

CALEDON EAST STREETSCAPE STUDY ADDENDUM

December 2009

The Town of Caledon
Planning and Development Department

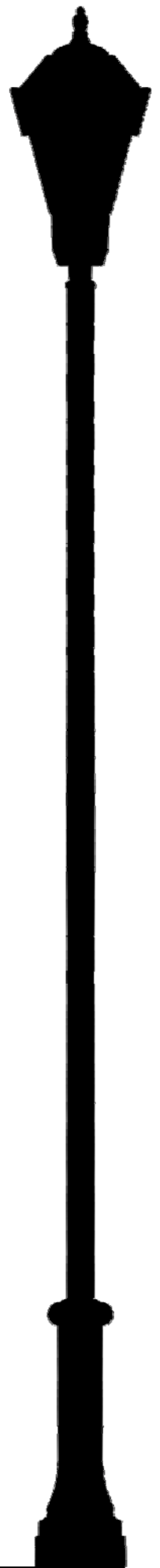


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1.0 INTRODUCTION

1.1 Background of Study

In 1992, the Region of Peel initiated the design and construction of sanitary sewers and pumping facilities in Caledon East. Consequently to this construction, development intensification of the area was expected.

In 1993, the Caledon East Streetscape Study was undertaken as a response to the construction, which was seen as a valuable opportunity to implement a comprehensive Streetscape Plan to preserve and enhance the community's village atmosphere. The study, prepared by Marshall Macklin Monaghan Limited in association with Quadrangle Architects Limited, was completed in February 1994. Council approved the Study later that year.

The Planning and Development staff was directed at the August 4, 2009 council meeting to update the Caledon East Streetscape Study to include an extension of the limits of the study area. Staff was instructed to bring forward a Council Report on the status of the updated Streetscape Study by December 2009.

1.2 Purpose of the Addendum

This Addendum studies additional areas in Caledon East and suggests an implementation strategy of the Caledon East Streetscape Study beyond the original limits. It also suggests other solutions for some of the originally proposed streetscape elements.

The study area for the original Caledon East Streetscape Study had been limited to Airport Road approximately 100 metres south of Hilltop Drive to 250 metres north of Walker Road; and Old Church Road between Airport Road and 40 metres east of Marilyn Street.

In terms of certain elements of the 1994 study, as for example the east entrance along Old Church Road (section 4.1.1 Village Entrances of the Caledon East Streetscape Study), the location was anticipated to happen beyond these limits. It was considered premature at the time to include in the study elements outside of the limits due to the uncertainty of the form of the future development in this area. Some of the streetscape elements which had been implemented as per the Caledon East Streetscape Study caused maintenance problems or created functioning issues over time; some were not implemented at all.

The Caledon East Streetscape Study Addendum is intended to be read in conjunction with the original Caledon East Streetscape Study. All aspects of the 1994 document apply unless stated otherwise.

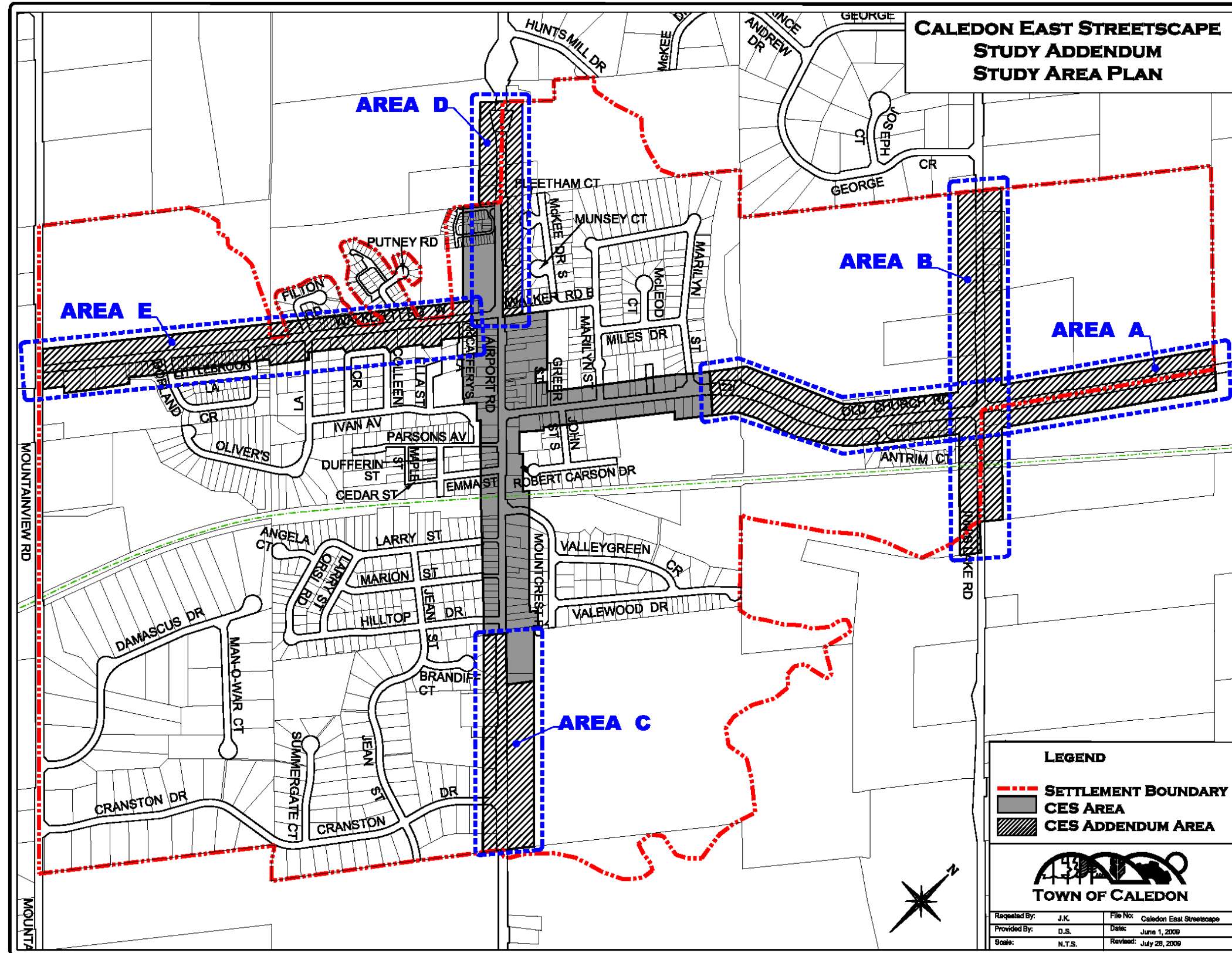
1.3 Study Area

The Addendum Study Area is graphically described in Figure 1. It generally includes the rights-of-way and adjacent land ownership of: Old Church Road from Marilyn Street to the settlement boundary on the east between Innis Lake Road and Centreville Creek Road

Caledon East Streetscape Study Addendum

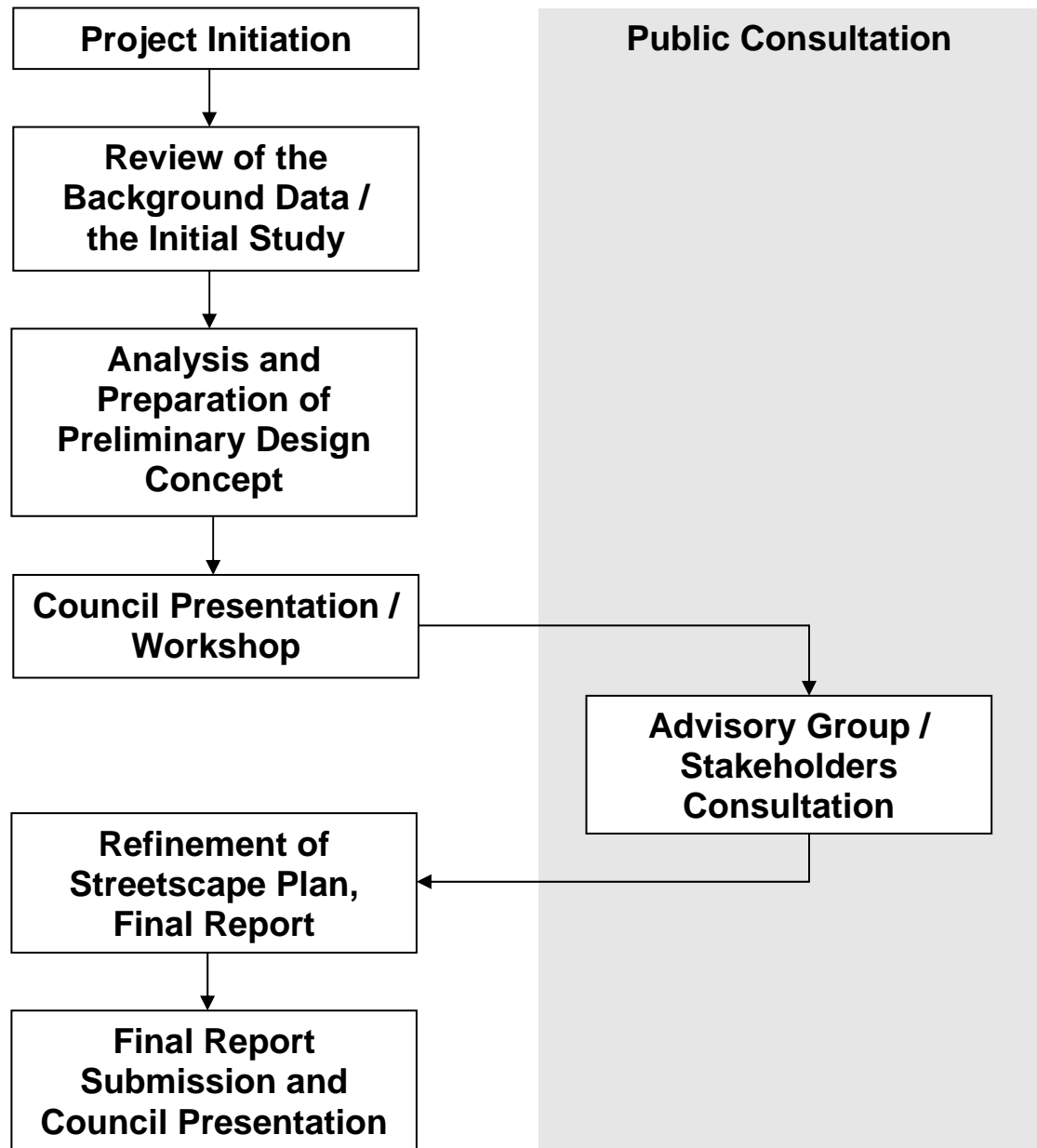
(Area A); Innis Lake Road in between the north and south settlement boundaries (Area B); Airport Road from approximately 100 metres south of Hilltop Drive to the settlement boundary on the south (Area C); Airport Road from Walker Road to the settlement boundary on the north (Area D); and Walker Road West from Mountainview Road to Airport Road (Area E).

Figure 1: Study Area Map



1.4 Work Program

CALEDON EAST STREETSCAPE STUDY
ADDENDUM WORK PROGRAM



2.0 SITE ANALYSIS

Detailed site analysis has been performed in the Caledon East Streetscape Study Report. This report defined the stakeholders, reviewed the architecture of the area, determined the land uses and their relations to each other, and reviewed other issues such as engineering, landscaping, recreation and tourism.

In this Addendum, findings from the Caledon East Streetscape Study Report are used. Following is an analysis of the issues and concerns which evolved after the implementation of the Design Concept of the Caledon East Streetscape Study, as well as an analysis in regard to the new areas included in this Addendum.

2.1 Area Covered by the Original Caledon East Streetscape Study

- The originally installed splash pad unit pavers require a high level of maintenance in terms of weeding. The pavers are also susceptible to move as they settle and therefore shift in place over time.
- There are thirty-eight areas within the original study area where street trees were planted but did not survive. In these locations, trees have been removed and replaced with inconsistent paving alternatives. Fifty-three trees have survived and have not been removed.
- Some of the originally installed bollards have been replaced with designs that vary from the proposed design concept.
- New trash and recycling receptacles have been implemented including plastic bins placed by the Region of Peel. These new units are not consistent with the existing receptacles creating a disorderly appearance. In addition, the originally located receptacles are deteriorating and allow for one stream collection only.
- Existing newspaper stands come across as untidy looking because of the diversity in shape, colour, size and poor installation.
- Garbage receptacles and benches have been located within close proximity to each other. The appearance and smell created by the location of these receptacles might repel people who would use them otherwise.
- All benches throughout the original study area in Caledon East are installed in pairs except for one case. These groups of two benches provide an ample supply of seating, and are almost never occupied. The conditions of these benches have deteriorated over time.
- Existing light fixtures do not have receptacles to supply energy for seasonal lighting. The fixtures also do not have a hanging arm to showcase flags or banners. An improvised device has been added to accommodate the current flags and banners hanging in Caledon East.
- There is a lack of proper signage to assist in guiding people to the trail networks of Caledon East and to public parking areas.
- Users of the Caledon Trailway are parking on Airport Road to access the trail and therefore are significantly reducing the number of parking spaces available; especially on weekends.

- Not all of the elements outlined in the Caledon East Streetscape Study have been followed. This includes planting and screening requirements which were not implemented due to the concerns expressed by the Region, as well as the creation of a Special Sign District.

2.2 Area A – Old Church Road (Figure 2)

2.2.1 Land Use

The land use within Area A is a mix of predominately institutional and residential. The institutional uses within the area include a police station, community complex with an attached hockey arena, and the Town of Caledon Town Hall. On the northeast corner of Old Church Road and Innis Lake Road there is a secondary school with a public library. The historic St. James Cemetery is located at the southeast corner of this intersection. The remainder of the land is occupied by a newer residential neighbourhood and large undeveloped property.

2.2.2 Built Form

The housing stock is made up of new and older development. The newer area of development has access on to neighbouring Antrim Court and therefore has mostly reverse frontage onto Old Church Road. The three older residential properties across from the Town Hall building front on to Old Church Road. These units are surrounded by mature trees and have moderate setbacks fitting in with the characteristic rural feel of the area. The Town Hall and the OPP station are relatively close to Old Church Road with the Community Complex set further back allowing for parking between it and Old Church Road.

The buildings throughout Area A range between one and two storeys in height. The Town Hall and the Caledon Community Complex have the largest mass.

2.2.3 Open Space

The majority of open space is located on the property owned by the Town of Caledon, adjacent to Area A. This space contains sports fields for recreational purposes.

2.2.4 Views

Throughout Area A there are few significant views. One noteworthy view corridor is of St. James Cemetery on the southeast corner of Old Church Road and Innis Lake Road (Figure 2, view 10). An additional important view is of the Town Hall building (Figure 2, view 3).

2.2.5 Parking

Parking within Area A is available at the municipal building, the sports and community complex, and at the high school. These locations provide adequate parking for visitors

including those accessing the trail or using the sports fields located behind the Town Hall building.

2.2.6 Streetscape Components and Appearance

Old Church Road is a two lane road providing access to Caledon East for those travelling from the east. The road has a sidewalk on the south side providing access to the Town Hall and residential development. On the north side of Old Church Road, east of Innis Lake Road, sidewalk is provided in front of the secondary school. This streetscape is lined with trees (Figure 2, view 11).

In terms of the appearance of Old Church Road, there are utility poles lining the street mostly along the north side. In front of the Caledon Town Hall building there is a substantial amount of manicured landscape (Figure 2, view 3). Light posts closely matching those installed in the core area are located along the south sidewalk adjacent to the residential development.

2.3 Area B – Innis Lake Road (Figure 3)

2.3.1 Land Use

This area features an elementary school, secondary school, St. James Cemetery (Figure 3, view 8), single detached housing units, and larger undeveloped lands.

2.3.2 Built Form

The built form is characterized by the two schools and the few residential units that are one to two storeys in height. The elementary school and all of the residential properties within the study area have frontages on to Innis Lake Road.

The building setbacks are substantial supporting the rural nature of the street.

2.3.3 Open Space

Neighbouring open spaces to Area B can be accessed by using the Caledon Trailway connection having entrance points on Innis Lake Road. This trail provides an entry to the Town Hall sports fields. The unpaved trail sees frequent pedestrian activity and crosses Innis Lake Road south of the intersection with Old Church Road (Figure 3, view 10). The grounds of the two schools are also accessible for public recreational purposes.

2.3.4 Views

Innis Lake Road is lined with mature vegetation and is characterized by its hilly appearance. The significant views in Area B are of this landscape and the continuously changing terrain (Figure 3, views 1, 7). Additional views of importance are those looking along the trail to the east and west of Innis Lake Road (Figure 3, view 10).

2.3.5 Parking

Parking along Innis Lake Road is available at the two schools and in a lot accommodating the sports fields and Caledon Trailway. There is a locked gate blocking the access to this lot, therefore restricting its use. This parking lot is located on the west side of Innis Lake Road, south of Old Church Road.

2.3.6 Streetscape Components and Appearance

Innis Lake Road is two lanes wide and does not have any streetscape components with the exception of where the road intersects with Old Church Road.

The appearance of the road is lined with wooden utility poles which are somewhat hidden amongst the mature vegetation.

2.4 Area C – Airport Road – South Portion (Figure 4)

2.4.1 Land Use

The southern stretch of Airport Road provides an entry point into Caledon East. The predominant use throughout this stretch of road is agricultural land (Figure 4, view 8) and low-density residential (Figure 4, view 6). Also within Area C is an elementary school that backs onto the west side of Airport Road.

2.4.2 Built Form

Few buildings within Area C have frontages onto Airport Road. This includes several residential units and a farmhouse. The Foodland plaza and a Bell Canada building (Figure 4, view 4) are located north of the agricultural property and are contained within the original Caledon East Streetscape Study. All of these buildings range from one to two storeys in height and have varied setbacks from the road.

2.4.3 Open Space

Within study Area C there are no open spaces owned by the Town of Caledon. In terms of recreation, the predominant space within the site is the elementary school property. There is no pedestrian connection from the rear lot line to Airport Road. However, there is a one-way driveway from the school property, north of the residential properties, exiting on to Airport Road.

2.4.4 Views

The most recognizable views throughout Area C are those seen when travelling north to enter Caledon East (Figure 4, views 1, 3, 7). Characterized by mature trees, there is a gradual transition from agricultural lands on the exterior of the settlement boundary to the downtown core of Caledon East seen from atop of the hill.

2.4.5 Parking

There is no on-street parking or visitor parking within the area at the present time. The Foodland parking lot is reserved for patrons only.

2.4.6 Streetscape Components and Appearance

Airport Road is a two lane road that is frequently travelled by automobiles and trucks. The road widens at Cranston Drive providing motorists a turning lane in each direction into the residential development.

Airport Road, within Area C, has gravel shoulders. On the west side of the road, there is an existing sidewalk. This sidewalk is a continuation of the original streetscape design and terminates at the lane providing a connection to the school grounds. Within Area C on the east side of the road there are no sidewalks. However, adjacent to the Foodland plaza, terminating at the Bell Canada Building, there is treatment continuing from the downtown area. This area overlaps with the original study context. Where these sidewalks are located, unit paving splash pads are introduced with areas of sod between the splash pads and the sidewalk (Figure 4, view 2).

Wooden utility poles line the east side of Airport Road within Area C. The west side of the road contains vegetation surrounding the residential units fronting on to the road. Vegetation also provides somewhat of a buffer to the fence of the properties on Cranston Drive flanking Airport Road (Figure 4, view 2).

2.5 Area D – Airport Road – North Portion (Figure 5)

2.5.1 Land Use

Area D represents the extension of the original streetscape study limits on Airport Road to the north settlement boundary. The land within this area contains large residential properties with some wooded areas, as well as the Biosphere Escarpment property and a designated commercial property at the northwest corner of Airport Road and Walker Road West. The residential unit on this property is of historical significance.

2.5.2 Built Form

The area consists of a residential area with a window street development on the west and reverse frontage development on the east. The development on the west is a smaller, newer development consisting of townhouse units. The lower density development in the northern portion of the study area creates a more rural feel. Buildings range in height from one to two storeys.

Building setbacks vary within Area D. To the north end of the settlement boundary, units are setback a substantial distance from Airport Road. Within the townhouse development, units have little distance from the street.

2.5.3 Open Space

The Town of Caledon does not own any open space within Area D. However, the privately owned Escarpment Biosphere on the west side of Airport Road is a wooded area providing access to neighbouring trails (Figure 5, view 1). Similarly, the east side also consists of densely forested woodlot.

2.5.4 Views

Similar to section 2.4.4 regarding view corridors in Area C, the significant views throughout Area D are those seen as one enters Caledon East (Figure 5, views 3, 7, 8).

A gateway feature is located at the entrance to the townhouse development (Figure 5, view 7).

2.5.5 Parking

There is no parking located within Area D.

2.5.6 Streetscape Components and Appearance

The portion of Airport Road north of the Caledon East downtown core is a two lane road. This road consists of gravel shoulders throughout the entirety of the study area.

Other than the existing sidewalk on the west side of Airport Road adjacent to the study area, there are no other urban streetscape components. Wooden utility poles and small wooden traffic barricades are contained amongst the mature vegetation.

2.6 Area E – Walker Road West (Figure 6)

2.6.1 Land Use

Except for few informal open spaces and a farm operation on the northwest side of Walker Road West, this area is entirely residential.

2.6.2 Built Form

The housing stock consists predominantly of new development. Within Area E there are few houses flanking on the street and a relatively long stretch of units with reverse frontage on the southeast side of Walker Road West (Figure 6, view 9).

Even though the newer houses are built close to each other, building setbacks from the street are in most cases generous supporting the more rural, rather than urban, character of the street (Figure 6, view 5).

Buildings are generally one to two storeys in height in this area.

2.6.3 Open Space

There are few informal open space areas adjacent to Walker Road West owned by the Town. One larger area is located on the southwest end of the street. Other locations are on the north side of Walker Road West surrounding the development on Filton Road (Figure 6, view 8).

2.6.4 Views

The most noteworthy view opens on the northwest side of the street where the landscape of the undeveloped farmland raises into a hill. This elevated terrain is characterized by several significant trees and a wooded area (Figure 6, views 1, 2, 3).

2.6.5 Parking

On-street parking is provided on one side of the road. Due to the ample front yard setbacks and very wide driveways, there is enough visitors parking provided on the private properties (Figure 6, view 5).

2.6.6 Streetscape Components and Appearance

Walker Road West is a two lane street with concrete sidewalks on both sides and deteriorating asphalt splash strips in between the sidewalks and the road (Figure 6, view 10). Approximately half way along the street, the sidewalk on the south side adjacent to the newest development on the west side continues further from the road and therefore creates a larger green boulevard with street trees. There is a missing section of sidewalk on the north side of the road spanning from Airport Road to the end of the commercial property at this intersection; just prior to Putney Road.

Wooden utility poles are located on the south side of the street, with incorporated street light fixtures (Figure 6, view 6). Utility boxes along Walker Road West are very visible and are not screened from view (Figure 6, view 4).

The street does not feature any street furniture at the present time.

Figure 2: Views – Area A

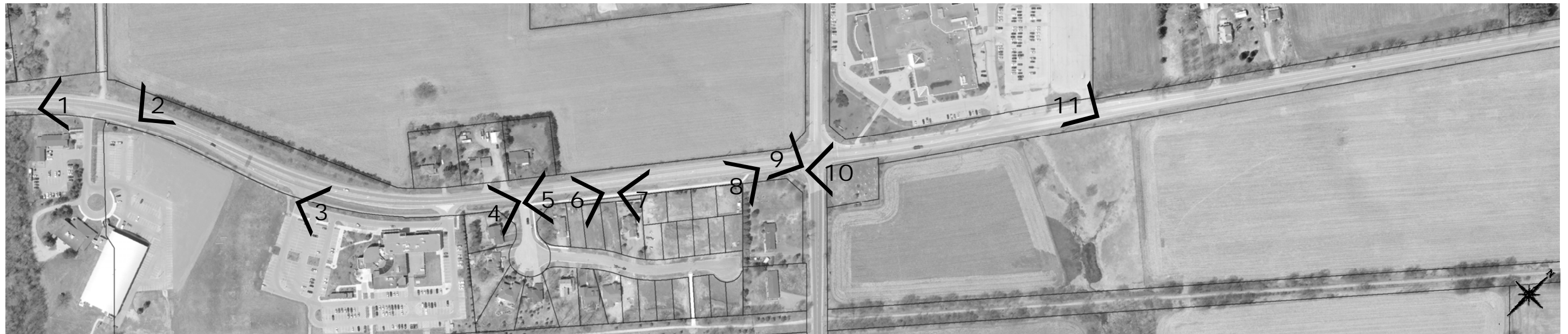


Figure 3: Views – Area B

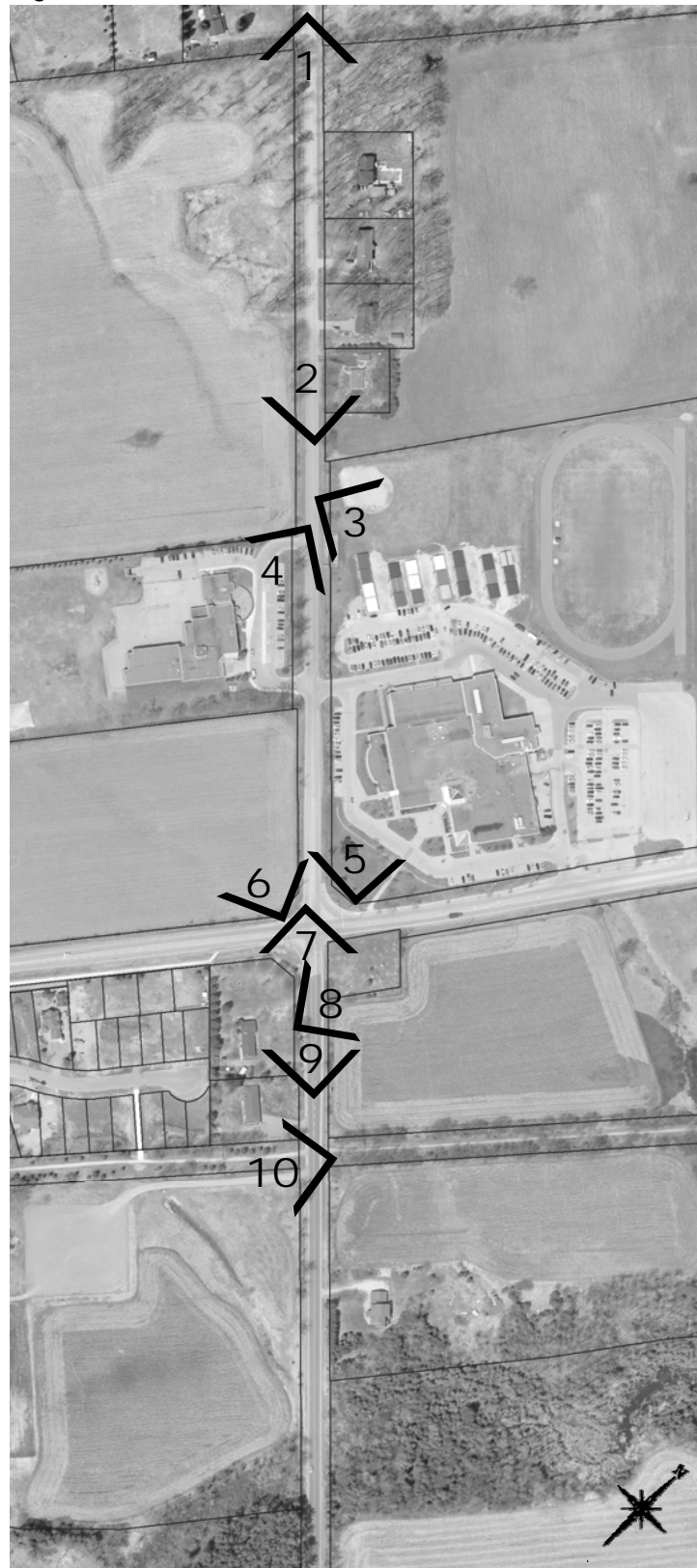


Figure 4: Views – Area C

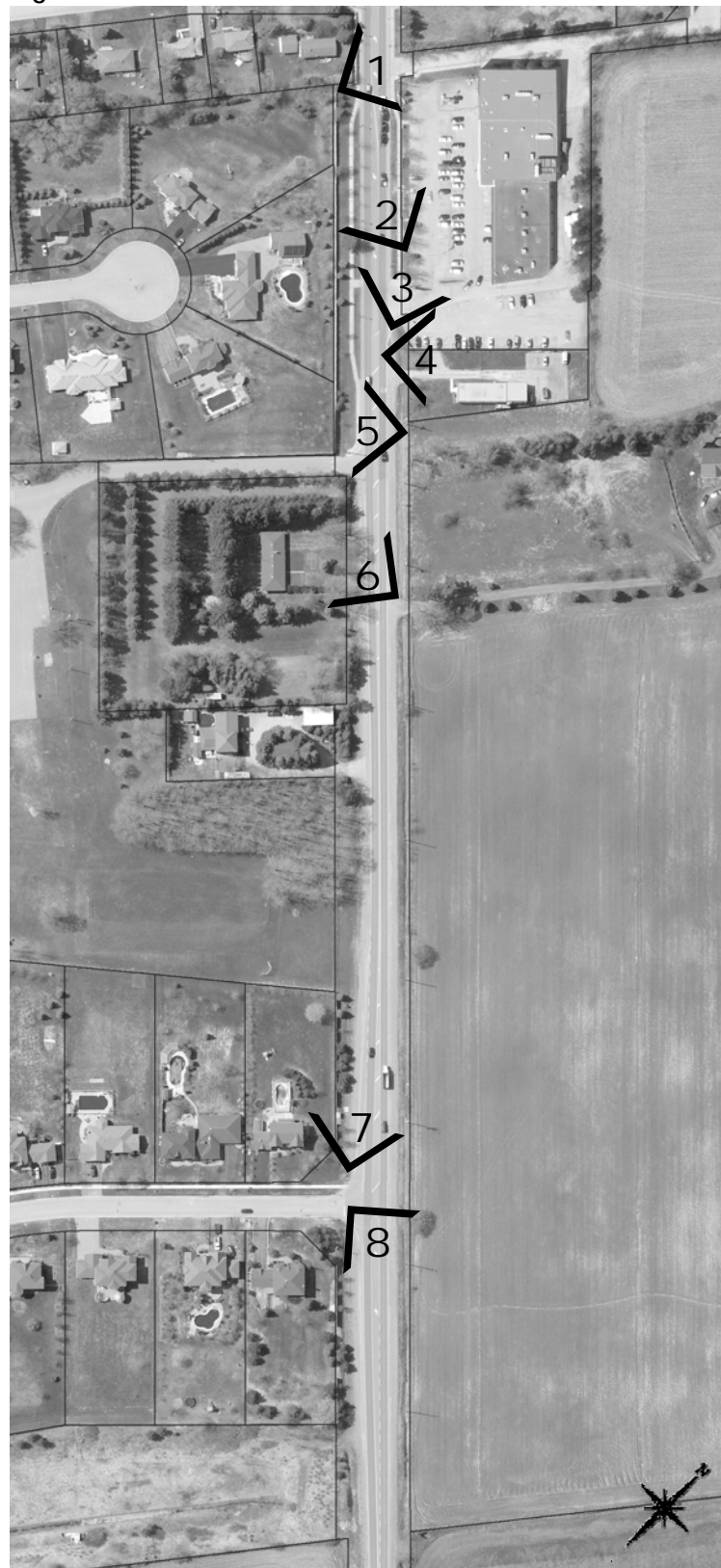


Figure 5: Views – Area D

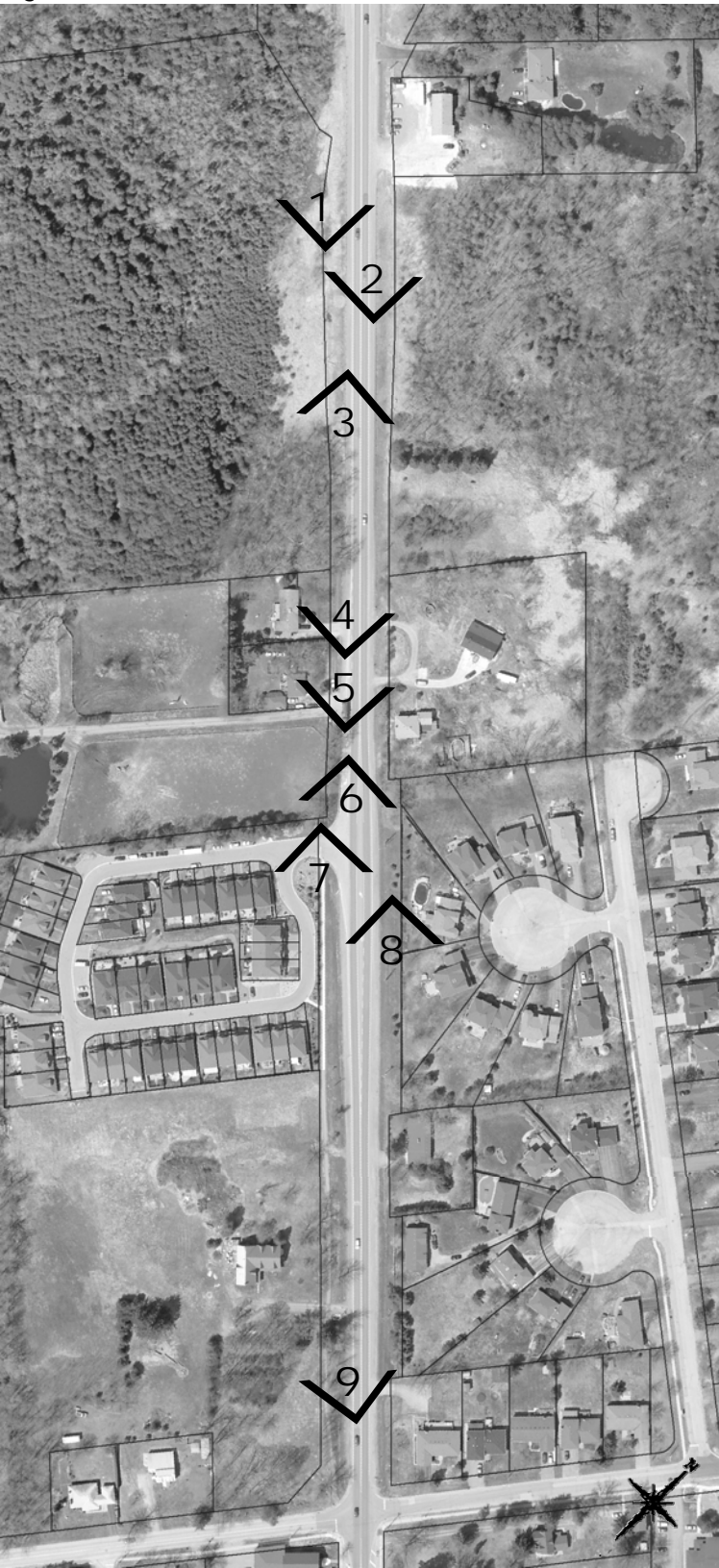


Figure 6: Views – Area E



3.0 DESIGN CONCEPT

Generally, the design concept defined in the Caledon East Streetscape Study Report is to be followed, unless stated otherwise.

Where not specified otherwise, streetscape detail as per the original study, with a change of the splash pad material, should be followed. The typical detail is indicated in Appendix E.

Following are the requirements for improvements of the area based on the Caledon East Streetscape Study Report, as well as the design concept for the new areas. All proposed designs must be consulted with, and approved by the Town of Caledon.

3.1 General Requirements for all Areas

- Change the original splash pad paver detail to specify impressed asphalt instead of unit paving. StreetPrint by Integrated Paving Concepts, or an approved equivalent, should be used. The spacing and colours are to match the previously installed pavers. This adjustment will reduce the maintenance required in terms of the settlement and weeding of the pavers.
- In new areas adjacent to uses other than residential property (i.e. mixed use, retail, commercial, institutional), sidewalks should be a minimum of 1.5 metres wide.
- Whenever adjacent to on-street or lay-by parking, splash pads should be 1.6 metres wide.
- All new development should have strong street presence on the streets. Built form should front onto the roads. Rear laneways or window streets are to be designed to eliminate multiple accesses from the main roads. Reverse frontage lots should not be permitted.
- For any new development, street trees will be required.
- All new street trees planted in the boulevard should follow the specified planting detail by Greenleaf Canada (Appendix C-1 and C-2), or approved equivalent. This will assure that the street trees will have sufficient space to grow their root systems.
- All new or replaced light fixtures are to match the light fixtures outlined and implemented through the original study. In the case that through the new design or re-design of the road (i.e. widening) it is recognized that these fixtures would not provide sufficient light levels to illuminate the road as well as the sidewalk, light fixtures of the same design and character featuring taller poles with a fixture illuminating the road, as well as a lower arm with a fixture illuminating the sidewalk are to be installed.
- New light fixtures should have electrical receptacles for seasonal lighting as well as have hanging arms for banners and flower baskets.
- All significant intersections are to be enhanced. Light poles with two fixtures should be introduced at each corner. Crosswalks should be installed using impressed asphalt of similar colour and pattern as the splash pads. Additional landscaping is encouraged where appropriate.
- All trail connections should be integrated into the streetscape design.
- New crosswalks should be installed using impressed asphalt.

- New trash receptacles are to be two or three streamed to accommodate recycling needs. The Brunel Litter Bin by Classic Displays should be used (Appendix D), or approved equivalent. Trash receptacles should be of the same character as other street furniture elements throughout the study area.
- All new or replaced street furniture is to match the street furniture outlined in the original study. Exceptions might be granted to furniture which is also a form of public art.
- Any front yard fencing proposed adjacent to the streets included in this study should be designed as low decorative fencing to maintain the views.
- Screen all of the noise attenuation walls and rear yard privacy fencing by the addition of planting, creation of living walls, or a combination of both options. This will create a more comfortable and visually pleasing pedestrian environment.
- Existing utility boxes, such as those on Walker Road West (Figure 6, view 4), and any new utility boxes should be screened from view. The screening should not interfere with the accessibility to the units.
- All new retaining walls are to be the same material and style as the existing retaining walls on the south hill.

3.2 Area Covered by the Original Caledon East Streetscape Study

- It was stressed by the residents that traffic safety, as well as parking provision on Airport Road are of a concern. A parking study should be prepared to identify the possible areas of improvement (i.e. potential for additional parking). Additional safety measures for pedestrians should be examined, such as countdown pedestrian signal lights at signalized intersections.
- Implement a crosswalk on Airport Road to provide a safe connection for the Caledon Trailway.
- Public art is encouraged to replace areas where the street trees were planted and did not survive. These 'public art coins' will create interest along the street while showcasing local talent and artistry. The initiative will also instigate further pedestrian movement in the area. Where multiple consecutive trees are missing, a vertical element should be introduced.
- Trash and recycling receptacles are to be replaced to create a unified look (Appendix D).
- All trash and recycling receptacles should be relocated a greater distance away from benches.
- An additional seating area should be introduced on the northwest corner of the intersection of Airport Road and Walker Road West.
- Recently implemented bollards are to be replaced with those identified in the Caledon East Streetscape Study.
- Create a Special Sign District as proposed by the original Caledon East Streetscape Study. This will affect the street signage as well as the signs of stores and local businesses.
- Signage should be added on Airport Road adjacent to the Caledon Trailway indicating parking areas adjacent to the trail system, such as in front of the community complex and around the Town Hall building.

- The existing newspaper stands should be relocated in a single housing unit consistent with the character and style of the street furniture proposed in the original Streetscape Study.

3.3 Area A – Old Church Road (Figure 7)

- Streetscape treatment should continue as per the original study on Old Church Road, west of Innis Lake Road. Streetscape is required to connect with the existing elements ending at the intersection of Old Church Road and Marilyn Street.
- In front of the Town Hall and community facilities, streetscape should create a prominent pedestrian environment through the implementation of a wider sidewalk, enhanced landscaping, an open view toward the Town Hall and added street furniture. Trees are to cover the sidewalk in a canopy creating an ornamental alley to these facilities (Appendix F-1).
- Additional landscaping should be added to screen parking in front of the Caledon Community Complex.
- The treatment of the right-of-way in front of the live work units on the north side of Old Church Road is required to have wider sidewalks and enhanced landscaping (Figure 2, view 9).
- Splash pads, as per the original study, should be added on the south side of Old Church Road between the Town Hall entrance and Innis Lake Road.
- Due to the fact that the settlement boundary only allows for the lands on the north side of Old Church Road (east of Innis Lake Road) to be developed at this time, street trees should be planted on the south side of the Road, east of Innis Lake Road to soften the contrast of the undeveloped lands.
- If the lands on the north side of Old Church Road, east of the secondary school, be developed as sports fields or parkland, sidewalk and street trees will be a sufficient treatment and will provide a transition into the more rural areas east of Innis Lake Road. If developed as residential, urbanized streetscape should continue as per the original study.
- If the entrance in front of the Town Hall building is developed as a roundabout, special attention should be given to this area including a landscape feature in the centre of the roundabout.
- A gateway feature should be added on Old Church Road, east of the secondary school (Figure 2, view 11) to welcome travelers from the east and to aid in the screening of the school parking lot.
- Due to the prominence of the institutional uses within the study area, it should be requested for all new development to have the built form front onto Old Church Road. Rear laneways or window streets are encouraged to reduce the number of accesses on Old Church Road.
- A Town Square across from the Town Hall on Old Church Road will provide a prominent outdoor public gathering space. The Town Square must be of high quality design, provide an ample amount of seating areas and include a significant feature; such as a form of public art or a water feature.

3.4 Area B – Innis Lake Road (Figure 8)

- Continue the streetscape design as per the original study on the west side of the road up to the north property boundary of the school and south on Innis Lake Road to the most southern limit of the new OPP station.
- On the east side of the road, continue the streetscape from the Streetscape Study to the northern limits of the property designated as Policy Area – Residential.
- The north portion of the west side of the road will feature a parallel window street which will provide pedestrian connection to and from Innis Lake Road. Therefore, only enhanced landscaping and a sidewalk connecting to George Crescent in this area is suggested.
- Implement a crosswalk on Innis Lake Road, south of Old Church Road, to provide a safe connection for the Caledon Trailway.
- The gate restricting the use of the parking lot, accessible from Innis Lake Road, should be removed. The lot accommodates the Town Hall sports fields. Bollards or heavy landscaping should be added surrounding the perimeter of the lot to prevent vehicles from driving on the turf.

3.5 Area C – Airport Road – South Portion (Figure 9)

- The original streetscape concept design should be continued on the west side of Airport Road to Cranston Drive and on the east side to the southernmost limits of any proposed development.
- Additional landscaping, cluster of benches and trash receptacle should be added on the west side of Airport, north of Cranston where the school property backs on to the road.
- Create a new landscaped gateway feature at the terminating location of the streetscape design or edge of the settlement boundary.
- New developments should be designed with units having street frontage onto Airport Road. Access to units should be provided by rear laneways or window streets to eliminate multiple accesses from Airport Road.
- To reflect the mature, natural feel of the west side, a double row of street trees should be implemented on the east side of the road adjacent to any new development.
- Implement a crosswalk connecting streetscape on the west side of Airport Road to the east side in front of the Foodland plaza. Paving has already been implemented at this location as per the original study.
- A pedestrian connection toward Mono Road should be designed and implemented. This connection is suggested to be in the form of a pathway from Cranston Drive, located behind the existing swale.

3.6 Area D – Airport Road – North Portion (Figure 10)

- A pedestrian path should continue on the west side of Airport Road to the southern property line of the Escarpment Biosphere property accommodating any future trail connections. This path should be aligned with the existing sidewalk towards the south of Area D.

- If development occurs on the east side of Airport Road at a distance from Airport Road, a pedestrian crossing is recommended to connect the sidewalk on the west side of Airport Road to a trail connecting the path to the new development.
- If future development on the east side of Airport Road is close to Airport Road then full streetscape detail, as per the study, is recommended to extend from the core area to the northern limit of new development. If this is the case, it is a requirement that units front onto Airport Road and have individual access by a rear lane or window street.
- Street trees should be added on the west side of Airport Road from Leamster Trail to Walker Road West.
- Should the commercial property at the intersection of Walker Road West and Airport Road be developed, the streetscape as per the original study should be implemented on the west side of the road. It should continue around the corner onto the north side of Walker Road West to the access point of this commercial property.

3.7 Area E – Walker Road West (Figure 11)

- The sidewalk should continue to the end of the road; at minimum on the south side.
- To improve the visual appearance of the street and to tie Walker Road West with the village core, the asphalt splash pad on the north side of the road will be replaced by impressed asphalt of the same pattern and colour as the splash pad implemented on Airport Road. The splash pad on the south side should be replaced by sod, or soft landscaping to narrow the hard pavement area and to maintain the rural character of this road.
- If the commercial property at the intersection of Walker Road West and Airport Road is developed, the streetscape as per the original study should be implemented on the north side of Walker Road West. It should continue to the access point of this commercial property. Sidewalk should be implemented on the north side surrounding the commercial property where there is currently no pedestrian connection.
- The feasibility of the installation of traffic calming measures should be examined on Walker Road West.
- Due to the length of the street, one bench and a trash receptacle should be located in the informal open space on the north side of the road.
- New development is required to have street frontage onto Walker Road West. The built form is to front onto the road with accesses to individual units provided by rear laneways or a window street. This will reduce the number of access points on Walker Road West.
- Should the farmland be developed, every effort should be made to maintain the existing views. The grading, wooded areas and any recognized significant trees should be protected.

Figure 7: Design Concept – Area A

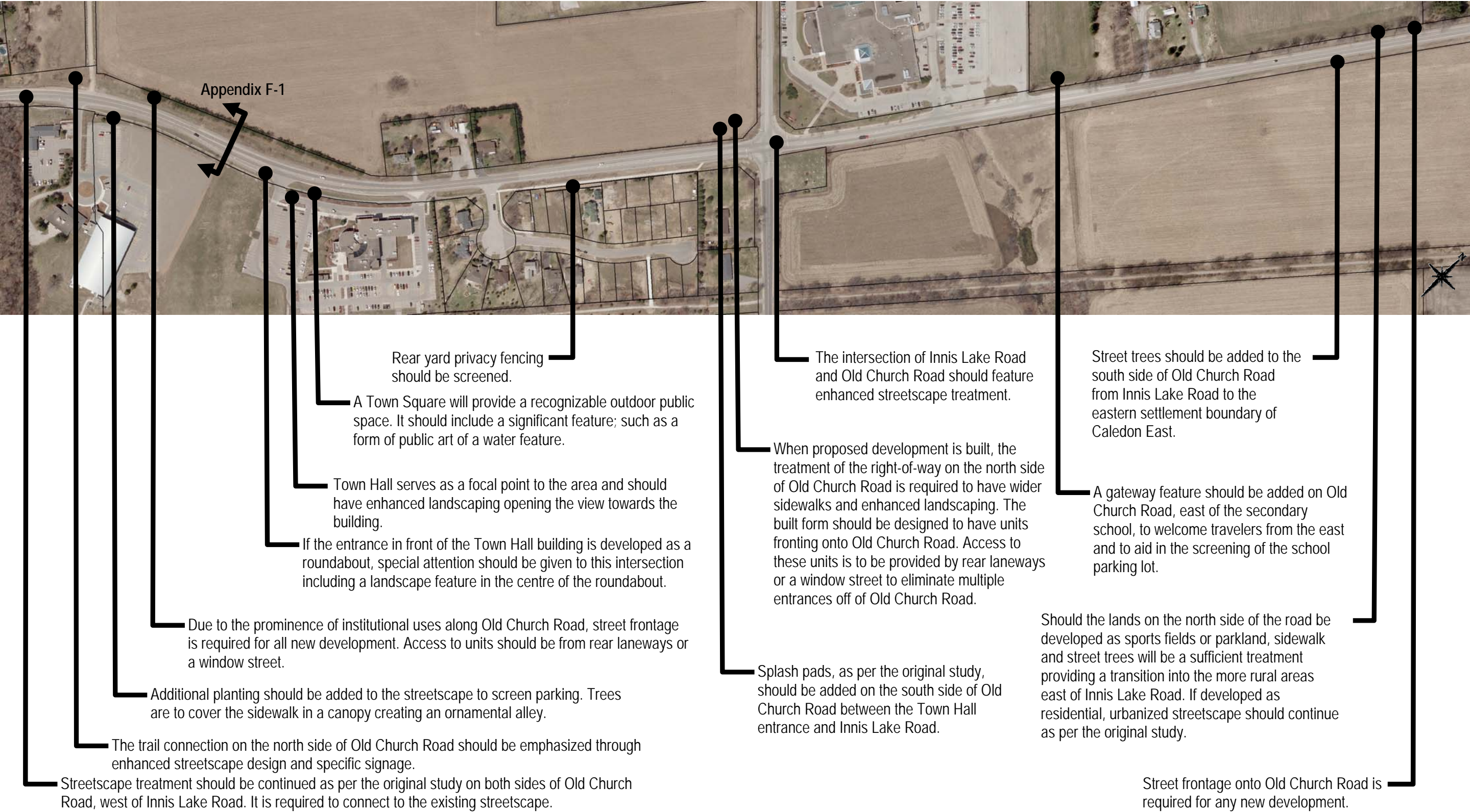


Figure 8: Design Concept – Area B

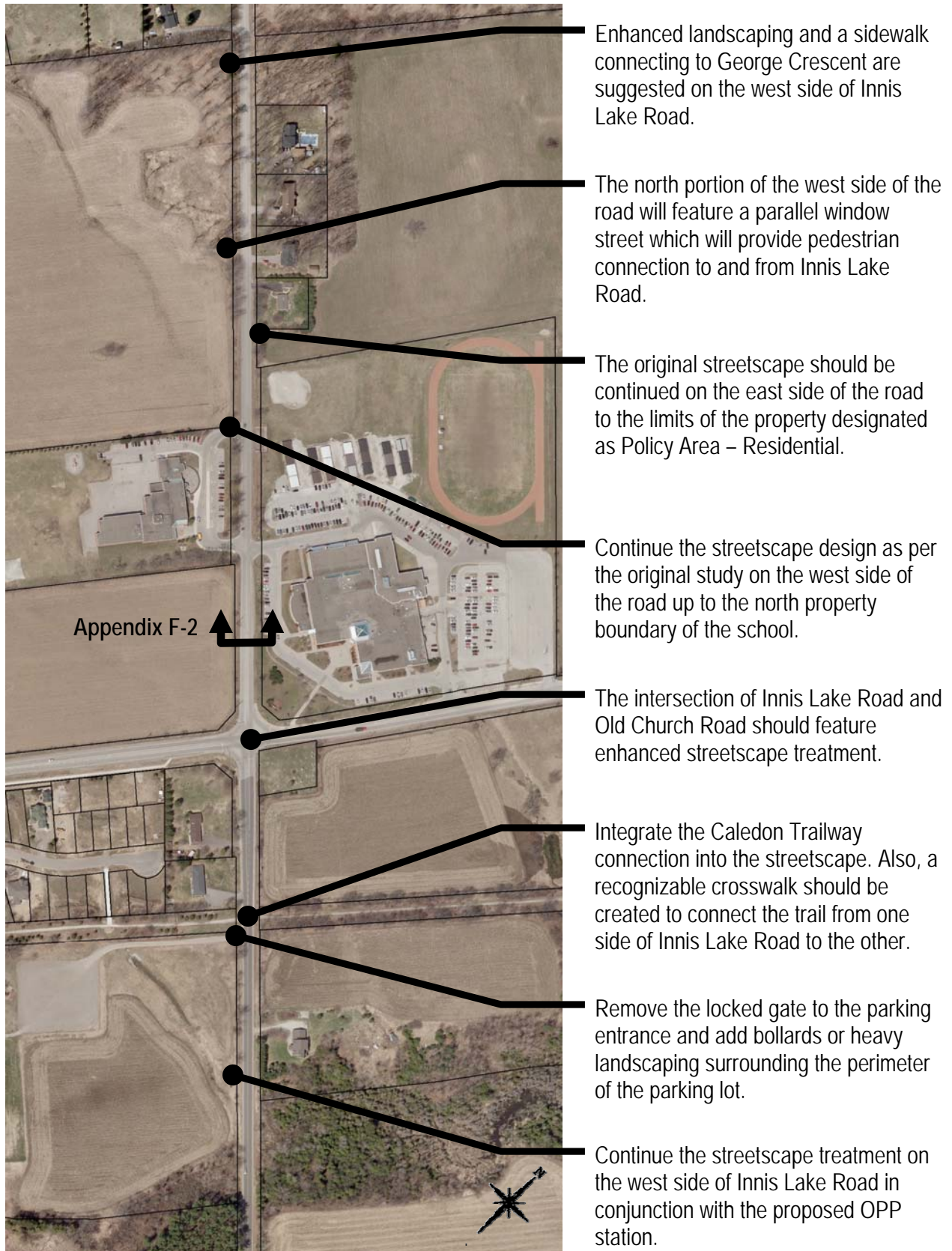


Figure 9: Design Concept – Area C

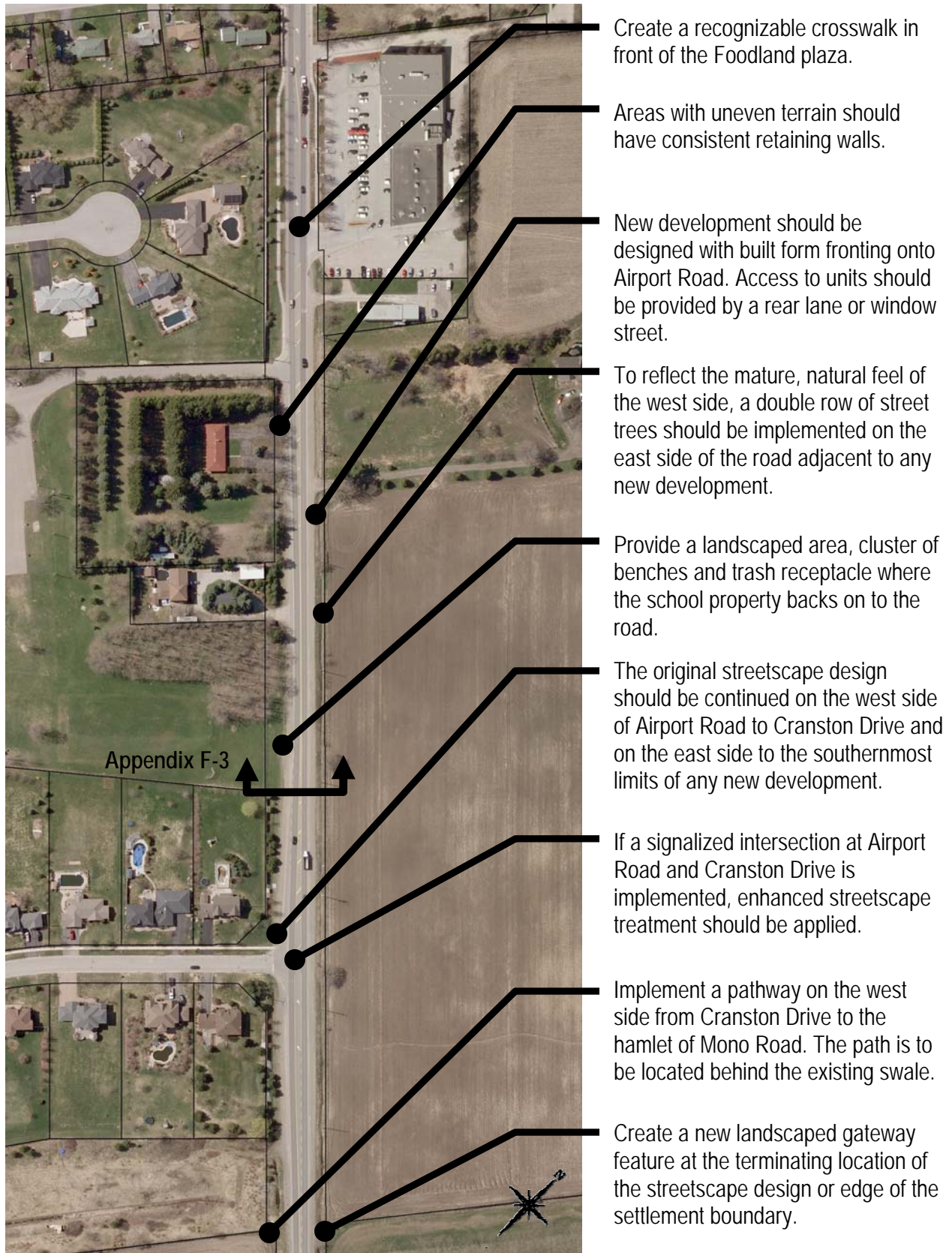


Figure 10: Design Concept – Area D



If future development on the east side of Airport Road is built close to Airport Road then full streetscape detail, as per the study, is recommended extending from the original study limits to the northern limits of the new development. If this is the case, it is a requirement that units front onto Airport Road and have access by rear lanes or a window street.

A crosswalk connecting the east side of Airport Road to the west side should be implemented.

If development occurs on the east side at a distance from Airport Road, a walkway through the development will connect Airport Road with Huntsmill Drive.

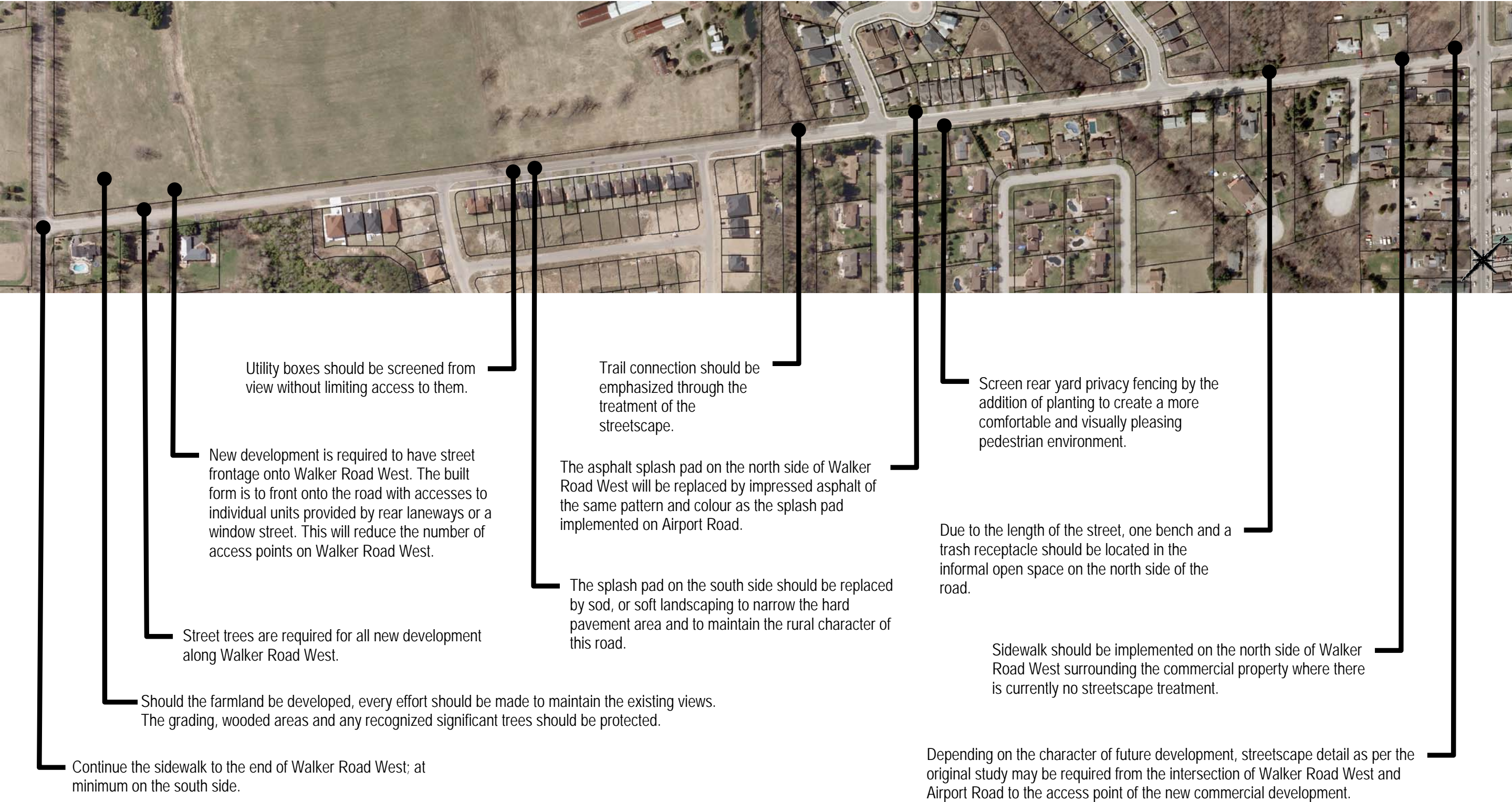
If a future trail is proposed or if development occurs on the east side of Airport Road, a pedestrian path should be provided on the west side of the road aligned with the sidewalk on the south (away from the road) to the southern property line of the Escarpment Biosphere property.

Enhanced landscaping should be provided to screen the noise attenuation fences.

Street trees are required on the west side from Leamster Trail to Walker Road West.

Depending on the character of future development, streetscape detail as per the Study may be required.

Figure 11: Design Concept – Area E



4.0 IMPLEMENTATION STRATEGY

After receiving a positive feedback from the public represented by the Caledon East Revitalization Committee on October 30, 2009, by the Community Advisory Group (November 11, 2009), as well as from Council (Council Workshop October 20, 2009), following are the recommendations on the implementation strategy for the project.

The design concepts consist of both guidelines applicable through the new development and redevelopment in the area (i.e. required design and construction details, specified street furniture), as well as specific actions to be undertaken by the Town (i.e. Sign By-law amendment, creation of Public Art Program, etc.). Some of the proposed actions are easier to achieve than others; some depend on new development or redevelopment of the area, partnerships with other levels of government, local businesses and non-profit organizations such as the Caledon East Revitalization Committee and the public, and means of funding.

To assure implementation of all elements of the project, following the approval of this Addendum, a detailed Action Plan should be prepared. The Action Plan will suggest targets for all of the actions, as well as propose the designated department(s)/individual(s) responsible for each action, and potential partners and funding opportunities.

Generally, most of the streetscape installation in the new areas included in the Addendum will be triggered by the new development in the area.

Following are the suggested actions which could be implemented prior to occurrence of the new development, or which will be implemented through development of new areas but should be initiated by the Town:

4.1 Parking

- Provide a parking study to identify the possible areas of improvement. Identify any potential properties along Airport Road which could be utilized to carry additional parking needs. Secure funding to follow up with the findings of the parking study.
- Design and implement a barrier along the perimeter of the parking lot accessible from Innis Lake Road to allow for full access to this area.

4.2 Landscaping

- Prepare detailed landscape design for the areas where the rear yard privacy fencing and noise attenuation walls are adjacent to the roads to screen them from the view. Include the area of the chain link fence separating the school property from Airport Road. Initiate discussion with the School Board about the possibilities of creating a connection to the school property through this area. Prepare design to add street trees within the boulevard on the west side of Airport Road in Area D. Create a program to engage the community in a "planting day". To obtain planting material, funding options such as donations from local garden centres, or fundraising program should be considered.

- Prepare detailed design of enhanced landscaping in front of the Town Hall and Community facilities to screen the parking and to create a prominent pedestrian environment in this location, as well as open the views toward the Town Hall as a focal point. This action could be implemented also through the engagement of Town staff.
- If the intersection in front of the Town Hall is developed as a roundabout, work with the Region to prepare landscape design for the centre of this feature.
- Work closely with the designer of the proposed Town Square to assure that this only exterior gathering space in Caledon East is of a high design quality and features all the aspects of the successful public space.
- Plant street trees along the south side of Old Church Road, east from Innis Lake. This action will be triggered by the development on the north side of the road.
- Prepare detail design for the landscape gateway feature on the south settlement boundary on Airport Road and assure implementation through the planning application process for the new development on the east side of the road.
- Design and install a new gateway feature for the entry point from the east (Old Church Road).
- Revise design of the landscaped areas on the east and west sides of Airport Road where the south gateway feature is currently located. For stronger presence of this gateway, landscaped areas on both sides should be complimentary and balanced.

4.3 Public Art

- Prepare and organize a competition for paved public art coins in the core area. Review the opportunity to create a Public Art Policy to guarantee funding. Prepare a separate plan to identify the opportunities to obtain the vertical sculptural pieces. Partner with local artists and prepare a marketing campaign.

4.4 Street Furniture

- Secure funding to replace all garbage receptacles with new, two or three stream collection bins, similar in character with the originally installed street furniture. Locate the garbage receptacles further from the seating areas.
- Secure funding to replace all recently implemented bollards. An alternative design to the originally proposed bollards might be specified. In such a case, the design of the new bollards should be in keeping with the character of the street furniture implemented through the original study.
- Secure funding to refurbish the existing benches.
- Secure funding to install an additional bench and garbage receptacle on Walker Road West.
- Additional seating areas should be installed on the west side of Airport Road, close to the school property, as well as at the northwest corner of the intersection with Walker Road West (commercial property). Installation of these benches will be triggered by the new development in this area. Cluster seating should be secured for all future installations in large areas and areas with large setbacks from the road.
- Prepare the design and implement the screening of utility boxes from view.

- Prepare the design of housing units for the newspaper stands. Engage in discussion with the publishers of the papers for potential funding opportunities.

4.5 Intersections

- Detailed designed in terms of location of the street lighting posts with two fixtures, crosswalks, and enhanced landscaping wherever possible will be prepared for the following intersections: Airport Road and Cranston Drive; Airport Road and Old Church Road, Airport Road and Walker Road; Old Church Road and future Street A; and Old Church Road and Innis Lake Road. This design will be followed at the time of redevelopment in the area.

4.6 Trail Connections

- Prepare and implement a detail design for the integration of all of the trail connection points with the road network. The design should feature a unifying concept and should include special vertical signage, as well as signage in the sidewalk, or splash pad pavement.
- Additional signage will be implemented on Airport Road to indicate parking areas adjacent to the trail system, such as in front of the community complex and around the Town Hall building.

4.7 Pedestrian Connections

- Throughout the study of Caledon East, several areas have been identified requiring the implementation of pedestrian connections. Some of them are reasonable to be implemented through the new development, such as a walkway connecting the future window street adjacent to Innis Lake Road to George Crescent, a walkway connecting the existing sidewalk on the west side of Airport Road to the Escarpment Biosphere property, sidewalk from this point through the new development on the east side of the road toward Huntsmill Drive, and sidewalk on the west side of Airport Road connecting Cranston Drive to the sidewalk further north, implemented through the original Study. The missing pedestrian connection on the north side of Walker Road West, as well as continuation of the sidewalk on the south side toward Mountainview Road should be planned independently upon new development in this area. The sidewalk on the west side of Airport Road in Area D should be designed in consultation and cooperation with the Escarpment Biosphere.
- Partner with the Region to design and install a pathway connecting Caledon East and Mono Road.

4.8 Crosswalks

- Three new impressed asphalt crosswalks should be implemented on Airport Road: to connect the Caledon Trailway, to connect the Foodland plaza with the west side of the road, and to connect proposed pedestrian walkways north from Walker Road, in front of the Biosphere Escarpment property (to connect to Huntsmill Drive on the east side). One crosswalk should be added on Innis Lake Road, south of Old Church Road, to

connect the Caledon Trailway. Partner with the Region to assure that design of the crosswalks is as per their standard practice and that a Ministry of Transportation warrant for installation has been met.

- Prepare design and implement an additional crosswalk connecting the Caledon Trailway through Innis Lake Road.

4.9 Signage

- As per the original Study, the Sign By-law should be amended to acknowledge Caledon East as a Special Sign District. A unifying concept respecting streetscape elements will be prepared for all street signs, as well as commercial/retail signs.
- The aforementioned signage for trial connection points will be designed with respect to the future Special Sign District concept.

4.10 Splash Pad Resurfacing on Walker Road West

- Secure funding for resurfacing of the deteriorating asphalt splash pads on Walker Road West. Implement impressed asphalt splash pads on the north side of the road, and sod areas on the south side.

4.11 Pedestrian Safety

- Partner with the Region to install a countdown pedestrian signal at the intersection of Airport Road and Old Church Road.
- Provide a speed survey to identify if traffic calming measures are required on Walker Road West.

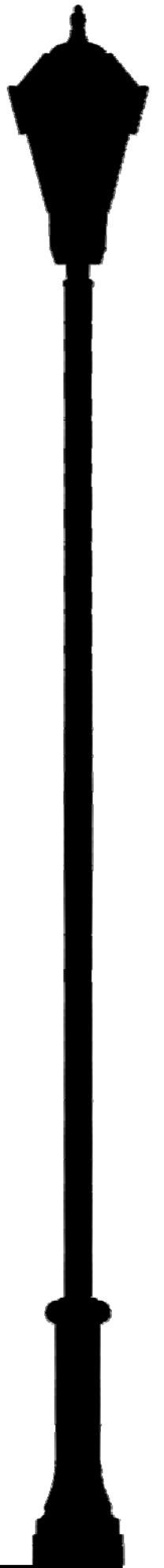
5.0 SUMMARY

The original Caledon East Streetscape Study created streetscape design for two main streets in Caledon East. This study helped in defining the character of these two streets and the village as a whole.

This Addendum addresses the streetscape of the original study area; as well it includes additional areas of potential development. Some of the suggested actions are achievable within a short period of time; others will be triggered by new development accommodating the growing population of Caledon East. It is suggested to prepare a detailed Action Plan to clearly identify targets, designated department(s)/individual(s), partners and funding for the actions which could be addressed at this time.

The continuation of the originally proposed streetscape design through other significant areas, as well as the addition of elements such as public art, landscaping, and signage to indicate trail connections will help in creating a more vibrant village core and promote further pedestrian movement. The proposed concepts will also strengthen the character of the area as the administrative centre for the Town. The implementation of the addendum will assist Caledon East to become a recognizable destination.

APPENDICES



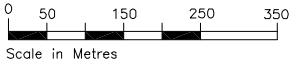
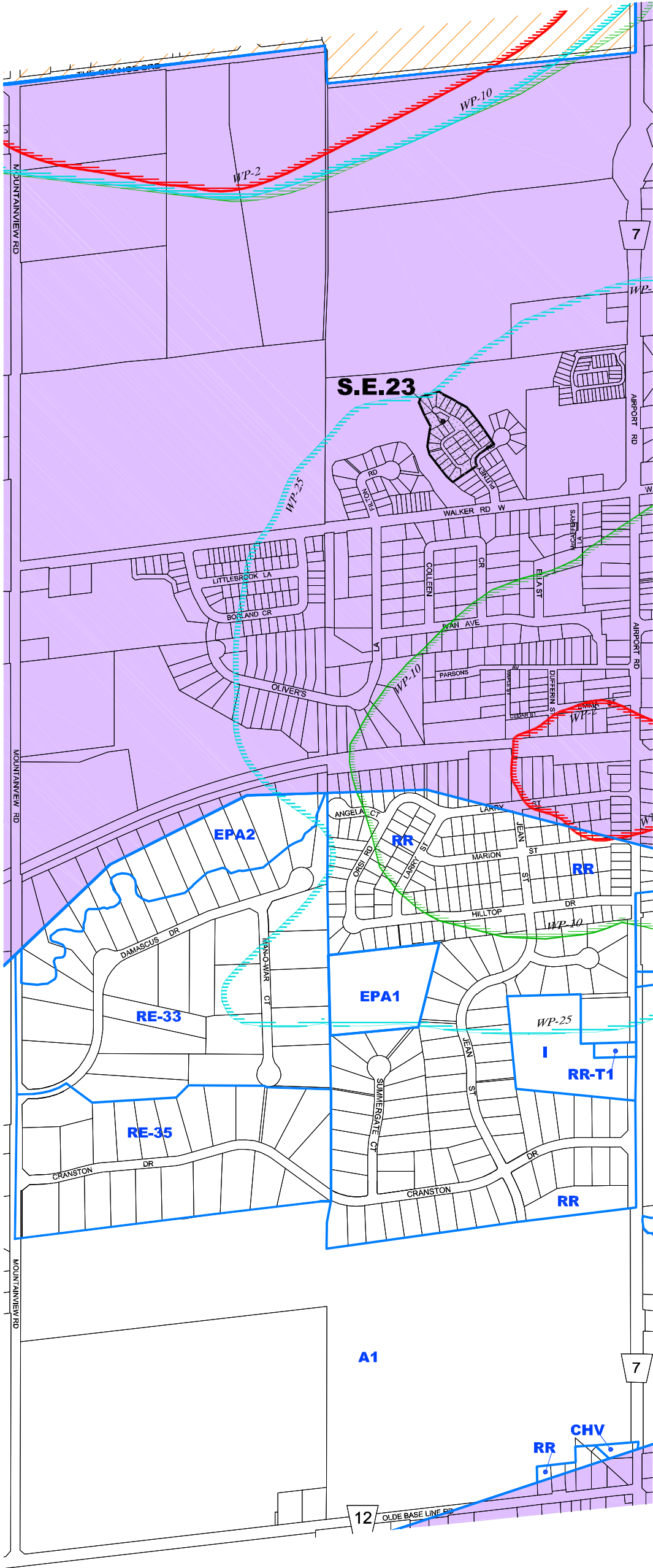
LOT 5

LOT 4

LOT 3

LOT 2

LOT 1



This copy is provided for convenience only. If necessary, the original may be referred to in the office of the Town Clerk.

LEGEND

- A2 ZONE SYMBOL**
- A2-### ZONE SYMBOL**
NOTE: Number suffixes represent Exceptions which can be looked up in the Exceptions section of the By-law.
- ZONE BOUNDARY**
- SE STRUCTURAL ENVELOPE MAP**

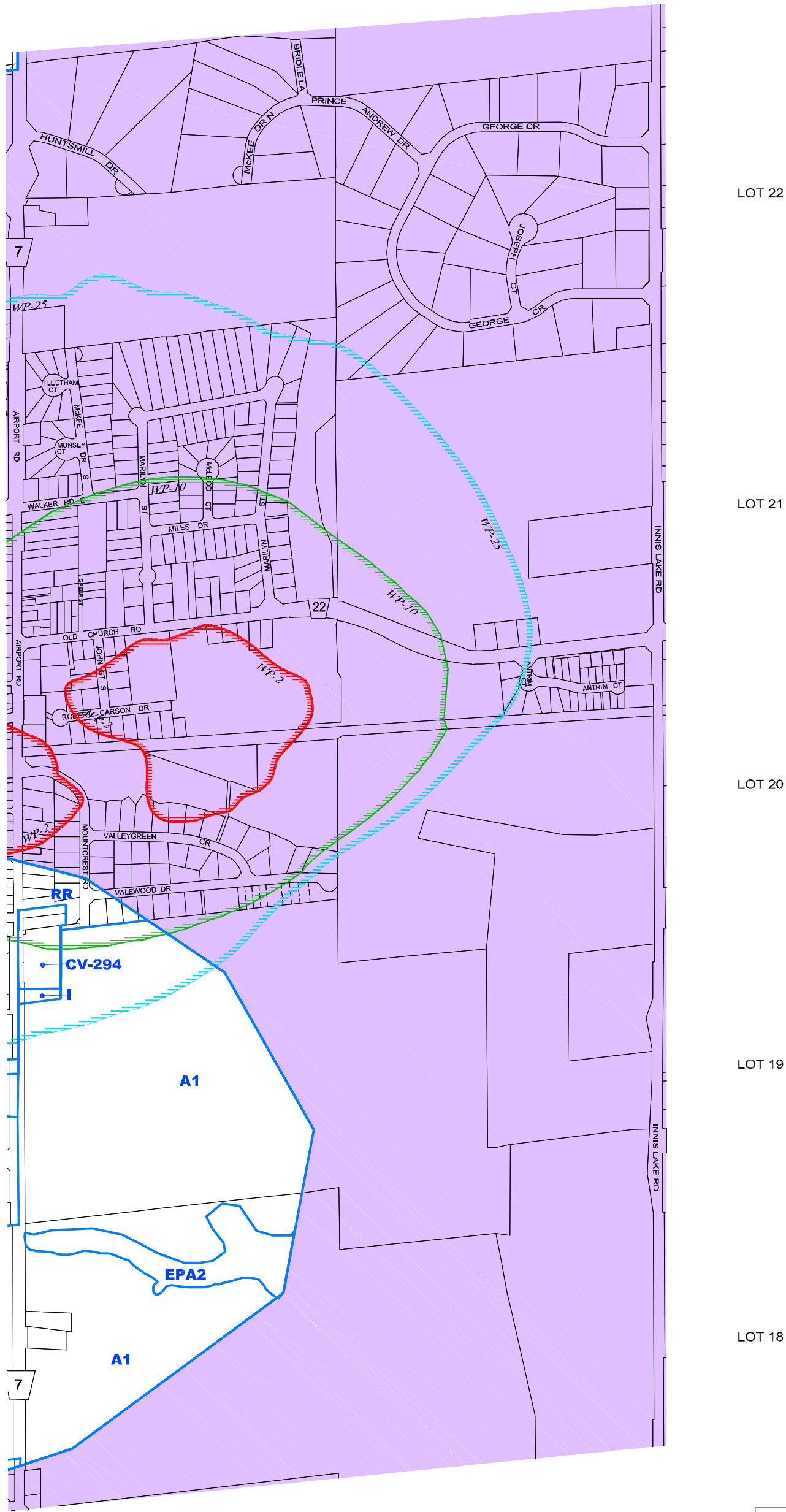
- NIAGARA ESCARPMENT DEVELOPMENT CONTROL AREA**
Lands lying within the Development Control area pursuant to the Niagara Planning and Development Act are subject to permit requirements under Ontario Regulations 685/80, as amended.
- OAK RIDGES MORaine CONSERVATION PLAN AREA BOUNDARY**
- WELLHEAD PROTECTION AREA BOUNDARY**
WP-2 WP-5 WP-10 WP-25
Zone Maps amended to indicate the 2, 5, 10 and 25 year Wellhead Protection Areas.

For applicable zoning see By-law 87-250, as amended.

BY-LAW 2006-50
ZONE MAP 36a
SCHEDULE "A"



DATE: April 3, 2006 REVISED: Dec. 9, 2008
CREATED BY: D.S. CHECKED BY: S.M.
FILE: Zone Maps 2006-50 and 2007-42



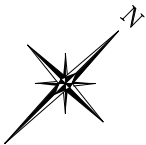
LOT 22

LOT 21

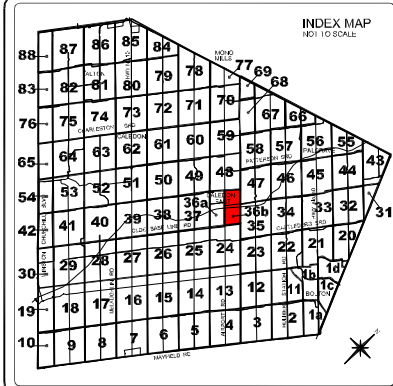
LOT 20

LOT 19

LOT 18



This copy is provided for convenience only. If necessary, the original may be referred to in the office of the Town Clerk.



LEGEND

- A2** ZONE SYMBOL
- A2-###** ZONE SYMBOL
- NOTE: Number suffixes represent Exceptions which can be looked up in the Exceptions section of the By-law.
- ZONE BOUNDARY**
- SE** STRUCTURAL ENVELOPE MAP

- NIAGARA ESCARPMENT DEVELOPMENT CONTROL AREA**
Lands lying within the Development Control area pursuant to the Niagara Planning and Development Act are subject to permit requirements under Ontario Regulations 685/80, as amended.
- OAK RIDGES MORaine CONSERVATION PLAN AREA BOUNDARY**
- WELLHEAD PROTECTION AREA BOUNDARY**
WP-2 WP-5 WP-10 WP-25
Zone Maps amended to indicate the 2, 5, 10 and 25 year Wellhead Protection Areas.

For applicable zoning see By-law 87-250, as amended.

BY-LAW 2006-50
ZONE MAP 36b
SCHEDULE "A"



DATE: April 3, 2006
CREATED BY: D.S.
CHECKED BY: S.M.
FILE: Zone Maps 2006-50 and 2007-42

SCHEDULE D
CALEDON EAST
LAND USE PLAN



LEGEND

- MEDIUM DENSITY RESIDENTIAL
- LOW DENSITY RESIDENTIAL
- SPECIAL RESIDENTIAL
- POLICY AREA - RESIDENTIAL
- GENERAL COMMERCIAL
- SPECIAL USE AREA
- FLOOD POTENTIAL STUDY AREA
- INSTITUTIONAL
- SPECIAL STUDY AREA
- ENVIRONMENTAL POLICY AREA
- OPEN SPACE POLICY AREA

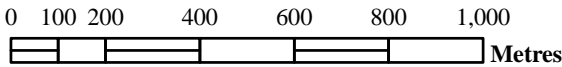
- OPEN SPACE LINK
- OAK RIDGES MORaine CONSERVATION PLAN AREA
- 2021 SETTLEMENT BOUNDARY
- LOT & CONCESSION LINES
- LOCAL ROADS
- REGIONAL ROADS
- CALEDON TRAILWAY
- CONCEPTUAL VEHICULAR CONNECTION

- CONCEPTUAL PARKETTE LOCATION
- CONCEPTUAL NEIGHBOURHOOD PARK LOCATION
- GATEWAY FEATURES

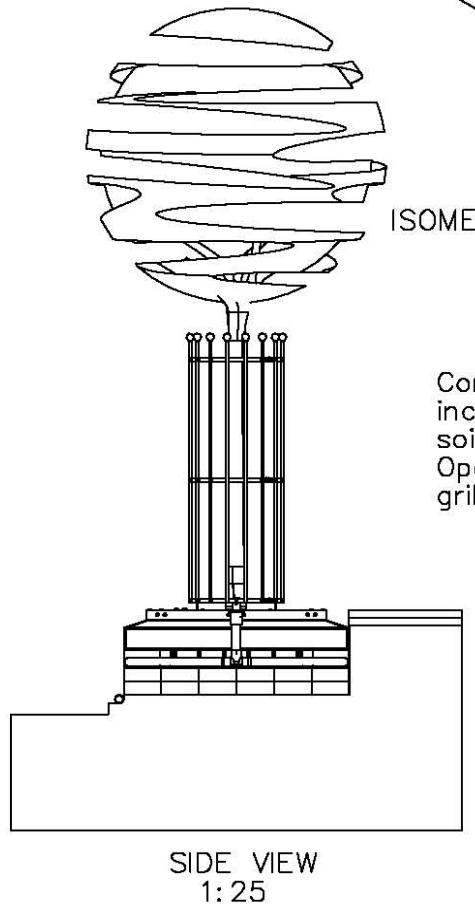
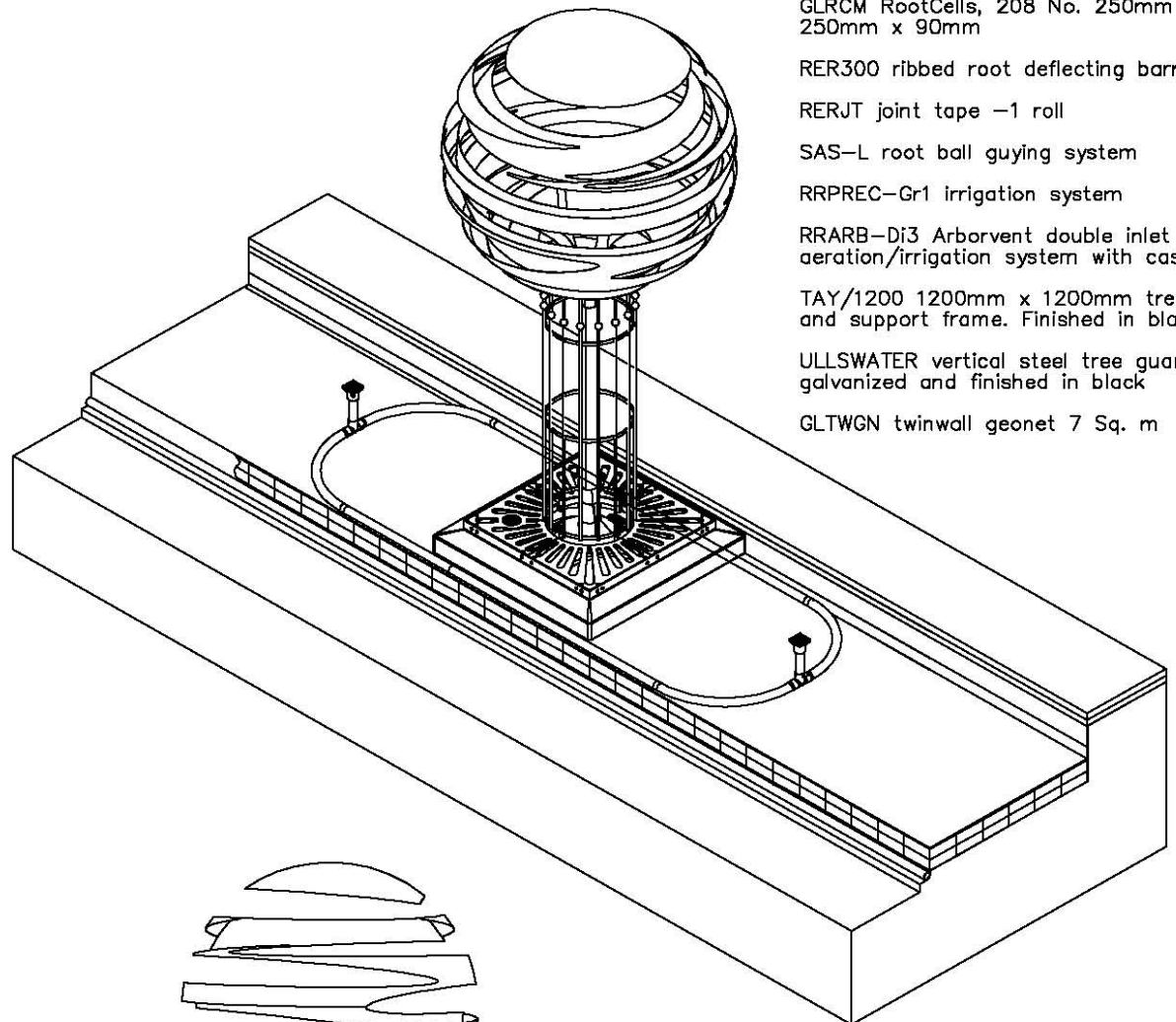
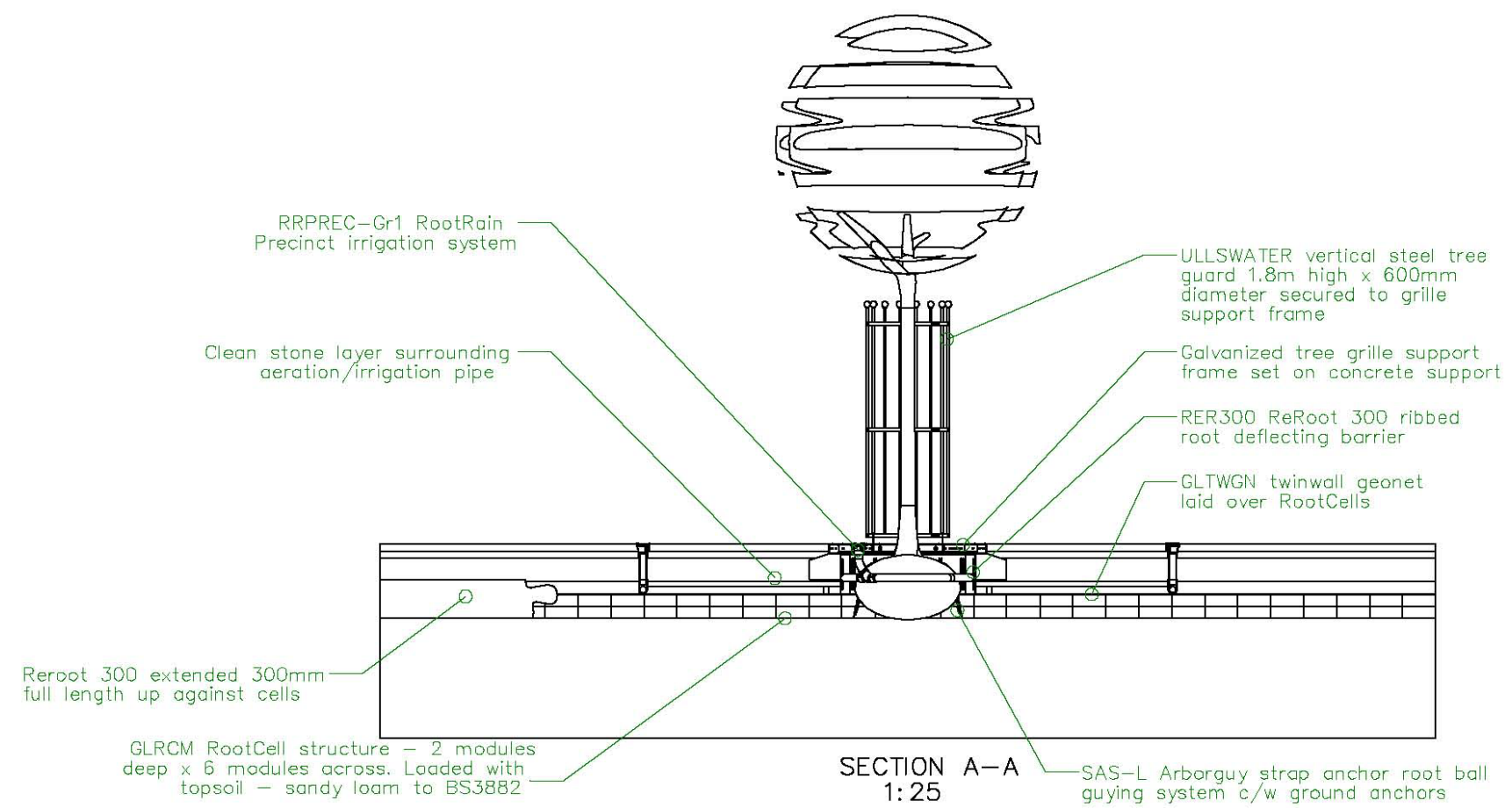
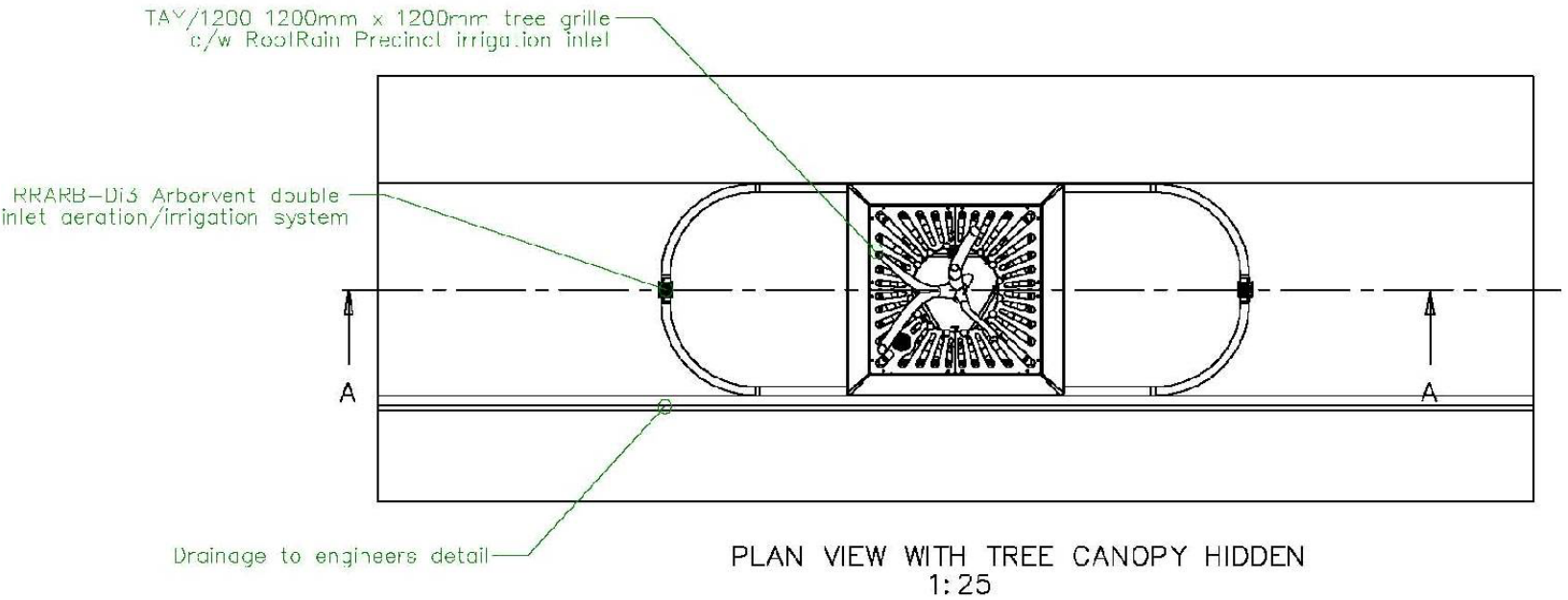
NOTE:
This Schedule represents the consolidated Official Plan Schedule, as of December 31 2008.

If necessary, reference should be made to the Town of Caledon Planning & Development Department or the Town Clerk for confirmation of current Minister approved policies.

Base Data Source: Town of Caledon



Street Tree within Hard Surface Boulevard **Greenleaf Canada**

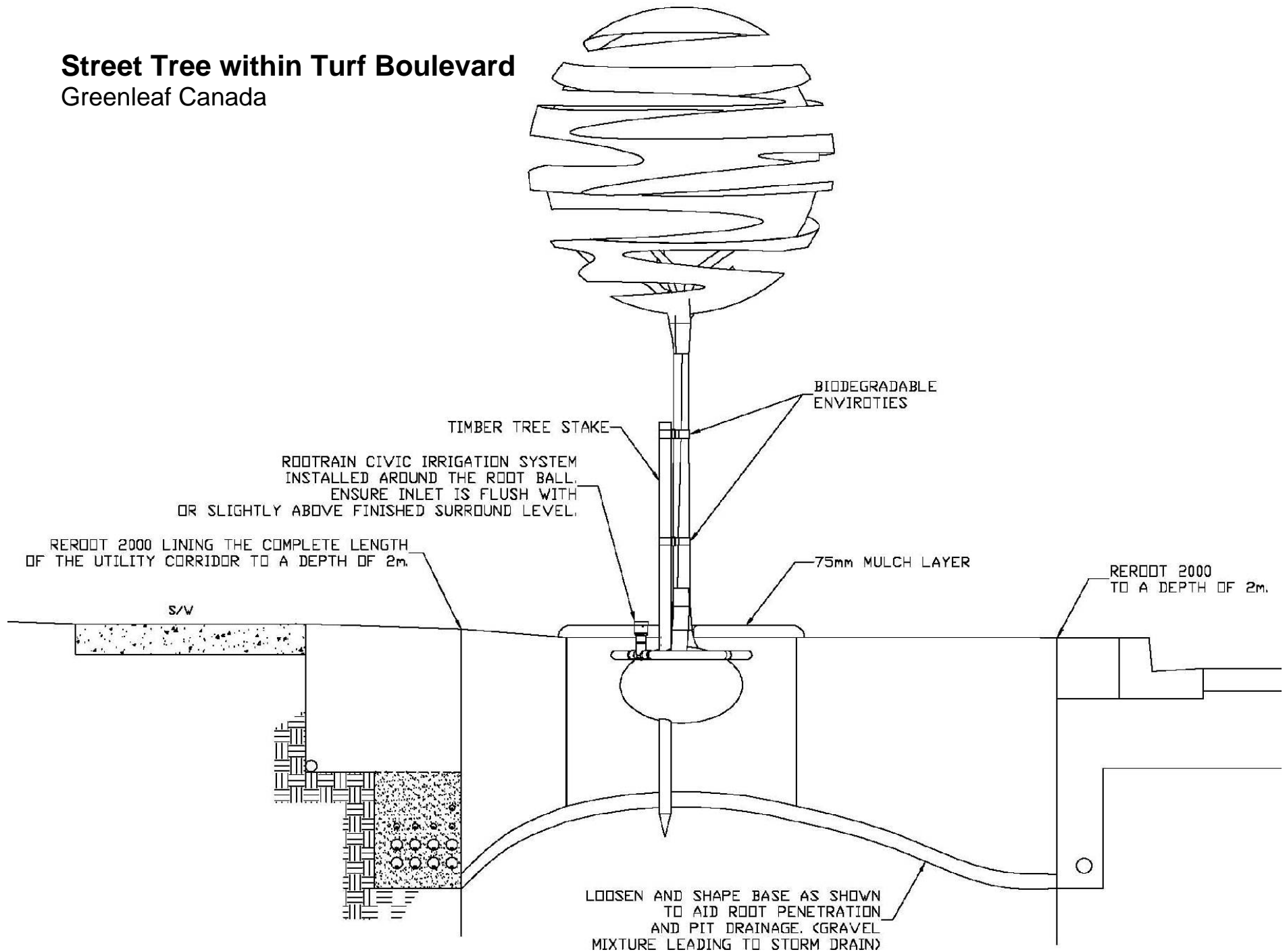


- Product descriptions
- GLRCM RootCells, 208 No. 250mm x 250mm x 90mm
 - RER300 ribbed root deflecting barrier - 5.0m
 - RERJT joint tape -1 roll
 - SAS-L root ball guying system
 - RRPREC-Gr1 irrigation system
 - RRARB-Di3 Arborvent double inlet aeration/irrigation system with cast inlets
 - TAY/1200 1200mm x 1200mm tree grille and support frame. Finished in black
 - ULLSWATER vertical steel tree guard, galvanized and finished in black
 - GLTWGN twinwall geonet 7 Sq. m

Continuous tree corridor for paved areas incorporating a linear load bearing RootCell zone soil module structure for trees to share space. Openings in pavement with root management, tree grille and guard, irrigation, aeration and guying.

Street Tree within Turf Boulevard

Greenleaf Canada



Suggested Trash Receptacles

Brunel Litter Bin

Classic Displays Inc.



COLOURS:	
STANDARD	NON STANDARD
 JUBILEE MAROON	 DEEP GREEN
 JUBILEE GREEN	 DARK BLUE
 BLACK	MAY BE SUBJECT TO MINIMUM ORDER QUANTITY

*Customization and Recycling
option available*

BRUNEL™

Constructed from tough DURACORE™ material, BRUNEL bin is probably the most rigid litter bin made relative to its weight. The large knuckle-hinge door allows easy access to the liner which can then be removed for emptying with minimal lifting.

BRUNEL bin is supplied complete with an elegant moulded-in crest detail on all sides except the door.

Standard Features

- Bolts for permanent ground fixing into pavements or concrete. (We strongly advise ground fixing where possible.)
- Moulded knuckle-hinge door.
- Two point locking system - key to open, slam to lock.
- Zinc-coated steel liner, moulded plastic liner or sack retention system.
- Gold litter legends and bands.

Optional Extras

- **FIREXPIRE** fire extinguishing device - please see page 20a for details.
- Choice of ground fixings - please see page 20b/c for details. (We strongly advise ground fixing where possible.)
- Cigarette stubber plate kit - please see page 21 for details.
- Plastic sacks (for use with sack retention system).
Size: 458mm (18") diameter x 990mm available in packs of 30.
- Personalisation - your own crest or logo can be moulded into BRUNEL bin or a polycarbonate plaque can be personalised and inserted into the recessed area, please see page 39 for details.
- Hood-mounted ashtray - please see page 21 for details. (Retrofitting kits available for use on existing litter bins.)

Specifications

Width: 535mm Height: 954mm

Weight with steel liner: 27.9kg Liner Capacity: 100ltr

Materials

Bin body & door: DURACORE.

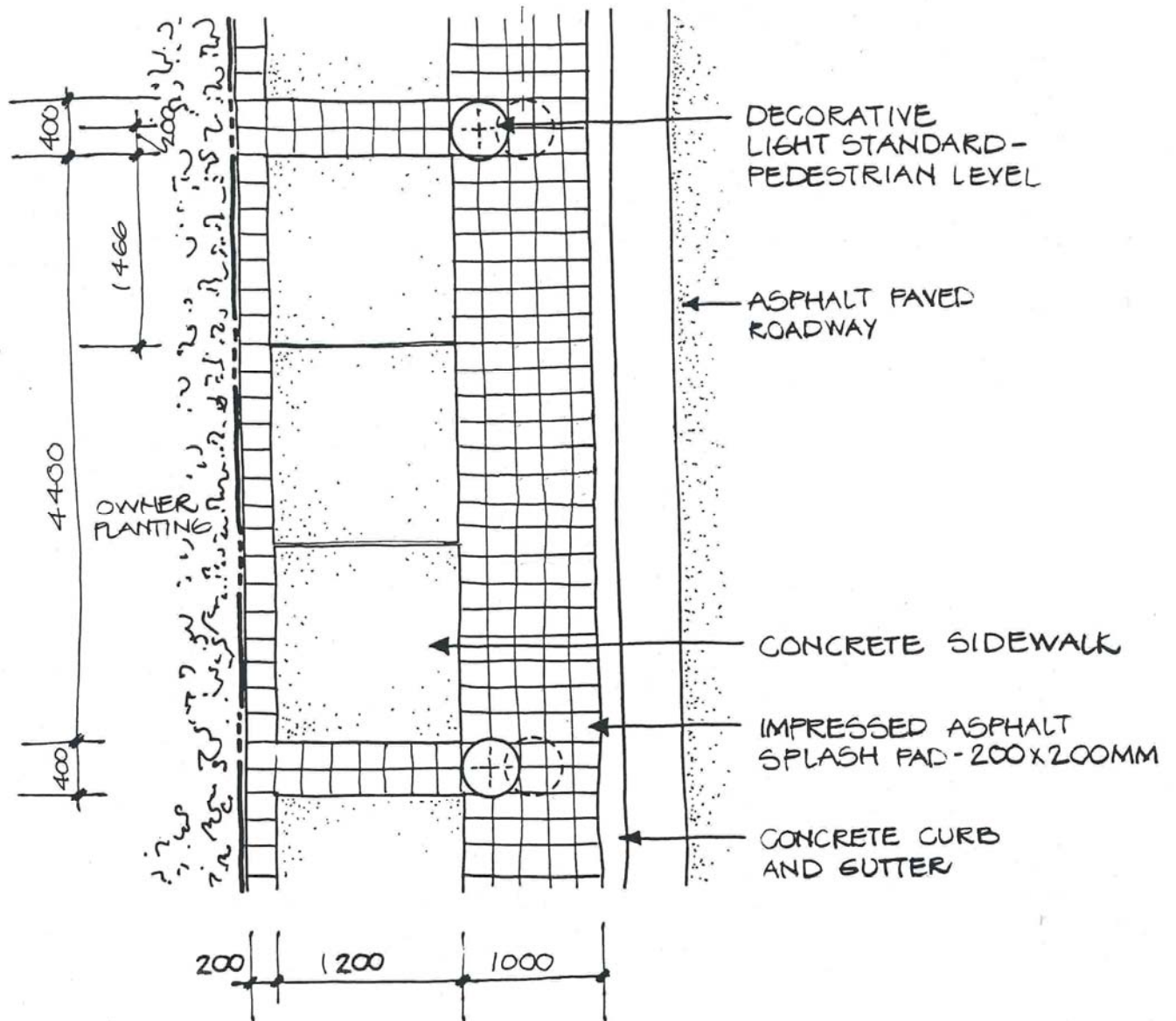
Metal liner: Zinc-coated steel.

Moulded plastic liner: Polyethylene.

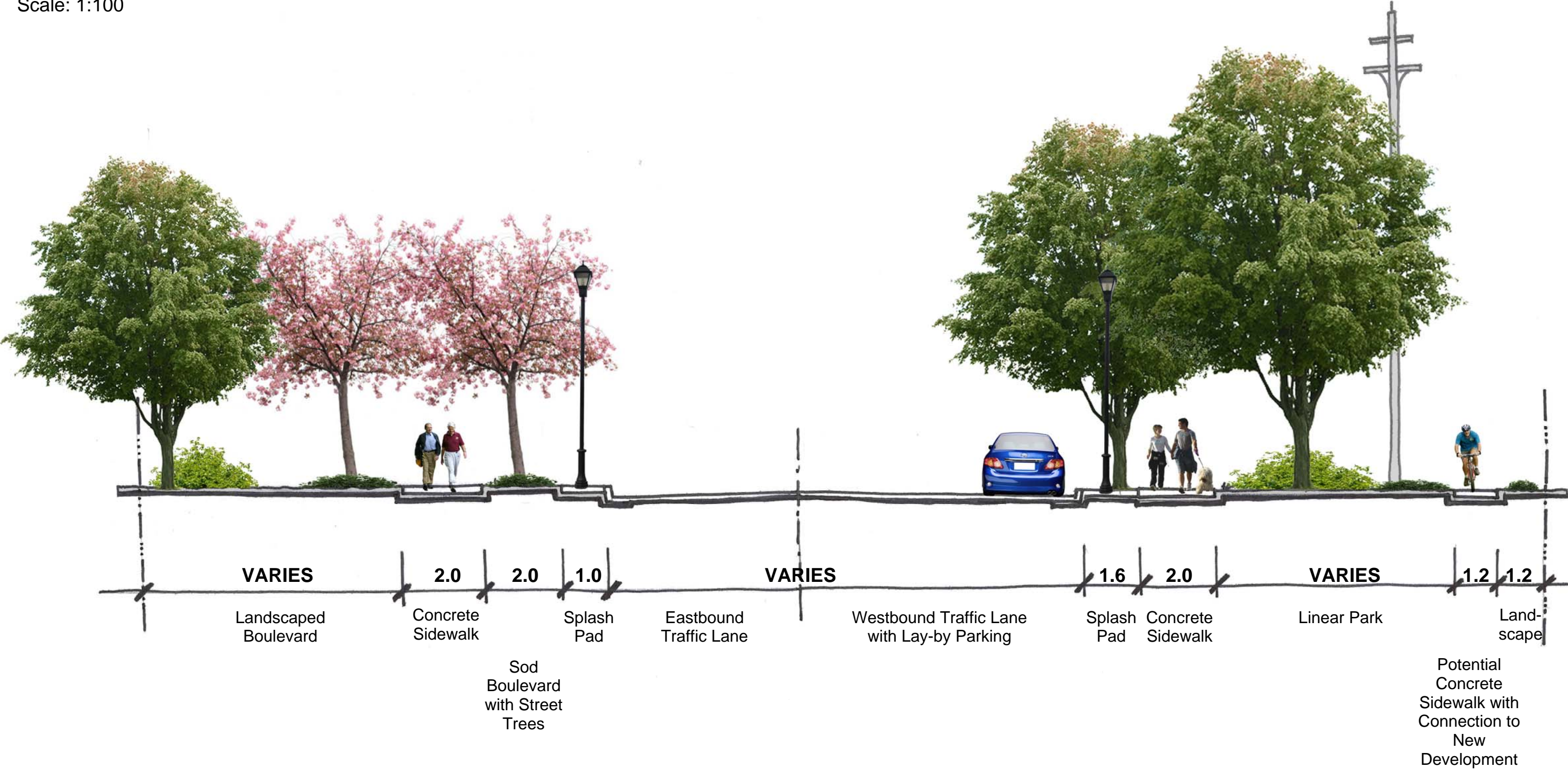
Sack retention system: Stainless steel.

Caledon East Streetscape Typical Sidewalk Layout

Scale: 1:50

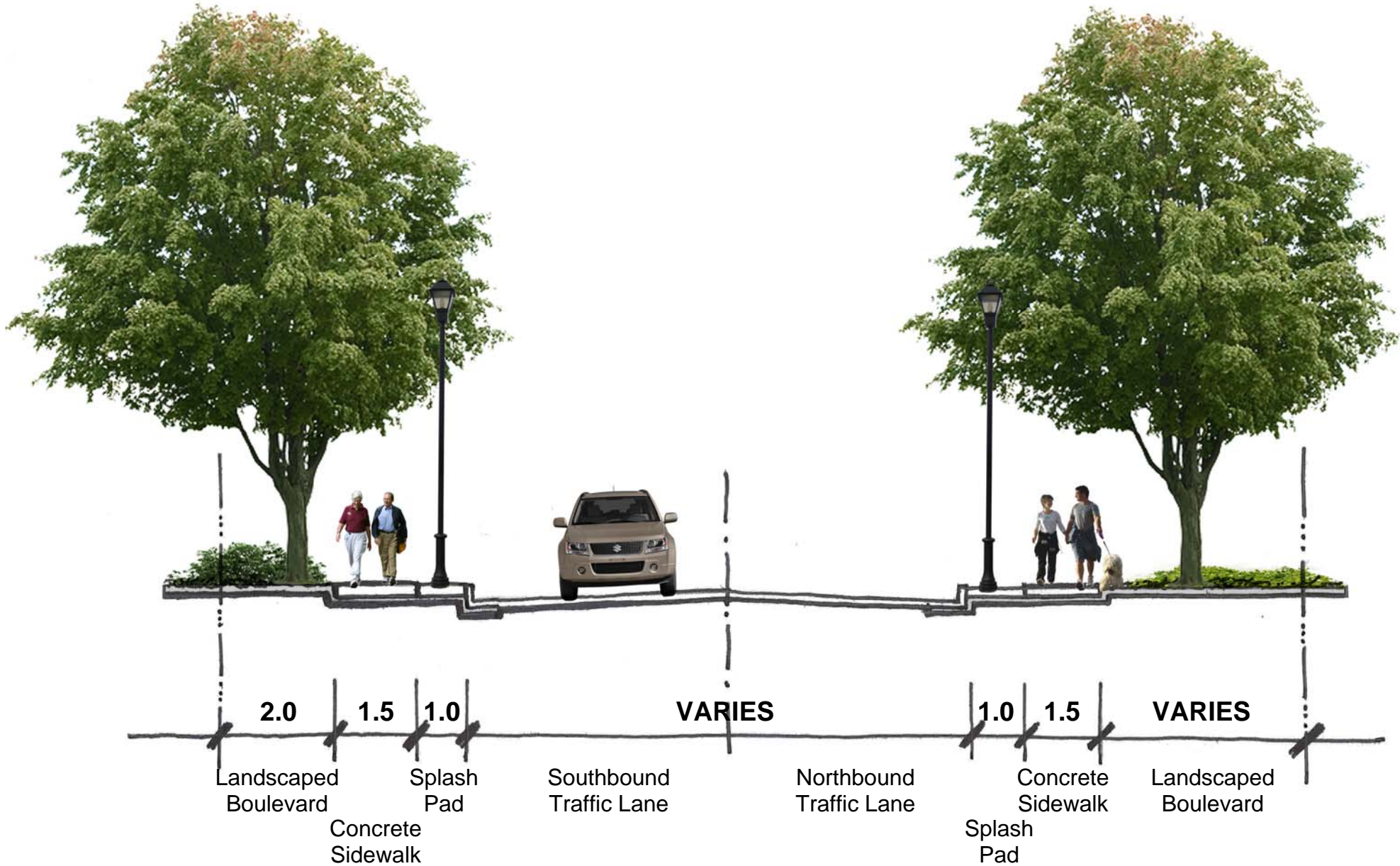


Caledon East Streetscape
Street Section – Old Church Road
Scale: 1:100



Caledon East Streetscape
Street Section – Innis Lake Road

Scale: 1:100



Caledon East Streetscape
Street Section – Airport Road – South Portion
Scale: 1:100

