

**PLANNING JUSTIFICATION REPORT
ZONING BY-LAW AMENDMENT, SITE PLAN,
DRAFT PLAN OF SUBDIVISION AND
DRAFT PLAN OF CONDOMINIUM
APPLICATIONS**

9229 5th Sideroad
Town of Caledon

Villalago Residences Inc.

MAY 2017

PREPARED BY:

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FILE No: P-2658

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1.0 INTRODUCTION

KLM Planning Partners Inc. are the planning consultants for Villalago Residences Inc. of a 2.81 hectares (6.94 acres) parcel of land consisting of Blocks 118, 152-154, 165, 167, 178, 181 & 182 on 43M-1251 in the Town of Caledon, Region of Peel. The proposed development also includes the portion of the 5th Sideroad between Highway 50 (Queen Street South) and Landsbridge Street currently owned by the Town of Caledon and a daylight triangle on the southeast corner of the 5th Sideroad and Highway 50 currently owned by the Region of Peel. Villalago Residences Inc. have begun the process with the Town of Caledon and Region of Peel to acquire the above noted lands. The total land holdings subject to the applications is 3.15ha (7.80 acres) which includes Villalago Residence Inc. lands, Town of Caledon and Region of Peel lands.

On behalf of Villalago Residence Inc., KLM Planning Partners Inc. has prepared a Draft Plan of Subdivision, Draft Plan of Condominium and a proposed implementing Zoning By-law Amendment which has been submitted to the Town of Caledon to facilitate the development of residential uses on the subject lands.

As part of the review process associated with the above noted development applications, Town of Caledon staff has requested a Planning Justification Report be submitted. This report provides a planning analysis and justification in support of the proposed amendments to the Town's Zoning By-law as well as the proposed Draft Plan of Subdivision, and Draft Plan of Condominium.

2.0 SUBJECT LANDS & SURROUNDING AREA

2.1 Subject Lands

Villalago Residences Inc. land is located in Bolton and is bounded by Queen Street South to the west, Landsbridge street and existing residential development to the east, 5th Sideroad and the commercial plaza beyond to the north and the Railway tracks to the south as shown on Figure 1. As noted above, Villalago Residence Inc. land holding is approximately 2.81 hectares (6.94 acres) in size.

Further, the lands subject to the proposed development applications include the right-of-way for the 5th Sideroad (currently owned by the Town of Caledon) and a daylight triangle on the southeast corner of the 5th Sideroad and Highway 50 (currently owned by the Region of Peel). It is intended that the Town of Caledon, as per their DART comments, will close the road allowance for 5th Sideroad and it will be sold to Villalago Residences Inc. An application to purchase the 5th Sideroad right-of-way has been submitted to the Town of Caledon as well as an application to the Region of Peel to purchase the daylight triangle on the southeast corner of the site.

The total land holdings subject to the applications is 3.15ha (7.80 acres) which includes Villalago Residence Inc. lands, Town of Caledon and Region of Peel lands.

The subject lands are located at a lower elevation than Queen Street South and generally are flat with minor topographical deviations. The residential dwelling on the property has been removed and the site is currently vacant.

2.2 Surrounding Area

The surrounding area is characterized by an existing residential neighbourhood and a commercial plaza. The subject lands are bounded by the following:

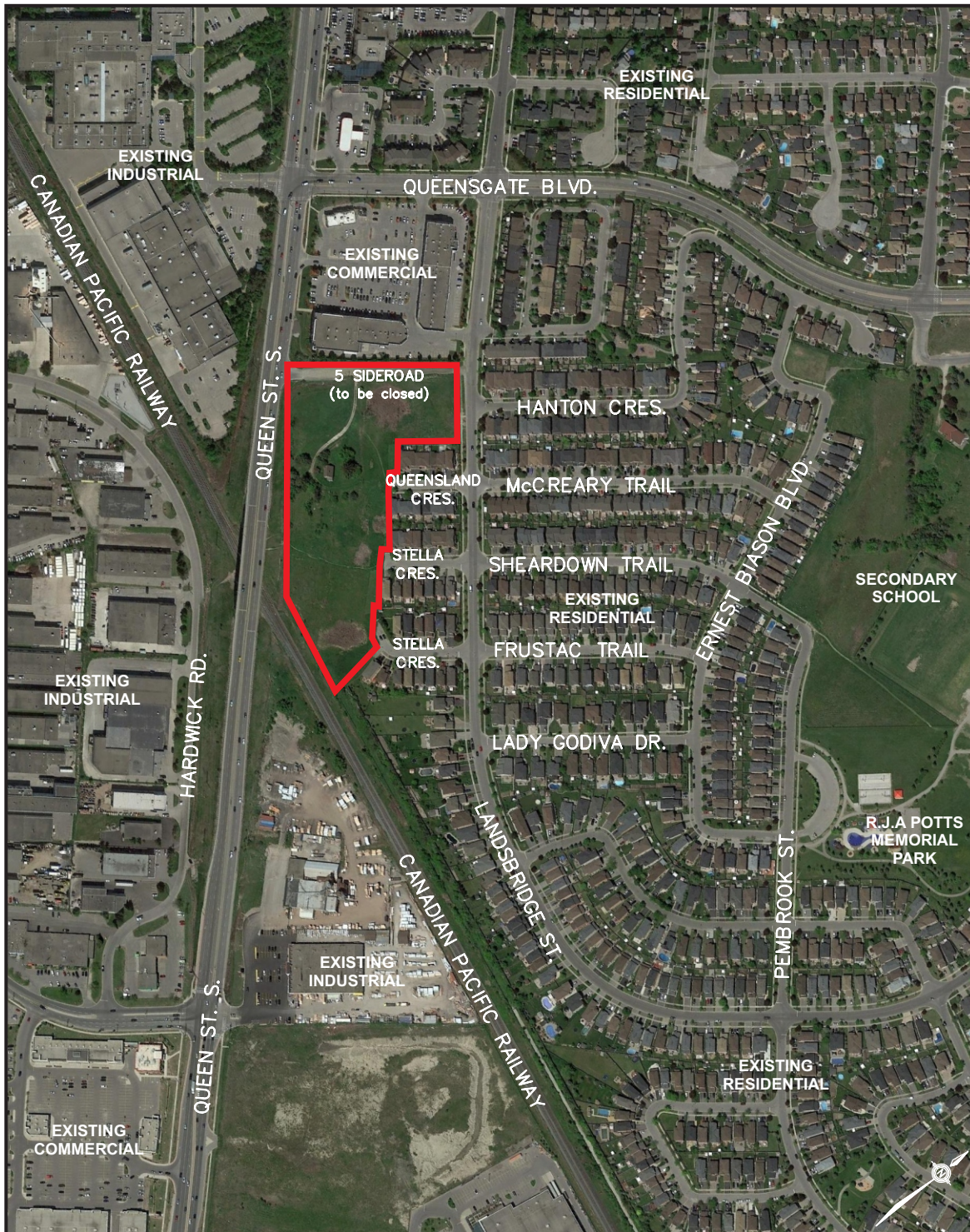
North: Existing commercial plaza.

South: C.P. Railway tracks and an existing residential neighbourhood

East: Existing residential neighbourhood.

West: Queen Street South and existing industrial development.

Figure 1 - Context Map



SUBJECT LANDS

Part of Lot 5 and Part of the Road Allowance between
West Halves of Lots 5 and 6 Concession 7 and
Blocks 118, 152-154, 165, 167, 178, 181 and 182,
Registered Plan 43M-1251
Town of Caledon

**PLANNING JUSTIFICATION REPORT
VILLA LAGO RESIDENCE INC.
TOWN OF CALEDON**

3.0 PROPOSAL

Description

The proposed Draft Plan of Subdivision, Draft Plan of Condominium and Zoning By-law Amendment have been submitted to the Town of Caledon to permit the development of the subject lands for residential purposes.

As noted previously, the proposed development includes a total land holding of approximately 3.15 hectares (7.80 acres) which is made up of the following:

- Approximately 2.81 hectares (6.94 acres) of land owned by Villalago Residences Inc.
- Approximately 0.34 Hectares (acres) of land owned by the Town of Caledon and by the Region of Peel

The proposed development contains the following as demonstrated on Figure 2:

- 1 single detached unit,
- 4 semi-detached units,
- 7 freehold townhouse units,
- 102 townhouses with common element tenure comprised of:
 - o 34 townhouse units with unit widths of 4.6 metres containing double car garages,
 - o 7 townhouse units with unit widths of 5.5 metres containing single car garages,
 - o 61 townhouse units with unit widths of 6.0 metres containing double car garages,
- 20 visitor parking spaces dispersed throughout the site,
- The extension of Queensland Crescent via the proposed Street '1' to connect to Stella Crescent; and
- A private amenity area.

The proposed common element condominium townhouses will contain a mix of rear accessed townhouses and street accessed townhouses. The rear access townhouses will have frontage along Queen Street South, Landsbridge Street and the extension of Street "1" and garage access from the common element private road network from the garages behind. There are also a number of townhouses internal to the site that have frontage onto a common element walkway with garage access from the condominium road network. Each unit will have an outdoor amenity space and depending on its location within the development the amenity space may be provided over the front and rear entry doors and terraces above the garage.

There is one street accessed townhouses block (Block 6 on the site plan) which will be accessed by the common element condominium road network.

The townhouse blocks along Highway 50/Queen Street South, Landsbridge Street and Street "1" will address the street and will have vehicle access at the rear via a private road (refer to the conceptual elevations in Figure 3). This treatment provides for a pedestrian friendly environment along both Highway 50/Queen Street South, Landsbridge Street and Street "1".

The seven (7) freehold townhouse units will front onto Street "1". The freehold townhouse units will contain an architectural style similar to the common element townhouse units throughout the proposed development. These units will have a private amenity area in the rear yard.

Block 1 contains a single detached unit and Blocks 2 and 3 contain two semi-detached units each and are adjacent to existing single detached units along Stella Crescent and Queensland Crescent. These units will front onto Queensland Crescent and Stella Crescent and are proposed to have a complementary architectural style and street presence to the existing residential dwellings.

The zoning by-law amendment proposes a maximum building height of 12.5m to provide flexibility to accommodate the varying grades which have yet to be confirmed. The proposed height increase is appropriate for the proposed infill development which assists the Town with their intensification targets within the built up area. In addition, the proposed development provides for a larger built form adjacent to Queen Street South and provides a transition from the existing two storey residential units.

The south west portion of the site adjacent to the Canadian Pacific (C.P.) Railway tracks has limited development potential because the C.P. railway requires a 30 metre buffer from the tracks where structures or buildings are not permitted. In order to make the most efficient use of land, the buffer for the C.P. railway tracks contain 7 visitor parking spaces as well as a private amenity area to service the proposed development. A 2.5 metre high berm is proposed adjacent to the C.P. railway to provide the appropriate noise and safety mitigation measures for the proposed development since it is adjacent to a principle main line of the C.P. railway.

A total of 20 visitor parking spaces are being provided throughout the proposed development which are connected to the surrounding residential units via the condominium walkway system. Additionally, a minimum of two (2) parking spaces are required for each townhouse dwelling unit as per the Town's parking requirements in the Zoning By-law. Two (2) spaces have been provided for each unit, one in the driveway and one in the garage. The proposed development provides additional 95 parking spaces for the townhouse units which have double car garages.

The Town's Zoning By-law require 0.25 visitor parking spaces per unit which means 26 visitor parking spaces are required for the 102-unit common element townhouse

development (102 common element townhouse units $\times 0.25 = 25.5$). The proposed development provides a total of 20 visitor parking spaces. The Transportation Study prepared by NexTrans concludes that the 20 visitor parking spaces provided is sufficient to service the proposed development since there are additional parking spaces being provided for in condominium townhouse units with double car garages.

The proposed development provides sidewalks throughout the development to facilitate pedestrian connectivity throughout the development and to the surrounding community. A 1.5 metre sidewalk has been provided along Queen Street South and along the northern limit of the site adjacent to the existing commercial plaza to connect Queen Street South to Landsbridge Street. The proposed development also intends to extend the sidewalk along the existing right-of-way of Stella Crescent and Queensland Crescent onto Street "1" as well as along Landsbridge Street.

The proposed development provides for a municipal right-of-way (Street "1") which links the Queensland Crescent and Stella Crescent.

Figure 2 - Conceptual Site Plan



SUBJECT LANDS

Part of Lot 5 and Part of the Road Allowance between
West Halves of Lots 5 and 6 Concession 7 and
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Figure 3 – Conceptual Building Elevations



TYPICAL 4.62m FRONT ELEVATION



TYPICAL 6.00m FRONT ELEVATION - A

4.0 PLANNING APPLICATIONS

The following amendments to the Town of Caledon's Zoning By-law and application for development are requested at this time to implement the proposed development:

4.1 Draft Plan of Subdivision

A Draft Plan of Subdivision has been prepared which containing a total of 6 blocks to facilitate the proposed development (refer to Figure 4) which consists of:

- One single detached unit on Lot 1
- 4 semi-detached units on Lots 2 and 3
- 7 freehold townhouses on Block 4
- 102 condominium townhouse units within Block 5
- A road widening is provided as Block 6
- Street "1" municipal right-of-way to connect Queensland Crescent and Stella Crescent

4.2 Zoning By-law Amendment

A Zoning By-law Amendment application has been prepared to implement the above noted Draft Plan of Subdivision. The Zoning By-law Amendment application rezones the subject lands from Prestige Industrial (MP-310), Residential Townhouse Exception (RT-67 and Residential One Exception (R1-68) to a site-specific Townhouse Residential exception and Residential One exception as well as Open Space for a future park.

The following exceptions to the Zoning By-law are required to implement the proposed development:

- Rezone the subject lands to permit freehold townhouse dwellings, common element townhouse dwellings, semi-detached, single detached and private home day care uses;
- Prescribe building requirements including:
 - Reduction to the lot area from 200m² to 100m²;
 - Reduction in lot frontage for the 4.5m wide units;
 - Reduced front, rear, interior and exterior side yard setbacks;
 - Increase to building height for both common element townhouse units and freehold units from 10.5m to 12.5m;
 - Increase the lot coverage from a maximum of 30% to 56%; and
 - Reduction to the Backyard amenity area and clarification that the balcony above the garage provides the backyard amenity area.
- Prescribe the permitted encroachment for canopies/porticos and uncovered steps within any required yard.

A zoning matrix has been provided as part of the submission materials for each block to assist staff in reviewing the proposed development.

4.3 Draft Plan of Condominium Application

A Draft Plan of Condominium has been prepared which identifies the common element area of approximately 0.975 ha (2.409 acres) including the common laneways and condominium common element areas (walkways, visitor parking, private amenity area, berm and acoustic fence). Blocks 1-12 include the residential blocks which will be further divided through a future application for part lot control to create the parcels of tied land. (refer to Figure 5). In addition to maintenance of the common element road way, the condominium would also be responsible for the maintenance of the acoustic fence and berm, the private amenity area, visitor parking spaces and the walkways within the condominium.

Figure 4 - Draft Plan of Subdivision

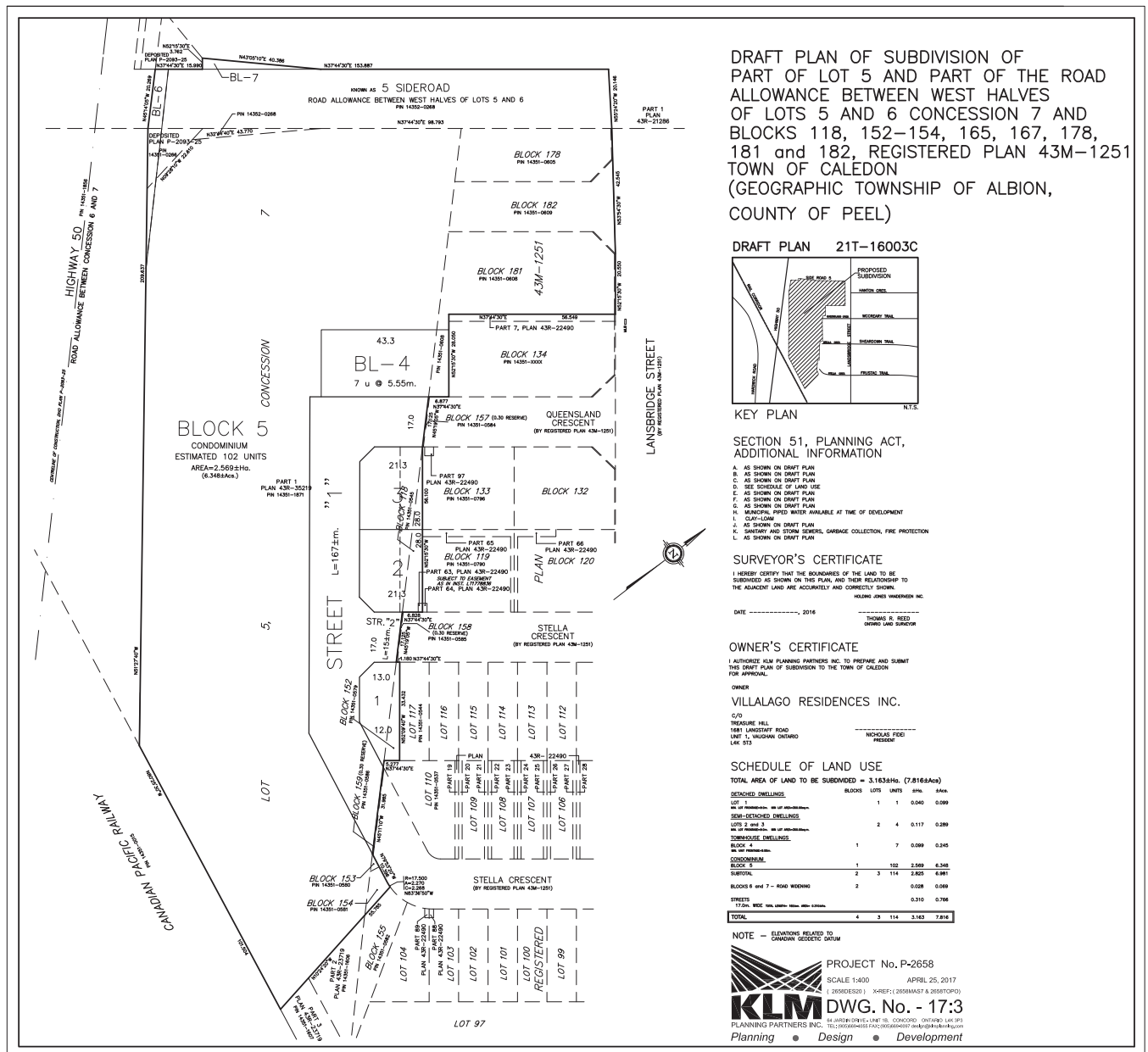
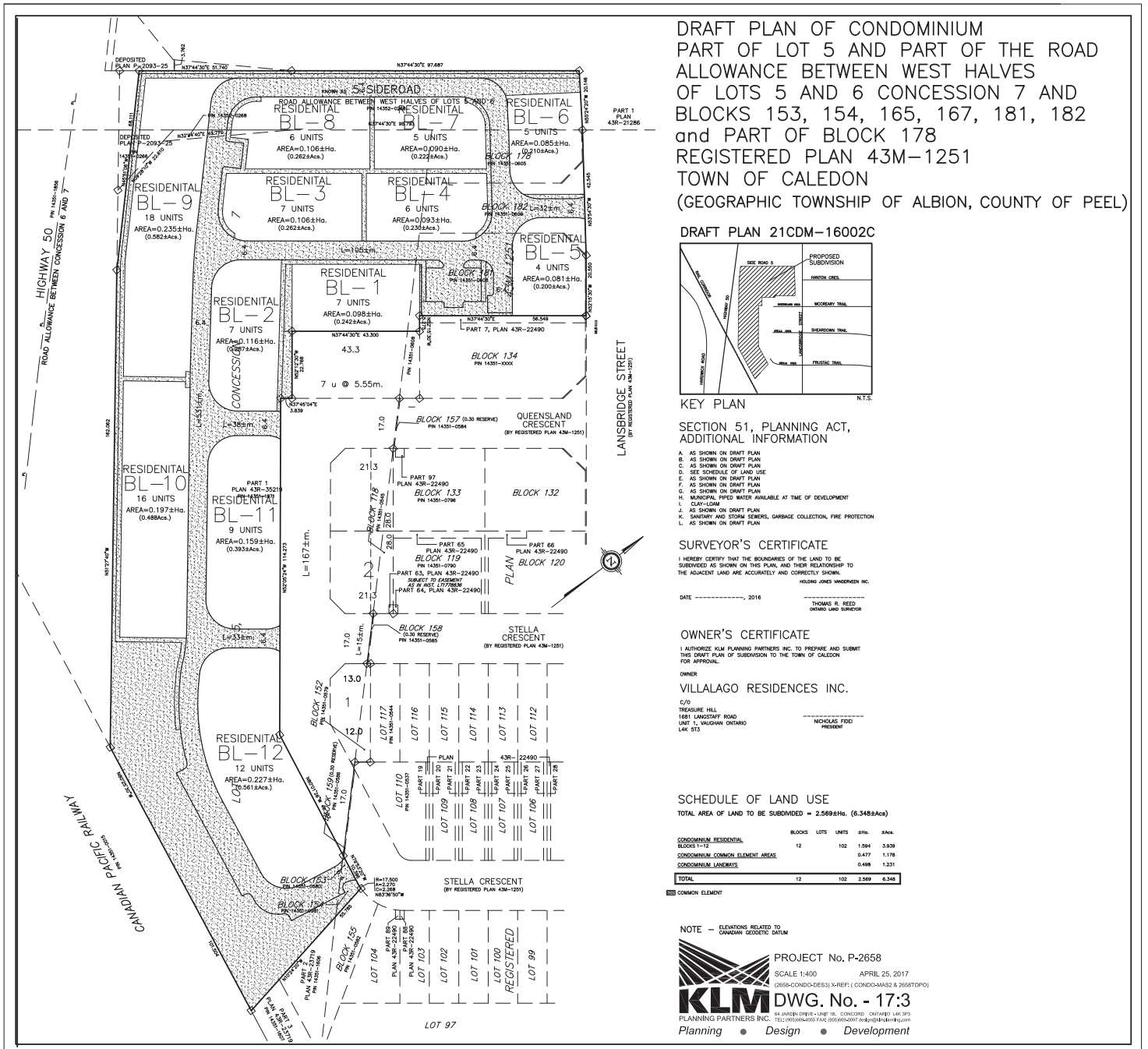


Figure 5 - Draft Plan of Condominium



4.4 Supporting Documents

The proposed development has been prepared together with various supporting documents. These have been submitted separately but the findings are summarized below:

Functional Servicing Report

Prepared by RAND Engineering Corporation

The report makes the following conclusions and recommendations in support of the above noted applications:

- A technical assessment of the municipal servicing requirements indicates that the development plan may be adequately serviced by the proposed storm drainage, sanitary and water distribution systems
- The existing SWM Pond 4, located southeast of the intersection of Albion-Vaughan Road and the CPR rail line, has sufficient capacity to accommodate the drainage from the proposed Villalago subdivision. The facility provides water quality, erosion and quantity control for the development
- A detailed Stormwater Management Implementation Report will be prepared in conjunction with the final engineering design.

Transportation Study

Prepared by nexTrans Consulting

The study identifies the following findings and conclusions:

- The subject development application proposes to provide 109 condominium townhouse units of which seven (7) freehold units, four (4) semi detached units and one (1) single detached dwelling unit.
- The proposed and future background residential development is expected to generate 77 two-way trips (19 inbound and 58 outbound) during the weekday morning peak hour and 90 two-way trips (52 inbound and 38 outbound) during the afternoon peak hour.
- The proposed site will have a minor impact on the study area intersections operations during Weekday AM and PM peak hours.
- A private condominium road with multiple connections to the external public municipal road network will service the condominium blocks.
- Due to the low traffic activity at site entrances, the closure of Side Road 5 will have no impact to the remaining site entrances operations. As a result, site traffic accessing multiple roadways (ie. Hanton Crescent, Queensland Crescent and Stella Crescent (i.e. north and south) will not be detrimental to Landsbridge Street considering traffic and site operations are minimal for vehicle transportation purposes.

- Garbage and emergency vehicles will be able to circulate within the proposed site and exit the site in a cab forward manner.
- The overall quantum of parking supply as proposed is acceptable and will not negatively affect the overall peak period visitor parking demands, as visitor overflow (if required) can be provided within individual units.
- The overall quantum of parking supply as proposed is acceptable and will not negatively affect the overall peak period visitor parking demands, as visitor overflow (if required) can be provided within individual units.
- A functional design has been prepared for the Highway 50 right-of-way.

Environmental Noise Impact Study

Prepared by Valcoustics Canada Ltd.

The study investigates the transportation noise sources (road and rail traffic) potential to impact the site and recommends the following noise control measures:

- A 2.5 m high safety berm adjacent to the principal main line of the C.P railway. The preliminary grading plan prepared by RAND Engineering shows the cross sections of the berm.
- Sound levels from the adjacent Queensgate plaza have the potential to exceed the MOE sound level limits. To address the potential noise issues, a 4.5 m sound barrier on the northern property line as well as site specific dwelling designs (no noise-sensitive windows at certain locations at the third storeys) are proposed for Blocks 9 and 12.
- Significant noise impact from the Monterra Mill is not anticipated. This conclusion is based on the assessment done in the AAR prepared for that facility as part of their C of A application, the existence of closer residential dwellings and sound level predictions using a predictive model.
- Husky has not provided information about their activities, operations or noise emissions. Thus, to assess the impact from this facility, sound level measurements were done on the subject site. The measurement data indicates that sound levels on site are entirely due to road or rail traffic. Noise from the Husky facility was not significant (evident or audible) relative to road and rail traffic noise.
- Blocks 1 to 4 and 13 to 17 require brick veneer exterior wall construction and windows of up to STC-38.
- All other blocks as well as the semi-detached dwellings and the single-family dwelling require exterior wall construction meeting STC 41 and window construction meeting STC 28. These are considered minor upgrades to the minimum non-acoustical requirements of the OBC.
- Blocks 1 to 4 and 13 to 17 require central air conditioning. All other Blocks (Blocks 5 to 12 and 18) as well as the single family and semi-detached dwellings require the provision to allow the future installation of air conditioning by the occupant.

- Where the guideline sound level limits are exceeded, appropriate warning clauses should be registered on the title or included in the development agreement that is registered on title. The warning clauses should also be included in agreements of Offers of Purchase and Sale and lease/rental agreements to make future occupants aware of the potential noise situation. A summary of the warning clauses are provided in the Valcoustics report.

The study concludes that with appropriate design of the development, a suitable acoustical environment can be provided and the applicable MOE noise guideline requirements met. Thus, the proposed development is considered feasible with respect to transportation and stationary noise, by means of proper design, and will implement the development objectives (outlined in the Official Plan and Zoning By-laws) of the Town of Caledon.

Railway Vibration Analysis

Prepared by Valcoustics Canada Ltd.

The report assessed the potential vibration impacts due to train movements on the Canadian Pacific Railway (CPR) MacTier Subdivision on the proposed residential development. The report concludes that ground-borne vibration velocity magnitudes, measured on-site June 24, 2016, due to railway trains on the CPR MacTier Subdivision, did not exceed the Federation of Canadian Municipalities (FCM)/Railway Association of Canada (RAC) vibration guideline limit. Therefore, vibration mitigation measures are not mandatory for this development.

Phase 1 Environmental Site Assessment

Prepared by exp Services Inc.

The report makes the following conclusions and recommendations:

- Based on the Phase I ESA findings, the potential environmental concerns associated with the Site are summarized as follows:
 - Regulated substances equipment present on site. A pole mounted transformer associated with the former buildings located on Site which potentially contains PCBs, was observed to be present in the north-central portion of the property. The presence of these substances does not present any threat to human health or the environment if maintained in good condition and disposed of in accordance with EPA regulations when no longer required.
 - Presence of fill material of unknown quality and origin at location of former residential dwelling, and of containers and piles of domestic solid waste. Fill associated with backfill of possible basements of former structures may be encountered during future development. Numerous small piles of household solid wastes, scrap construction materials, steel

drums containing domestic solid wastes, and uprooted and/or burnt vegetation were also noted across the Site property.

- Based on the findings of the Phase I ESA and the conclusions drawn, no requirement for a Phase II ESA (i.e., sampling and testing) has been identified. Additional due diligence recommendations are summarized as follows:
 - Regulated substances equipment present on site the pole mounted transformer and potentially PCB containing contents noted on site should be properly disposed of prior to future development.
 - Presence of fill material of unknown quality and origin at location of former residential dwelling, and of containers and piles of domestic solid waste. Any fill associated with former structures or levelling of lands for ease of cultivation encountered during future development should be chemically assessed for suitability for re-use on site or for disposal off-site. Dispose of all domestic solid domestic waste at a suitably licensed landfill site.

Urban Design Brief

Prepared by NAK Design Strategies

The Urban Design Brief emphasizes and describes those elements that are fundamental in creating an attractive, compact, pedestrian-friendly urban environment situated within the largely existing residential context. The Urban Design Brief outlines the following:

- The proposed development provides for pedestrian connection and circulation through the site as well as connects to existing infrastructure in the surrounding community.
- The character of the public realm within Villalago Residence will largely be influenced by the streetscape treatment and planting scheme proposed for areas interior to the development and those associated with the adjacent extended road network. The planting of trees and shrubs are associated with the front yard and open space opportunities, rather than traditional street boulevard conditions.
- A unique dwelling condition is the orientation of townhouses fronting onto an internal walkway, with parking access from a private lane on the rear side.
- A parkette feature will be proposed in the south-west corner of the development to serve the immediate recreation needs of Villalago Residences. The parkette and adjoining open space will supplement the existing J.S. Potts Memorial Park located within walking distance to the east.
- The townhouses are sited and designed to provide an appropriate transition from existing single-detached and townhouse residential neighbourhood immediately to the east. Consistent with a contemporary modern style, the form of the buildings will include a mix of masonry with stucco and/or stone face treatment, pitched roof and ample fenestration.

- Priority dwellings have been identified in the report to include corner dwellings, gateway dwelling at the intersection of Landsbridge Street and Hanton Crescent, view terminus dwellings and enhanced front and side elevation dwellings.

Arborist Report

Prepared by Beacon Environmental

The Arborist Report identifies that the majority of the trees are located within the central western area, clustered around the former residence. A total of eighty-five trees were documented within or adjacent to the subject property. Twenty-five of these trees are larger trees, individually assessed and tagged with numbered tags. There are also six groups of trees totaling approximately sixty-two trees of various sizes. The majority of the trees in this inventory are planted, non-native species and ornamental or fruit trees.

All of the eighty-five trees included in Beacon's inventory are proposed for removal to accommodate construction as they conflict with the proposed site plan's building footprints, roads and grading and servicing requirements. Given the young age of the predominantly non-native species of trees within an urban context and few natural features, the trees in Beacon's inventory have low ecological value.

5.0 PLANNING ANALYSIS

5.1 Provincial Policy Statement (2014)

The Provincial Policy Statement (2014) ("PPS") provides broad based policy direction on matters of provincial interest related to land use planning and development. The policies that directly apply to the proposed development of the subject lands include:

"1.1.1 Healthy, liveable and safe communities are sustained by:

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;*

- f) improving accessibility for persons with disabilities and older persons by identifying, preventing and removing land use barriers which restrict their full participation in society;*
- g) ensuring that necessary infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities are or will be available to meet current and projected needs; and*
- h) promoting development and land use patterns that conserve biodiversity and consider the impacts of a changing climate.”*

The development of the subject lands for residential purposes will provide a mix of residential housing forms to meet long term needs of future residents. In addition, the form and scale of development provides for a cost-effective pattern of development minimizing land consumption and servicing costs as it takes advantage of existing services.

Section 1.1.3.1 of the PPS states:

“Settlement areas shall be the focus of growth and development, and their vitality and regeneration shall be promoted.”

The subject lands are located within the Rural Service Centre of Bolton, one of the Town of Caledon’s settlement areas. The proposed development contributes growth to Caledon’s settlement area of Bolton.

Section 1.1.3.2 of the PPS is also of particular relevance as it states:

“Land use patterns within settlement areas shall be based on:

- a) densities and a mix of land uses which:*
 - 1. efficiently use land and resources;*
 - 2. are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
 - 3. minimize negative impacts to air quality and climate change, and promote energy efficiency;*
 - 4. support active transportation;*
 - 5. are transit-supportive, where transit is planned, exists or may be developed; and*
 - 6. are freight-supportive; and*
- b) a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in policy 1.1.3.3, where this can be accommodated.”*

The proposed development for residential purposes utilize the existing municipal and regional infrastructure currently in place to service the subject lands. The subject lands

are an underutilized parcel of land within the settlement area and the proposed residential development will provide for the completion of the surrounding neighbourhood through the efficient use of land and resources. The proposed development provides intensification with the Town of Caledon's settlement area that is compatible with the existing community. Section 1.1.3.3 of the PPS which states:

"1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs."

Intensification and redevelopment shall be directed in accordance with the policies of Section 2: Wise Use and Management of Resources and Section 3: Protecting Public Health and Safety."

The proposed development is an example of intensification which takes into account the existing building stock and surrounding area. Furthermore, the proposed development offers a range and mix of housing types in a compact built form that is compatible with the surrounding neighbourhood and suitable for the existing or planned infrastructure required to accommodate the projected needs of the current and future residents. This is consistent with Section 1.4.1 of the PPS which states:

"To provide for an appropriate range and mix of housing types and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:

- a) maintain at all times the ability to accommodate residential growth for a minimum of 10 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development; and*
- b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned to facilitate residential intensification and redevelopment, and land in draft approved and registered plans."*

Section 1.6.6.2 of the PPS outlines the preferred method of servicing for new development and states:

"Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas. Intensification and redevelopment within settlement areas on existing municipal sewage services and municipal water services should be promoted, wherever feasible."

The proposed development will be serviced by regional water and sanitary services and municipal storm sewers that are currently available in close proximity to the subject lands.

Given the above policies, the proposed Draft Plan of Subdivision, Draft Plan of Condominium and Zoning By-law Amendment applications are consistent with the policies of the PPS (2014).

5.2 “Places to Grow” The Growth Plan for the Greater Golden Horseshoe (2006)

The Provincial Growth Plan for the Greater Golden Horseshoe sets out policies for growth within the Greater Golden Horseshoe. The Growth Plan directs growth to built-up areas where the capacity exists to best accommodate the expected population and employment growth, while promoting compact built form and densities with a mix of residential land uses. The subject lands are located in the designated “Built-up Area” of the Growth Plan.

The Provincial Growth Plan in Section 2.2.2 Managing Growth states:

“2.2.2.1 Population and employment growth will be accommodated by -

- a. directing a significant portion of new growth to the built-up areas of the community through intensification*
- b. focusing intensification in intensification areas*
- c. building compact, transit-supportive communities in designated greenfield areas*
- d. reducing dependence on the automobile through the development of mixed-use, transit-supportive, pedestrian-friendly urban environments*
- e. providing convenient access to intra- and inter-city transit*
- f. ensuring the availability of sufficient land for employment to accommodate forecasted growth to support the GGH's economic competitiveness*
- g. planning and investing for a balance of jobs and housing in communities across the GGH to reduce the need for long distance commuting and to increase the modal share for transit, walking and cycling*
- h. encouraging cities and towns to develop as complete communities with a diverse mix of land uses, a range and mix of employment and housing types, high quality public open space and easy access to local stores and services*
- i. directing development to settlement areas, except where necessary for development related to the management or use of resources, resource-based recreational activities, and rural land uses that cannot be located in settlement areas*

- j. directing major growth to settlement areas that offer municipal water and wastewater systems and limiting growth in settlement areas that are serviced by other forms of water and wastewater services*
- k. prohibiting the establishment of new settlement areas.*

The proposed development provides for growth within the built-up area of Caledon through the intensification of an underutilized parcel of land. The development's compact built form contributes to a pedestrian friendly environment which has easy access to local stores and services. The subject lands are located adjacent to a commercial plaza and the proposed development provides for pedestrian connectivity from the site to the local commercial plaza. Furthermore, the development will make efficient use of existing municipal water and wastewater systems.

Furthermore, Provincial policy requires a minimum of 40% of all new residential development occurring annually to be located within the designated "Built-up Area" as part of each municipality's intensification target. Local municipalities are required to implement through their Official Plans strategies and policies to achieve the intensification targets. The proposed development contributes to the Town's intensification target by providing growth within the built-up area of the Town.

Section 2.2.3.7 of the Provincial Growth Plan states that intensification areas will be planned and designed to:

- a) "Cumulatively attract a significant portion of population and employment growth;*
- b) Provide a diverse and compatible mix of land uses, including residential and employment uses, to support vibrant neighbourhoods;*
- c) Provide high quality public open spaces with site design and urban design standards that create attractive and vibrant places;*
- d) Support transit, walking and cycling for everyday activities;*
- e) Generally, achieve higher densities than the surrounding areas; and*
- f) Achieve an appropriate transition of built-form to adjacent areas."*

The proposed development is an efficient use of land and infrastructure on an underutilized and vacant parcel of land within the Built-up area of the Town of Caledon. Further, the proposed development provides a compact built form and a range of residential uses which completes the build out of the surrounding community. The design of the development contributes to creating an attractive and vibrant neighbourhood which supports pedestrian activities.

Therefore, the proposed Draft Plan of Subdivision, Draft Plan of Condominium and Zoning By-law Amendment conforms to the objectives and policies in the Provincial Growth Plan.

5.3 Region of Peel Official Plan (Office Consolidation October 2014)

The subject lands are within the Rural System and designated “Rural Service Centre” as per Schedule D – Regional Structure of Peel Region’s Official Plan. As per the Region’s Official Plan, growth is to be directed towards the Rural Service Centres in the Town of Caledon which include Bolton, Mayfield West and Caledon East. The subject lands are located within the Rural Service Centre for Bolton. The Regional Official Plan states:

“5.4.3.2.1 Designate three Rural Service Centres, as shown on Schedule D, as locations for growth outside of Peel's Urban System, providing a range and mix of residential, commercial, recreational and institutional land uses and community services to those living and working in the Rural System.”

“6.3.2.1 Require and provide full municipal sewage and water services to accommodate growth in the Urban System to the year 2031, and the three Rural Service Centres to the year 2021. The provision of full municipal sewage and water services in the Urban System and the three Rural Service Centres will be subject to the Regional financial and physical capabilities.”

The proposed development provides for a mix of residential uses within the Rural Service Centre of the Region’s Official Plan. Municipal water and sewage services are available adjacent to the proposed development and as demonstrated through the FSR prepared by RAND Engineering Corporation, these services have the capability of servicing the proposed development.

5.4 Town of Caledon Official Plan

The subject lands are located within the 2021 Settlement boundary within the Rural Service Centre of Bolton as per Schedule C - Bolton Land Use Plan of the Town’s Official Plan. The Town’s Official Plan outlines the following housing objectives and policies:

“3.5.2.1 To promote and foster the development of a diverse mix of housing types and tenure in order to meet the needs of current and future residents of the Town of Caledon, including the needs of different income groups, people with special needs and accessibility challenges and the needs of people through all stages of their lives.

3.5.2.2 To encourage all forms of residential intensification in parts of built up areas that have sufficient existing and planned infrastructure and community services.

3.5.3.1 *In conformity with the Strategic Direction of the Plan, the majority of new housing shall be located in settlements where full water, sewer and community support services can be provided in an effective and efficient manner.*

Other settlements which have limited capacity to absorb and support new housing, especially housing at medium to high densities are planned to have a more moderate increase in housing.

3.5.3.5.1 *A full range of housing types and densities will be planned as an integral part of each Rural Service Centre.”*

The proposed development achieves the above housing objectives and policies through the development of a mix of housing types and tenures which provide housing options to meet the needs of different income groups. The proposed development also provides an alternative housing option for people at different stages in their lives.

The proposed development represents an appropriate form of intensification within the built-up area that has regard for the scale and type of housing in the adjacent neighbourhood. Further, there are existing services available to service the proposed residential development, including but not limited to existing water and wastewater infrastructure, existing schools and existing parks.

Section 3.5.3.8 of the Official Plan contains policies related to universal design which are meant to encourage the design of housing to provide for the ability for its occupants to make future changes easily and with minimum expense to meet their evolving needs. The proposed development has had regard for providing universal design options to the townhouse units. Villalago Residence Inc. intends to provide an option to perspective purchases to have a self-contained unit (bedroom, washroom and kitchenette) on the ground floor should they opt for their alternative design. This design option meets the intent of the Town’s universal design policies by providing new residential units within intensification areas with universal design features.

The Town’s Official Plan contains the following applicable growth management policies with respect to intensification:

“4.2.1.3.1 *Caledon will encourage intensification within the built-up area and undelineated built-up areas shown on Figure 1 of this Plan and will work to overcome barriers to intensification, where consistent with Section 3 of the Provincial Policy Statement.*

4.2.1.3.4 *To 2031, the minimum amount of residential development allocated within the built-up area shall be 1,500 units.*

The subject lands are located within the delineated built-up areas as shown on Figure 1 of the Town's Official Plan and are therefore appropriately located for development and intensification. The proposed development contributes to the Town achieving their minimum residential development allocation target for the built-up area.

The Town's general settlement area policies reiterate:

"5.10.3.5 Development of settlements will occur in an orderly manner that make efficient use of services, and discourages scattered or fragmented land development."

The proposed development is categorized as an infill development which completes the development surrounding community. Currently, Queensland Crescent and Stella Crescent end at the subject lands and it is clear that the intended that these two streets connect. The proposed development links Queensland Crescent and Stella Crescent through a proposed municipal road which will connect the existing community to the proposed development. This represents an orderly development within the settlement which makes efficient use of existing services and provides for connections to the surrounding neighbourhood.

"5.10.3.10 The land uses and the design of any proposed development will be compatible with, or enhance, the community character of the settlement, and development will be compatible with the land use patterns, densities, road systems, parks and open space system, and streetscape(s) of the community."

The proposed residential development provides a compatible land use pattern and scale of development to the surrounding neighbourhood. The surrounding neighbourhood is comprised of a mix of townhouses dwellings and single detached units and although the proposed development contains a more contemporary design, the scale and height of the proposed development is consistent with the townhouse dwellings along Queensland Crescent and Stella Crescent. The development also enhances the community by providing for a link between Queensland Crescent and Stella Crescent and providing pedestrian connectivity to Queen Street South and the commercial plaza.

5.4.1 Bolton South Hill Secondary Plan

The Bolton South Hill Secondary Plan Area is nearly built out and the subject lands are one of the last remaining underutilized parcel of land within the area. The subject lands are designated lands High Density Residential and Mixed Low/Medium Density Residential on Schedule C-2 – Bolton South Hill Land Use Plan. Figure 6 shows identifies the Secondary Plan designations on the subject lands.

The general policies of the Secondary Plan identify that *“senior citizen housing and other similar uses will be encouraged to locate in close proximity to commercial facilities, in the vicinity of Queensgate Boulevard and Highway 50”* (Section 7.2.4.8). Villalago Residence Inc. intend to provide an option for perspective purchases for a ground floor plan which would include a bedroom, washroom and kitchenette. This design option provides an alternative for seniors who are interested in the proposed development.

The southern portion of the subject lands are located within the Mixed Low/Medium Residential designation. The Bolton South Hill Secondary Plan states that the Mixed Low/Medium Residential designation permits townhouse units with a minimum density of 16 units per net hectare with a maximum density of 24.8 units per net hectare. Section 7.2.5.3 contains the following policies with regards to Mixed Low/Medium Residential:

- “7.2.5.3.1 Development in the Mixed Low/Medium Residential designation shall occur at a density of not less than 16 units per net ha with a maximum on 24.8 units per net ha where net hectare is defined to be exclusive of public rights-of-way, parks, school sites, environmental policy area and open space policy area. An overall average density of 19.8 units per net hectare shall be achieved on those lands designated Mixed Low/Medium Residential.*
- 7.2.5.3.2 Permitted uses shall include single-family, residential semi-detached units, link units, street and en-bloc townhouse units.*
- 7.2.5.3.3 There shall generally be no more than 6 units per townhouse block.*
- 7.2.5.3.4 To achieve the housing mix goals of the Official Plan which are to provide approximately 30% of housing units within the South Hill Area for medium and high density purposes, each plan of subdivision submitted shall be encouraged to have approximately 30% of the proposed units for medium density development.”*

The northern portion of the subject lands are designated High Density Residential. High Density Residential permits the following:

- “7.2.5.5.1 Lands designated High Residential may be developed for high density residential uses with a density range of 49.4 to 85 units per net residential hectare. The maximum building height shall be 10.5 metres.*
- 7.2.5.5.2 Non-residential uses may be permitted in the ground floor provided they will not negatively impact residential development in the immediate vicinity. Site development shall include provision for suitable landscaping*

and building siting to ensure compatibility with the adjacent residential uses.”

The proposed townhouse dwelling units are permitted uses in both the Mixed Low/Medium Residential designation and the High Density designations. Section 7.2.5.1.2 of the Secondary Plan states that *“Development of residential areas shall be based on a net basis where net hectare is defined to be exclusive of public rights-of-way, parks-school sites, environmental policy area and open space policy area.”* Figure 7 clearly identifies the portion of the lands used to calculate the density of the two designations. The lands which will extend the public rights-of-way have been excluded from the calculation of density. However, the private condominium roads and private open space/amenity area have been included as part of the density calculation.

The Mixed Low/Medium Density portion of the subject lands contains a net residential density of 19.7 units per net residential hectare which is within the permitted density range outlined in the Secondary Plan policies.

The portion of the site within the High Density designation contains a net residential density of 46.7 units per hectare (101 units/ 2.16 ha). This represents a minor variation to the density requirements outlined in the Secondary Plan. The High Density Residential designation allows for some flexibility since it states these lands *“may be developed for high density residential uses with a density range of 49.4 to 85 units per net residential hectare”*. Further, the High Density designation specifies in Section 7.2.5.5.1 that the maximum building height shall be 10.5 metres. Since there is no definition for building height in the Official Plan, we have relied on the definition for building height in Zoning By-law 2006-50 which states that:

“Building Height means the vertical distance between the finished grade of the lot on which the building is situated and:

- a) The highest point of the roof surface of a flat roof or domed roof; or*
- b) The deck line of a mansard roof; or*
- c) The median level between the eaves and ridge of a gable, gambrel or hip roof.”*

Given that the finished grades have yet to be determined for the site and it is anticipated that the grades will vary across the proposed development, the proposed draft zoning by-law proposes an increased building height of 12.5 metres to accommodate the variations. This increase in building height is a minor variation to the building height to accommodate the varying grades which may have not been considered when the Secondary Plan restricted the building height in Section 7.2.5.5.1.

The proposed height increase in the draft zoning by-law is appropriate for the proposed development which assists the Town with their intensification targets within the built up area and the density requirements in the Secondary Plan. In addition, the proposed development provides for a larger built form adjacent to Queen Street South and provides a transition from the existing two storey residential units.

The proposed development and density of the site provides for an efficient use of land and infrastructure through a compact built form typical for intensification areas. The compact built form of the proposed development represents an appropriate form of intensification for the Bolton South Hill Secondary Plan. The built-form and associated density achieve a balanced approach to providing intensification within the built-up area and providing development that is compatible with the adjacent established residential area. It is our opinion that the proposed development achieves the objectives of the Secondary Plan and provides a built form that ensures compatibility with the adjacent residential uses.

In addition, the proposed development generally contains townhouse blocks with 6 units or less. However, there are 4 blocks that contain 7 units per townhouse block and 4 block which contains 8 townhouse units. Given that the majority of the blocks within the development have 6 units or less, the development complies with the general intent of Section 7.2.5.3.3 of the Secondary Plan.

The Bolton South Hill Secondary Plan contains the following policy related to the 5th Sideroad:

“7.2.9.4 Portions of the Fifth Side Road will be closed as development approvals are given and Queensgate Boulevard is extended towards the Albion-King Road. The alignment shown on the Land Use Plan is approximate and priority will be given to protecting access for existing residents on the Fifth Side Road.”

The majority of the 5th Sideroad has been closed as part of the development of the surrounding neighbourhood. Currently, the 5th Sideroad from Queen Street South, east towards Landsbridge Street remain opened. The Town has expressed their intention, through their DART comments, to close this portion of the 5th Sideroad so that the lands can be added to the proposed development. The 5th Sideroad lands have been included into the proposed development as requested by the Town and it is anticipated that these lands be transferred to Villalago Residences Inc.

The transportation policies of the Secondary Plan state:

“7.2.9.7 Reverse frontage development will be generally discouraged, except along the Albion-Vaughan/King Road, and the use of berming and/or service roads will be encouraged. Noise and vibration studies may be a

factor in determining the road layout for those developments located adjacent to the C.P. Railway line.”

The townhouse units along Queen Street South front onto Queen Street and connect with a sidewalk and pathway in order to provide a pedestrian friendly environment along the streets edge. These units along Queen Street South are accessed from the rear laneway. Additionally, reverse frontages have been proposed along Street “1” to provide a compatible and pedestrian friendly interface with the existing community.

A Transportation Noise Impact Study has been completed by Valcoustics in support of the subject applications. The study concludes that with appropriate design of the development, the applicable MOE noise guideline requirements will be met. Development of residential units have not been proposed adjacent to the C.P Railway line. The lands adjacent to the C.P Railway line have been proposed for the visitor parking spaces in order to make efficient use of those lands.

The Bolton South Hill Secondary Plan states:

“7.2.10.5 The final approval of specific development proposals will be based on the following:

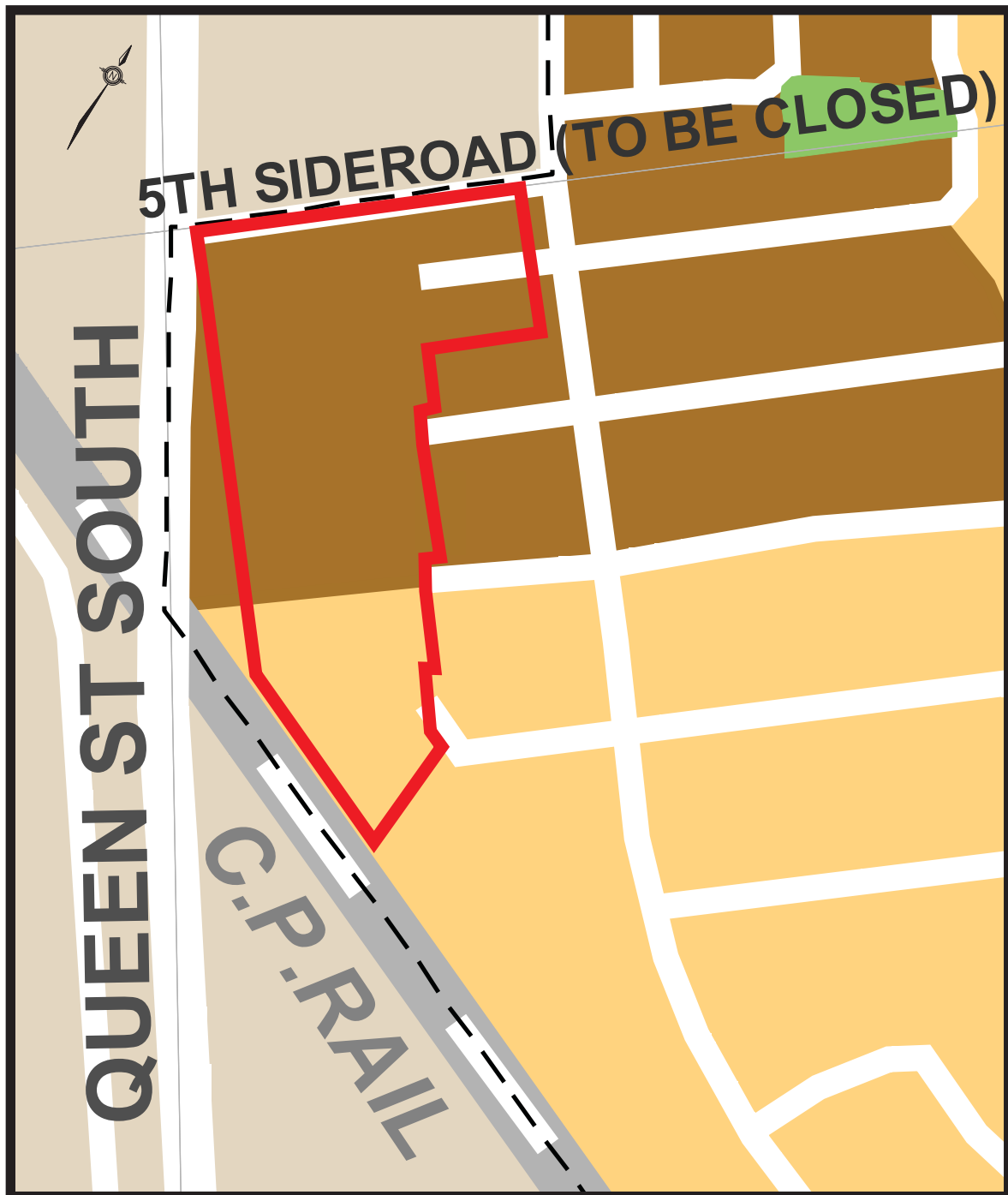
- a) Orderly provision of services and facilities*
- b) Sequential development of neighbourhood areas*
- c) Provision of schools and parks*
- d) Construction of the collector road system*
- e) Adequacy of the stormwater management system”*

The proposed development completes the surrounding neighbourhood as it is one of the last large underutilized parcel in the area. The development also proposes to utilize existing services adjacent to the subject lands as well as link the existing sections of Stella Crescent and Queensland Crescent by a municipal road. A network of private roads which are accessed off of the new municipal road and Landsbridge Street are proposed to service the condominium residential development. A private amenity area is also proposed on the southern end of the site to service the proposed development.

A stormwater management assessment has been prepared by RAND Engineering Corporation to ensure there is an adequate stormwater management system in place. The subject lands drain to a SWM Pond 4 located north east of the C.P. Railway crossing the Albion Vaughan Road. The Stormwater management report has determined that the Permanent Pool of this pond is sufficient to accommodate the proposed development.

The proposed Draft Plan of Subdivision, Draft Plan of Condominium and Zoning By-law Amendment applications comply to the policies of the Bolton South Hill Secondary Plan and are therefore in conformity with the Town of Caledon’s Official Plan.

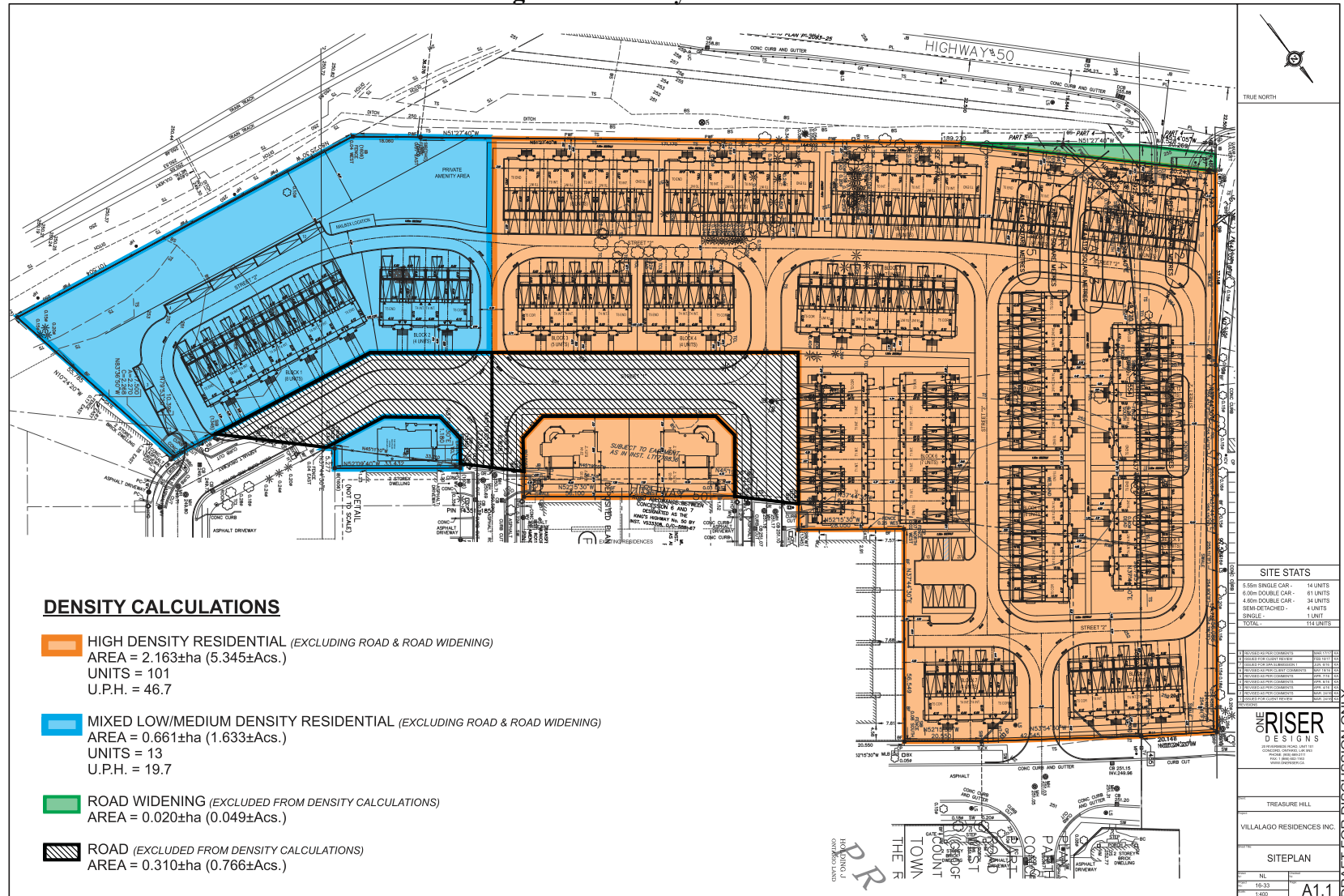
Figure 6 - Official Plan Split Designation
EXCERPT OF SCHEDULE C-2 TO THE CALEDON OFFICIAL PLAN



BOLTON SOUTH HILL - LAND USE PLAN

- | | | | |
|--------------------------------------|---------------------------|---------------------------|---|
| High Density Residential | Environmental Policy Area | Collector Road 30m R.O.W. | SUBJECT LANDS
Part of Lot 5 and Part of the Road Allowance between West Halves of Lots 5 and 6 Concession 7 and Blocks 118, 152-154, 165, 167, 178, 181 and 182, Registered Plan 43M-1251 Town of Caledon |
| Medium Density Residential | Open Space Policy Area | Collector Road 26m R.O.W. | |
| Low Density Residential | School | Local Road 22m R.O.W. | |
| Mixed Low/Medium Density Residential | Secondary Plan Area | Regional Road | |
| Special Residential | Bolton Settlement | Local Road | |
| | Future Grade Separation | Railway | |

Figure 7 - Density Calculation



5.5 Town of Caledon Zoning By-law 2006-50

The subject lands are currently zoned Prestige Industrial (MP-310), Townhouse Residential (RT-67) and Residential One (R1-68).

The MP-310 zone permits a range of Prestige Industrial uses. The MP-310 zone is not consistent with the land use designation and policies described by the Town's Official Plan as discussed in the above sections.

The RT-67 zone permits Townhouse dwellings and private home day care. The R1-68 zone permits detached dwelling, apartment accessory, private home day care and home occupation.

A Zoning By-law amendment will be required to implement the proposed development and is included as part of the submission materials.

6.0 Summary

As discussed in the above sections, the proposed residential development is consistent with the objectives and intent of the Provincial Policy Statement, Provincial Growth Plan, Peel Region Official Plan and the Town of Caledon Official Plan. The proposed development provides infill development within the built-up area and takes advantage of the existing infrastructure while providing for the anticipated housing needs in a density and compact built-form that is land efficient, pedestrian-scaled and which contributes to shaping vibrant neighbourhoods.

Further, the proposed built-form is compact, appropriately massed and articulated to define a pedestrian scaled street edge along Queen Street South and Street "1" contributing to an attractive urban streetscape while providing an appropriate built-form scaled to the surrounding residential neighbourhood. The development also contains a continuous pedestrian connection through the site from Queen Street South to the existing community and commercial plaza.

Given the above, it is our opinion that the Draft Plan of Subdivision, Draft Plan of Condominium and Zoning By-law amendment represents good planning.



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