TULLAMORE COMMUNITY DESIGN GUIDELINES

The Corporation of the
Town of Caledon

Prepared by:

Paul Cosburn Associates Limited

Adopted by the Town of Caledon Council

May 15, 2000

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TULLAMORE COMMUNITY DESIGN GUIDELINES

1.0 INTRODUCTION

The Tullamore Community is located on the north side of Mayfield Road at the intersection of Airport Road and Mayfield Road, as illustrated on Figure 1. It is currently dominated by active agricultural land although it contains a mixture of some commercial, industrial and residential land uses. The Tullamore Industrial/Commercial Secondary Plan provides direction for future development in the area as illustrated on Figure 2.

2.0 DESIGN GUIDELINES... SCOPE AND INTENT

The purpose of this document is best described by the following objectives:

a) To establish a coordinated approach to landscaping in the area that will assist in creating an attractive and desirable community, which could be used as strong marketing tool by the developers and the Town.

b) To provide direction to individual developers and their Landscape Architects when preparing the detailed plans for each subdivision and site plan. This should have the effect of simplifying and accelerating the drawing approvals process.

c) To assist the Town staff in the review and approval of each individual subdivision and site plan application. This will ensure that plans are consistent with the overall "vision" for the Tullamore Community.

The following document deals with design guidelines and landscape standards as they pertain to minimum expectations for landscape performance, materials, etc. to be employed in the development of a cohesive community.

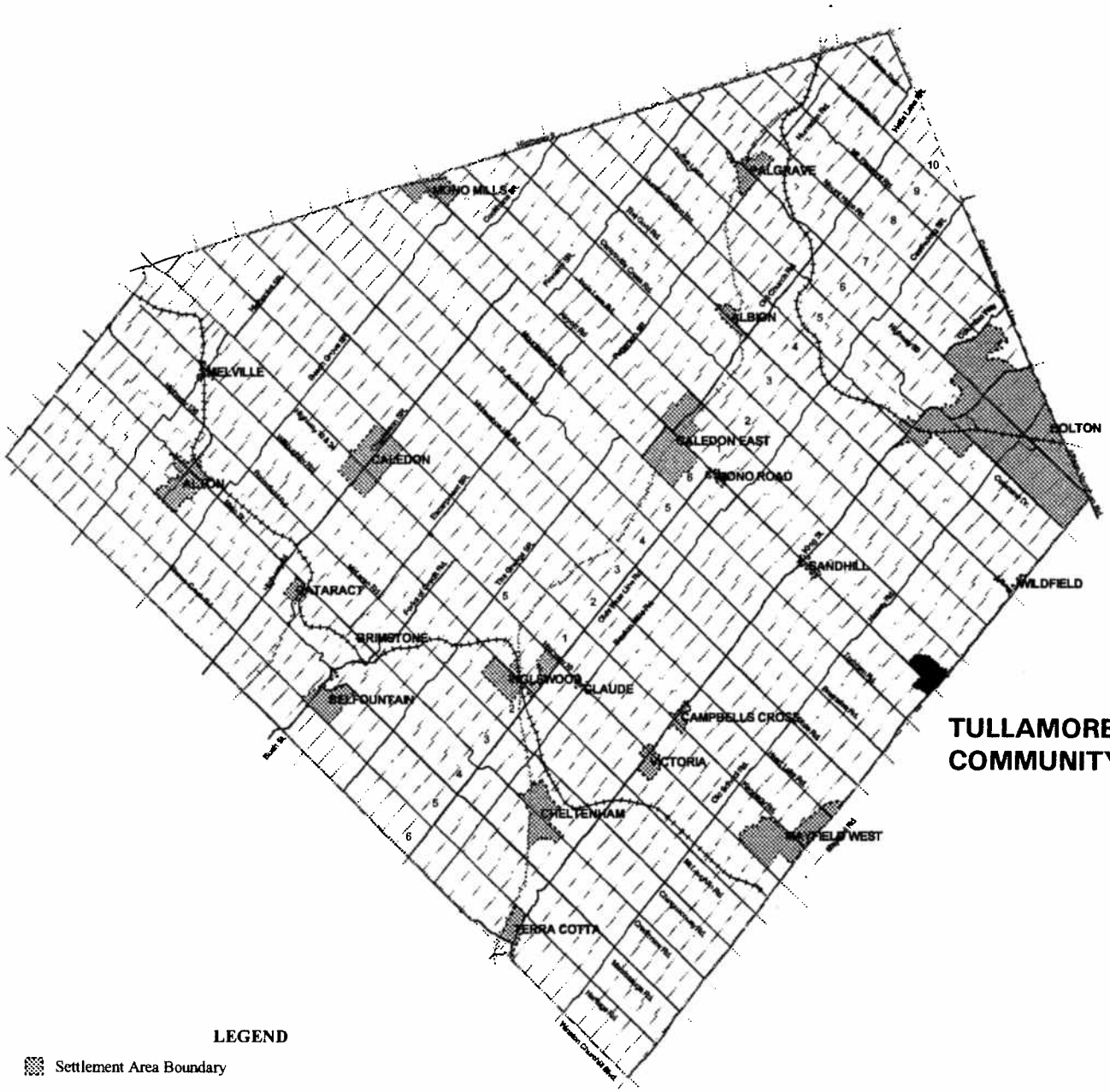
The design guidelines are made up of written descriptions, plan views, and cross sections to be considered by an applicant, to design the streetscapes, select the trees and shrubs, and create a scheme that will be in keeping with the character established by this document. In several cases, alternative options are provided for the treatment of specific areas.

Various landscape standards are contained in Appendix A of this document. They include construction details, specifications and notes specific to the various landscape categories and are to be considered the minimum technical requirements for development.

All the landscape standards in this report (found in construction detail format) are available to the developer or landscape architect in a digital format so that they can be imported directly into computer generated landscape drawings.

The consultant for each development site is responsible for reviewing this package and ensuring the site is designed within the parameters established by these standards and guidelines, and that the relevant details and specifications are included in the landscape submission.

FIGURE 1

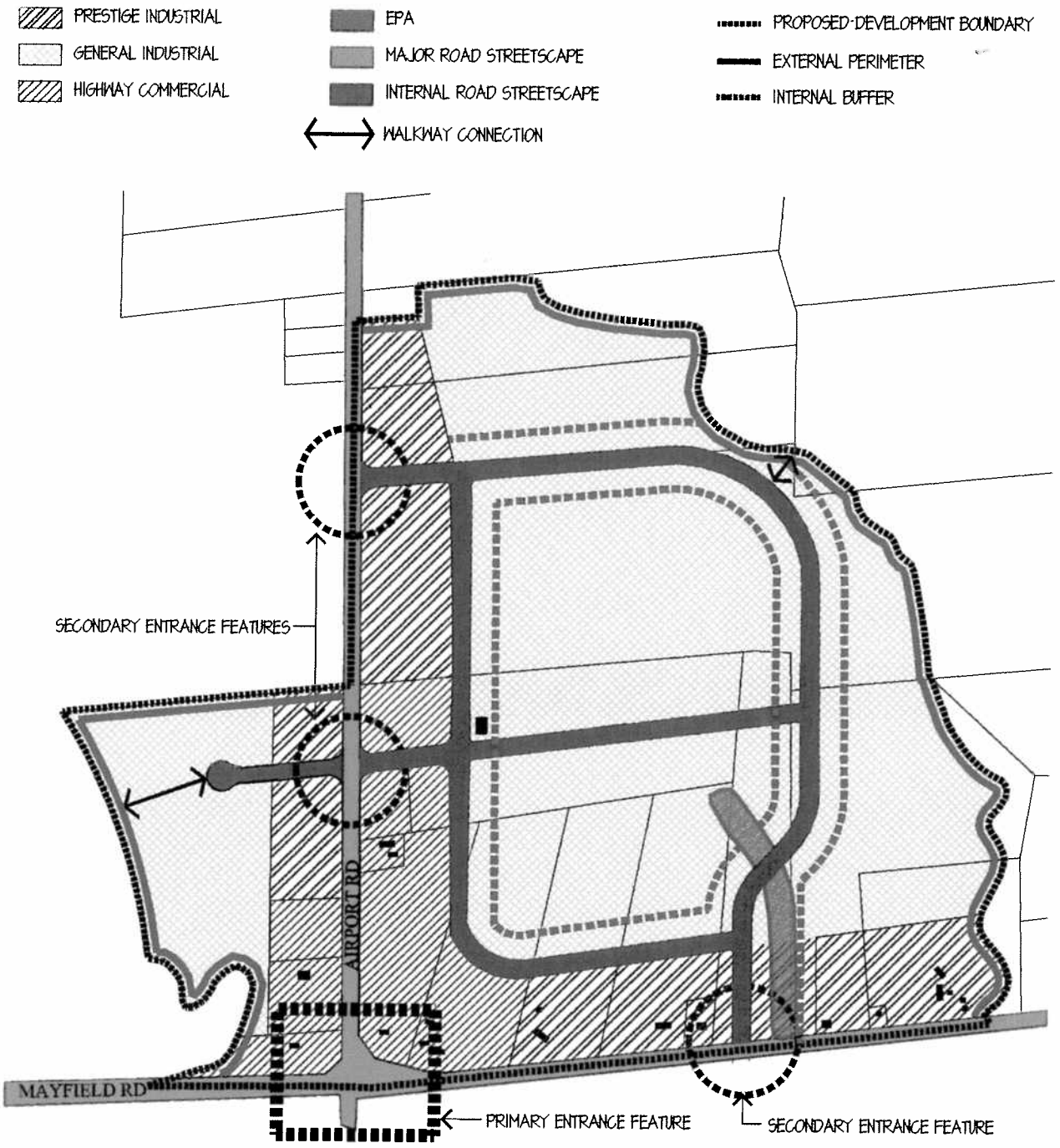


LEGEND

- Settlement Area Boundary
- Lot & Concession Lines
- Rail Trails
- Rail Road Lines
- Local Roads
- Regional Roads
- Provincial Roads

TOWN OF CALEDON

FIGURE 2



THE CORPORATION OF THE TOWN OF CALEDON

TULLAMORE COMMUNITY DESIGN GUIDELINES

3.0 *MAJOR ROADS – STREETSCAPE*

The Tullamore Secondary Plan has two major roadways – Airport Road and Mayfield Road. These roadways provide frontage for primarily commercial and prestige industrial land uses. These guidelines address both design features within the roadway right-of-way of these major roadways, and the frontage of the adjacent properties.

3.1 Public Lands

The ultimate width and design of Airport Road and Mayfield Road had not been determined at the time these guidelines were prepared. Only preliminary design concepts for the enhancement of the right-of-way are illustrated in this document. These concepts are recommendations to the Region of Peel for their consideration during the detailed design of the roadway.

3.1.1 Boulevard Treatment

Based on experience with other major roadways, the opportunities for boulevard planting are limited by the location of aboveground and underground services. However, during the future design of the roadway it is recommended that primary and secondary planting locations be established within the road cross section. Trees of 100 caliper with a 2.5m minimum branching height are recommended at 7.5m o.c. average spacing along the boulevards.

The concepts envision an urban cross-section for the ultimate road development. Interim planting programs will be considered with each phase of road reconstruction. Although commercial and industrial property will be developed prior to the completion of the ultimate roadway, adjacent property owners are advised that landscape treatments similar to that proposed in Figure 3a will be implemented.

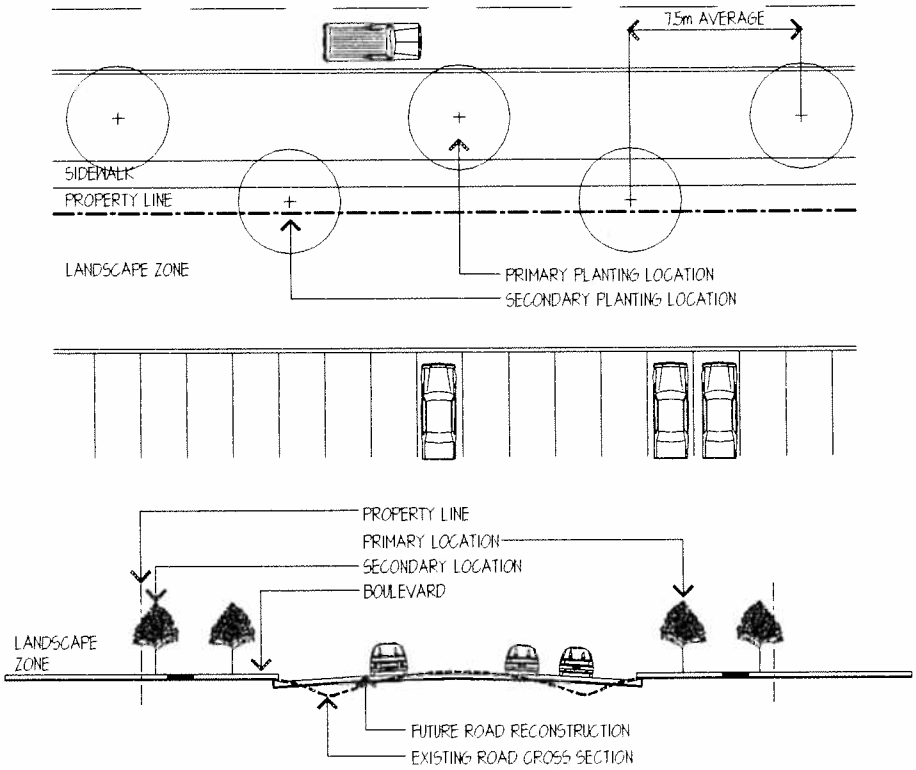
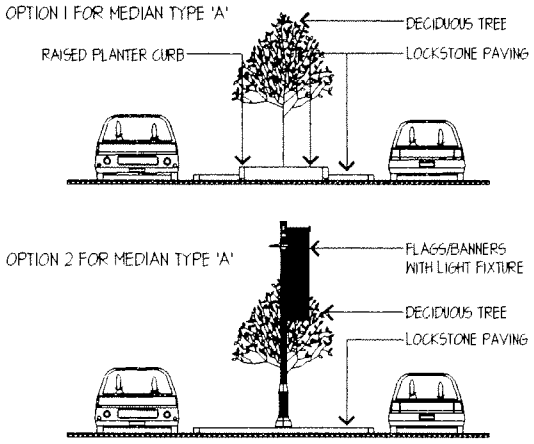


FIGURE 3a

3.1.2 Median Treatment

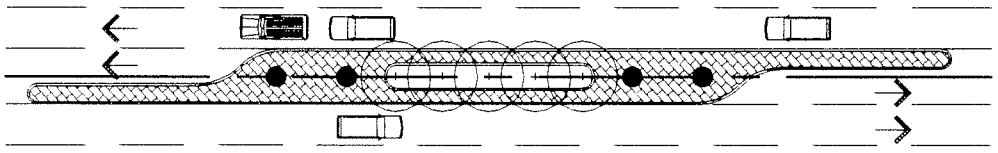
Median treatments, similar to that illustrated in Figure 3b, are recommended to the Region of Peel for consideration in preparing the ultimate designs for Airport Road and Mayfield Road.



MEDIAN TYPE 'A'

- **At the intersection of Mayfield Road and Airport Road**

FIGURE 3b



MEDIAN TYPE 'B'

- **North along Airport Road**
- **East and west along Mayfield Road**

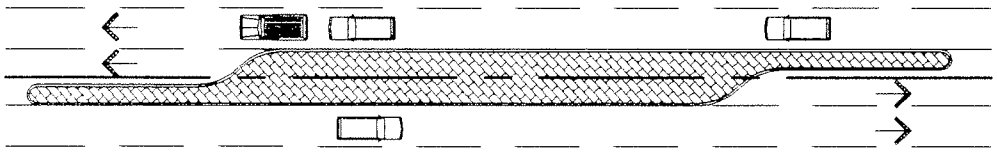


FIGURE 3c

3.2 Private Lands

The design of lands fronting Airport Road and Mayfield Road will include the following design elements:

- Landscape Zones
- Decorative Elements
- Site Entrances
- Building Façade Treatment
- Landforms
- Planting
- Peninsulas
- Sideyard Treatment

3.2.1 Landscape Zones

Highway commercial frontage along major roads shall have a 6.0m wide Landscape Zone.

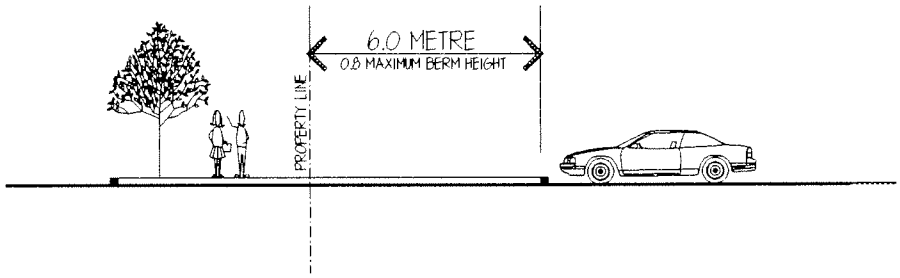


FIGURE 3d

For all prestige industrial frontage along major roads, the Landscape Zone shall be 12.0m in width.

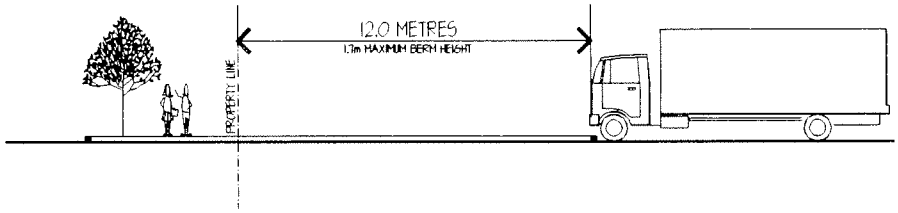


FIGURE 3e

3.2.2 Landforms

3.2.2.1 Height and Slope

Landscape Zone within highway commercial properties and prestige industrial properties along major roadways will be dominated by 3:1 slopes reaching heights between 0.6m to 0.8m and 1.2m to 1.7m respectively. This landform treatment will reduce visibility of parking areas. The combination of sloping berms and decorative elements (ie. limestone ledgerock and granite stone) will be encouraged.

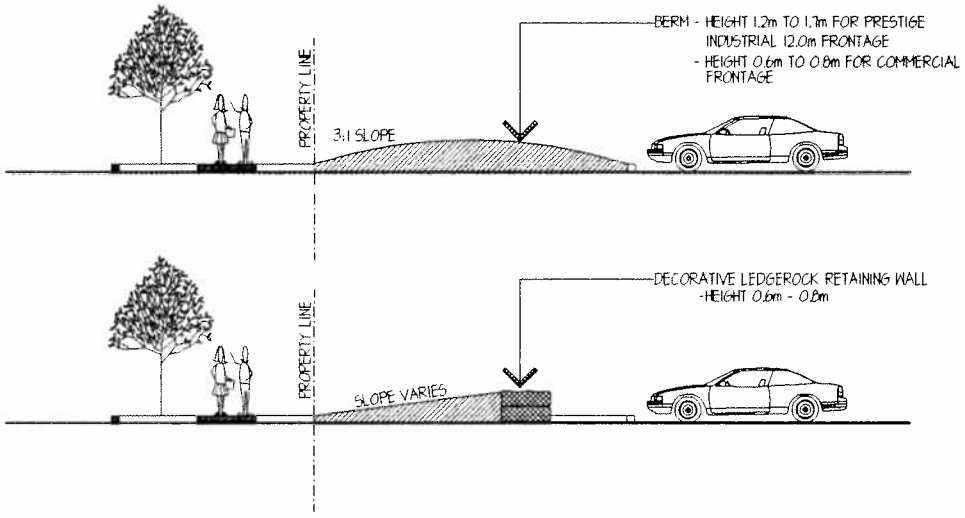


FIGURE 3f

3.2.2.1 Style

In both highway commercial and prestige industrial lands, the landform will have a rectilinear style.

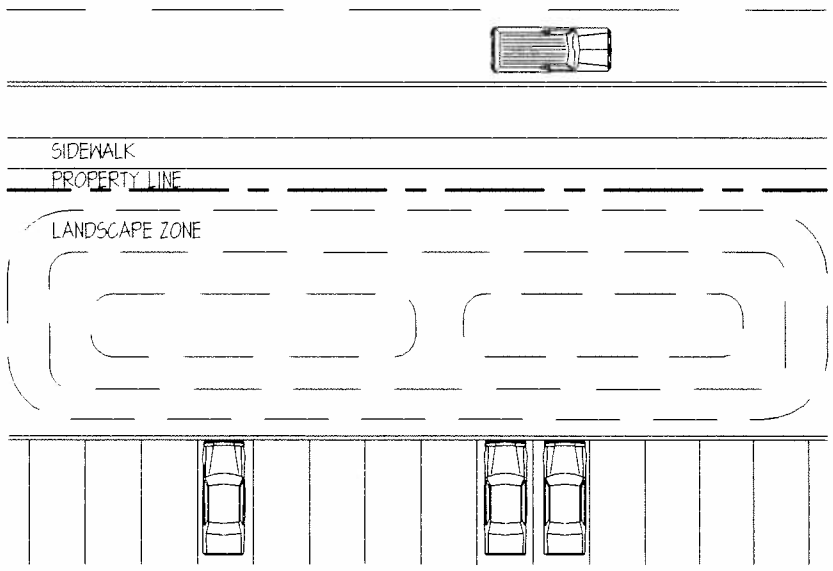


FIGURE 3g

3.2.3 Decorative Elements

3.2.3.1 Types

Highway commercial and prestige industrial properties along major roadways will contain limestone ledgerock and granite stones as decorative elements within the landscape zones.

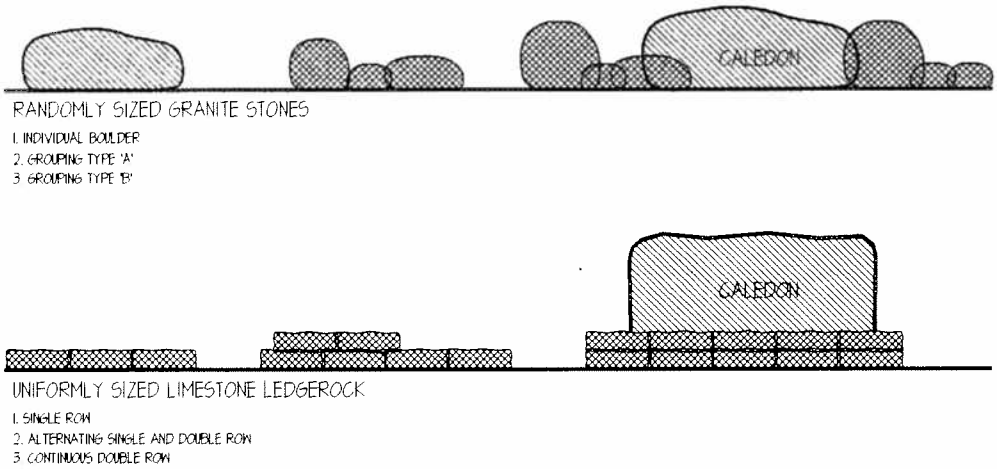


FIGURE 3h

3.2.3.2 Density

The decorative elements will be used with a frequency of 5.0 lin.m. per 10.0 lin.m. of frontage for limestone ledgerock and 2.5 sq.m. per 10.0 lin.m. of frontage for granite boulders. The ledgerock may vary in thickness from 250 to 750 mm in one or two bedding courses and have an average width of 1.0 m. The granite stones shall be arranged in well-defined beds.

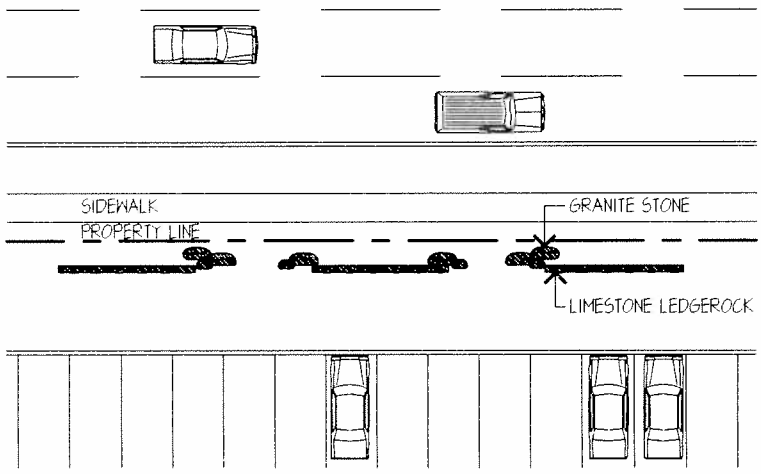


FIGURE 3i

3.2.4 Planting

3.2.4.1 Size

For highway commercial and prestige industrial properties along major roadways, all deciduous trees shall be 50, 60 or 70 caliper in size and all coniferous trees shall be 1.5, 2.0, 2.5m in height (in equal proportions).

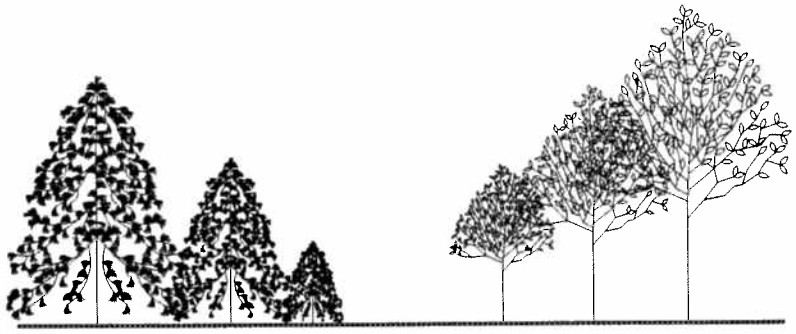


FIGURE 3j

3.2.4.2 Density and Style

For highway commercial and prestige industrial properties along major roadways, deciduous trees shall be planted at a rate of one tree per 10 lin.m. of frontage, coniferous trees at one tree per 20 lin.m. of frontage and 2 shrubs per lin.m. of frontage. Daylilies will be planted in a continuous double row at a spacing of 4 plants per lin.m. of frontage for all highway commercial properties. Daylily beds along prestige industrial properties will be planted at the discretion of the developer. For highway commercial and prestige industrial properties along major roadways, plantings shall be installed in a 'linear' form.

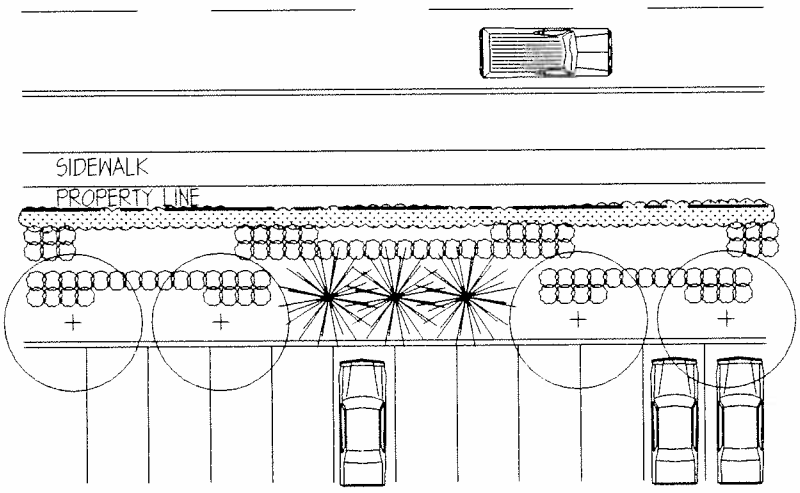


FIGURE 3k

3.2.5 Site Entrances

All site entrances for highway commercial properties will provide accent areas with decorative features and planting. The density and style of the landscape treatment shall be an extension of the frontage design. All landscape treatment will be low in profile to ensure that visibility is not hampered.

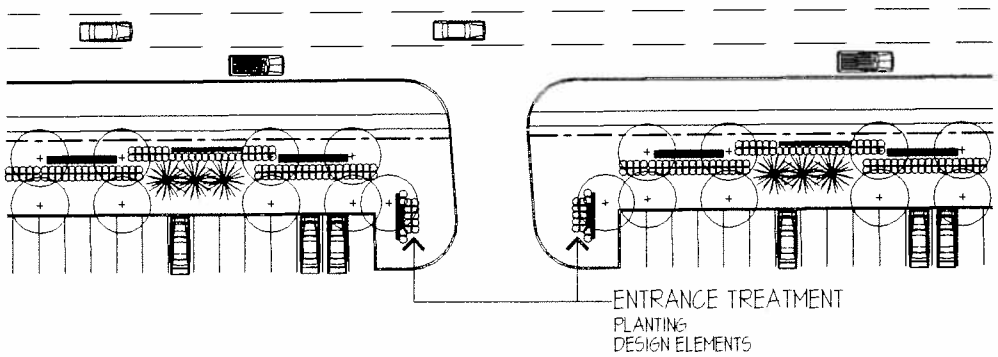


FIGURE 3l

3.2.6 Peninsulas

For highway commercial properties along major roads, peninsulas will be provided in the parking areas at the rate of one peninsula (5.0m minimum width) for every 25 parking stalls.

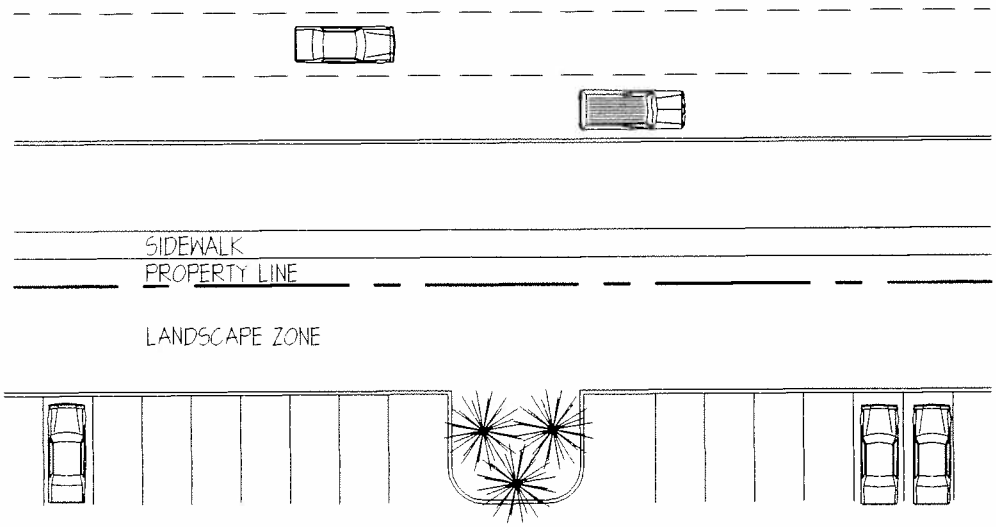


FIGURE 3m

3.2.7 Building Façade Treatment

Shrub and tree planting along the front building façade is strongly recommended.

3.2.8 Sideyard Planting

Planting along the yards from the front corner of lot to the building is strongly recommended except when joint entrances are provided.

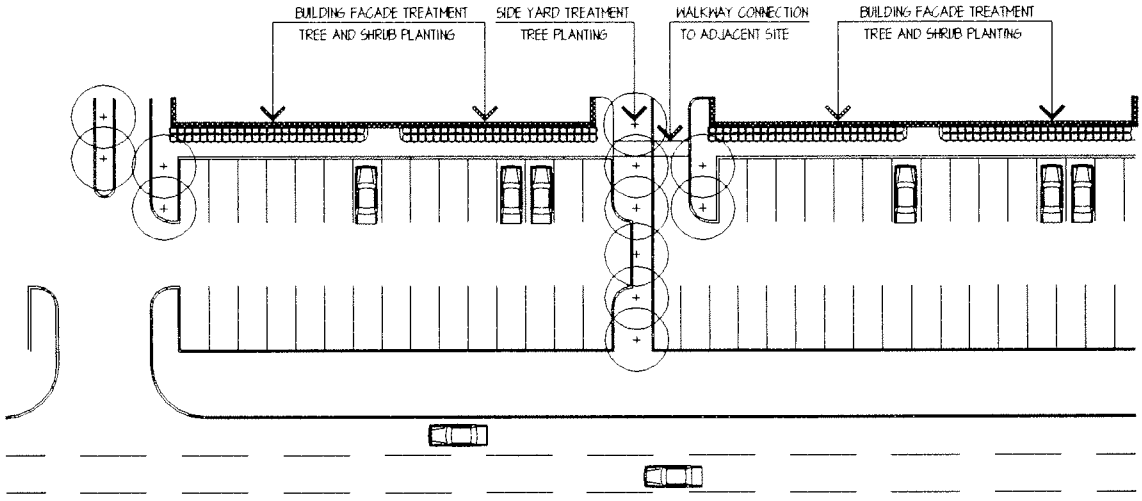


FIGURE 3n

4.0 INTERNAL ROADWAYS ... STREETSCAPE

4.1 Public Lands

4.1.1 Boulevard Treatment

On internal roadways, a 2.0m wider municipal road right-of-way is recommended to facilitate additional planting opportunities along the streetscape. Planting plans will use both the conventional planting location and the new planting location created by the boulevard widening.

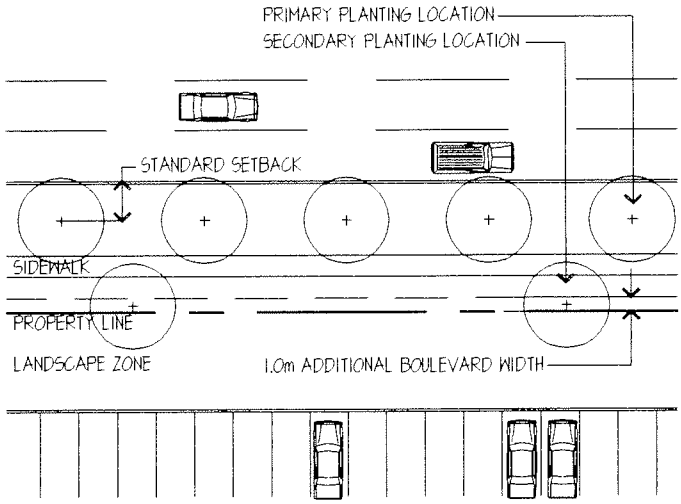


FIGURE 4a

4.2 Private Lands

4.2.1 Landscape Zone

An 8.0m wide landscape zone is recommended for all land uses along internal roads.

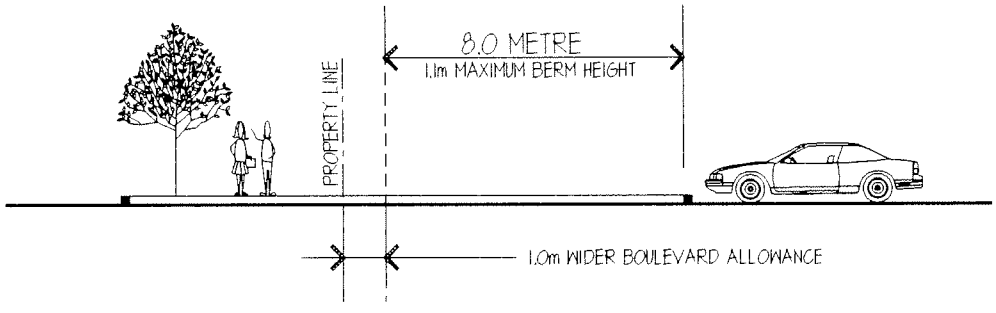


FIGURE 4b

4.2.2 Landform

4.2.2.1 Height and Slope

The Landscape Zone along all internal roads will be dominated by 3:1 slopes and conform to a rectilinear grading style.

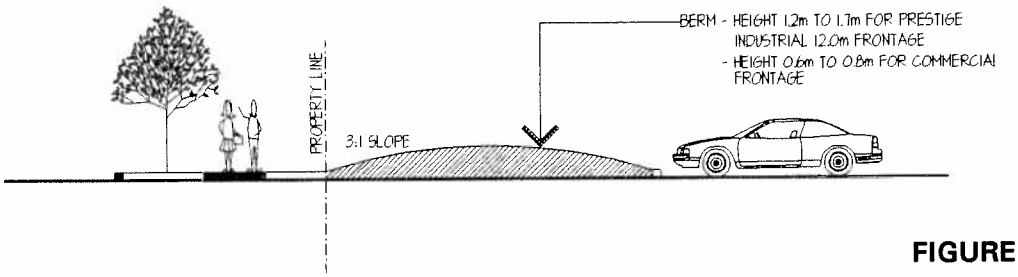


FIGURE 4c

4.2.3 Planting

4.2.3.1 Size

Deciduous trees shall be 50, 60 or 70 caliper in size and all coniferous trees shall be 1.5, 2.0, 2.5m in height (in equal proportions).

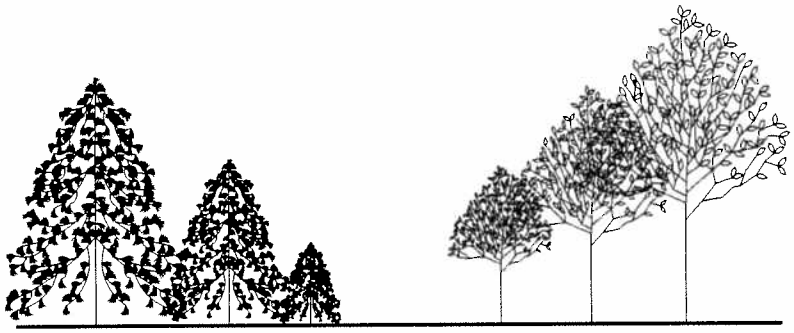


FIGURE 4d

4.2.3.2 Density and Style

All planting will conform to a density of 1 deciduous tree per10.0 lin.m of frontage, one coniferous tree per 15.0 lin.m frontage and one shrub per lin.m of frontage. Planting style shall be at the discretion of the owner.

5.0 PRIMARY ENTRANCE - GATEWAY

The intersection of Airport Road and Mayfield Road forms an important vehicular gateway into the Town of Caledon, as illustrated on Figure 5a. To accentuate the entrance into the municipality, a strong landscape treatment and signage is recommended. In combination with municipal signage, the landscape treatment will also recognize the Tullamore Commercial and Industrial Community. The accompanying concept plan illustrates the level of treatment envisioned for the intersection.

The gateway is situated on private property and is subject to a satisfactory agreement being reached with the respective landowners. In addition, the location and configuration of the design is strongly dependent on future roadway improvements at this intersection.

The gateway design has an estimated construction cost of $150,000.00. It is anticipated that this cost will be distributed between the Tullamore development community and the Town of Caledon.

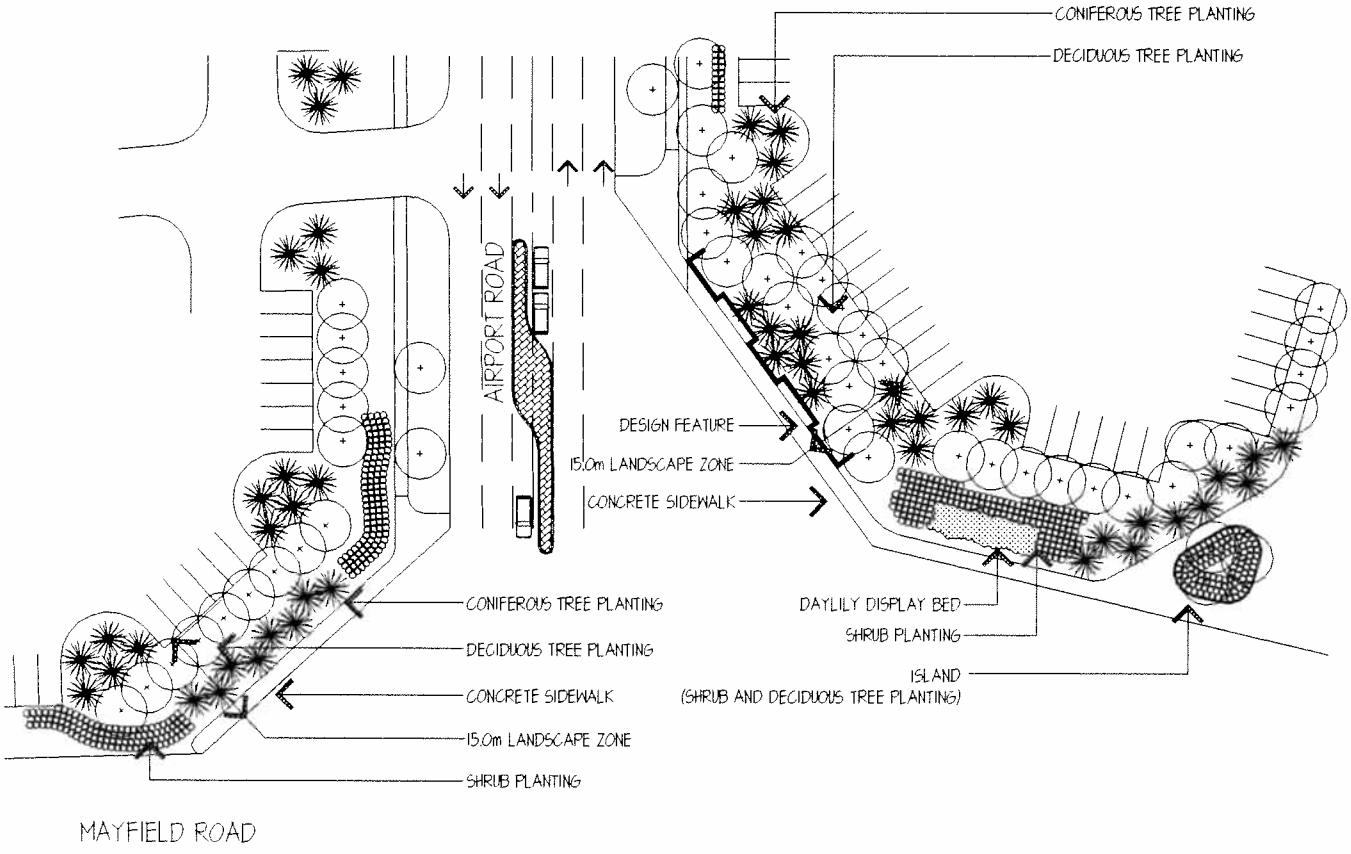


FIGURE 5a

6.0 *SECONDARY ENTRANCE*

Along Airport Road and Mayfield Road there are three proposed intersections. At each intersection, as illustrated on Figure 2, landscape treatment will enhance the streetscape and provide signage.

The landscape features at each intersection will be positioned on private property along the "daylight triangles". The anticipated cost for the construction is $150,000.00 for a complete intersection and $75,000.00 for half intersection.

The construction cost of the secondary entrances will be the responsibility of the Tullamore development community.

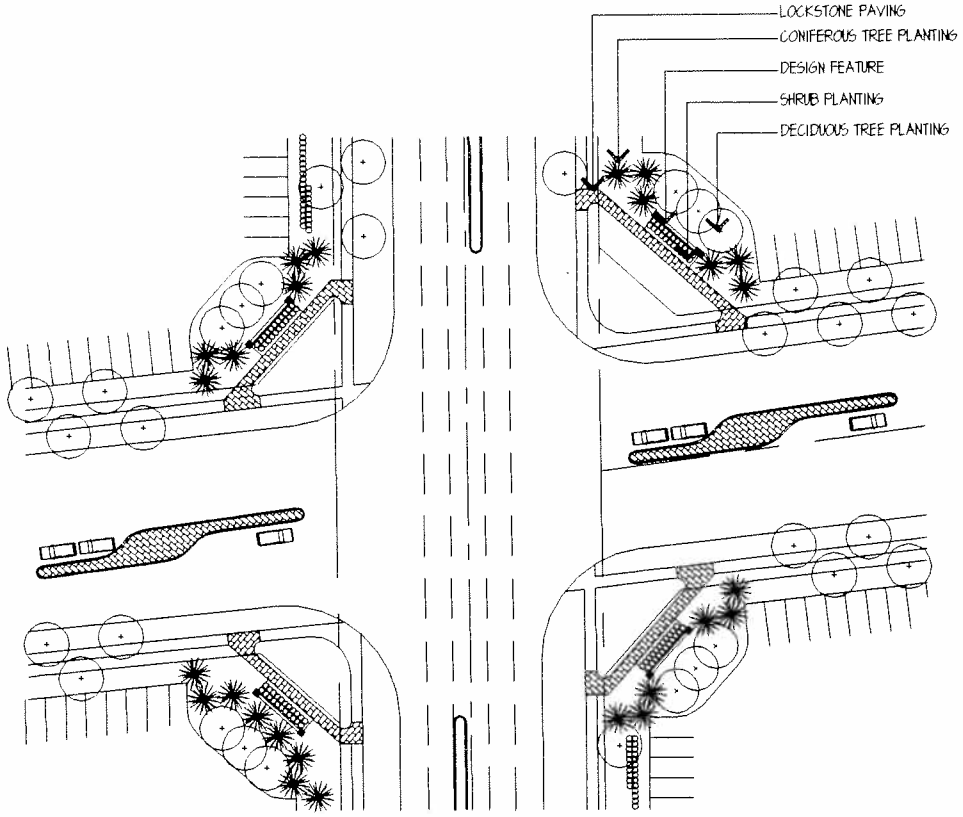


FIGURE 6a

7.0 BUFFERS

Internal and External Buffers are required within the Tullamore Community. External Buffers will be provided around the limit of the secondary plan area where there are adjoining agricultural lands or Environmental Policy Areas as illustrated on Figure 2. These buffers, when situated near major roadway, will provide both a visual buffer and a planting area to enhance any adjacent environmental features. Internal Buffers will be provided to reduce visibility and improve the streetscape appearance of commercial and industrial properties.

7.1 External Buffer

The width of the external buffer zone will be 15.0m and will contain a naturally shaped landform. The height of the landform will depend on the nature of the adjoining land uses and the potential visibility of the facilities on these lands. As a minimum, landforms will be 1.5m in height.

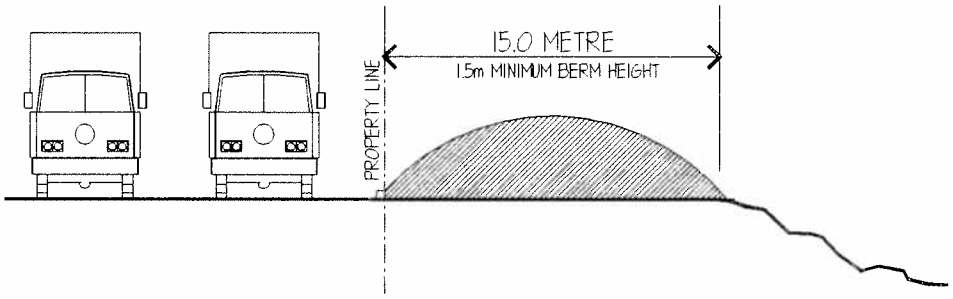


FIGURE 7a

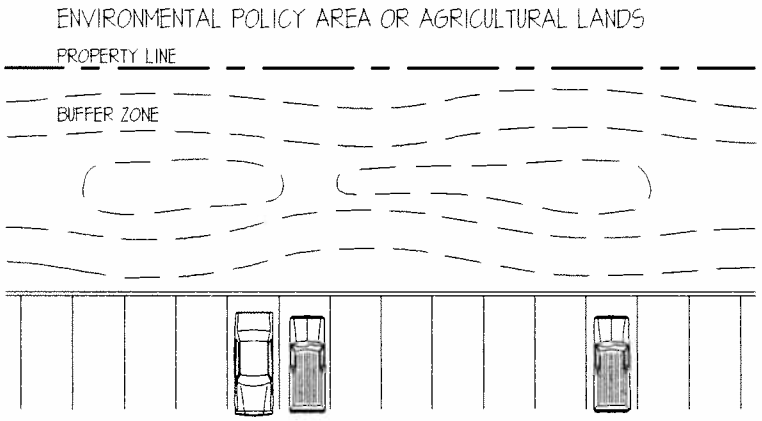


FIGURE 7b

When the buffers are situated adjacent to an environmental policy area, the planting program will focus on environmental enhancement. It will form a continuous ecological screen and use native tree, shrub and groundcover species. It will be low maintenance allowing plants to naturally spread and colonize. The planting style will have a natural form, using deciduous trees 2.0, 2.5, 3.0m in height and coniferous trees 0.75, 1.0, 1.25m in height, in equal proportions. The planting density will be an average of one deciduous tree per 2.5 lin.m buffer, one coniferous tree per 2.5 lin.m buffer and 4 shrubs per 1.0 lin.m. of berm.

Where the external buffer is situated near a major roadway, applicants must prepare designs and demonstrate the grading and planting program will reduce the visibility of the commercial or industrial property.

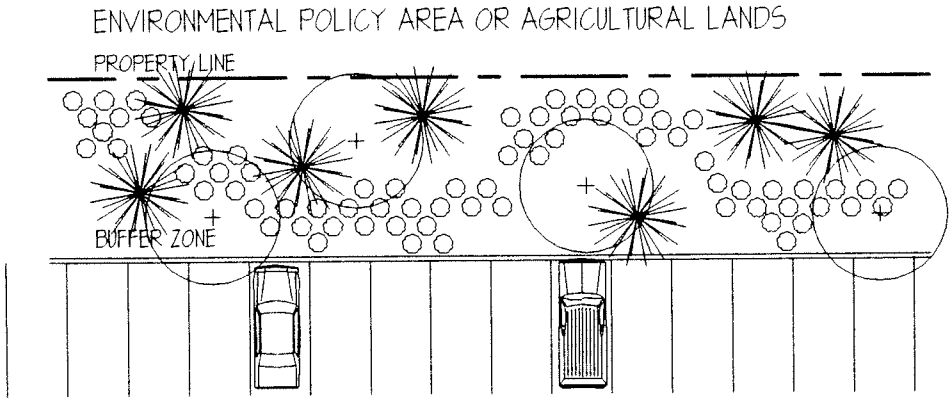


FIGURE 7c

7.2 Internal Buffer

Buffering views from internal roadways into industrial land uses is an important landscape requirement. Due to the wide variation in facilities and site plan configurations for industrial uses, specific guidelines are not provided in this document. However, as a general rule, the landscape treatment used in buffering industrial uses must have aesthetic appeal and block or screen views to the full height of industrial facilities when being viewed from adjacent public roadways. The Town of Caledon may require the appropriate cross sections to demonstrate that this design requirement is met with each site plan application.

8.0 APPROVALS AND IMPLEMENTATION PROCEDURES

8.1 Approvals

A Landscape Architect will be hired by a developer to prepare the necessary drawings and cost estimates for the landscape components of a new subdivision or a site plan. These Community Design Guidelines address the streetscape design requirements for the Tullamore Secondary Plan Area, the landscape design of secondary entrances, boulevards, landscape zones and buffers.

After the developer's Consulting Engineer has prepared the complete utility plan for a new subdivision, the Landscape Architect will use this as the base plan for the streetscape and entry designs. Similarly, the landscape architect will use information provided by other design team members in preparing designs for site plan approval.

All landscape site plan submissions will conform to the requirements available at the Town of Caledon at the time of submission.

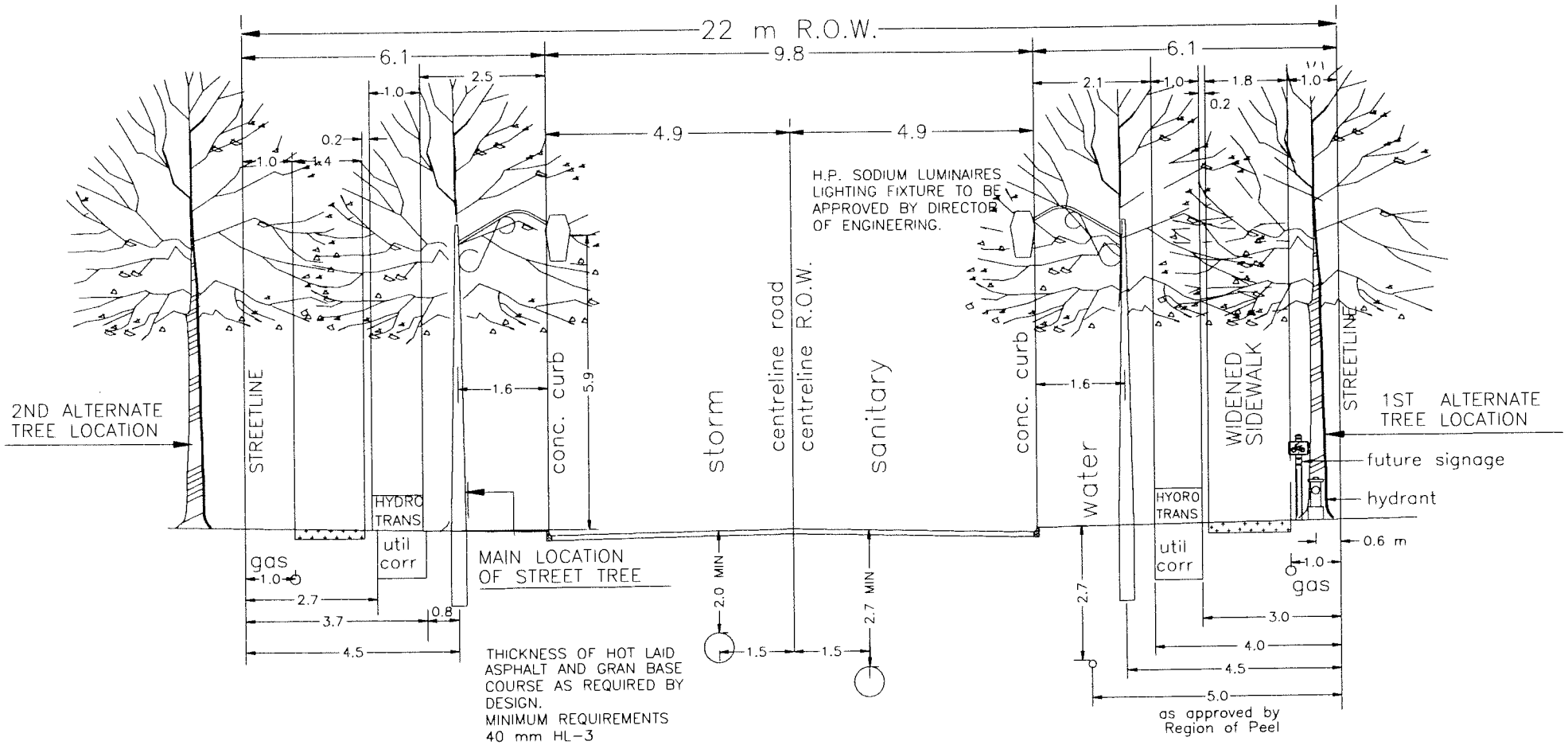
8.2 Implementation Procedures

After the infrastructure of the subdivision has been constructed, the street tree plantings will be established in conjunction with each site plan application until application is made for assumption of subdivision.

APPENDIX A
LANDSCAPE STANDARDS, SPECIFICATIONS, STANDARD DRAWING NOTES

- Local Industrial Road Cross Section – 26 Metre ROW
- Deciduous Tree Planting Details
- Coniferous Tree Planting Details
- Shrub Planting Details
- Specifications For Streetscape
- Standard Drawing Notes

ALL DIMENSIONS IN METRES UNLESS OTHERWISE NOTED



GRANULAR ROAD BASE TO BE COMPACTED TO 100% S.P.D. AS PER O.P.S.S. 501.07.02
MAXIMUM DRIVEWAY GRADE 10%

THICKNESS OF HOT LAID ASPHALT AND GRAN BASE COURSE AS REQUIRED BY DESIGN.
MINIMUM REQUIREMENTS
40 mm HL-3
90 mm HL-8
150 mm GRAN A
300 mm GRAN B
CROSS FALL 2%
SUBDRAIN AS REQUIRED BY DESIGN

SIDEWALK TO HAVE MAXIMUM CROSSFALL OF 3% AND 2% AT DRIVEWAYS.
MAXIMUM GRADE OF BLVD. 6%
MINIMUM GRADE OF BLVD 2%

ALL DIMENSIONS SHOWN FOR COVER ARE MIN. UTILITY LOCATIONS IN ACCORDANCE WITH TOWN OF CALEDON PROCEDURE

NOTE:
THIS CROSS SECTION IS PROVIDED TO ILLUSTRATE TREE LOCATIONS ONLY. REFERENCE SHOULD BE MADE TO THE APPROVED TOWN OF CALEDON ENGINEERING CROSS SECTION FOR ALL OTHER PURPOSES.

THE CORPORATION OF THE
TOWN OF CALEDON

TULLAMORE COMMUNITY DESIGN GUIDELINES: LANDSCAPE STANDARDS

DRAWING TITLE: URBAN SUBDIVISION ROAD CROSS SECTION FOR 22 M R.O.W. (With increased sidewalk width – One side)

PLAN NO: SEC-1

PREPARED BY:
alexander budrevics and associates limited
modified by Landscape Planning Limited

May 1998

WILTPROOF IN NURSERY
PRIOR TO DELIVERY

PRUNING SHALL BE LIMITED
TO DEAD OR BROKEN BRANCHES
AFTER PLANTING. MAINTAIN
ORIGINAL SHAPE OF TREE
DO NOT TRIM LEADER BRANCH.

SET TREE 75-100mm HIGHER THAN
ADJACENT FINISHED GRADE TO
ALLOW FOR SETTLEMENT

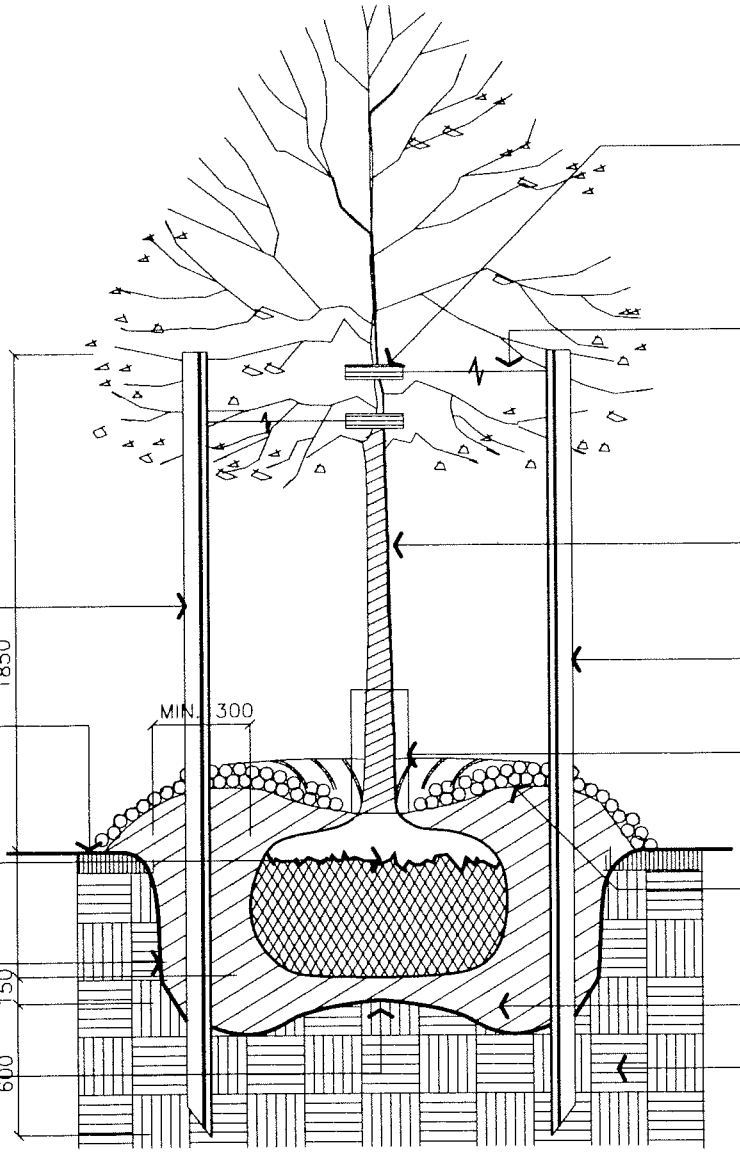
SET TREE STAKES JUST
INSIDE TREE PIT AS SHOWN

FINISHED GRADE

CUT AND REMOVE TOP 1/3 OF
BURLAP FROM ROOTBALL INCLUDING
ALL TIE ROPE AND WIRE

SCARIFY, LOOSEN, IRRIGATE AND
FERTILIZE THE INSIDE OF THE TREE PIT
PRIOR TO PLANTING WITHIN THE UPPER
50 % OF THE TREE PIT.

PROVIDE 75mm SOIL MOUND AT
BASE OF PIT AS SHOWN



12mm DIAMETER BLACK RUBBER
HOSE LOOPED ABOVE FIRST
STRONG BRANCH

12 GAUGE GALVANIZED WIRE
ENCLOSED IN 12mm DIAMETER
RUBBER HOSE SECURED AROUND
TREE TRUNK. PROVIDE WIRE
TURNBUCKLE FOR TENSION
ADJUSTMENT.

IF CONTRACTOR ELECTS TO WRAP TREE
TRUNK, TREE TO BE WRAPPED WITH
APPROVED TREE WRAP AFTER VISUAL
INSPECTION BY LANDSCAPE ARCHITECT.
WRAP TO EXTEND FROM TOP OF ROOTBALL
TO ABOVE GUYWIRE HOSE LOCATION.
PROVIDE MIN. 10mm OVERLAP. WRAP TO
BE REMOVED PRIOR TO FINAL INSPECTION.

TWO 2400mm LONG 50mm SQUARE
PRESSURE TREATED WOOD STAKES
SECURED INTO GROUND AS SHOWN

PROVIDE APPROVED RODENT
GUARD WHERE REQUIRED

CONSTRUCT 100mm SOIL SAUCER
AROUND TREE BASE AND COVER
WITH 75mm APPROVED SHREDDED
WOOD MULCH

SPECIFIED SOIL MIXTURE FIRMLY
COMPACTED TO ELIMINATE AIR
POCKETS AND PREVENT SETTLEMENT

COMPACTED SUBGRADE

NTS

THE CORPORATION OF THE
TOWN OF CALEDON

TULLAMORE COMMUNITY DESIGN GUIDELINES: LANDSCAPE STANDARDS

DRAWING TITLE: DECIDUOUS TREE PLANTING DETAIL (80mm OR LESS)

DETAIL NO: PLA-1

PREPARED BY:
alexander budrevics and associates limited
modified by Landscape Planning Limited

May 1998

WILTPROOF IN NURSERY
PRIOR TO DELIVERY

PRUNING SHALL BE LIMITED
TO DEAD OR BROKEN BRANCHES
AFTER PLANTING. MAINTAIN
ORIGINAL SHAPE OF TREE
DO NOT TRIM LEADER BRANCH.

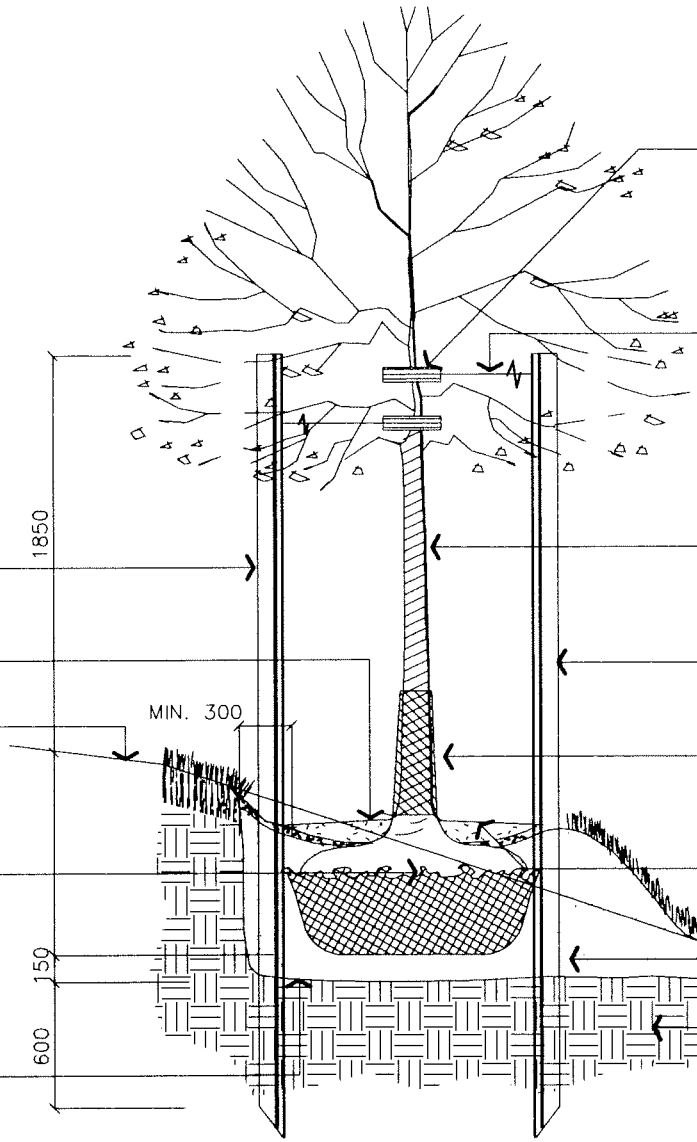
SET TREE STAKES JUST
INSIDE TREE PIT AS SHOWN

MAINTAIN ORIGINAL GRADE OF TREE
BASE AFTER PLANTING OR SLIGHTLY
HIGHER TO SUIT SOIL CONDITIONS

FINISHED GRADE

CUT AND REMOVE TOP 1/3 OF
BURLAP FROM ROOTBALL INCLUDING
ALL TIE ROPE AND WIRE

SCARIFY, LOOSEN, IRRIGATE AND
FERTILIZE THE INSIDE OF THE TREE PIT
PRIOR TO PLANTING WITHIN THE
UPPER 50% OF THE TREE PIT.



12mm DIAMETER BLACK RUBBER
HOSE LOOPED ABOVE FIRST
STRONG BRANCH

12 GAUGE GALVANIZED WIRE
ENCLOSED IN 12mm DIAMETER
RUBBER HOSE SECURED AROUND
TREE TRUNK. PROVIDE WIRE
TURNBUCKLE FOR TENSION
ADJUSTMENT.

IF CONTRACTOR ELECTS TO WRAP TREE
TRUNK, TREE TO BE WRAPPED WITH
APPROVED TREE WRAP AFTER VISUAL
INSPECTION BY LANDSCAPE ARCHITECT.
WRAP TO EXTEND FROM TOP OF ROOTBALL
TO ABOVE GUYWIRE HOSE LOCATION.
PROVIDE MIN. 10mm OVERLAP. WRAP TO
BE REMOVED PRIOR TO FINAL INSPECTION.

TWO 2400mm LONG 50mm SQUARE
PRESSURE TREATED WOOD STAKES
SECURED INTO GROUND AS SHOWN

RODENT GUARD MANDATORY DURING
3 YEAR MAINTENANCE PERIOD

CONSTRUCT SOIL SAUCER 100mm DEEP,
600mm DIA. AROUND TREE BASE AND
COVER WITH 75mm APPROVED SHREDDED
WOOD MULCH

SPECIFIED SOIL MIXTURE FIRMLY
COMPACTED TO ELIMINATE AIR
POCKETS AND PREVENT SETTLEMENT

COMPACTED SUBGRADE

NTS.

THE CORPORATION
OF THE
TOWN OF CALEDON

TULLAMORE COMMUNITY DESIGN GUIDELINES: LANDSCAPE STANDARDS

DRAWING TITLE: DECIDUOUS TREE ON SLOPE PLANTING DETAIL

PLAN NO: PLA-6

PREPARED BY:
alexander budrevics and associates limited
ADAPTED FROM DETAIL PREPARED BY LANDSCAPE PLANNING LIMITED

November 1996

TREE SHALL BE MEASURED TO
HEIGHT OF LAST YEARS GROWTH

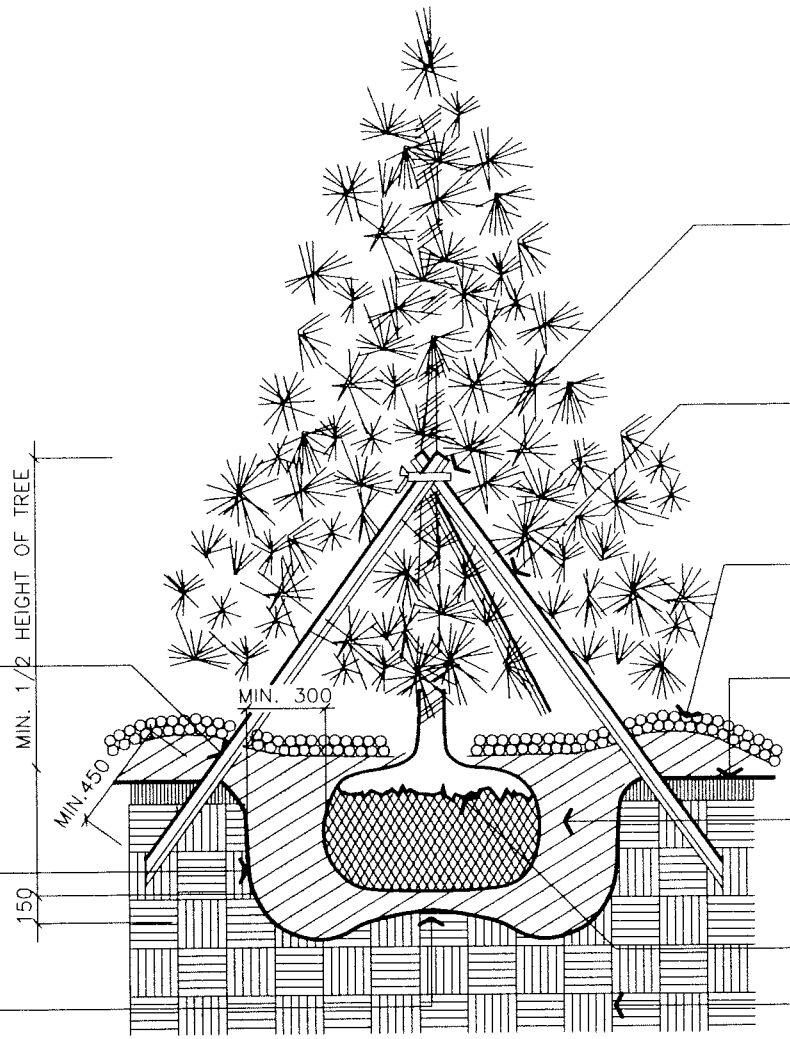
PRUNING SHALL BE LIMITED
TO DEAD OR BROKEN BRANCHES
AFTER PLANTING. MAINTAIN
ORIGINAL SHAPE OF TREE
DO NOT TRIM LEADER BRANCH.

SET TREE 75 TO 100mm HIGHER THAN
ADJACENT FINISHED GRADE TO
ALLOW FOR SETTLEMENT

SET TREE STAKES JUST
INSIDE TREE PIT AS SHOWN

SCARIFY, LOOSEN, IRRIGATE AND
FERTILIZE THE INSIDE OF THE TREE
PIT PRIOR TO PLANTING WITHIN THE UPPER
50 % OF THE TREE PIT.

PROVIDE 75mm SOIL MOUND AT
BASE OF PIT AS SHOWN



SECURE STAKES TO MAIN TREE
TRUCK WITH BURLAP ROPE.
ENSURE THAT THE STAKES DO NOT
CONTACT EXPOSED TREE BARK

THREE 50mm SQUARE PRESSURE
TREATED WOOD STAKES SECURED
INTO GROUND A MINIMUM OF
450mm AS SHOWN
LENGTH OF STAKES AND HEIGHT OF
CROSSING TO BE ADJUSTED TO
ACCOMMODATE TREE SIZE

CONSTRUCT 100mm SOIL SAUCER
OUTSIDE OF DRIPLINE
AROUND TREE BASE AND COVER
WITH 75mm APPROVED SHREDDED
WOOD MULCH

FINISHED GRADE

SPECIFIED SOIL MIXTURE FIRMLY
COMPACTED TO ELIMINATE AIR
POCKETS AND PREVENT SETTLEMENT

CUT AND REMOVE TOP 1/3 OF
BURLAP FROM ROOTBALL INCLUDING
ALL TIE ROPE AND WIRE

COMPACTED SUBGRADE

NTS

THE CORPORATION OF THE
TOWN OF CALEDON

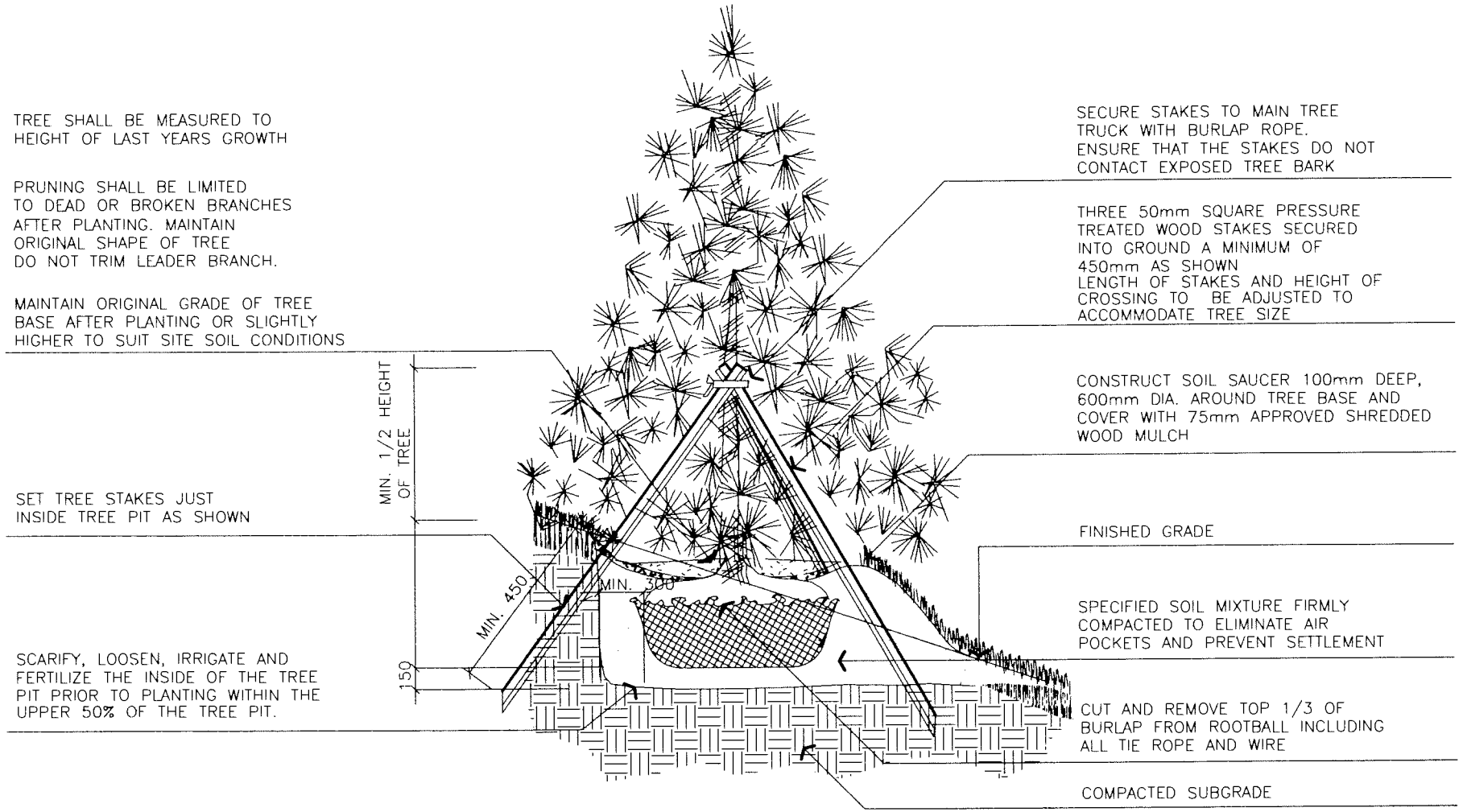
TULLAMORE COMMUNITY DESIGN GUIDELINES: LANDSCAPE STANDARDS

DRAWING TITLE: CONIFEROUS TREE PLANTING DETAIL

DETAIL NO: PLA-2

PREPARED BY: alexander budrevics and associates limited
modified by Landscape Planning Limited

May 1998



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THE CORPORATION OF THE TOWN OF CALEDON

TULLAMORE COMMUNITY DESIGN GUIDELINES : LANDSCAPE STANDARDS

DRAWING TITLE: CONIFEROUS TREE ON SLOPE PLANTING DETAIL

PLAN NO: PLA-7

PREPARED BY: alexander budrevics and associates limited
ADAPTED FROM DETAIL PREPARED BY LANDSCAPE PLANNING LIMITED

November 1996

SHRUB HEIGHT SHALL BE MEASURED FROM FINISHED GRADE TO UPPER MAIN MASS OF SHRUB BRANCHES

SHRUBS PLANTED IN GROUPS SHALL BE SET IN CONTINUOUS BEDS AS SHOWN ON PLAN

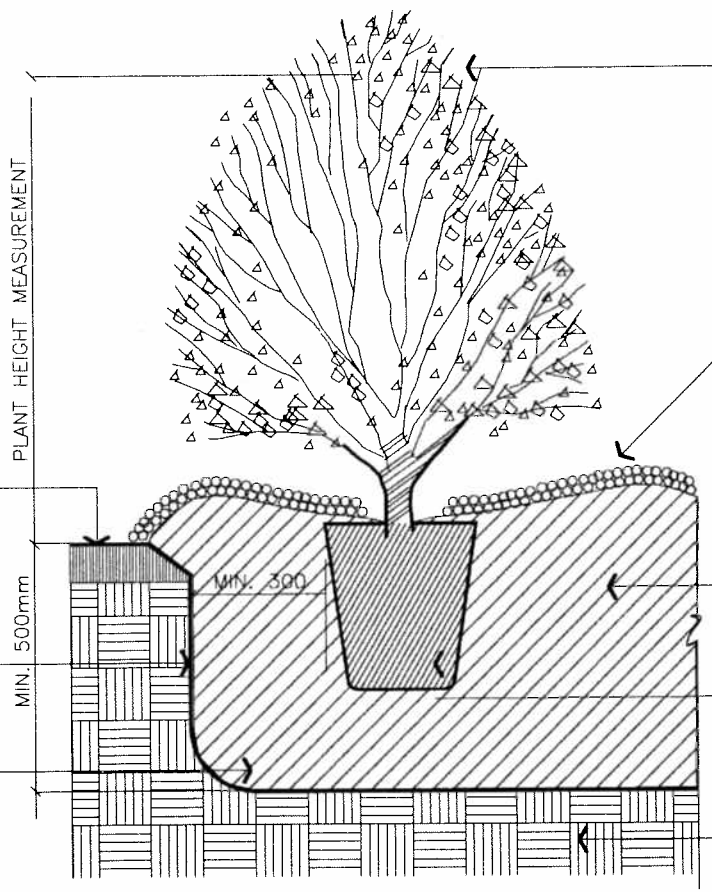
SET SHRUBS 50mm HIGHER THAN ADJACENT FINISHED GRADE TO ALLOW FOR SETTLEMENT

PLANTING METHOD ILLUSTRATED APPLIES EQUALLY TO BARE ROOT, POTTED OR B&B NURSERY STOCK

FINISHED GRADE

SCARIFY, LOOSEN, IRRIGATE AND FERTILIZE THE INSIDE OF THE SHRUB BED PRIOR TO PLANTING WITHIN THE UPPER 50% OF THE PLANTING PIT

EXCAVATE AND PREPARE PLANTING BED TO MINIMUM 500mm DEPTH AS SPECIFIED



PRUNING SHALL BE LIMITED TO DEAD OR BROKEN BRANCHES AFTER PLANTING. MAINTAIN ORIGINAL SHAPE OF SHRUB DO NOT TRIM LEADER BRANCH.

CONSTRUCT 100mm SOIL SAUCER AROUND TREE BASE AND COVER WITH 75mm APPROVED SHREDDED WOOD MULCH OVER ENTIRE BED AREA

SPECIFIED SOIL MIXTURE FIRMLY COMPACTED TO ELIMINATE AIR POCKETS AND PREVENT SETTLEMENT

REMOVE NURSERY POTS OR WRAP PRIOR TO PLANTING

COMPACTED SUBGRADE

NTS

THE CORPORATION OF THE TOWN OF CALEDON

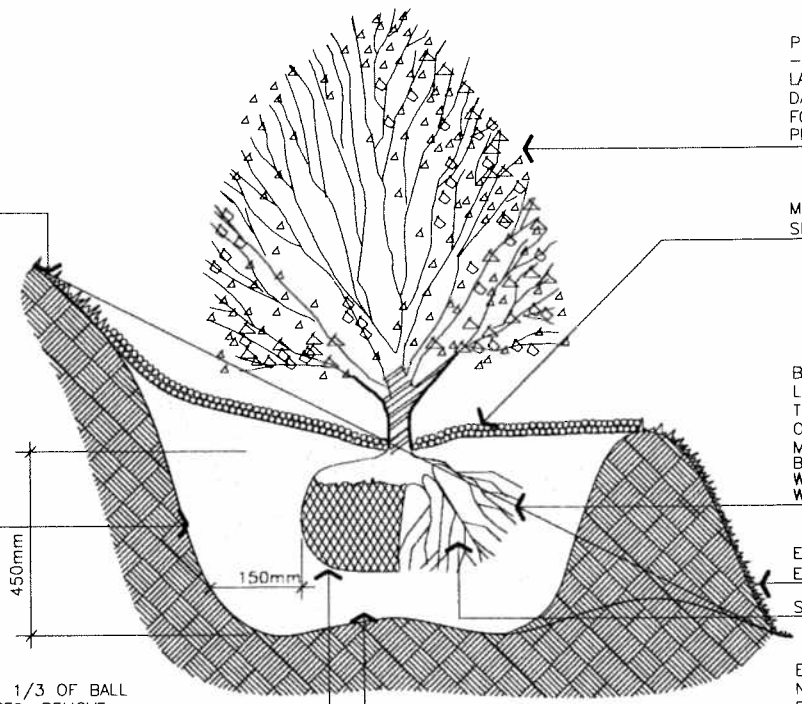
TULLAMORE COMMUNITY DESIGN GUIDELINES: LANDSCAPE STANDARDS

DRAWING TITLE: SHRUB PLANTING DETAIL

DETAIL NO: PLA-3

PREPARED BY: alexander budrevics and associates limited
modified by Landscape Planning limited

May 1998



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THE CORPORATION OF THE TOWN OF CALEDON

TULLAMORE COMMUNITY DESIGN GUIDELINES: LANDSCAPE STANDARDS

DRAWING TITLE: SHRUB AND CONIFEROUS SEEDLING PLANTING DETAIL ON SLOPES POTTED OR BARE-ROOT

DETAIL NO: PLA-8

PREPARED BY: alexander budrevics and associates limited

NOTE 1: THE STREET TREES DEPICTED ON THIS PLAN INDICATE THE SPECIES AND THE GENERAL LOCATION OF THE TREES. ONCE THE DRIVEWAYS AND ALL THE UTILITIES AND LIGHT STANDARDS HAVE BEEN INSTALLED, THE EXACT LOCATION OF STREET TREES WILL BE DETERMINED BY THE LANDSCAPE ARCHITECT AND APPROVED BY THE TOWN PRIOR TO PLANTING.

NOTE 2: CLEARANCES TO UTILITY FURNITURE AND INTERSECTIONS FOR STREET TREES (WHEN TREES ARE 1.5 m FROM THE CURB):

*2 m FROM WATER HYDRANTS
*2 m FROM DRIVEWAYS
*2 m FROM COMMUNITY MAILBOXES
3 m FROM HYDRO TRANSFORMERS
5 m FROM STREET LIGHTS
15 m MINIMUM FROM STREET LINE (STREET INTERSECTIONS AS MEASURED FROM THE BACK OF THE CURB) AND BEHIND THE DAYLIGHT TRIANGLE AS PER THE GEOMETRIC DESIGN STANDARDS FOR ONTARIO HIGHWAYS
18 m FROM FACE OF ALL WARNING AND REGULATORY SIGNS

WHEN THE MINIMUM DISTANCES NOTED ABOVE ARE NOT ACHIEVABLE, TREES MAY BE PLANTED 0.5 m FROM THE PROPERTY LINE AS AN ALTERNATE LOCATION (BEHIND THE SIDEWALK), ADJACENT TO FENCES, OR JUST INSIDE THE STREET LINE, ON PRIVATE PROPERTY, AS IN THE CASE OF A CUL-DE-SAC. *IF THE TREE IS PLANTED IN AN ALTERNATE LOCATION, THE DISTANCES MARKED WITH AN ASTERISK MUST STILL BE MAINTAINED.

NOTE 3: THE TREE PITS AND PLANTING BEDS FOR ALL TREES AND SHRUBS LOCATED WITHIN 1 METRE OR LESS OF UNDERGROUND UTILITIES ARE TO BE HAND DUG.

NOTE 4: ALL PLANT MATERIAL IS TO BE INSPECTED AND APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO SHIPMENT TO THE SITE. THIS DOES NOT LIMIT THE RIGHT OF THE LANDSCAPE ARCHITECT OR THE MUNICIPALITY TO REJECT THE MATERIAL DUE TO POOR QUALITY, DAMAGE DURING SHIPPING AND INSTALLATION, OR POOR PERFORMANCE DURING THE WARRANTY PERIOD.

NOTE 5: ALL PLANTINGS AND HARD LANDSCAPE FEATURES ARE TO BE LAID OUT ON SITE BY THE LANDSCAPE ARCHITECT TO THE SATISFACTION OF THE TOWN OF CALEDON PRIOR TO INSTALLATION. ANY DEVIATIONS FROM THE APPROVED LANDSCAPE PLANS REQUIRE PRIOR TOWN APPROVAL.

NOTE 6: UPON COMPLETION OF LANDSCAPING, THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLIANCE TO THE PLANNING DEPARTMENT CERTIFYING THAT ALL LANDSCAPE WORKS HAVE BEEN COMPLETED IN ACCORDANCE WITH THE APPROVED PLANS, AND AN INSPECTION CHART INDICATING PLANT SPECIES, QUANTITY, LOCATION, PLANTING DATE(S), AND ALL OTHER RELEVANT INFORMATION AS SPECIFIED BY THE TOWN. UPON RECEIPT OF THE CERTIFICATE OF COMPLIANCE, THE PLANNING DEPARTMENT SHALL CONDUCT A PRELIMINARY INSPECTION OF THE SITE AND, PROVIDED THAT THE WORKS ARE IN SATISFACTORY CONDITION, SHALL GRANT PRELIMINARY ACCEPTANCE OF THE LANDSCAPING.

NOTE 7: ALL STREETSCAPE LANDSCAPING IS SUBJECT TO A TWO (2) YEAR WARRANTY PERIOD, COMMENCING FROM THE DATE THAT PRELIMINARY ACCEPTANCE IS GRANTED BY THE MUNICIPALITY. AFTER THE FIRST AND SECOND YEARS, THE LANDSCAPE ARCHITECT WILL CONDUCT A PERFORMANCE INSPECTION AND MAKE RECOMMENDATIONS REGARDING REPLACEMENTS AND/OR WORKS TO ACHIEVE THE INTENT OF THE APPROVED LANDSCAPE PLAN. ALL WARRANTY REPLACEMENTS ARE TO BE RECORDED ON THE INSPECTION CHART. AT THE END OF THE WARRANTY PERIOD, THE MUNICIPALITY WILL CONDUCT A FINAL INSPECTION AND, PROVIDED THE WORKS ARE STILL IN SATISFACTORY CONDITION, SHALL GRANT FINAL ACCEPTANCE. FOR THE WORKS REPLACED OR REPAIRED DURING THE WARRANTY PERIOD, THE WARRANTY PERIOD FOR SUCH WORKS SHALL BE EXTENDED FOR AN ADDITIONAL TWO YEAR PERIOD, AND SHALL NOT BE GRANTED FINAL ACCEPTANCE UNTIL EXPIRY OF THE EXTENDED WARRANTY PERIOD, OR AS OTHERWISE DETERMINED BY THE MUNICIPAL PLANNING DEPARTMENT.