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planning + urban design

# U R B A N DESIGN.BRIEF

ENIO D'AMATO  
4 WALKER ROAD WEST  
TOWN OF CALEDON

AUGUST 2019  
FILE #8809





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# 1.INTRODUCTION



This urban design Brief [the Brief] has been prepared by Weston Consulting on behalf of Enio D'Amato to illustrate the proposed design for the construction of two single detached residential dwellings at 4 Walker Road West on the south side of Walker Road West and east of McCaffery's Lane, in the Town of Caledon.

This Brief is in support of an Official Plan Amendment, Zoning By-law Amendment and Site Plan Approval to facilitate the development of the proposed dwellings. This Brief is based on the presentation of the role of Urban Design Briefs as outlined in section 5.1 of the Town of Caledon Comprehensive Town-Wide Design Guidelines [2017] and contains an overview of key design principles inherent to the proposed development. A draft outline of the following discussion contained within this Brief was circulated to Town Staff on the 19th of July 2019.

The Brief is intended to provide an overview of fundamental design principles that will guide the detailed design of the proposed development to ensure that the development is compatible with the surrounding neighbourhood and meets the Town of Caledon's key Official Plan policies and recommendations.

The purpose of the Urban Design Brief is to:

- Introduce the development proposal, illustrating the vision, objectives and overall character of the proposed development;
- Discuss compliance with key Official Plan Policies and recommendations contained within the Town of Caledon Comprehensive Town-Wide Design Guidelines;
- Demonstrate the compatibility of the proposed development with the local context and built form character.

A number of reports, plans and drawings have been prepared in support of the proposed development and should be reviewed in conjunction with this Brief. The contents of this Brief supplement the Planning Justification Report [August 2019] also prepared by Weston Consulting.



Figure 1: Aerial of Proposed Development Site Location.



## 1.1 THE VISION & KEY OBJECTIVES

- To support housing supply in the Caledon East area through a moderate increase in residential density.
- To support incremental intensification within the neighbourhood to complement the nearby commercial corridor of Airport Road.
- To provide a 'compact' residential development to help facilitate walkable communities and enliven the public realm.
- To enhance existing connections from Walker Road West to Airport Road and nearby recreational trails.
- To reinforce the street edge with new landscaping, tree planting and establishing a legible building line along Walker Road West.

Key site statistics are detailed in Table 1 outlining the proposed dwellings:

## 1.2 CHARACTER OF THE PROPOSAL

The following discussion describes the key design elements of the proposed development.

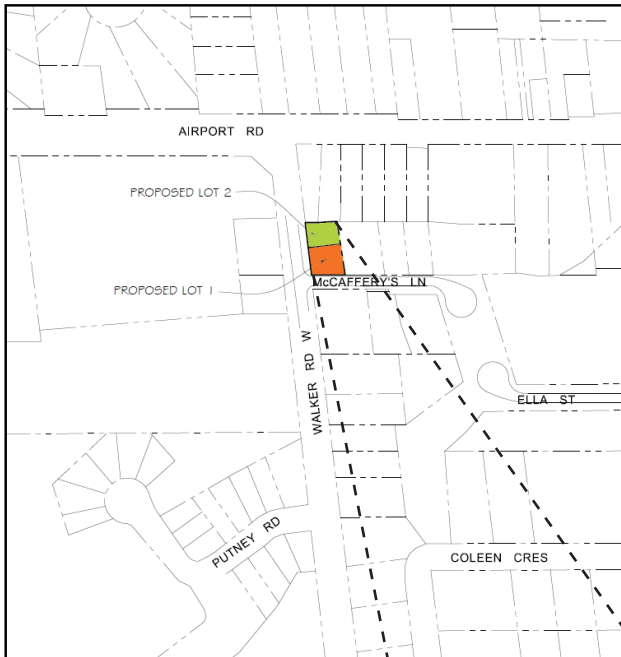
The proposed development lot currently has 40.18m of frontage along Walker Road West and 25.47m along McCaffery's Lane. The subject property is presently vacant and is located within a residential neighbourhood. Both proposed dwellings are 2 storeys in height, comparable to the typical buildings heights in the vicinity. These include a 2 storey building to the west on the north side of Walker Road West, 1 and 2 storey buildings to the north fronting Airport Road, and 1 and 2 storeys buildings along McCafferey's Lane [see neighbouring building heights diagram figure 20].

		PROPOSED LOT 1 [SOUTH]	PROPOSED LOT 2 [NORTH]
LOT AREA [EXISTING = 993.51m <sup>2</sup> ]		573.43m <sup>2</sup>	420.08m <sup>2</sup>
LOT FRONTAGE		21.55m	18.63m
LOT COVERAGE		28%	34%
SETBACKS [MIN]	FRONT	7.5m (main wall)	3.30m (porch) 4.5 m (main wall)
	SIDE EXTERIOR	4.5m	n/a
	SIDE INTERIOR	1.5m (Driveway side)	2.0m (Driveway side) 2.5 m (other side)
	REAR	5.0m	7.5m
GROSS FLOOR AREA TOTAL [GFA EXCLUDING BASEMENT]		287.85 m <sup>2</sup>	258.84 m <sup>2</sup>
FLOOR SPACE INDEX [FSI]		0.50	0.62
BUILDING HEIGHT		8.2m	8.6m
PARKING SPACES [2.6M WIDE X 5.8M LENGTH]		2 driveway + 2 garage	2 driveway + 2 garage

Table 1: Key Site Statistics



Figure 2: Context Plan Showing Lots 1 and 2; V&A Architects, 2019.07.15



The design rationale of the proposed development seeks to maintain the rural residential character of the neighbourhood. The proposal offers a moderate variance in lotting size, compatible with neighbouring residential lots which are similarly occupied by detached single family dwellings. The Context Plan [figure 2] in conjunction with the Site Plan [figure 3] prepared by V&A Architects illustrates the proposed development concept in context.

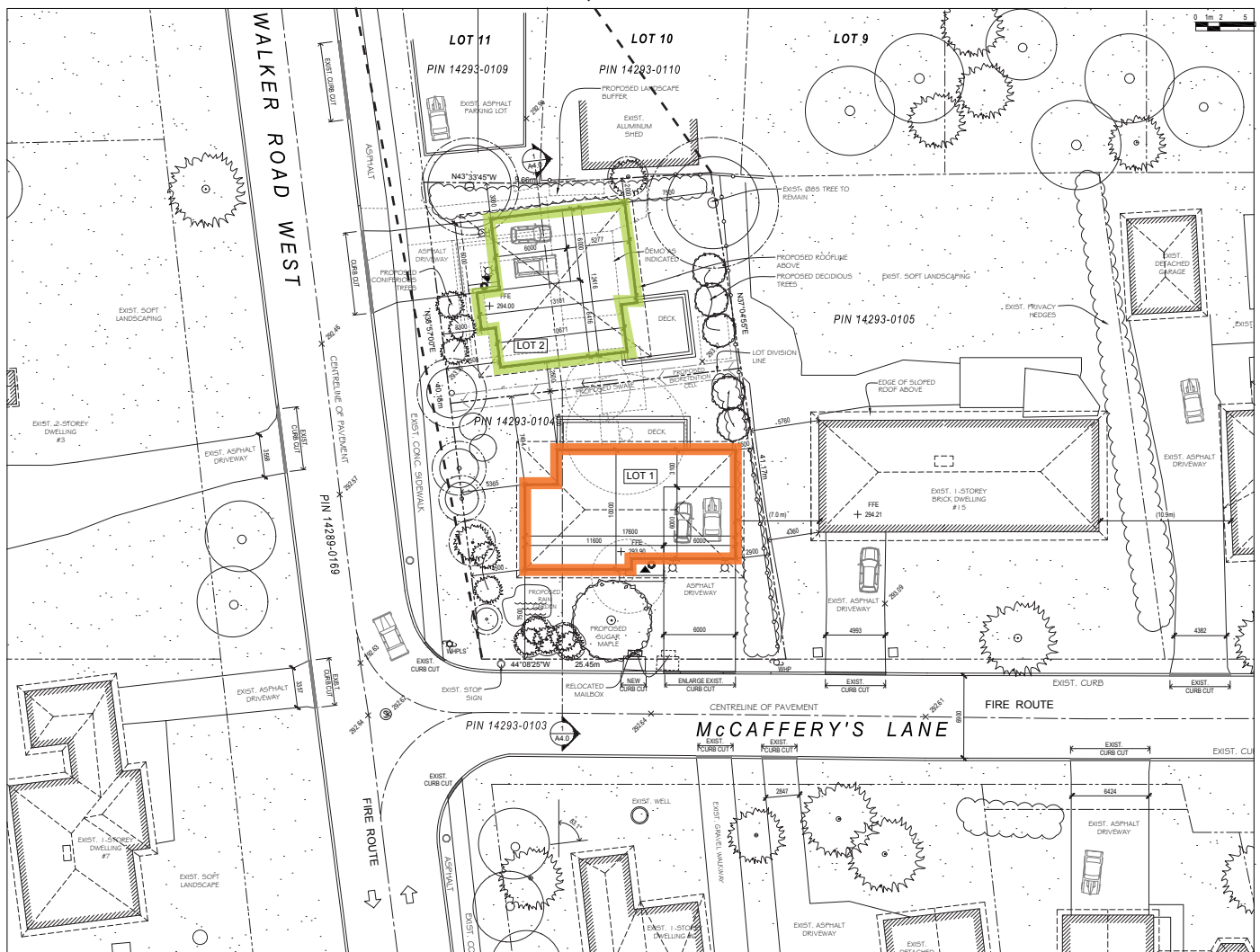


Figure 3: Proposed Site Plan Showing Lots 1 and 2; V&A Architects, 2019.07.15

## 1.2.1 KEY CHARACTERISTICS OF LOT 1

### Built Form

The dwelling proposed for Lot 1 faces McCaffery's Lane with the west flank facing onto Walker Road West. The proposed dwelling is 2-storeys [8.2 metres] in height with a total Gross Floor Area [GFA] of 288.15m<sup>2</sup> [see table 1 for key Proposed Development statistics]. The proposed dwelling will sit within a corner lot accessed by a 6m wide driveway from McCaffery's Lane, expanding the existing curb cut necessitating the relocation of the existing mailbox. The driveway accommodates 2 parking spaces with an additional 2 spaces in the double car garage. To the rear of the building, a deck occupies the rear yard which meets the south perimeter of Lot 2, bordered by a proposed swale and bio-retention cell. The approximate rear yard setback of the dwelling on Lot 1 is 5m.

### Architectural Characteristics

Preliminary features of the proposed dwelling of Lot 1 include:

- A. Shadow stone masonry [sierra]: Corner cladding treatment
- B. Wood panels [iron gray]: second and upper first storey cladding
- B2. Wood panels [pearl gray]: lower first storey cladding
- C. Concrete [at grade]
- D. Shingle roofing

The preliminary material palette is contemporary and understated, featuring muted grays and sierra shades. Window surrounds maintain complementary materials and colour palette. Decking on the north elevation provides a minor set-up from grade to the glazed sliding door entranceway to the rear yard of the dwelling. See figure 4 for a sample rendering of the the proposed materiality of the Lot 1 dwelling.



Figure 4: Render of North Elevation Lot 1; V&A Architects, 2019.06.10



## Landscaping

A rain garden [see site plan figure 5], is proposed at the corner on the lot where Walker Road West meets McCaffery's Lane, with a sugar maple tree also proposed at the front of the dwelling beside the driveway accessed by McCaffery's Lane. The existing coniferous trees will be retained. On the east side of the driveway is a proposed shrubbery which will act as a landscape buffer between the new dwelling and the neighbouring existing building to the east along McCaffery's Lane. Along the Walker Road West frontage, further planting of coniferous trees is proposed to strengthen the tree-line and reinforce the sidewalk which runs along the perimeter of the property. A rain garden and the planting of a sugar maple tree are proposed at the front of the dwelling. The rear of the proposed dwelling to the north, further tree planting of deciduous trees is proposed alongside soft landscaping.

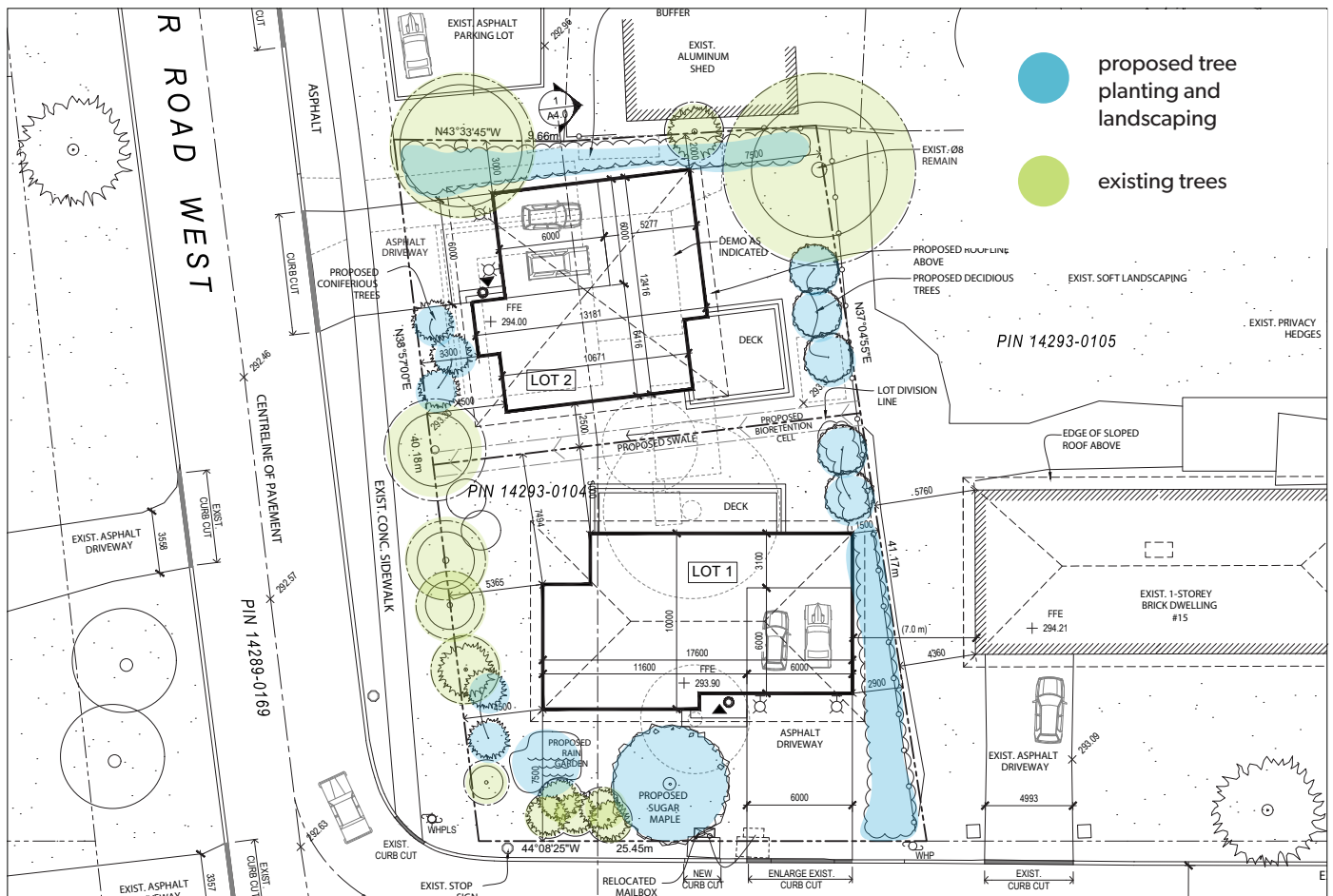


Figure 5: Site Plan of the Proposed Development Indicating Retained and New Planting; V&A Architects, 2019.07.26

## 1.2.2 KEY CHARACTERISTICS OF LOT 2

### Built Form

The dwelling proposed for Lot 2 faces Walker Road West accessed by a 7m wide driveway accommodating 2 parking spaces. The proposed dwelling on Lot 2 is 2-storeys [8.6 metres] in height and has a total GFA of 259.02 m<sup>2</sup> and an FSI of 0.62. The proposed dwelling on Lot 2 is 2-storeys [8.6 metres] in height and has a total GFA of 259.02 m<sup>2</sup>. Lot 2 has a frontage of 18.63m along Walker Road West [see table 1].

### Architectural Characteristics

Preliminary features of the proposed dwelling of Lot 2 are similar to those of the Lot 1 dwelling, and include:

- A. Shadow stone masonry [sierra]: Corner cladding treatment
- B. Wood panels [iron gray]: second and upper first storey cladding
- B2. Wood panels [pearl gray]: lower first storey cladding
- C. Concrete [at grade]
- D. Shingle roofing

To the rear of the property, a partially recessed decking area opens out onto the landscaped rear yard, with generous glazing provided for the sliding access door. The main entrance is partially recessed beside the garage entrance. The second storey, similar to Lot 1 dwelling, is clad in wooden panel to provide a visual differentiation between levels.



Figure 6: Render of West Elevation Lot 2 Along Walker Road West; V&A Architects, 2019.06.10

## Landscaping

The rear of the property includes a generous deck with soft landscaping and deciduous tree planting. A proposed shrubbery provides a landscape buffer at the north edge of the property line. Coniferous tree planting is additionally proposed aside the asphalt driveway entrance.



## 2. LOCAL CONTEXT

## 2.1 LOCAL CONTEXT



Figure 7: Community Context Map within 400m Radius of the Proposed Development Site.



The proposed development site lies within an existing low rise residential area, characterized by primarily 1 and 2 storey dwellings. The general character of the area is rural in nature [see section 2.2 for a descriptive analysis]. The site itself is surrounded on the northwest side by natural green space which includes the Simmons Nature Reserve and agricultural lands to the south and east. To the northeast of the proposed development are an abundance of commercial uses located along Airport Road. Airport Road serves as a transport and commercial corridor and is subject to moderate intensification. Surface parking is located along Airport Road. Numerous local services and facilities also lie along Airport Road, which include banks, a church, Canada post and a range of retail outlets and eating establishments.

Caledon East is designated as a Rural Service Centre in the Official Plan. Several major educational and institutional services are located within Caledon East, including the Town Hall and a Public Library. In addition, the neighbourhood is supported by an elementary and secondary school.

### Lotting Patterns

The lotting patterns within Caledon East are generally modest in scale and relatively uniform. A number of smaller streets are discontinued leading on from main streets such as Putney Road and Filton Road, which terminate in cul-de-sacs. The character of the streets is rural, specifically, there is no predominant grid pattern which informs the shape of residential blocks [see section 3.3 on block for a comparative block and lotting pattern discussion].

### Street Patterns

The streets are often curvilinear and do not uniformly maintain a sidewalk [see figures 12 and 13]. The sidewalk is continuous along Airport Road and Walker Road West along the southeast edge. Buildings are typically set back generously from the street edge and are buffered by a landscaped or lawned area in the front yard. Both Airport Road and Walker Road West are 2 lane roads. The intersection of the two roads [figure 9] is broad with three corners accessible by sidewalk, though there is no pedestrian crossing currently demarcated.



Figure 8: Site Visit Photo: Walker Road West looking north



Figure 9: Site Visit Photo: Intersection of Walker Road West and Airport Road



## 2.2 LOCAL CHARACTER

### Built Form and Architectural Character

The built form of Caledon East is primarily characterized by low-density, residential subdivisions consisting of 1-2 storey single detached dwellings.

Typically, buildings within Caledon East are modest and rural in character, using muted tones and understated materials. Buildings are often orientated towards the street, many with traditional projections, porches and porticos at the fronting properties. Commercial buildings along Airport Road [see figure 16] retain a traditional character consisting of neo-colonial and Victorian style buildings, as well as a number of newer buildings with fewer traditional elements.



Figure 10: Site Visit Photo: Residential Building on McCaffery's Lane



Figure 11: Site Visit Photo: Residential Building on Airport Road



Figure 12: Site Visit Photo: Commercial Building at the Corner of Airport Road and Walker Road West.



Figure 13: Site Visit Photo: Residential Building Walker Road West

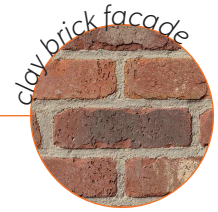
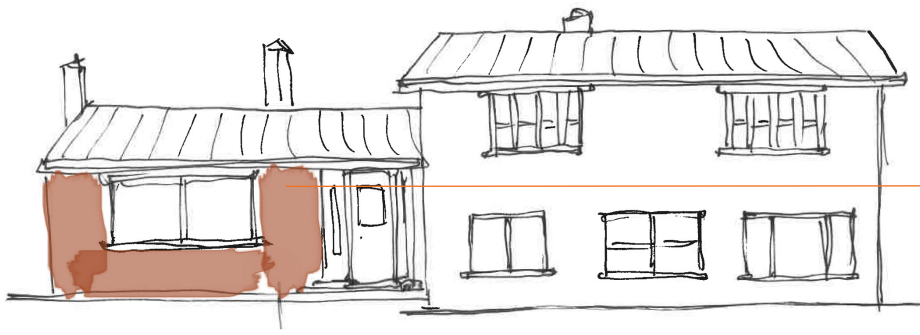


Figure 14: Sketch of Typical Residential Buildings along Walker Road.

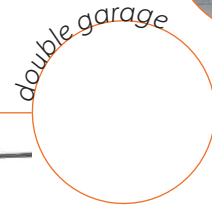
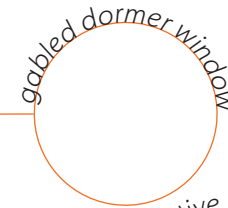
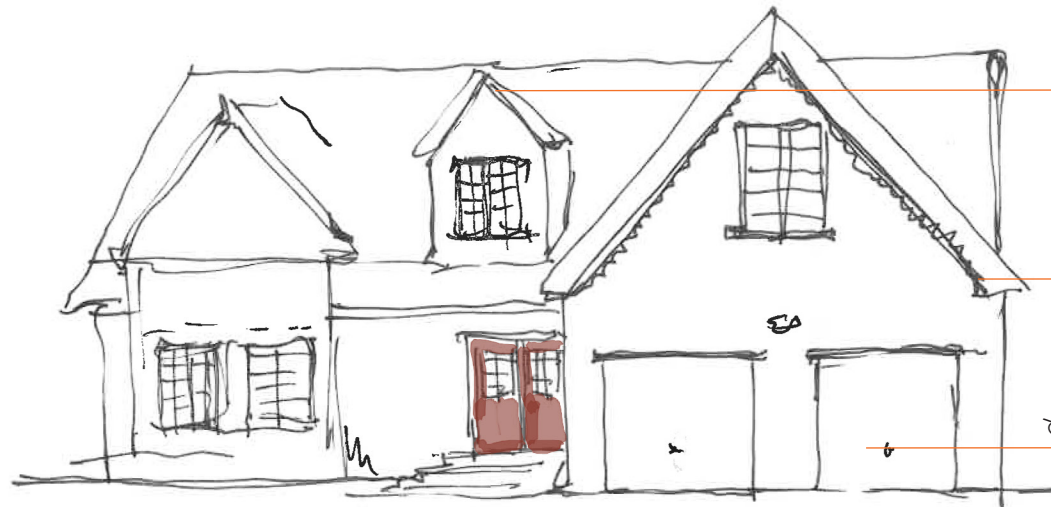
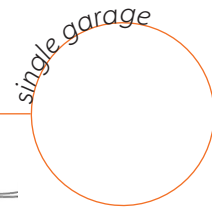
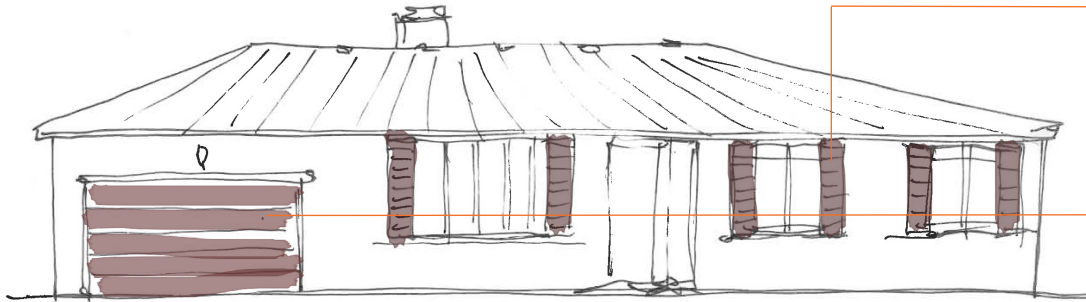


Figure 15: Sketch of Typical Residential Buildings along McCaffery's Lane.



Figure 16: Sketch of Typical Commercial Buildings along Airport Road.

## Materiality

Common materials found within the local vicinity include clay brick and wooden panelling of a variety of shades and textures. The material palette reflects the rural character of the area and traditional building types which include Victorian-styled dwellings. Residential buildings often feature neo-traditional detailing such as gable trim and window shutters. Commercial buildings along Airport Road are generally clad with wooden panels. Storefronts are identified with awnings and window canopies.

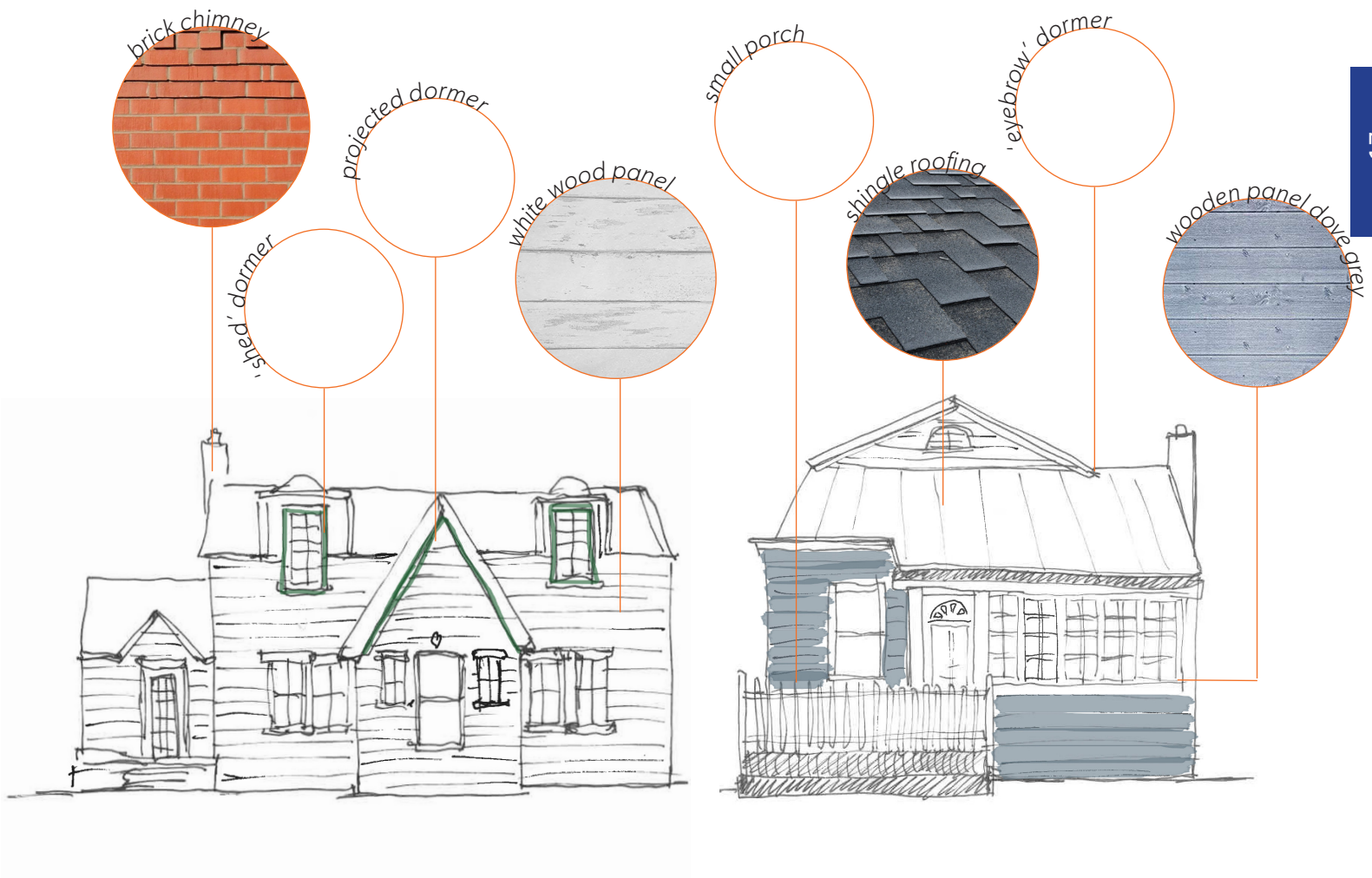


Figure 17: Sketch of Typical Residential Buildings along Walker Road.



### 3. CHARACTER COMPATIBILITY ANALYSIS

### 3.1 LAND USES

This section discusses the compatibility of the proposed development within the context of the local neighbourhood. A study area has been developed based on blocks in the immediate vicinity to the site which share similar patterns of land use. The proposed development site is designated 'Low Density Residential' in the official plan, permitting single detached and semi-detached dwellings, linked dwelling units, duplexes and freehold townhouse units. In addition, the Caledon Zoning By-law

2006-50 [amended], zones the subject property 'Rural Residential'. The study area also consists primarily of lots with these designations. The study area boundary has been based on Schedule "A" of the Town of Caledon By-law 2006-50, zone Map 36a [see figure 19].

The local area is characterized by low-rise residential properties of generally detached and semi-detached dwellings. Whilst almost entirely residential, as noted in section 2 of this brief, there are exceptions to this land use within the study area. A 1 storey commercial establishments lies to the northeast directly neighbouring the property, designated as an institutional use as per Schedule D of the OP.

The proposed development is highly compatible with the predominant residential character of the immediate area, and provides additional density to support the commercial corridor along Airport Road. As Airport Road is also a higher-order transportation corridor, this minor increase in density is duely supported .

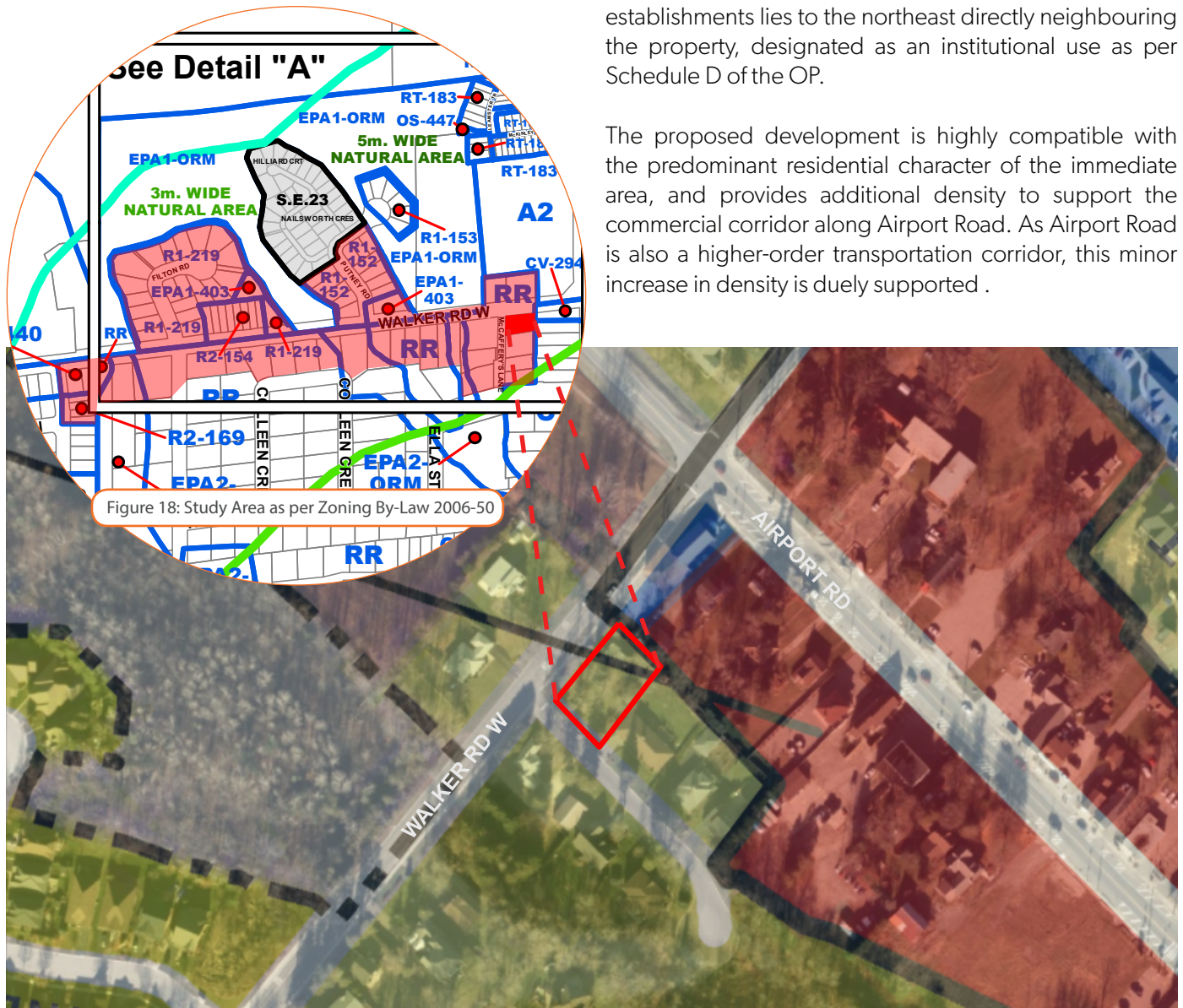


Figure 19: Land Uses within the Study Area: Schecule D, Caledon East Land Use Plan.

## 3.2 BUILDING HEIGHTS & MASSING

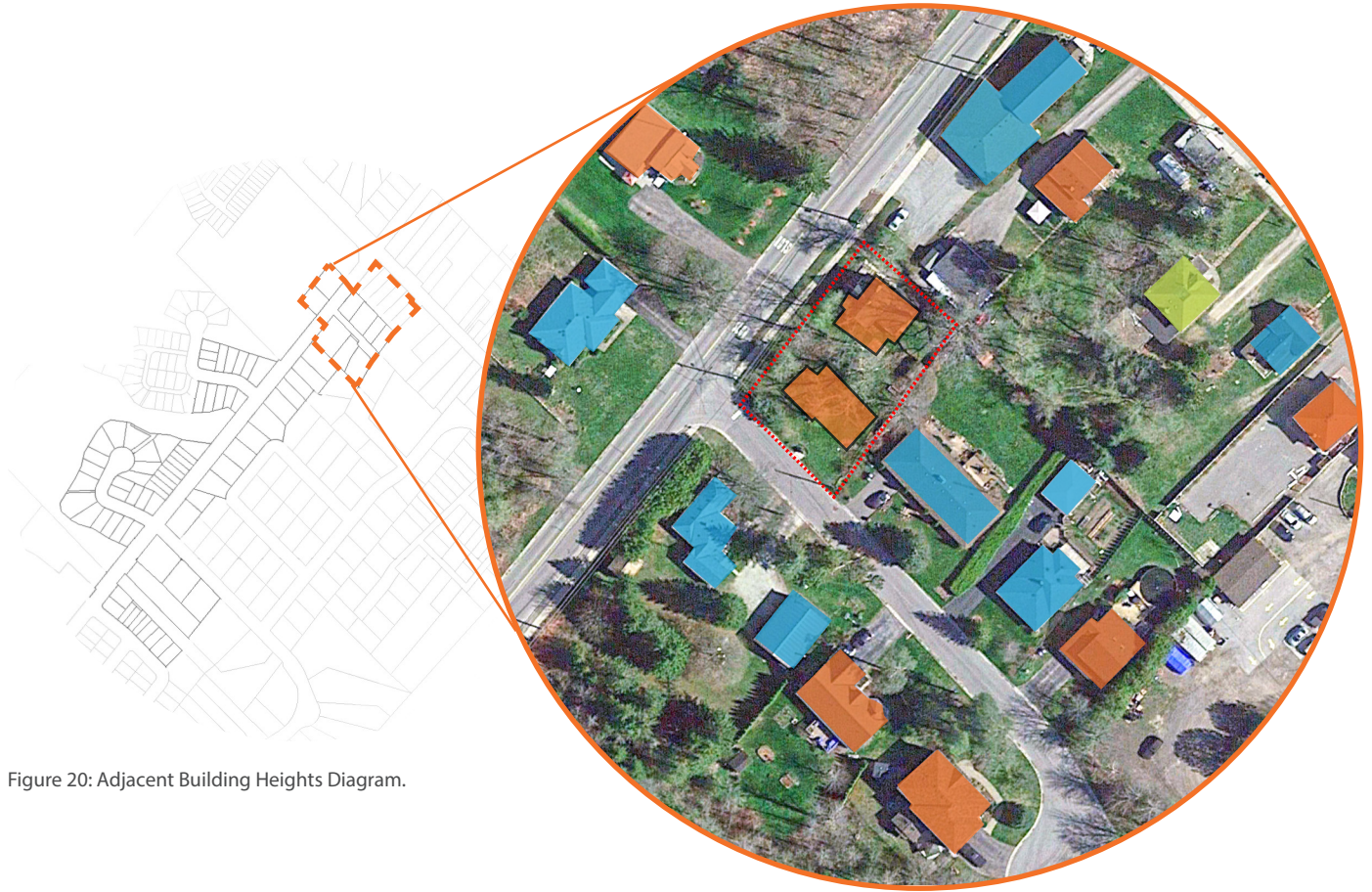


Figure 20: Adjacent Building Heights Diagram.



Figure 21: Lot 1 South Elevation showing Adjacent Dwellings and Built Form Envelope



The surrounding buildings outlined in figure 20 outline adjacent residential properties to the proposed development. This sample area shows buildings heights of between 1-3 storeys. The building illustrated as 3 storeys is generally massed as a 2 storey building with a 3rd storey loft with protruding dormer window facing Airport Road at the upper level. The remainder of buildings in this sample area are balanced between 1 and 2 storey dwellings. At 2 storeys, the proposed development is therefore compatible with the neighbouring context.

In addition, the building footprint is of a compatible shape and scale to those in the vicinity, reflecting a square form that is reflective of typical residential footprints seen in the local neighbourhood. The building footprint informs

the basic massing of the proposed dwellings which is contained within the modest 2 storey building envelope at a height of 8.2m [Lot 1] and 8.6m [Lot 2]. Figure 21 illustrates the gradual grading of building heights down from Walker Road West to the smaller terminating lane of McCaffery's Lane. This also reflects the compatibility of the roofline and overall massing of the proposed development in relation to the dwellings immediately adjacent. Figure 22 illustrates Lot 2 in relation to the adjacent commercial building to the north which fronts onto Airport Road. The proposed dwelling offers a modest height increase towards the commercial corridor contributing to moderate densification towards the main street.



Figure 22: Lot 2 West Elevation showing Adjacent Dwellings.



### 3.3 BLOCK & LOTTING PATTERNS

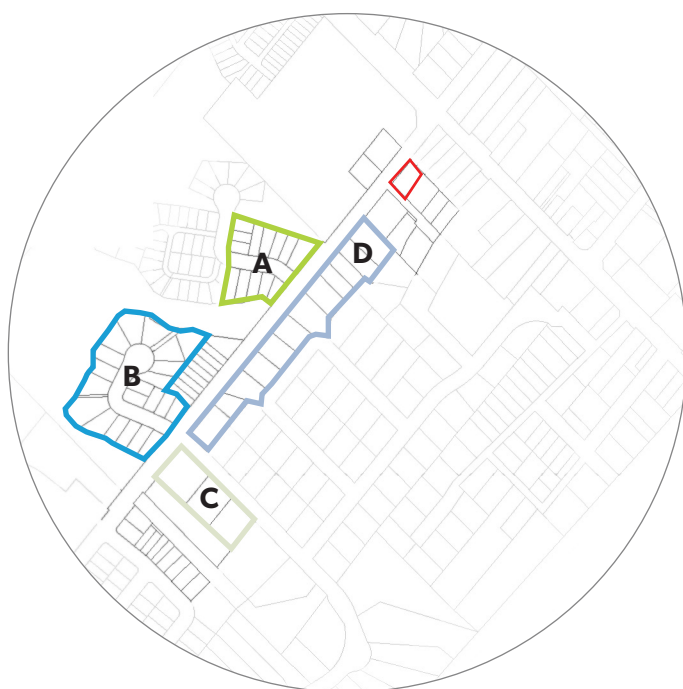
#### Block Patterns

The pattern of development within the study area is typical of older and contemporary patterns of spatial residential growth. Blocks fronting streets such as Walker Road West and Airport Road illustrate regimented, rectilinear blocks, whilst newer blocks [block B, figure 23]. Larger blocks containing fewer, larger lots can be seen at block C. These lots typically contain larger dwellings with multiple garages.

The proposed development lies within a block pattern typical of older patterns of development, which generally feature square or rectilinear blocks containing lots which follow the street line in relatively regimented patterns.

#### Lotting Patterns

The present lot is rectilinear in shape and has a lot area of approximately 0.099 hectares [0.24 acre]. It has a frontage along Walker Road West of approximately 40.18m and 25.45m along McCaffery's Lane. The proposal contemplates the division of the current rectilinear lot into two smaller lots. The lots will take on a square form, with one dwelling on each lot. The current lot will be divided by a swale with potential for a green buffer between the new dwellings. The shape of the proposed lots reflects the development patterns illustrated in figure 21, block D, respecting the existing grain of the lots fronting Walker Road West.



— Proposed development site  
\*illustration approximate scale

Proposed  
development:  
existing lot



Proposed  
development:  
proposed lot



**A**



**B**



**C**



**D**

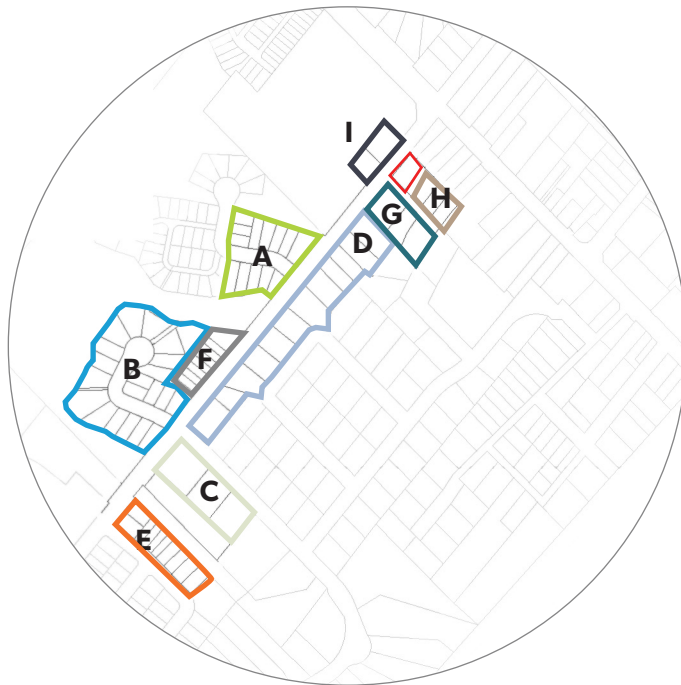


Figure 23: Block Pattern Diagram with Lot Details.

The proposed lotting size reflects the existing block pattern illustrated in figure 21 block A, which lies in close proximity to the site. Block A similarly contains lots between approximately 400m<sup>2</sup> to 700m<sup>2</sup>, comparable to the proposed Lot 1 at 573m<sup>2</sup> and Lot 2 at 420m<sup>2</sup>. The proposed development has frontage onto both a cul-de-sac lane which terminates in a turnaround, and onto Walker Road West. The lots in block A follow a similar pattern, with side yards facing onto Walker Road West and a cul-de-sac lane terminating in a turnaround.

The range of frontages within the study area for residential lots varies. Smaller lots with narrower frontages tend to relate to lots belonging to blocks A and B. Generally these are seen in newer development patterns such as cul-de-sacs. Within the study area, frontages range from 9m to 53m. Lots with larger frontages generally contain

buildings which are sited in the centre of large lots and include generous side, front and rear yards with significant landscaped areas. The existing frontage of the proposed development site is 40.18m. The proposal would break up the frontage, which is amongst the largest typically found in area, and propose a Lot 1 frontage of 21.55m and a Lot 2 frontage of 18.63m. These proposed frontages are comparable with nearby lots with moderate frontages, chiefly including those seen in blocks A and B.



BLOCK	± AVERAGE SIZE m <sup>2</sup>	±AVERAGE FRONTAGE m
A	392 - 734	13 - 20
B	547 - 1176	11 - 20
C	1256 - 2842	43 - 53
D	917 - 1814	20 - 51
E	359 - 678	9 - 18
F	402 - 879	10 - 34
G	1125 - 2063	17 - 41
H	744 - 1249	18 - 31
I	1089 - 1495	31 - 43

Figure 24: Blocks Detailing average Lot and Frontage dimensions [geowarehouse source]



## 3.4 ARCHITECTURAL CHARACTERISTICS

Key architectural features prevalent in the surrounding neighbourhood consist of the following elements, as outlined in section 2.2 of this brief:

### Roof Types

There are a variety of roof types in the neighbouring vicinity. The most prevalent forms include a variation on a hip roof [example A in figure 25] and generally correspond to traditional built forms seen in older buildings which features multiple pitches. Some buildings have open gable roofs which shows a simpler roof footprint - see example B. More complex roof types identified within the area include hip and valley roofs [example C] usually occurring where additions or garages adjoin the main building. As the main garage is contained within the main structure of the proposed dwellings, there are no ancillary

buildings attached. As such the simplicity of the roof forms are maintained. Lot 1 features a hip roof form, and Lot 2 a pyramid roof form. These forms are seen reflected in the adjacent dwellings, such as example D and A.

### Projections & Features

Roof forms commonly feature traditional architectural elements such as dormers and projecting porticos or small porches which are reflected in the roof footprint. Projections such as these are common at entrance or doorways. Example E shows an a roof type with a dormer window projecting from the facade. Example F shows a more complex roof type, with a dormer-style projection forming part of the overall massing of the building frontage. The proposed dwellings have the main entrances and garage access partially recessed, rather than projected, which preserves the simplicity of the roof form.

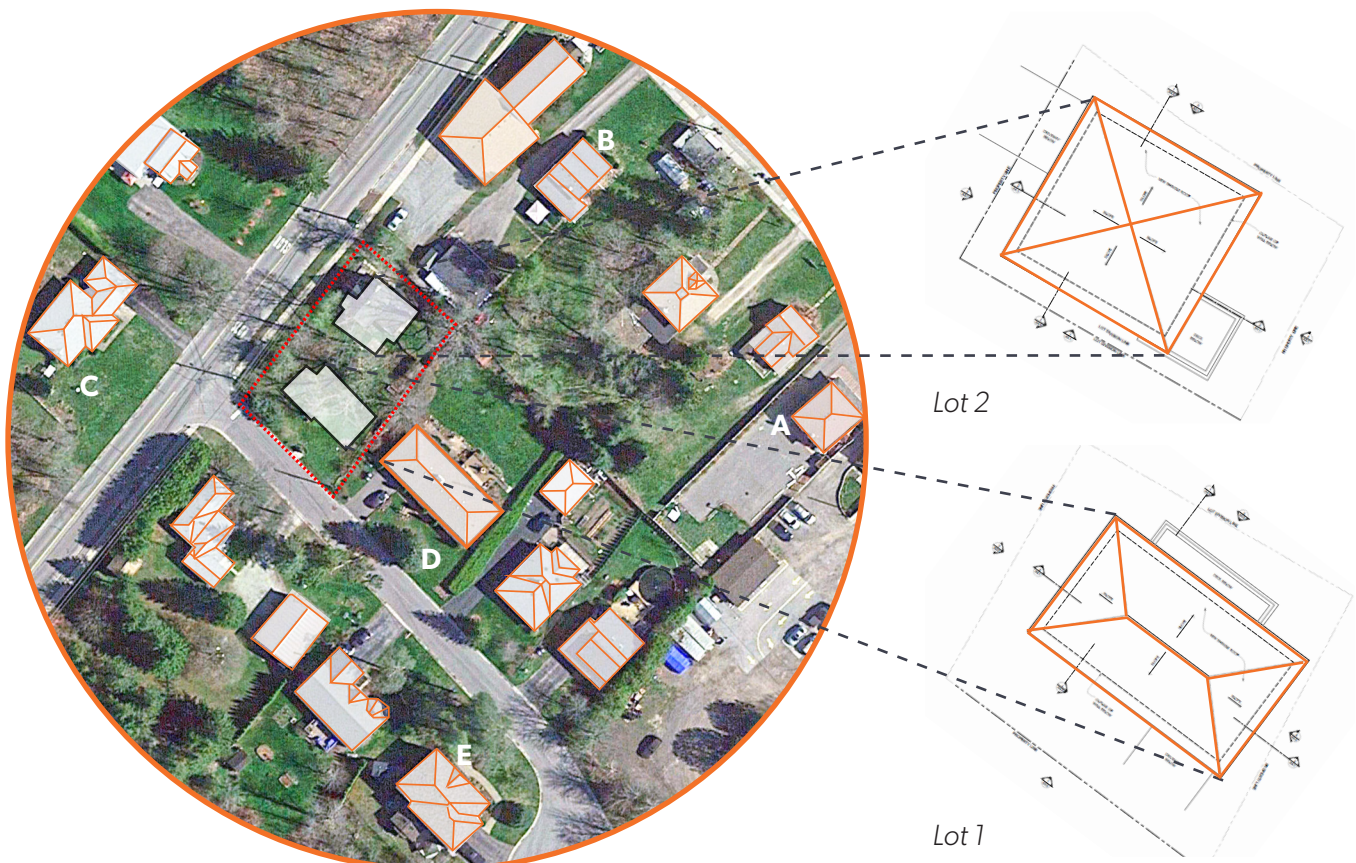


Figure 25: Adjacent Roof Types.

## Proposed Materiality

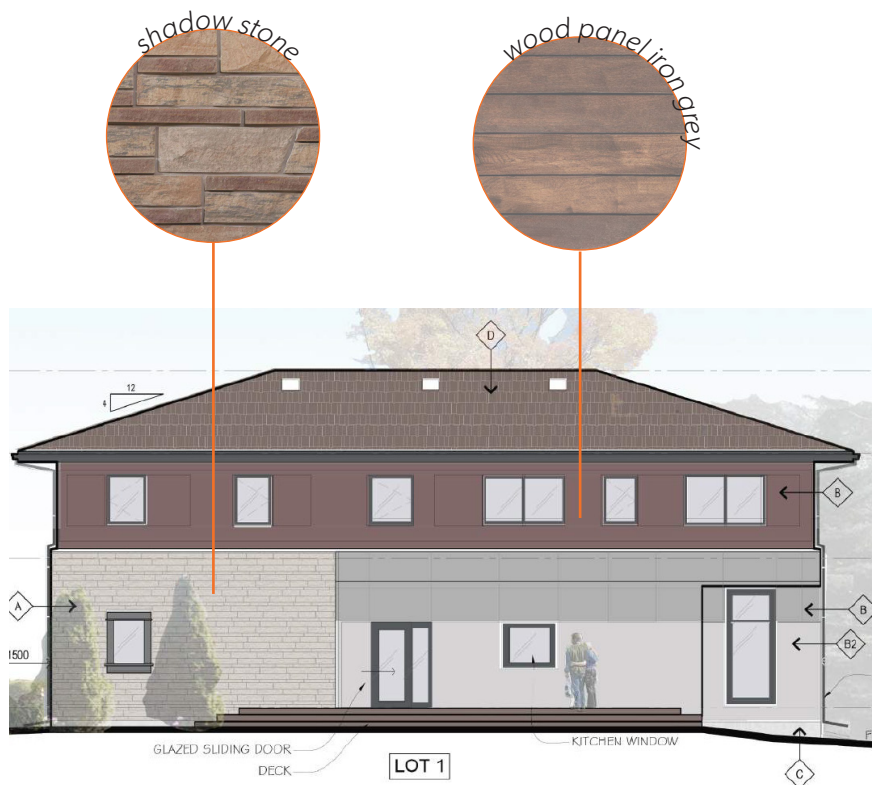


Figure 27: Render of North Elevation Lot 1; V&A Architects, 2019.06.10

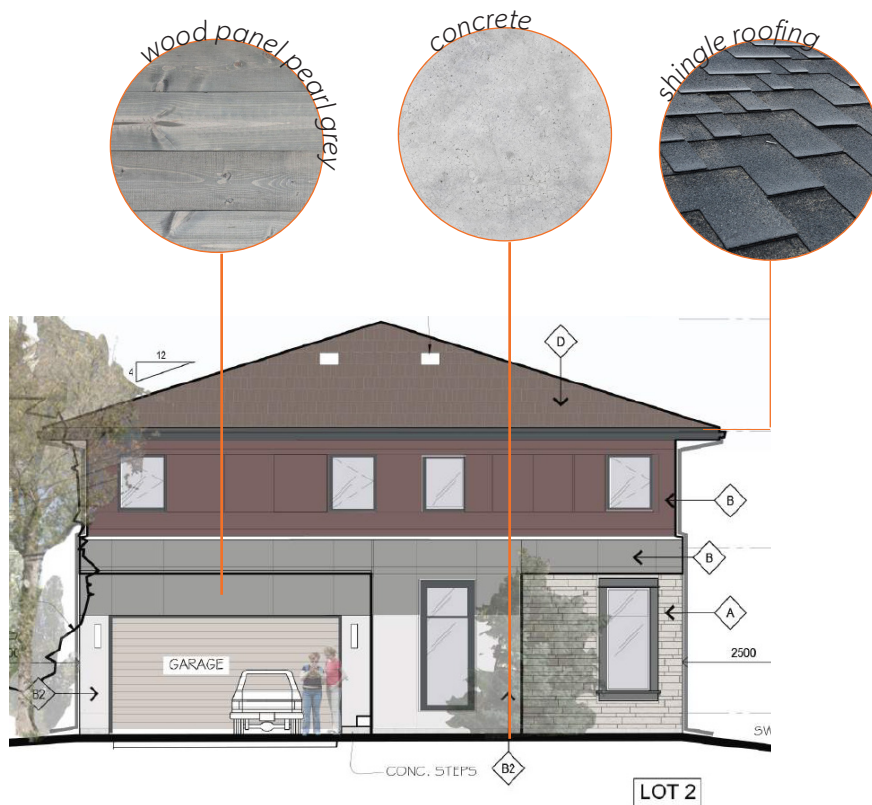
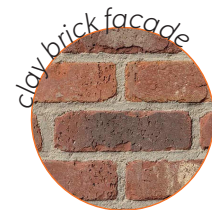


Figure 26: Render of West Elevation Lot 2 Along Walker Road West; V&A Architects, 2019.06.10

## Comparable Materiality in the Local Context



## 4. POLICY CONTEXT



## 4.1 TOWN OF CALEDON OFFICIAL PLAN

The Town of Caledon encourages the development of a diverse range and mix of housing types, density and tenure to meet current and future population demands. Section 3.5 of the Town of Caledon Official Plan [OP] provides policies to meet this objective [see detailed discussion in the Planning Justification Report Prepared by Weston Consulting]. Design consideration within the Official Plan provide the following provides the following policies that are relevant to the proposed development:

### 5.10.4 Rural Service Centres

“Rural Service Centers are designated as the primary growth areas for the planning period.” Caledon East, within which the proposed development site sits, is designated as a rural service center. These are described as focus areas for new residential and employment growth in the are. Policy 5.10.4.4 Caledon East is subject

to a Secondary plan which comprehensively lays out development objectives for the area [see discussion in Section 4.2 of this brief].

The OP states that in order for “*Caledon East to fully realize the Rural Service Centre function, an appropriate range of opportunities related to service provision, housing mix and employment are necessary.*”

As outlined in the vision for the proposed development in section 1.1, the proposal supports the provision of housing in the area as well as supplying a modest, incremental increase in density, which additionally supplements new employment opportunities emerging in the area. In addition, the Servicing Plan [see figure 28] prepared by Crozier and Associates demonstrates that the proposed development is supportable in terms of utilizing existing infrastructure along Walker Road West.

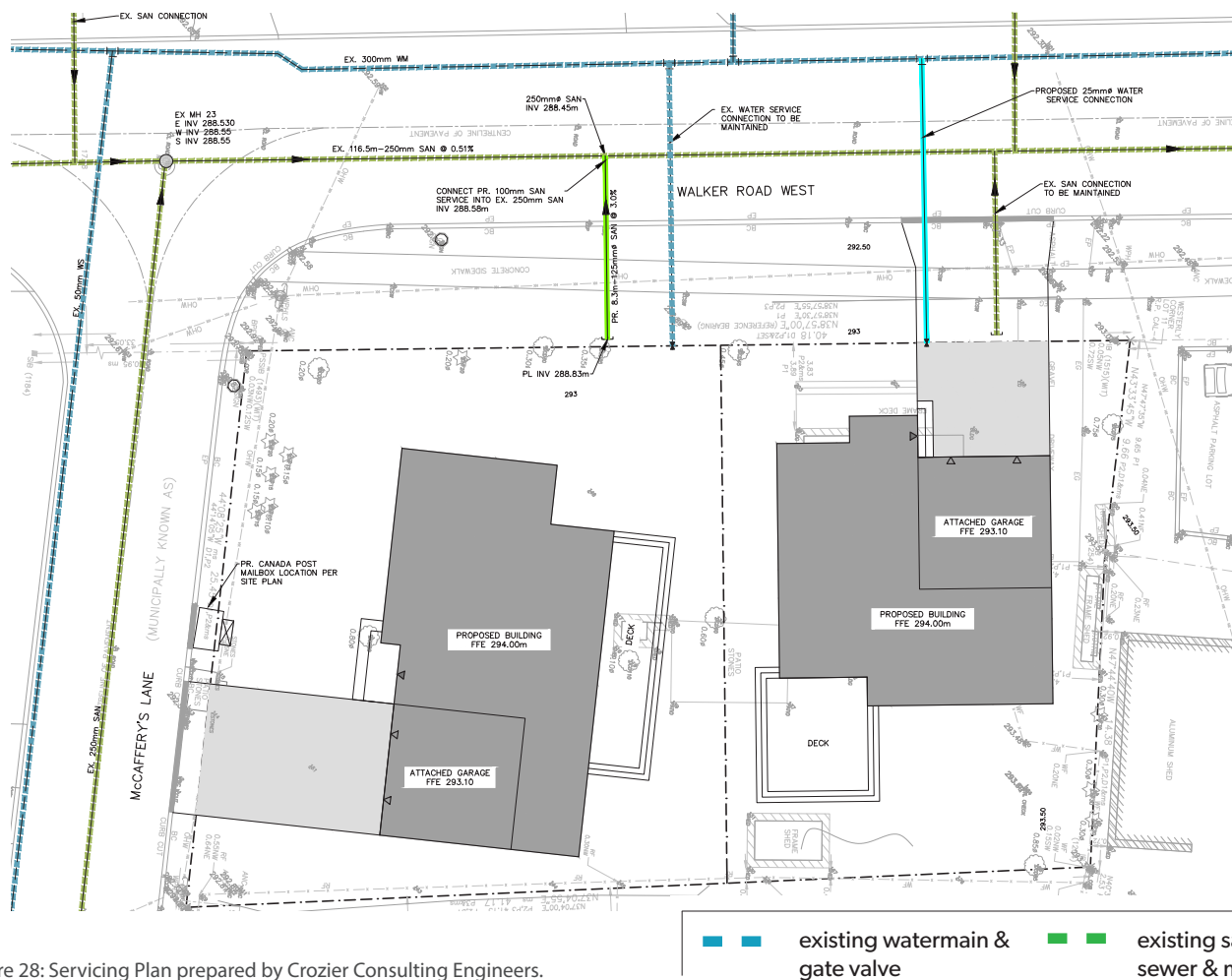


Figure 28: Servicing Plan prepared by Crozier Consulting Engineers.

## 4.2 CALEDON EAST SECONDARY PLAN

Section 7.7 of the Town of Caledon Official Plan provides a comprehensive policy overview for the Caledon East area within the Caledon East Secondary Plan. An overview of key policies of the Caledon East Secondary Plan pertinent to the proposed development are summarized as follows:

### 7.7.2 Objectives

- b. Creating a compact community; maintaining the character of the surrounding rural landscape; effective use of land and services that facilitates pedestrian and vehicular access to community facilities and services.
- h. Encouraging and facilitating pedestrian activity.
- i. Creating a streetscape that enhances the character of Caledon East and provides safe and pleasant pedestrian environments contributing to a sense of community.

The proposed development adequately meets the key objectives laid out in section 7.7.2. The proposal supports compact communities by providing additional density in the area to support moderate intensification in the area at a proposed rate of 20.13 units per hectare. Additional density supports walkability within the area and easy access to nearby recreation trails such as Simmons Nature Reserve to the east to further active recreation in the area. The proposal would take advantage of existing infrastructure and services in the area, generating negligible additional traffic along Walker Road West. Landscaping and additional tree planting along Walker Road West will reinforce the street edge and create a pleasant environment for pedestrians and cyclists accessing the commercial corridor along Airport Road. In summary, the proposal will support a safe and pleasant pedestrian environment that contributes to a sense of community, whilst supporting growth in Caledon East in-keeping with its role as a Rural Service Centre.

### 7.7.3 Structural Concept

The Structural Concept establishes the vision for the evolution of the Caledon East Area to 2031. The proposed development supports the goals outlined in 7.7.3, principally through the supply of new housing on this currently vacant lot in concurrence with the second phase of residential development envisioned for Caledon east

occurring between 2016 and 2021. The modest increase in density underpins intensification in proximity to the commercial corridor and serves to maximize pedestrian access to and from Airport Road by enlivening the public realm.

### 7.7.4 Community Design & Associated Design Principles

Policy 7.7.4 outlines key design principles for East Caledon and provides guidance which will maintain and enhance the “*rural small town character*” of the community. The policy outlines key values expressed by the community identified during the public consultation process:

- “1. Maintaining the historic character and desire for a traditional “mainstreet” commercial area with appropriate (and preferably traditional) architectural style;*
- 2. The preservation and enhancement of natural features;*
- 3. The integration of common open space within the community;*
- 4. Streetscapes that are greener, pedestrian friendly and safe;*
- 5. Further development of the railway system including improved and new connections to link all neighbourhoods, community facilities and services and open space;*
- 6. Integration of housing types;*
- 7. Good architectural design, particularly for medium density development, safety and crime prevention through design;*
- 8. Gateway entrance features to the community.*
- 9. Maintaining views of valued landscape features and the night sky.”*

The proposed development responds to the design principles outlined in section 7.7.4 in the following ways:

- Maintains a compatible architectural and built form character to that of the surrounding neighbourhood which maintains the appearance of a rural small town;
- Preserves trees currently on-site and proposed additional landscaping and planting schemes to support biodiversity;
- Facade orientation towards the street and fenestration at grade improves surveillance of the public realm along Walker Road West potentially providing safety

benefits for pedestrians; and

- Contributes moderate residential density which supports access from Walker Road West to nearby recreational trails and outdoor space.

Furthermore, policy 7.7.4.1 considers in detail design principles for guiding development in Caledon East. Policies specific to the proposed development include those considering streetscaping, housing style, lot size mix and layout, walkway, landscape buffers and in particular, low-density residential design considerations. Further discussion is detailed in section 4.5 of this brief.

With regard to low-density residential neighbourhoods, section [f] of policy 7.7.4.1 states:

*f) Low Density Residential neighbourhoods will generally consist of a mix of lot sizes and housing types, with single-dwellings predominating. The mixing of single-dwellings, semi-detached dwellings and townhouses is encouraged within each neighbourhood, subject to the maximum density specified in this Plan. Plans of subdivision should ensure compatibility of building types through consideration of appropriate scale, massing and siting.*

The proposed development responds to these design principles in the following ways:

- Provides additional single detached residential dwellings on the existing lot which are well integrated into the existing built fabric, reflecting the neighbouring scale, massing, and orientation and of neighbouring properties.
- Offers a compatible variance in lotting pattern, concurrent with goals laid out within the OP to increase residential supply in Caledon East.
- The proposed dwellings are architecturally compatible with their surroundings, offering a sensitive material palette with contemporary variations, such as recessed main entrances accessed via low step which adequately tackle minor grade changes on site.

#### 7.7.5.2 Low Density Residential

Policy 7.7.5.2 outlines considerations for new development in low-density designated zones. Relevant policies with regard to the proposed development include:

*"7.7.5.2.1 The uses permitted in the Low Density Residential designation shall consist of: single-detached and semi-detached dwellings..."*

*7.7.5.2.3 Where appropriate, a range of dwelling types and lot sizes shall be encouraged in the Caledon East Settlement Area...*

*7.7.5.2.4 A mix of single residential lot sizes with frontages generally ranging from 15 metres (50 feet) to 23 metres (75 feet) shall be required.*

*7.7.5.2.5 There shall generally be no more than 4 units per freehold townhouse block.*

*7.7.5.2.7 Low Density Residential development adjacent to Walker Road and Mountainview Road shall provide direct frontage and driveway access onto these roads where technically feasible and safe..."*

With regard to the policies outlined above, the proposed development:

- Contributes additional residential development in the form of two detached dwellings;
- Offers a compatible, moderate variation on typical lot sizes in the neighbourhood;
- Proposes a frontage of 21.55m [Lot 1] and 18.63m [Lot 2] within the parameters of policy 7.7.5.2.4 [15 metres to 23 metres];
- Proposed two single family dwellings on the existing lot, the equivalent of two units; and
- Lot 2 provides direct frontage and driveway access onto Walker Road West, with Lot 1 fronting onto McCaffery's Lane to mitigate potential congestion.



## 4.3 CALEDON EAST COMMUNITY IMPROVEMENT PLAN

The Community Improvement Plan for Caledon East was prepared in 2014 in order to develop long-term strategy to *“revitalize the community, promote private investment, and improve the quality of life of the community’s residents.”* Appendix A: Design guidelines [in particular ‘D. Built form guidelines’ for infill development], are of particular relevance to the proposed development.

### Appendix A: Design Guidelines

Guidelines to support the goals of the Community Improvement plan include the following key elements:

#### A. Streetscapes

*“Objective: To create a unified, pedestrian-oriented Village core, with safe, comfortable, accessible and attractive streets.”*

The proposed development supports the objective of the guidance provided in section A. Streetscapes in the following key ways:

- Retains and enhances the legibility and aesthetic experience of the sidewalk abutting the site through landscaping along the perimeter.
- Maintaining accessibility to recreation trails and outdoor amenity in the vicinity.
- Maintaining the sight lines along Walker Road West through reinforcement of the street edge through tree planting.

#### B. Façade Improvement

*“To encourage the restoration, improvement and maintenance of façades to their original historic character, and create an interesting, harmonious Village streetscape.”*

The proposed development supports the objective of the guidance provided in section B. Façade Improvement in the following key ways:

- Compatible facade design with neighbouring historic buildings.
- Utilizes traditional materials such as wood panels and shingle roofing to complement the built fabric of the neighbourhood.

#### C. Property Improvement and Landscaping

*“To establish a very high standard for property maintenance and sustainable landscaping solutions in Caledon East as a critical element of creating a picturesque village character. Private property improvements are to be consistent with a walkable and accessible urban environment and contribute to a vibrant main street.”*

The proposed development lies in close proximity of the main street along the commercial corridor of Airport Road and therefore plays a role in contributing to the vibrancy of the area in the following key ways:

- Landscaping is proposed at the front yard of Lot 1 which includes the planting of a sugar maple and proposed rain garden.
- Planting of coniferous and deciduous trees are proposed on both Lot 1 and Lot 2. The side yard of Lot 2 additionally proposes landscaping to provide a buffer between the dwelling and the existing parking lot at the rear of the property to the north.
- Site lines are maintained to ensure the visibility of the Lot 2 driveway access from Walker Road West, as it crosses the existing concrete sidewalk.

#### D. Built Form

*“To provide for the sensitive redevelopment and infilling of Caledon East’s historic core, and to contribute to creating a “picturesque village” through appropriate building character and scale”*

The proposed development provides an appropriate contextual response to the currently vacant site in the following key ways:

- Horizontal lines are repeated in the architectural detailing of both dwellings to maintain a street rhythm.
- The street wall is maintained through appropriate setbacks of the dwellings from the property line.
- The buildings heights of the proposed dwellings are compatible with adjacent structures.

#### E. Parking and Driveways

*“The visibility and intrusion of parking areas and driveways*

## 4.4 TOWN OF CALEDON TOWN-WIDE DESIGN GUIDELINES

*is to be minimized in the village core, where pedestrian movement and comfort are to be prioritized.”*

The proposed development responds to these guidelines in the following key ways:

- All parking is contained within an internal garage accessed at the frontage of the property.
- Garages are accessed via a 6m wide asphalt driveway with landscaping provided in conjunction with these paved areas.
- The driveways are designed to mitigate obstruction of the street and to be visible to pedestrians accessing the sidewalk along Walker Road West.

The following discussion provides an overview response to design requirements contained within the Town of Caledon Town-Wide Design Guidelines [TWDG] in relation too the proposed development. Key policies are ass follows:

### Policy 6.0 The Public Realm: 6.5.3 Corner Lots

a. Lot 1 of the proposed development is a corner lot. The south face and west face are visible from Walker Road West and McCaffery's Lane, and provide consistent proportions and consistency of detailing. Increased fenestration on the south frontage increases the opportunity for “eyes on the public realm”, particularly important as the sidewalk along Walker Road West where it meets the lane is discontinued.

b. The main entrance to lot 1 is located on the flankage side of the building facing onto McCaffery's Lane away from Walker Road West.

c. The roofline of Lot 1 is a simple hip roof form and is unobtrusive. Shingle roofing provides visual interest and reflects the aesthetic of the neighbourhood.

d. Steps up to the entrance frame the frontage, with the garage and driveway positioned beside the main entranceway.

e. The asphalt driveway is recessed and in the frontage of the building and is sufficiently setback from the property line along McCaffery's Lane by approximately 8m.

f. The main entry is located on the longer frontage of the lot. There is no secondary entrance.

g. The rear yard of the Lot 1 is treed and is not intrusively exposed tot he public realm along Walker Road West.

h. A bio-retention cell is proposed at the boundary line at the rear of Lot 1 to capture storm-water runoff.

### 8.0 Residential Development: 8.1.2 Single Detached & Semi-detached Housing

a. The presence of the garages on both Lot 1 and Lot 1 are sufficiently minimized through the recession of both garage entrance at the front facade of the dwellings. In the

case of Lot 1, the garage entrance is setback approximately 8m from the property line along McCaffery's Lane. The garage of Lot 2 is setback approximately 5m from Walker Road West.

b. Material transitions at the corner of Lot 1 dwelling where the corner lot meets the intersection of Walker Road West and McCaffery's Lane are seamless. Masonry cladding wraps around the corner of the dwelling to terminate at rear where it meets wood panel cladding and deck. Similarly the frontage of Lot 2 dwelling is treated with stone masonry at the south corner of the property.

c. A small area of exposed concrete is visible at the north elevation of Lot 1 due to a gradual slope on the site. This however is minimal and causes limited visual impact.

d & e. Steps to the main entrance of both properties are shallow and sufficiently relate to grade. The steps are clad to match the main material palette of the property.

f. The dwellings are of modest design with simple hip roof forms and discreet massing.

g. Ample fenestration is provided at street level to "*foster casual surveillance*." Smaller windows are placed at the second storey of the dwellings.

h. The garages and main entrances of both dwellings are partially recessed to articulate the form of the buildings.

i. The dwellings are located near to the street edge to establish a strong street edge.

k. Provision has been made for on-lot private landscaping at the rear and side yards of both dwellings.

l. Regard has been made to minor grade changes on site, with direct pedestrian access to the street edge and sidewalk along Walker Road West ensured.

## 9.0 Context Sensitive Development : 9.1 Infill Development & Alterations

The proposed dwellings consider the architectural language and built form of the local context in the following ways:

a. The proposed dwellings offer a complementary palette of materials and tones suitable to the local context. This includes shadow stone and wood panels.

c. Connections are maintained with the sidewalk along Walker Road West. This ensures the linkage to the commercial corridor along Airport Road and recreational trails in Simmons Nature reserve is maintained and reinforced.

d. The proposed dwellings maintain the existing line of the street wall in relation to existing neighbouring buildings.

e. The proposed dwellings are massed in a way that reflects the typical rectilinear forms of the surrounding buildings, and maintains the 1-2 storey average building height evident in the neighbourhood. The proposed dwellings hip roof forms are compatible to adjacent dwellings [see roofing diagram figure 25]. The placement of doors and windows are balanced proportionally and do not detract from the established rhythm of existing architectural themes in the locality. Smaller windows on the second storey of the dwellings serve to establish a visual datum line in the streetscape.

h. The proposed dwellings utilize similar cladding materials to those which predominate in the surrounding area, particularly shingle roofing, stone and wood paneling.

m. Lot 1 is a corner lot and as such, maintains balanced levels of architectural detailing on each facade visible from the public realm. Existing vegetation is maintained. The overall building height is modest in compatible with the neighbouring properties.



## 4.5 CALEDON EAST STREETSCAPE STUDY ARCHITECTURAL GUIDELINES

The Streetscape Study Architectural Guidelines was prepared in 1994 by the Town of Caledon. A series of guidelines and recommendations were developed in support of the character of Caledon East as:

- *“the capital of the Town of Caledon;*
- *as a recreational centre at the gateway to the Escarpment and Oak Ridges Moraine natural areas; and*
- *as a site of natural beauty. ”*

The proposed development broadly supports and upholds the characteristics described above. Alongside the retention of existing trees on site, additional landscaping which includes the planting of coniferous and deciduous trees supports the rural character of the neighbourhood. This enables the new development to blend seamlessly with the natural surrounding. This also serves to alleviate the modest increase in density proposed on the site. The sidewalk along Walker Road West is retained and enhanced, maintaining pedestrian linkages to the recreational trails nearby. In addition, architectural elements are compatible with the residential character of the area and broadly agree with the recommendations in this document. This includes the maintenance of the street wall, appropriate scale in relation to the public realm and a sensitive material palette in accordance with the traditional character of the area.

## 4.6 CALEDON EAST STREETSCAPE STUDY DESIGN CONCEPT

A Design Concept Streetscape Study Design was conducted alongside the Architectural guidelines in 1994 to develop a comprehensive streetscape plan in response to pending development pressures in the area resulting from as a result of infrastructure intensification. Though the subject property is not within the study area outline in 1.2 of the Guidelines, neighbouring properties along Airport Road north of the site are included in the study. The intersection of Walker Road and Airport Road is described in the study as the ‘North Gateway’, adjacent to the ‘Commercial Core’ along Airport Road. The following relevant objectives guiding the design of this area is as follows:

*“Any future residential developments nearby should provide pedestrian access to Airport Road and to local commercial functions.”*

As outlined in Figure 7, pedestrian access from the proposed residential dwellings along Walker Road West wholly maintain their connection with Airport Road. This will be enhanced by landscaping improvements which will further define the sidewalk, creating a pleasant pedestrian realm and reinforcing the pedestrian crossings at the intersection of Walker Road West and Airport Road.

## 5 . S U M M A R Y & C O N C L U S I O N S

The proposed residential development of two single detached dwellings offers an appropriate design solution for the current vacant lot. The development proposes an appropriate increase in density to support nearby commercial uses along Airport Road and moderate intensification in the area.

The existing form of the curb will be retained along McCaffery's Lane. In addition, existing greenery will be enhanced along Walker Road West supporting an attractive pedestrian realm with a strong, legible street edge. This supports active transportation in the area and maintains existing pedestrian crossings which lead to outdoor recreational trails of the Simmons Nature Reserve.

The buildings are scaled and massed similarly to their surroundings and complement the existing architectural style of the neighbourhood with a compatible palette of materials. The proposed building heights are in keeping with neighbouring properties and at 2 storeys, adequately mitigating adverse visual impacts on Walker Road West sidewalk.

It is our opinion that the proposed development will contribute to the vision for Caledon East in creating a compact, walkable neighbourhood which retains at its core, a rural character, whilst adding a compatible variation in lotting pattern akin to contemporary trends in land use development.





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