

URBAN DESIGN REPORT

Allison's Grove, Proposed Residential/Commercial Development

16114 Airport Road, Caledon East



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1.0 DEVELOPMENT

1.1 Introduction:

Montego Investments Limited has acquired a development site that fronts on Airport Road and Walker Road in north Caledon. The parcel consists of approximately 4.09 hectares (10.11 acres) of land. The owners propose to develop the site as a mixed-use development featuring residential and commercial components. The residential segment will be composed of condominium townhouses organized on a private road system. The commercial parts of the development comprise of restoring and converting the existing Allison Grove Heritage House into a commercial building and constructing a new one storey commercial building organized around a central parking lot.



Figure 1 – Subject Lands

1.2 Analysis of Guiding Principles and Documents:

The Town of Caledon has outlined the long term vision for its development in its Official Plan. The Caledon East Secondary Plan and the Community Design and Architectural Design Guidelines further define the ideals of Caledon.

1.3 Caledon East Secondary Plan:

The objectives of the Caledon East Secondary Plan are to allow for growth in a compact community that reflects the character of the surrounding rural landscape. This is to be done by integrating a diverse mix of housing types, while promoting commercial redevelopment within the towns core. The Plan states that it should provide for employment opportunities through office and commercial uses as part of mixed-use developments. The objectives also recognizing that it must identify and protect the cultural heritage of the area, including its heritage buildings.

The Caledon East Secondary Plan identifies this site as “Special Use Area A: Allison’s Grove”, and states that “The area north of Walker Road and west of Airport Road is intended to be an attractive northern gateway to the Caledon East Commercial Core Area. Development in Special Use Area A will protect, incorporate and complement the existing designated heritage residence known as Allison’s Grove and the natural setting and features of the site. Existing trees on the site will be protected and incorporated into the development the greatest extent possible.”

“Development may include an architecturally compatible addition to the existing house and/or separate architecturally compatible buildings as described in the Caledon East Community Design Guidelines.”

“Limited commercial, institutional and residential uses may be developed within the Special Use Area as follows:

- a) Tourism related commercial uses such as restaurant, inn, bed and breakfast, antique shop, craft shop, and specialty retail shall be permitted within the existing house, in an addition to the existing house, and in separate buildings. However, consistent with Section 7.7.8.2 of this Plan, Drive-thorough service facilities may be considered in Special Use Area A only where it can be demonstrated that the intent on the Plan related to Special Use Area A can be preserved.
- b) Apartments are permitted in the upper stories of commercial buildings subject to the provisions of Section 7.7.5 and within the existing house. Institutional development such as seniors' housing or nursing home may also be permitted.”

The Secondary Plan also sets out to show that community design should express the following values that were expressed by the community:

1. Maintaining the historic character and desire for a traditional “main street” commercial area with appropriate (and preferably traditional) architectural style;
2. The preservation and enhancement of natural features;
3. The integration of common open space within the community;
4. Streetscapes that are greener, pedestrian friendly and safe;
5. Further development of the railway system including improved and new connections to link all neighbourhoods, community facilities and services and open space;
6. Integration of housing types;
7. Good architectural design, particularly for medium density development, safety and crime prevention through design;
8. Gateway entrance features to the community.
9. Maintaining views of valued landscape features and the night sky.

1.4 Community Design and Architectural Design Guidelines:

The "Community Design and Architectural Design Guidelines , Caledon East" were prepared by Viljoen Architect Inc. and approved by Council 16 October, 2000.

Under Section 2.4 "Community Gateways" the site is defined as "North Community Gateway:

2.4.10 To minimize the visual impact of the garage and maximize the architectural character of the new residential buildings, the following lot types are preferred on Community Gateway Lots:

- a) Wide lots (provided that driveway access onto Airport Road is permitted by the Region of Peel.)
- b) Lots with frontage on a parallel service road
- c) Flankage lots
- d) Lots with driveway access from the rear
- e) Condominium development with driveway access and outdoor amenity space at the rear

2.4.11 The west side of Airport Road should be upgraded between the Gateway and Walker Road by removing the ditch and installing a landscaped boulevard with a sidewalk and Street Trees in accordance with the Caledon East Streetscape Concept."

The Architectural Guidelines also refer to the site as Special Use Area "A" and state "The Allison's Grove site is a Designated Heritage Property under the Ontario Heritage Act that will be developed as an attractive entrance to the Commercial Core of Caledon East. The following guidelines apply to new development that may include additions to the existing heritage home pictured on page 5 and/or separate

buildings:

- 2.8.1** All development will conform to the Caledon East Streetscape Concept and Architectural Guidelines prepared for the Commercial Core.
- 2.8.2** Additions and alterations to the Allison's Grove heritage home shall be architecturally compatible with the existing building and will be subject to approval by the Caledon Heritage Committee and the Heritage Resource Officer.
- 2.8.3** New buildings shall be either of the same Gothic Revival style and materials as the existing heritage home or resemble the compatible outbuildings (coach houses, sheds, barns, etc) of the same historical period.
- 2.8.4** Buildings constructed in the style of the 19th century outbuildings will be encouraged to have timber frame verandas and dormers."

The Community Design and Architectural Design Guidelines describe Allison's Grove on Airport Road in the following way, "This Gothic Revival home has a cross-gabled roof layout with a steep pitch, complemented by decorative verge boards, quoins, patterned brickwork, and a spindlework porch."



Figure 2 – Allison's Grove

1.5 Caledon Comprehensive Town Wide Guidelines:

The Caledon Comprehensive Town Wide Guidelines give direction to commercial development in Section 10.1 General Commercial Guidelines.

Under Section 10.1.1, relevant key design principles and objectives are listed:

- 3.1 Sustainable Design and Compact Development
- 3.2 Accessibility and Universal Design
- 3.3 Community Safety and Security
- 3.4 Complete Streets and Active Transportation
- 3.5 Cultural Heritage Conservation

The guidelines provide design standards and design requirements for site planning, building elevations, pedestrian circulation, landscape design, vehicular access, parking and servicing, lighting and signage.

Relevant design requirements for site planning include the requirement to locate parking loading and servicing to the rear of the building, the requirement to front commercial development onto the public realm and the requirement for commercial buildings to exhibit a high degree of visual appeal on all exposed frontages.

Relevant design requirements for building elevations include the requirement for use of the same materials on all elevations, the requirement for elevations to be pedestrian friendly, to have all elevations appear as the main façade and to provide visual interest and variety in roof scapes.

1.6 Allison's Grove, Urban Design Report:

The intent of this document is to review the Towns documents and guidelines in relation to the mixed-use development proposed for Allison's Grove, and to identify how the design aspects of the proposed development comply or differ from existing policy. The report will show how the proposed development will be a valuable addition to the Caledon East community by adopting many of the values of the Caledon East Secondary Plan and the Community Design and Architectural Guidelines and the Caledon Comprehensive Town Wide Guidelines.

Relevant design requirements for pedestrian circulation include the requirement for direct and easy connections to streets and sidewalks, the requirement to have hard surface paving along their frontages.

Relevant design requirements for vehicular access, parking and servicing, include the requirement to have parking broken up with landscaped parking islands which are to be curbed and located at the ends of rows. Parking should be screened from view by landscaping and fencing, should be paved and should be able to accommodate snow removal and storage.

Relevant design requirements for lighting include the requirement to be dark sky compliant, ensure safety at night, reflect the architecture of the community

Lastly, relevant design requirements for signage include the requirement to be compatible in scale and design and to be an integral element for the building facade.



Figure 3 – Site Location and Context

2.0 URBAN DESIGN

2.1 The Site:

The site at Walker Road West and Airport Road in the Town of Caledon East comprises approximately 4.09 hectares (10.11 acres). The site can be further broken down to a Woodlot & Buffer area of 2.51 hectares (6.22 acres), Residential area of 0.84 hectares (2.08 acres), Commercial area of 0.56 hectares (1.38 acres), Road Widening & Daylight Triangle area of 0.07 hectares (0.17 acres) and Park Area of 0.104 hectares (0.26 acres). The site has a street frontage of approximately 175 meters onto the west side of Airport Road, and approximately 30 meters onto the north side of Walker Road West. The site is located on the northern edge of the town and has been identified as the North Community Gateway.

The primary access for the Residential Development will be from Airport Road. Access points for the Commercial Development will be from Airport Road and Walker Road West. The residential road is integrated through the south end of the commercial parking.

There are two existing detached residential dwellings to the south of the site, and an existing condominium townhouse development on the north boundary of the site. The Woodlot & Buffer area make up the western boundary, and separate the site from existing detached residential dwellings to the west.

The road pattern provides a pedestrian connection to the Commercial Centre of town along Airport Road.

The proposed 2 storey condominium street townhouses front onto a private road in the same way as they do in the neighbouring development to the north. The two townhouses at the entrance to the site have their flankage elevation on Airport Road.

The existing Allison Grove heritage house will be at the northern end of the Commercial Development while a new one storey commercial building will be at the southern end. A parking lot will be between the two commercial buildings and behind the new commercial building. The commercial parking lot is connected with the Condominium Road at the South end.



Figure 4 – View of Heritage House from Airport Road

2.2 The Site Plan:

The site plan is organized around the Allison's Grove Heritage House. There are five blocks of 6.1m wide condominium townhouses for a total of 32 units arranged to the north and west of the heritage house. There is a one storey commercial building to the south with its frontage on Airport Road. The heritage house also fronts onto Airport Road and will be restored and converted to a commercial use.



Figure 5 – Site Plan

Rev. September 17, 2019

FBP Architects Inc.



Figure 6 – Residential Site Plan



Figure 7 – Commercial Site Plan

2.3 Allison's Grove, Heritage House:

The heritage house known as Allison's Grove on Airport Road is a Gothic Revival home. The house features decorative verge boards and an elaborate spindlework porch. The house itself has deteriorated in recent years and is currently missing its front porch and balcony. Much of the wood scroll work is rotted and missing. There is evidence of damage to some of the exterior brick, and the interior of the house will need to be completely gutted and rebuilt.

The addition of the residential and commercial units in the site plan will help to offset the cost of restoring and converting the heritage house to a commercial use. The new residential and commercial units will attract more people to the area, which will create an environment where the development of Allison's Grove Heritage House can thrive.



Figure 8 – Missing Front Porch & Balcony



Figure 9 – Missing Decorative Gable Woodwork



Figure 10 – Damage to Brick

2.4 Proposed Lot Sizes and Townhouse Design:

The residential component of this site plan proposes 6.1 meter wide townhouses that will flank onto Airport Road. The Community Design and Architectural Guidelines had indicated that one of the possible ways of establishing a North Community Gateway was with the use of “flankage lots”. The flankage lots, combined with the road layout helps to minimize the visual impact of the garages and maximize the architectural character of the new residential buildings. The design of the townhouses further reinforce this by recessing the garages behind the faces of the habitable rooms or in some cases porches.

The townhouses will have single car garages and single car driveways. The minimum interior garage dimension will be 3.0 meters wide by 6.0 meters long. There will be 9 visitor parking spaces including one accessible parking space, which exceeds the normal standard of 0.25 visitor parking spaces per condominium unit. The visitor parking spaces are adjacent to the commercial parking lot, yet they are physically separated and serve only the residential component of the site plan.

The townhouses are designed to have the following minimum setbacks:

- 4.5 metre Front Yard Setback to House & 6.0 metre to Garage Face
- 7.0 metre Rear Yard Setback
- 1.5 metre Interior Side Yard Setback
- 3.0 metre Exterior Side Yard

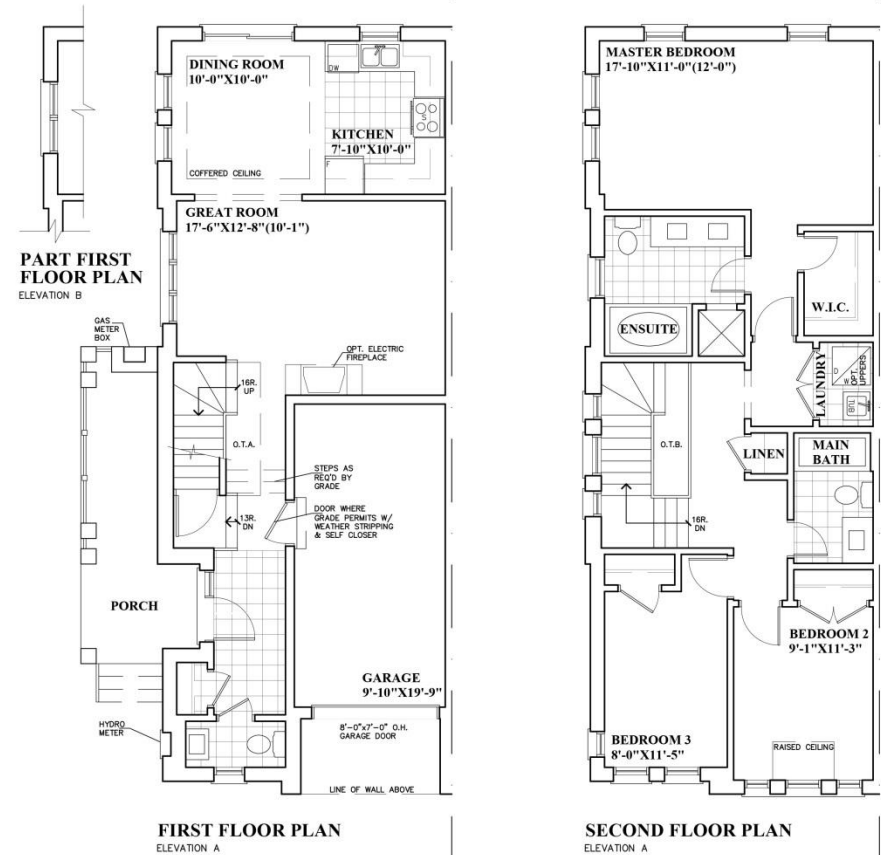


Figure 11 – Floor Plan of Corner Unit with Recessed Garage

2.5 Proposed Commercial Building:

The new one storey commercial building being proposed helps to reinforce the Community Design and Architectural Guidelines for Special Use Area "A", which states that the property be developed as an attractive entrance to the Commercial core of Caledon East. The position of the building on Airport Road will help to establish a visual gateway for the commercial core, as well as a pedestrian link to the Town Center for the residential properties to the north.

The primary building elevation addresses Airport Road, and the parking lot is mostly concealed behind the one storey commercial building.

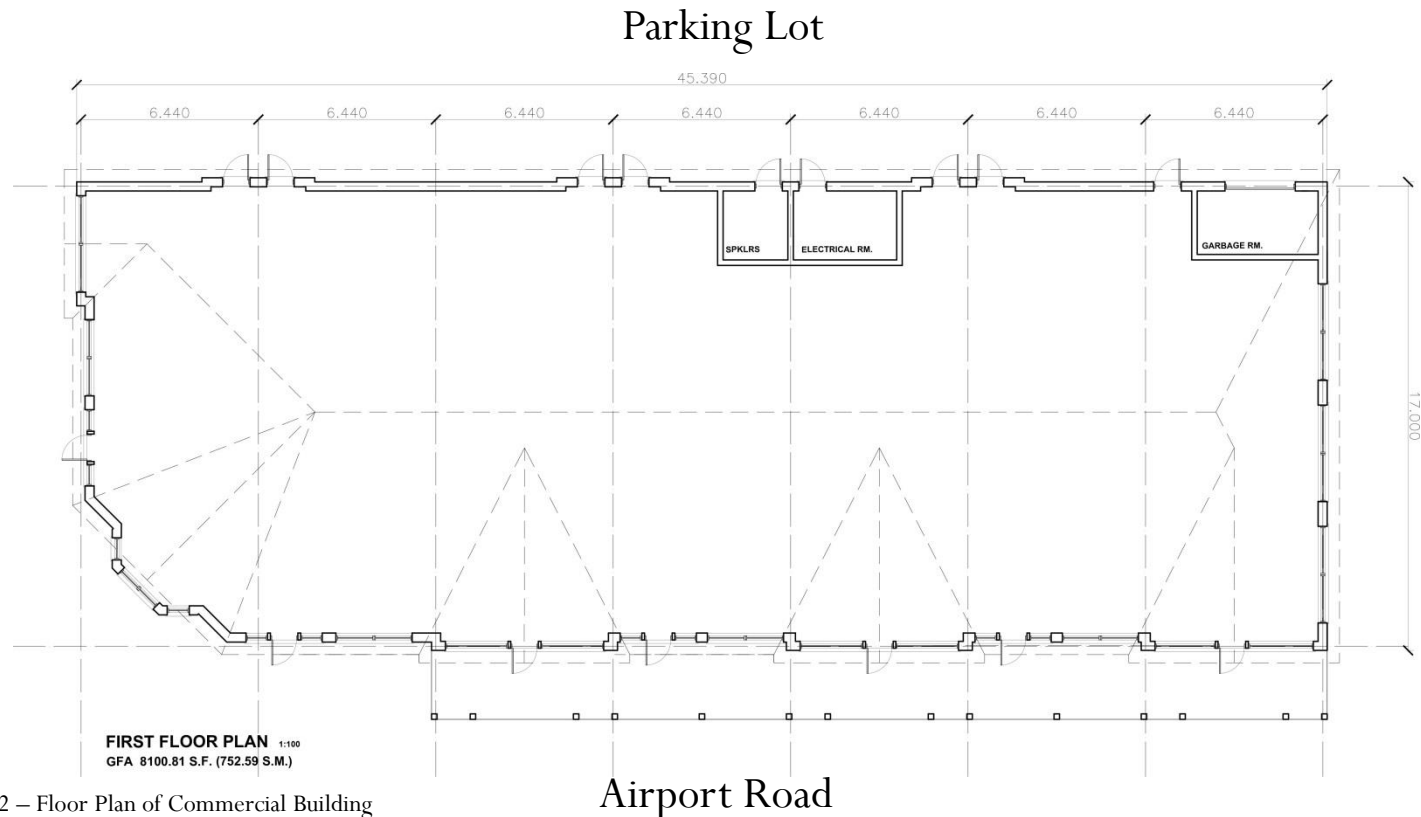


Figure 12 – Floor Plan of Commercial Building

3.0 ARCHITECTURAL DESIGN

3.1 Built Form and Massing – Condominium Townhouses:

The Allison's Grove heritage house is an example of the Gothic Revival style. The house features a cross-gabled roof layout with a steep pitch. There are large primary gable ends, and smaller secondary gables which highlight specific windows. There is a prominent porch that wraps around the ground floor, and a formal front porch with balcony above.

The proposed two storey condominium townhouses echo the design of the heritage house through the steep roof that features prominent primary and secondary gables. The two storey height of the townhouses is similar to the height of the heritage house. The entry porch recalls the size and scale of the front porch that is currently missing from the heritage house, while the recessed garages put the primary ground level focus on the entry porches.



Block 1 – Front Elevation

Figure 13 – Typical Townhouse Stone & Brick Elevation



Block 2— Front Elevation

Figure 14 – Typical Townhouse All Brick Elevation



Block 2 – Flankage Elevation ‘A’, Airport Road

Figure 15 – North Community Gateway



Block 1 – Flankage Elevation ‘B’, Airport Road

3.2 Built Form and Massing – Commercial Building:

The massing of the one storey commercial building borrows heavily from the massing of the heritage house. The height of the two buildings is similar, and the roof repeats the large primary gable that is the focus of Allison's Grove along the length of the building. The colonnade protecting the shop entrances recalls the wrap-around porch.



Front Elevation, Airport Road



Flankage Elevation, Corner of Walker
Road West & Airport Road



Rear Elevation, Parking Lot



Flankage Elevation, Parking Lot

Figure 16 – Commercial Building Elevations , Special Use Area “A”

3.3 Architectural Treatment:

The townhouses and the commercial building are both done in the same Gothic Revival architectural style as the heritage house.

The townhouses will have elevations that are all brick, or brick and stone. The all brick elevation will have an accent brick around the window openings. The brick and stone elevation will have a precast surround around the windows. Both types of elevations will feature gothic arched windows, and the porches will feature sloped metal roofs or flat roofs with railings. The brick window surrounds and the brick gable decorations recall the articulate brick work of the Allison's Grove heritage house.

The commercial building will have elevations that are all brick with an accent brick. The accent brick quoining at the corners of the heritage house have been reproduced on the retail building to articulate the jogs in the walls. The covered colonnade that faces Airport Road is reminiscent of the wrap-around porch, while the windows with their arched decorative masonry headers evoke the local vernacular. The patterned brick work in the gable end, and the decorative verge board also serve to enhance the façade and provide visual interest.



Figure 17 – Brick Quoining



Figure 18 – Window & Arched Brick Header

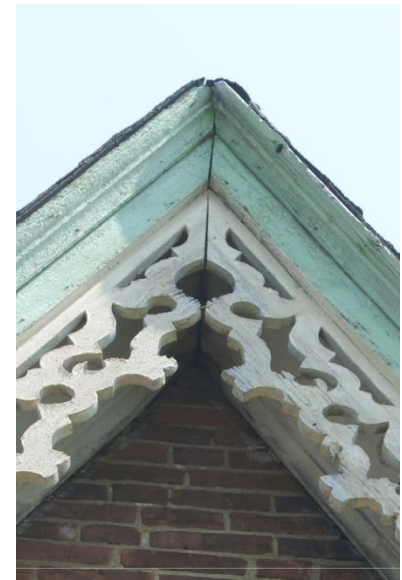


Figure 19 – Decorative Verge Board

4.0 CONCLUSION

The proposed mixed-use development at Allison's Grove reflects the objectives of the Caledon East Secondary Plan, the Community Design and Architectural Guidelines, and the Caledon Town Wide Comprehensive Guidelines for General Commercial Development, by responding to the requirements to create a North Community Gateway and a development that is architecturally compatible with the existing heritage building.

The site plan establishes a "gateway" with the flankage townhouse lots. The architectural detailing of the exposed side elevations is highly articulated and representative of the entire community.

The 6.1m wide two-storey condominium townhouses are in keeping with the Community Design and Architectural Guidelines framework for good community design, but do not meet the requirement of having three parking spaces per unit. Also, the unit frontage of 6.1m is less than the "encouraged" frontage of 7.5m for a freehold townhouse. The guidelines which were prepared in 2000, do not have a suggested frontage for condominium townhouses. The proposed 6.1m wide frontage does allow for one car parking in an attached garage, one car parking on a driveway and visitor parking that exceeds the industry norm of 0.25 spaces per condominium unit. The 6.1m frontage townhouse satisfies a market demand for a housing type that is affordable and accessible to more buyers. It is a relevant building type which can result in an attractive elevation that relegates the garage and promotes the porch, thereby enhancing community safety by providing more opportunity for eyes on the street.

The new commercial building serves to further reinforce the role of the site as an attractive entrance to the Commercial Core of Caledon East. The building will be served by a parking lot that will accommodate 59 parking spaces, as well as four accessible spaces. The increase in pedestrian traffic around the retail building will encourage pedestrian travel between this site and the nearby commercial center.

The proposed townhouses and commercial building compliment the existing heritage property. They will be the catalyst that make the redevelopment of the neglected building possible. The increase in traffic created by the 38 new townhouses and the new commercial buildings will help to generate an environment in which the re-purposed and restored Allison's Grove Heritage Building can thrive.