.	
I do not want a sidewalk because these are used in urban areas, not rural areas. We live in the Greenbelt, adding more concrete to the green spaces on Mill St. seems contradictory to the purpose of a Greenbelt.	
I do not want the extra pedestrian traffic from non-residents spilling from the Caledon trail to the roadway. More pedestrians = more noise, more garbage and pollution and more annoyance for the residents who live here.	

Asphalt Boulevard	Rank:
How do you feel this option accommodates pedestrians?	
I do not want a an asphalt boulevard, because these are used in urban areas, not rural areas. We live in the Greenbelt, adding more asphalt to the green spaces on Mill St. seems contradictory to the purpose of a Greenbelt.	
I do not want an asphalt boulevard on Mill St. This urban option would lead to an increase in other urban road features such as street lights, increased road signage. Increase in street lights=an increase in light pollution.	
Asphalt boulevards are unsightly. What happens when the asphalt breaks down from the weather conditions, or is broken up due to snow removal?	
These asphalt boulevards would eventually become hazardous for pedestrians, much like the road is now in its current state.	

Please provide any additional comments on your preferred option for pedestrian accommodation.

I prefer to keep this a rural road. No urban features like mountable curbs, sidewalks or asphalt boulevards.

I say no to street lights, additional road signage, and other urban road features. I say no to cutting down mature trees to make way for concrete walkways.

I say no to disrupting this charming neighbourhood, a place admired by many for its rural features and charm.

6. Please include other questions you have below:

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca
By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

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THANK YOU FOR YOUR TIME AND EFFORT!

MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

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NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

☒ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

☒ A Mill Street Resident

☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

☒ Personal Vehicle (Auto)

☐ Bicycle

☒ Walking

☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

☒ Daily

☐ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

If you answered "No", please provide your reason(s).

We, however would feel more
safe with traffic
calming features for cars.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank:
How do you feel this option accommodates pedestrians?	
Does not offer	
safety for pedestrians	
and cyclists.	

Sidewalk	Rank:
How do you feel this option accommodates pedestrians?	
It urbanizes our country	
setting which is not	
what we want.	

Asphalt Boulevard	Rank:
How do you feel this option accommodates pedestrians?	
We like this option as	
it accommodates pedestrians	
and cyclists and supports	
their safety.	
It also is more in	
keeping with our rural setting.	
Traffic calming features	
would help with slowing	
traffic to ensure public	
safety.	

*

Please provide any additional comments on your preferred option for pedestrian accommodation.

Our primary concern is with the improvement of the road, traffic ~~to~~ volume and speed will increase significantly. Safety measures need to be put in place for pedestrians and cyclists especially traffic calming features e.g. like they have in Terra Cotta and Belfountain Villages.

6. Please include other questions you have below:

Please provide your comments by July 6th, 2022 to:

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NAME (REQUIRED): _____

MAILING ADDRESS (REQUIRED): _____

EMAIL (OPTIONAL): _____

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1. You are:

☐ A Mill Street Resident

☒ Other (Please Identify -

CARA - Treasurer)

2. What is your primary mode of travelling along Mill Street?

☐ Personal Vehicle (Auto)

☒ Bicycle

☒ Walking

☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

☒ Daily

☐ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☐ No

If you answered "No", please provide your reason(s).

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 3
How do you feel this option accommodates pedestrians?	
I feel like this is the same as what we currently have on mill st. Can we get rid of the Guard rails? would these be all the way along mill st?	

Sidewalk	Rank: 1
How do you feel this option accommodates pedestrians?	
I feel this is a safer option than what we currently have on mill st. looks nicer than the asphalt.	

Asphalt Boulevard	Rank: 2
How do you feel this option accommodates pedestrians?	
I feel this is a safer option for pedestrians.	

Please provide any additional comments on your preferred option for pedestrian accommodation.

6. Please include other questions you have below:

Where would the guard rails
be located? Really would
look nice to have them all
the way along.

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☐ Other (Please Identify -)

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☐ Bicycle
☒ Walking
☐ Other (Please Identify -)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

There is not enough traffic to warrant a sidewalk.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 1
How do you feel this option accommodates pedestrians?	
Maybe it wont but may stop the snow plow from destroying the lawn.	
Not really interested in any other options as they just create more issues.	

Sidewalk	Rank:3
How do you feel this option accommodates pedestrians?	

Asphalt Boulevard	Rank:2
How do you feel this option accommodates pedestrians?	



By the way.... the sidewalk snow plow is not a good option. On Creditview there is not enough room for the snow. It went on to the road last year and blocked one half of the live traffic lane.

[illegible]

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EMAIL (OPTIONAL): _____

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☐ Bicycle
☒ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☒ Yes
☐ No

If you answered "No", please provide your reason(s).

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 3
How do you feel this option accommodates pedestrians?	
need somewhere to walk this street	

Sidewalk	Rank: 2
How do you feel this option accommodates pedestrians?	
It could be good, but don't want to take up much space	

Asphalt Boulevard	Rank: 3
How do you feel this option accommodates pedestrians?	
The road is in terrible condition and need asphalt	

Please provide any additional comments on your preferred option for pedestrian accommodation.

6. Please include other questions you have below:

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

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THANK YOU FOR YOUR TIME AND EFFORT!

MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

PUBLIC INFORMATION CENTRE (PIC) #2 COMMENT FORM

The Town is presenting three options for the north side of Mill Street. Option 1 is existing with mountable curb, Option 2 is dedicated sidewalk with barrier curb, and Option 3 is asphalt boulevard with mountable curb. All three options allow for grass boulevard and mountable curb on the south side of Mill Street. Please provide your input on the options presented by completing this comment form.

NAME (REQUIRED): _____

MAILING ADDRESS (REQUIRED): _____

EMAIL (OPTIONAL): _____

☒ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

- ☒ A Mill Street Resident
☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

- ☒ Personal Vehicle (Auto)
☐ Bicycle
☐ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☐ Daily
☒ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☐ No

I am indifferent usually on the shoulder off to the side.

If you answered "No", please provide your reason(s).

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: <u>3</u>
How do you feel this option accommodates pedestrians?	

Sidewalk	Rank: <u>1</u>
How do you feel this option accommodates pedestrians?	
safest option for pedestrians	

Asphalt Boulevard	Rank: <u>1</u>
How do you feel this option accommodates pedestrians?	
Enough to be off to the side ^{of road} , and not walk in the middle of the road. - cars can avoid.	

Please provide any additional comments on your preferred option for pedestrian accommodation.

I appreciate and value the safety aspect.
I also value the tranquil and rural
aspect of Mill Street. I moved from
the city w the deliberate intention to
be on a quiet ~~ex~~ - country street. ->
Trees are saved. - I want to preserve
the heritage of Cheltenham as reasonably
possible.

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3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

Walk, ride daily as is.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank:
How do you feel this option accommodates pedestrians?	
Not in favor of this option.	

Sidewalk	Rank:
How do you feel this option accommodates pedestrians?	
Not in favor of this option.	

Asphalt Boulevard	Rank:
How do you feel this option accommodates pedestrians?	
Not in favor of this option.	

Please provide any additional comments on your preferred option for pedestrian accommodation. I am not in favor of any options listed. All i want is for the road to be repaired to a safe condition, install proper guide railing near trail if road repair can not resolve the current issue.

Have lived in Cheltenham for 36 years and purchased this property because my wife and i wanted a nice country home with a quiet rural setting. All of a sudden the Town of Caledon wants to urbanize our community. I am in total diagrement of any curbs, sidewalks, walkways or street lighting. I don't see any of this on other streets, (Kennedy Rd., Station Rd., Grange sideroad etc.). The sidewalk on Creditview Rd. looks like hell and really took away from the original rural setting. 95% of people using the road are local residents. Just Repair the Road !!!

6. Please include other questions you have below:

I continue to hear about safety but it was not until this year that signing for pedestrians at the trail were installed. There has never been any Curve Warning / Speed tabs for the curve in the 36 years i have lived here. The guide rail at the trail is useless.

The signing for Pedestrians crossing the trail at Old Base Line Rd. is a disaster waiting to happen, very rarely cars stop or make any attempt to stop. Very poor design.

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MISSISSAUGA ROAD TO CREDITVIEW ROAD

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NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

☐ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

☐ A Mill Street Resident

☒ Other (Please Identify - My residence is on Creditview Rd.)

2. What is your primary mode of travelling along Mill Street?

☐ Personal Vehicle (Auto)

☐ Bicycle

☒ Walking

☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

☐ Daily

☒ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

If you answered "No", please provide your reason(s).

I don't feel walking
on Mill Street is an unsafe activity.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: ONE
How do you feel this option accommodates pedestrians?	
<p>ANY option that leaves the street in the most "natural" state would be, in my opinion, the most attractive solution to repairing Mill St. Mountable curbs help prevent breakage at the edges of the asphalt which is the only reason I see them as acceptable inside this option.</p>	

Sidewalk	Rank: 3
How do you feel this option accommodates pedestrians?	
<p>This option would, without doubt, ruin Mill St. The removal of the majestic 100-year old trees to widen Mill St flies in the face of Caledon's claim of being a eco-friendly place.</p>	

Asphalt Boulevard	Rank: 3
How do you feel this option accommodates pedestrians?	
<p>All I can think about this is why oh why was this not an option for Creditview Rd!!!</p>	

Please provide any additional comments on your preferred option for pedestrian accommodation.

~~n/a~~ I decided to
address the guardrails. NOT REQUIRED!!!!
There hasn't been a need for guardrails
on Mill Street in 200 years. There is no
need for them now.

Truly there should be a legislative exemption
for the small villages in Caledon in order to
allow them to be upgraded without the threat of
them being bastardized beyond recognition.

6. Please include other questions you have below:

I am a member of ~~the~~ C.A.R.A. People may say
"not my street, not my problem" but I disagree.
Chipping away at the uniqueness of Cheltenham
has already begun. Without input from the residents
there will be more unpleasant changes. Like the
abomination that now spans the west side of
Creditview Rd. I tried, to no avail, to convince the
powers that be to install a asphalt curb instead of
sidewalks (UBC has this all around its ringrd). Thank you
for listening. hopefully this time....

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**MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD**

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NAME (REQUIRED): _____

MAILING ADDRESS (REQUIRED): _____

EMAIL (OPTIONAL): _____

☒ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

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☒ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

- ☒ Personal Vehicle (Auto)
☐ Bicycle
☒ Walking
☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

- ☐ Daily
☐ Weekly
☒ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

I love walking around my neighbour
but find it challenging and unsafe
as there is no sidewalk and
road is deteriorated

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☒ Yes
☐ No

If you answered "No", please provide your reason(s).

Sidewalk would be beneficial
and safer for everyone.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 3
How do you feel this option accommodates pedestrians?	

Sidewalk	Rank: 1 Best
How do you feel this option accommodates pedestrians?	
I think it is time to make neighborhood safe for pedestrians - credit view Rd just had side walk built - great idea.	

Asphalt Boulevard	Rank: 2
How do you feel this option accommodates pedestrians?	
Cyclists tend to be out of towners who come to the area to ride their bikes - I believe believe Sidewalks for pedestrians who live in the area, is best solution	

Please provide any additional comments on your preferred option for pedestrian accommodation.

Mill Street is a winding and hilly Road and is unsafe for pedestrians. Side walker will make it safer for pedestrians, wheelchairs, walkers and strollers

I understand that current residents of mill street may be opposed to the encroachment on their property. However the safety of the community is paramount.

6. Please include other questions you have below:

I believe side walks are the solution for a safer community. Now that Creditview has a side walk, my family walks on it more often as it is finally safe.

Tara M. M. M.

Please provide your comments by July 6th, 2022 to:

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☒ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

If you answered "No", please provide your reason(s).

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Existing with Mountable Curb	Rank:
How do you feel this option accommodates pedestrians?	
I feel this is the best. Just a rural road repair upgrade.	

Sidewalk	Rank:
How do you feel this option accommodates pedestrians?	
Pedestrians walk on the road daily. Never any problems. Don't need to be like the suburbs. We are a small country community and wish to stay that way.	
No No No.	

Asphalt Boulevard	Rank:
How do you feel this option accommodates pedestrians?	
No No No	

Please provide any additional comments on your preferred option for pedestrian accommodation.

Pedestrians can walk on the road.
Never been a problem. Don't want the
road fixed to make cars drive faster.
People on mill walk their dogs and
kids and we all know. speed kills.

6. Please include other questions you have below:

Do the residence on mill get to vote on
final decision?

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☐ Other (Please Identify -)

3. Approximately how often do you walk along Mill Street?

- ☒ Daily
☐ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

The traffic is generally slow, respectful and infrequent. There is already plenty of road width and adequate sightlines

(other than the crest of the hill at the S-bend. The crest level could be adjusted without installing a specific pedestrian route) to be safe for pedestrians. A dedicated

pedestrian route gives motorists a sense that they are entitled to drive faster....and they do, especially more so since the installment of sidewalk on Creditview Rd.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 2
How do you feel this option accommodates pedestrians?	
This does not accommodate pedestrians. A mountable curb restricts pedestrian entry and egress.	
Mountable curbs funnel water and given the current	
topography, the discharge will likely add to flooding which will	
restrict access as well as damage to property.	
There are no proposals to deal with flooding in the current study.	

Sidewalk	Rank: 3
How do you feel this option accommodates pedestrians?	
A sidewalk categorizes pedestrians. A sidewalk instructs pedestrians that they	
must only walk where designated and it projects that viewpoint on motorists	
It speeds up traffic which is detrimental to pedestrians and is antithetical to	
accommodating pedestrians	
Sidewalks undermine the quiet rural nature of the community	
which makes it it less appealing to pedestrians,	
thereby failing to accommodate pedestrian.	
To summarize, a sidewalk requires road widening, which introduces a risk to the road that does not currently exist.	

Asphalt Boulevard	Rank: 1
How do you feel this option accommodates pedestrians?	
An asphalt blvd widens the road for motorists, which increase speeds.	
Increased speeds do not accommodate pedestrians	
This removes trees and significantly alters the streetscape.	
Urbanizing the streetscape does not accommodate pedestrians.	
The ranking does not reflect my view	
of these proposals in the community	
I originally ranked them 3-3-2	

Please provide any additional comments on your preferred option for pedestrian accommodation. The basic premise of this form and the presentation appears to be accommodating pedestrians.

No one has asked, or stated, what accommodation means for this study. What it appears to mean from the proposals, is to get pedestrians off the road in order to accommodate motorists. This is NOT what Mill St. residents want. Given the quiet rural character of the road, I suggest that the Town and its consultants are asking the question incorrectly or at least placing the emphasis in the wrong place. Perhaps it is the behaviour of the motorists that the design needs to alter in favour of the pedestrians.

Perhaps such an approach may also help in maintaining the character of the street and the sense of community. The work done over the years on Creditview is evidence of how to damage a rural village. Urbanizing everything is not the answer. Maintain a rural street environment.

6. Please include other questions you have below:

The EA has not communicated a need for reconstruction/realignment based on quantifiable information and the analysis is based on flawed data.

The comments form is poorly drafted with leading questions.

The comment form is very poorly formatted, making it less user friendly and is not a saveable pdf. The less user-friendly, the less likely people are to complete it. It restricts community engagement.

Ranking poor proposals against each other creates a false ranking of the proposal, generally. Again, this can create a misrepresentation that may be used to justify a particular proposal. A ranking of "1" does not mean it is something the community likes or WANTS.

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☒ Weekly
☐ Monthly
☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☐ Yes
☒ No

If you answered "No", please provide your reason(s).

The traffic is generally slow, respectful and infrequent. There is already plenty of road width and adequate sightlines (other than the crest of the hill at the S-bend) to provide a safe pedestrian route. A dedicated pedestrian route gives motorists a sense that they are entitled to drive faster....and they do as evidenced on Creditview Rd.

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 2
How do you feel this option accommodates pedestrians?	
This does not accommodate pedestrians. A mountable curb restricts pedestrian entry and egress.	
Mountable curbs funnel water and given the current	
topography, the discharge will likely add to flooding which will	
restrict access as well as damage property.	
There are no proposals to deal with flooding in the current study.	

Sidewalk	Rank: 3
How do you feel this option accommodates pedestrians?	
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It speeds up traffic which is detrimental to pedestrians and is antithetical to	
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Sidewalks undermine the quiet rural nature of the community	
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thereby failing to accommodate pedestrian.	

Asphalt Boulevard	Rank: 1
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Increased speeds do not accommodate pedestrians	
This removes trees and significantly alters the streetscape.	
Urbanizing the streetscape does not accommodate pedestrians.	
The ranking does not reflect my view	
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I originally ranked them 3-3-2	

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NAME (REQUIRED):

MAILING ADDRESS (REQUIRED):

EMAIL (OPTIONAL):

☒ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

- ☐ A Mill Street Resident
☒ Other (Please Identify - CheltenhamResident)

2. What is your primary mode of travelling along Mill Street?

- ☐ Personal Vehicle (Auto)
☐ Bicycle
☒ Walking
☒ Other (Please Identify - Golf Cart)

3. Approximately how often do you walk along Mill Street?

- ☐ Daily
☐ Weekly
☐ Monthly
☒ Rarely or Never

If you answered "Rarely or Never", please identify why.

Because there isnt anywhere really to walk. We walk to the bridge and look at the river. My kids go to the park instead.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

- ☒ Yes
☐ No

If you answered "No", please provide your reason(s).

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb	Rank: 2
How do you feel this option accommodates pedestrians?	
I think if we arent going to put in a sidewalk then just do the simple one.	
I also dont think the road needs all the city things like road paintings	
and it definitely doesnt need guard rails and things like that. It will look terrible for cheltenham.	

Sidewalk	Rank: 1
How do you feel this option accommodates pedestrians?	
I personally believe if we dont put the sidewalk in now it will never get done.	
We waited like 20 years to get a sidewalk on creditview.	
I think it will make it safer for people to walk.	
I do understand the people that live on Mill streets concerns though.	
I dont live on mill street but we might walk to more if there was somewhere to walk.	
But no need for all the extra road stuff like guard rails and city looking things.	

Asphalt Boulevard	Rank: 3
How do you feel this option accommodates pedestrians?	
I say either go for it with the sidewalk or just dont bother.	

Please provide any additional comments on your preferred option for pedestrian accommodation.

I am also noticing this is all just focussed on pedestrian accommodation. Why is that?

Shouldnt it also be about road safety? About keeping the Cheltenham Charm?

About saving as many trees and much more. Why is this comment sheet only addressing somewhere for pedestrians?

6. Please include other questions you have below:

Will we be notified once a decision is made? With our creditview sidewalk we werent told of the final decison (that I am aware of)

When will we find out this answer?

I hope we can come to some agreement as a community becasue we really just need something done with the road.

Thanks for all the extra time working with the community and providing extra meetings etc.

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca
By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

With the exception of personal information, all comments will become part of the public record of study. The study is being conducted according to the requirements of the Municipal Class Environmental Assessment, which is a planning process approved under Ontario’s Environmental Assessment Act.

THANK YOU FOR YOUR TIME AND EFFORT!

MILL STREET CLASS ENVIRONMENTAL ASSESSMENT STUDY
MISSISSAUGA ROAD TO CREDITVIEW ROAD

PUBLIC INFORMATION CENTRE (PIC) #2 COMMENT FORM

The Town is presenting three options for the north side of Mill Street. Option 1 is existing with mountable curb, Option 2 is dedicated sidewalk with barrier curb, and Option 3 is asphalt boulevard with mountable curb. All three options allow for grass boulevard and mountable curb on the south side of Mill Street. Please provide your input on the options presented by completing this comment form.

NAME (REQUIRED): _____

MAILING ADDRESS (REQUIRED): _____

EMAIL (OPTIONAL): _____

☐ PLEASE ADD ME TO THE STUDY MAILING LIST

1. You are:

☒ A Mill Street Resident

☐ Other (Please Identify - _____)

2. What is your primary mode of travelling along Mill Street?

☒ Personal Vehicle (Auto)

☒ Bicycle

☒ Walking

☐ Other (Please Identify - _____)

3. Approximately how often do you walk along Mill Street?

☒ Daily

☐ Weekly

☐ Monthly

☐ Rarely or Never

If you answered "Rarely or Never", please identify why.

4. Would you be more likely to walk along Mill Street if there was a dedicated pedestrian facility?

☐ Yes

☒ No

If you answered "No", please provide your reason(s).

I walk the road at least every day, sometimes twice doubt any "walkway" would up that!

5. Please rank the options for pedestrian accommodation presented today from 1 to 3, based on which option you feel best accommodates pedestrians walking along the corridor (with 1 being the best and 3 the least) and provide comments.

Existing with Mountable Curb

Rank: 1

How do you feel this option accommodates pedestrians?

The question is biased. There are many accommodation requirements. If the road had traffic calming measures, lower than 40 km/h posted speeds, narrowed lanes that don't invite cars to speed, then pedestrians would be very much accommodated to walk on the road edge and sod/grass edge.

Sidewalk

Rank: 3

How do you feel this option accommodates pedestrians?

A concrete sidewalk is a no brainer for accommodating pedestrians - of course it's the option if this is the only consideration. But it's not just pedestrians to accommodate.

Asphalt Boulevard

Rank: 2

How do you feel this option accommodates pedestrians?

Again, a paved shoulder is the same as a concrete sidewalk - so it's a no brainer accommodation for pedestrians of course! So again, the question is biased.

Please provide any additional comments on your preferred option for pedestrian accommodation.

Again this is a biased "comment" request. To suggest that pedestrian accommodation is the only issue to comment on is again slanted, biased and rather insulting and diminishing to ALL the concerns and needs to accommodate. I believe pedestrians need better/safer accommodations, but it is the cars that need less accommodations; narrow lanes, tighter turns, slower speeds, limited sight lines, traffic calming etc etc - SLOW the cars down, and the pedestrian issue takes care of itself.

6. Please include other questions you have below:

- A.) List specific traffic calming measures that will be implemented.
- B.) Explain in detail why the posted speed limit can't be lower than 40 km/h.
- C.) What is required to allow the Town to drop this minimum 2 lane width mantra?
- D.) Can someone give detailed cross section dimensions of the "S" bend retaining wall and guard rail?

Please provide your comments by July 6th, 2022 to:

By Email: shun.cheung@caledon.ca

By Mail: Shun Cheung
c/o The Corporation of the Town of Caledon
6311 Old Church Rd
Caledon, ON L7C 1J6

With the exception of personal information, all comments will become part of the public record of study. The study is being conducted according to the requirements of the Municipal Class Environmental Assessment, which is a planning process approved under Ontario's Environmental Assessment Act.

THANK YOU FOR YOUR TIME AND EFFORT!

APPENDIX 3-2

Comments Received: Technical Agency & Key Stakeholder Groups

Connor MacIsaac

From: Winnie Wong
Sent: August 31, 2022 11:01 AM
To: Saddi, Asha; Shun Cheung
Cc: Connor MacIsaac; Mannie, Sharon
Subject: RE: Town of Caledon: Mill Street Class EA

Hi Asha,

Thank you for your comments. Please see below responses in [blue](#).

Thanks,

Winnie Wong, P. Eng., PMP, M.Eng.
Associate, Project Manager
R.V. Anderson Associates Limited

From: Saddi, Asha <asha.saddi@peelregion.ca>
Sent: July 15, 2022 8:49 AM
To: Shun Cheung <shun.cheung@caledon.ca>
Cc: Winnie Wong <Winnie.Wong@rvanderson.com>; Connor MacIsaac <cmacIsaac@rvanderson.com>; Mannie, Sharon <sharon.mannie@peelregion.ca>
Subject: RE: Town of Caledon: Mill Street Class EA

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Hi Shun,
Attached are comments from our Traffic Engineering teams:

Traffic Operations - Sharon Mannie

- Does this EA cover the entire intersection of Mississauga Road and Mill Street? <<RVA_20220831: EA does not include the intersection of Mississauga Road.>>
- Please provide soft copies of the materials for reference after the PIC Meeting #2 <<RVA_20220831: Town forwarded the link to PIC #2 presentation to the Region on June 14.>>

Traffic Signals - Rebecca Caughey

- Will Town of Caledon be conducting a Trail Crossing Treatment Warrant for the Trail Crossing at Mill Street? <<RVA_20220831: Pedestrian crossing improvements including new traffic signs and pavement markings are proposed and details to be confirmed during detailed design.>>
- Would the Town of Caledon consider a connection between Mill Street and the Trail crossing as well along Mississauga Road? <<RVA_20220831: Trail currently intersects both Mill Street and Mississauga Road.>>

I will be in touch again if we have additional comments from other teams and in the meantime we look forward to receiving your response comments.

Thanks,

Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca



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From: Saddi, Asha
Sent: July 12, 2022 11:26 AM
To: Shun Cheung <Shun.Cheung@caledon.ca>
Cc: winnie.wong@rvanderson.com; Connor MacIsaac <cmaclsaac@rvanderson.com>
Subject: RE: Town of Caledon: Mill Street Class EA

Hi Shun,
This is to let you know that due to vacations some of our teams are still reviewing materials and I will be able to get back to you by the end of the week with any additional comments.

Thanks,

Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca



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From: Saddi, Asha
Sent: June 23, 2022 11:37 AM
To: Shun Cheung <Shun.Cheung@caledon.ca>
Cc: winnie.wong@rvanderson.com; Connor MacIsaac <cmaclsaac@rvanderson.com>; Khan, Sabrina <Sabrina.Khan@peelregion.ca>
Subject: RE: Town of Caledon: Mill Street Class EA

Hi Shun,
Our Transportation System Planning team have the following comment/question:

Transportation System Planning – Sabrina Khan

- Option 2 – Sidewalk is our preferred alternative as it provides the safest option for pedestrians
- Why is the focus only on pedestrians and not active transportation (cyclists)?

I will be in touch if other teams have comments and in the meantime we look forward to receiving your response comments.

Thanks,

*Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca*



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From: Saddi, Asha
Sent: June 20, 2022 3:27 PM
To: 'Shun Cheung' <Shun.Cheung@caledon.ca>
Cc: winnie.wong@rvanderson.com; 'Connor MacIsaac' <cmacisaac@rvanderson.com>; ZZG-PWI <pwi@peelregion.ca>; Ponce Vanelli, Italia <Italia.Ponce@peelregion.ca>
Subject: FW: Town of Caledon: Mill Street Class EA

Hi Shun,
 Our Water and Wastewater team have the following comments:

The Region has a 250mm and 300mm watermain that runs predominantly on the north side of Mill Street. This is the only source that provides water to the residents in the area. Impact to our underground infrastructure, Valve, chambers and fire hydrants will need to be taken into consideration during this project. Furthermore, in order to comment on the solutions presented we would like the project material to be available for viewing digitally as many of us could not attend the in person meeting and the Town's website does not contain the material online.

I will be in touch again if other teams have comments and in the meantime, we look forward to your response comments.

Thanks,

*Asha Saddi, BA(Hons), PMP
Technical Analyst, Infrastructure Programming & Studies
Transportation Division
Public Works, Region of Peel
Asha.Saddi@peelregion.ca*

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From: Connor MacIsaac <cmacisaac@rvanderson.com>

Sent: May 30, 2022 10:57 AM

Cc: Shun Cheung <shun.cheung@caledon.ca>; Winnie Wong <Winnie.Wong@rvanderson.com>

Subject: Town of Caledon: Mill Street Class EA - Notice of Public Information Centre #2

CAUTION: EXTERNAL MAIL. DO NOT CLICK ON LINKS OR OPEN ATTACHMENTS YOU DO NOT TRUST.

Dear Sir/Madam,

On behalf of the Town of Caledon, please find attached the Public Information Centre (PIC) #2 for the **Mill Street Class Environmental Assessment Study Mississauga Road to Creditview Road (Schedule B MCEA)**. The purpose of this meeting is garner feedback on the options for pedestrian accommodations developed for Mill Street in consideration of the feedback received during and following PIC #1, held online October 5, 2021.

The PIC will be held as an informal "Open House" with materials pertaining to the study on display, with members of the project team on hand to answer questions and discuss issues related to the project. Attendees will be asked to review the materials on display and provide their preferences for pedestrian accommodations. A final recommendation for Mill Street will be developed following the PIC, in consideration of the input received.

The details of the PIC are below:

Date: Wednesday June 15, 2022
Time: 5:30 to 7:00 pm
Location: Cheltenham Community Centre - 14190 Creditview Rd, Caledon, ON L0P 1N0
Entrance is located at the south side of the building

The project team values the participation of all stakeholders and wishes to ensure that the community's interests and concerns are taken into consideration. Please contact the project team members listed below if you require a hard copy of the materials or for further information on this project.

Town of Caledon

Shun H. Cheung, P.Eng., PMP
Project Manager, Capital Infrastructure Engineering Services
905.584.2272 X.4040
shun.cheung@caledon.ca

R.V. Anderson Associates Limited

Winnie Wong, P.Eng., PMP., M.Eng.
Project Manager
416.497.8600 X.1471
winnie.wong@rvanderson.com

If you would like to be removed from the project mailing list, please let us know by responding to this email.

Regards,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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