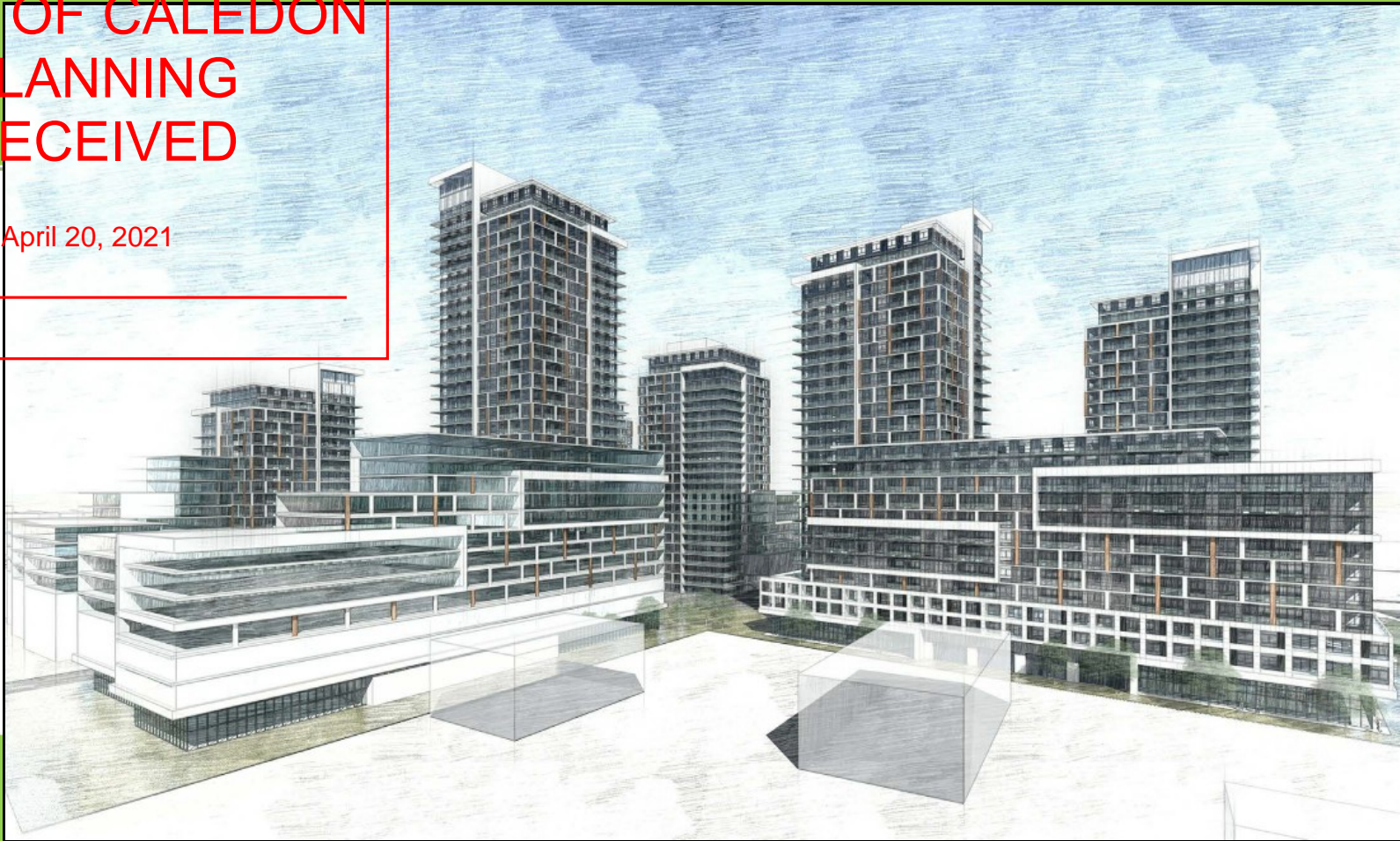


URBAN DESIGN BRIEF AND HIGH-RISE GUIDELINES

**TOWN OF CALEDON
PLANNING
RECEIVED**

April 20, 2021



PROPOSED MIXED-USE DEVELOPMENT
12563 & 12599 HWY 50,
BOLTON, ON

In support of

APPLICATIONS FOR OFFICIAL PLAN AMENDMENT & ZONING BY-LAW AMENDMENT

PROPOSED MIXED USE DEVELOPMENT

12563 & 12599 HWY 50, 2 INDUSTRIAL ROAD

BOLTON, ON

12599 Hwy 50 Ltd.



STRYBOS BARRON KING
LANDSCAPE ARCHITECTURE



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1.0 INTRODUCTION

1.1 Purpose and Intent/Description of Project

12599 Hwy 50 Ltd. (The Proponent) has acquired a development site at 12563 & 12599 HWY 50, 2 INDUSTRIAL ROAD, BOLTON, ONTARIO.

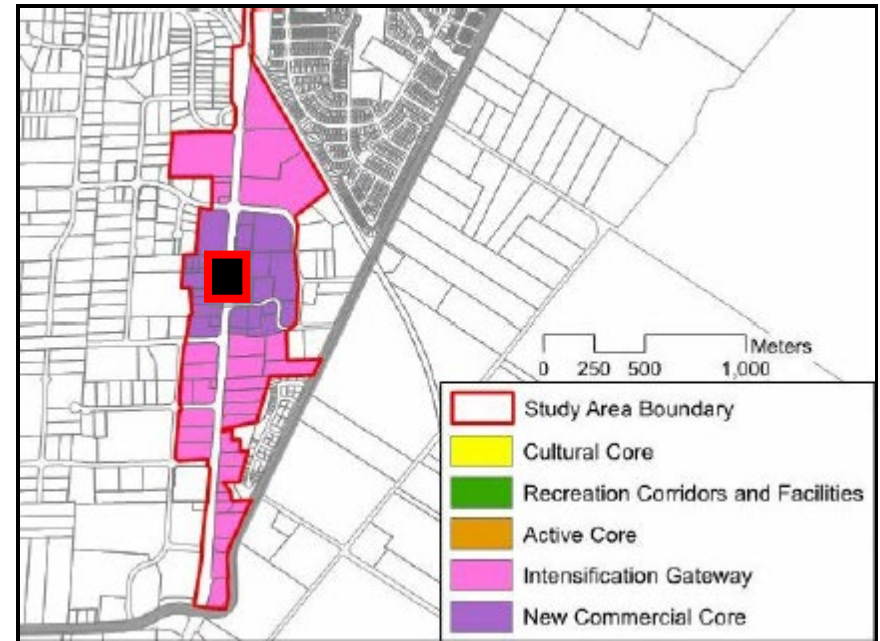
The parcel consists of approximately 3.52 hectares (8.69 acres) of land. The Proponent proposes to develop the site as a mixed-use, commercial and residential community comprising 35,000sq.ft. of commercial space and 2,229 residential units on a common-element condominium road network. The residential units comprise a broad range of accommodation, from 1-bedroom units to 3-bedroom units and some 2-storey townhouses.

Parking is largely underground on 3 levels with some surface visitor parking to serve the commercial component.

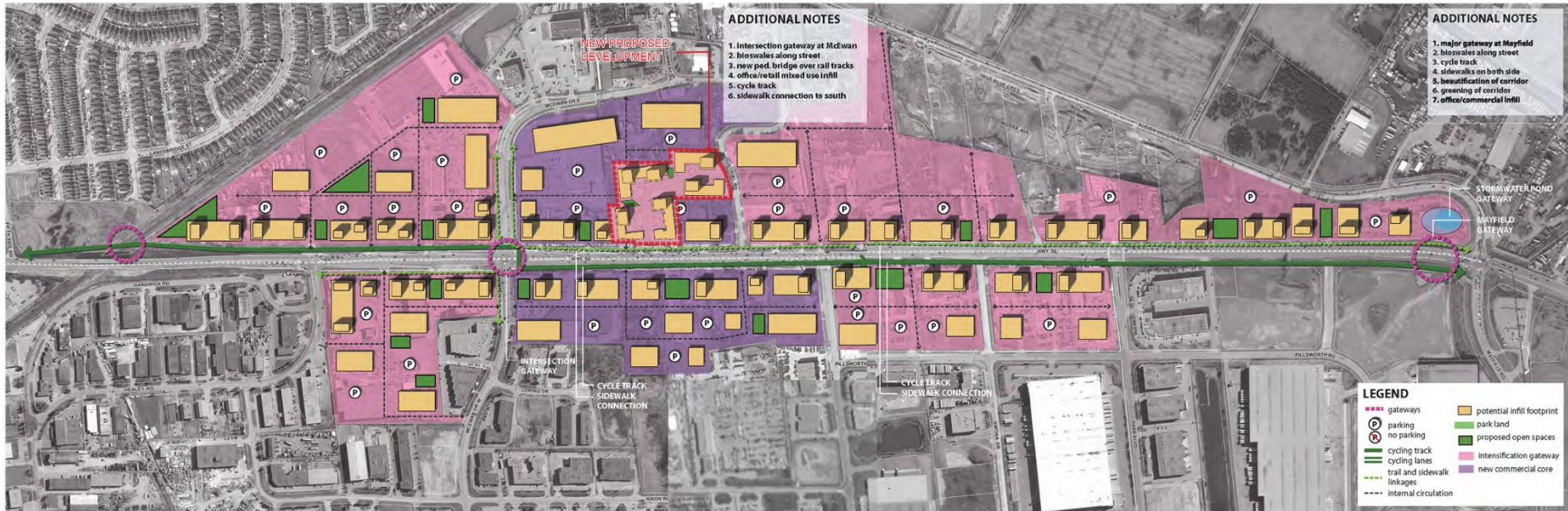
Refer to Section 2 for more details on the Development Site Plan.

The site fronts onto Highway 50 in Bolton and is part of the Bolton Highway 50 Commercial Area and also features in the Queen Street Corridor Study.

The Town of Caledon has formulated its long-term goals and vision as a community in its Official Plan and other Municipal documents. Refer to Section 1.5 for more detail of the Policy Framework.



Bolton Highway 50 Commercial Area

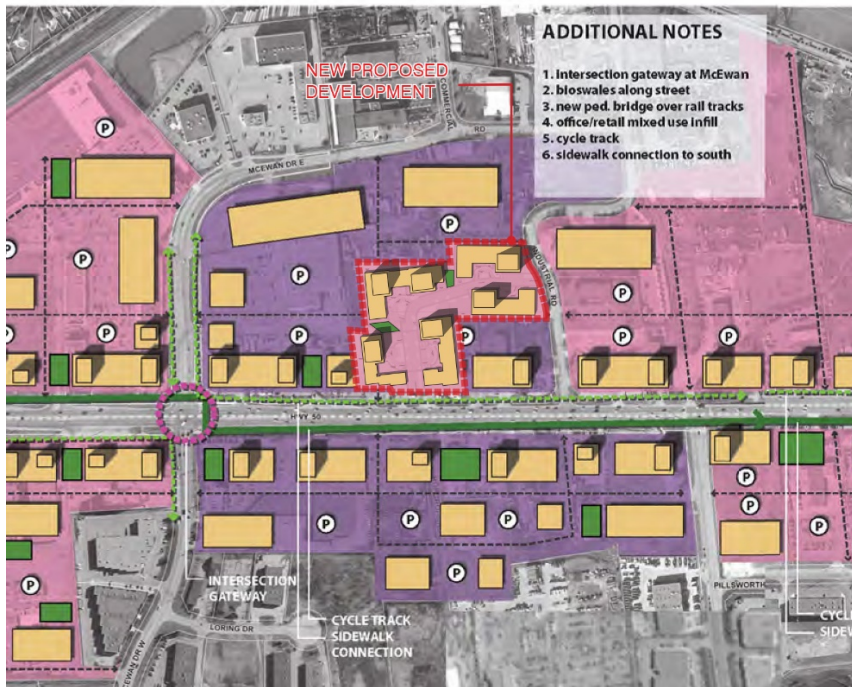


CORRIDOR PLAN - FIGURE 8D

BOLTON - QUEEN STREET CORRIDOR STUDY - TOWN OF CALEDON
 DESIGN CHARENTE - GEORGE BOLTON PKWY. TO RAIL CORRIDOR



0 82.5 125 250 375 500
 Meters

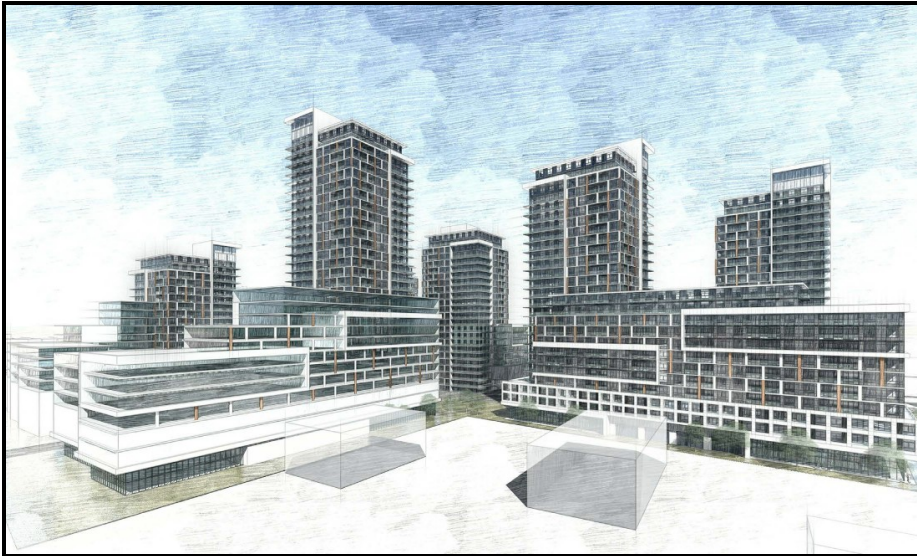


Neighbourhood Concept Plan

1.2- Design Vision

The Design vision for the Development is a self-contained, modern, vibrant residential community with supporting commercial facilities, set in the heart of a semi-rural GTA landscape with quick access to the local countryside, nearby villages and Metropolitan Toronto.

Surrounding Commercial and Industrial development will provide shopping and employment opportunities for the future residents



Development Concept Sketch

1.3 Design Objectives

Relevant urban design objectives that will be applied to the proposed development includes the following;

- *Good community design is important to successfully integrate the new growth projected for Bolton while maintaining and potentially enhancing the character of the community as a rural small town.*
- *The preservation and enhancement of natural features;*
- *The integration of common open space within the community;*
- *Streetscapes that are greener, pedestrian friendly and safe;*
- *Further development of the railway system including improved and new connections to link all neighbourhoods, community facilities and services;*
- *Good architectural design, particularly for medium and high-density development, safety and crime prevention through design;*
- *Gateway entrance features to the community;*
- *Maintaining views of valued landscape features and the night sky.*

1.4 Site Content

COMMUNITY SERVICES

■ PARK



Tormina Parkette



R. J. A. Potts Memorial Park

■ SCHOOL



Creative Children's Montessori School and Day care



Countryside Montessori & Private School



St John the Baptist Elementary

■ HEALTH CLINIC



Bolton Medical Practice And Therapy



■ WHOLESALE RETAILER



TruHealth Medical Clinic Centre



■ CAR DEALERSHIP



1.5 Policy Framework

1.5.1 Provincial Policy Statement

The 2020 Provincial Policy Statement (PPS) outlines the policies for managing and directing land use to achieve efficient development and land use patterns. The PPS promotes the focus of urban growth and development to settlement areas and away from significant or sensitive resources. Optional growth is to be obtained through efficient development patterns which efficiently use land and resources and ensure the effective use of infrastructure and public service facilities. The PPS promotes a mix of housing types and intensification within the urban area and the efficient use of municipal services in order to create sustainable living cities.

The proposed development contemplates the redevelopment of the subject property, currently an under-utilized site given its context within an urban settlement form that is consistent with the vision established within the PPS. The proposed development is located in a designated settlement area (Town of Caledon Official Plan, Schedule A1), which is appropriate for intensification.

The proposed development will contribute to creating compact, healthy, liveable and safe communities through the efficient development of a residential use on an existing infill site which utilizes existing infrastructure and public facilities and will contribute to the long-term needs of the local community. The proposed development accommodates a

mix of differing sized apartment units and can accommodate multi-generational households. The development also does not cause environmental or public health and safety concerns.

The proposed development will allow for an efficient use of existing infrastructure and access to public transit along a Major Road, which provides both inter- and intra-regional connections.

The mixed-use format of the proposed development contributes to the further diversification of uses within the community, will contribute to shortening commute journeys and decrease congestion, which will help to support climate change adaptability as identified by the PPS.

1.5.2 A Place to Grow:

A Place to Grow: Growth Plan for the Greater Golden Horseshoe 2019 (Growth Plan) outlines the policies for managing and directing where and how growth should occur within the Greater Golden Horseshoe. Amendment 1 to the Growth Plan was released in August 2020 and extends the planning horizon to the year 2051 and provides updated population forecasts anticipating a population 14.87 million people and 7.01 million jobs. Of this total, the Region of Peel is forecast to accommodate a population of 2.28 million residents and 1.07 million jobs.

The Growth Plan directs growth to settlement areas and prioritizes intensification, particularly in strategic growth areas. The Growth Plan also provides population and employment forecasts for municipalities within the Region of Peel, establishes the built boundaries for settlement areas, and provides targets for intensification.

The proposed development is within a settlement area situated within the delineated built-up area boundary and takes advantage of existing infrastructure and servicing. It represents an intensification of the existing subject site, supports the achievement of Growth Plan intensification targets and allocations, and provides for housing options that are

appropriate within the context of the subject site. The proposed development supports the efficient use of municipal infrastructure and contributes to the achievement of complete communities by adding to the range and mix of housing options currently available in the Town of Caledon and more particularly the Community of Bolton.

1.5.3 Region of Peel Official Plan

The Region of Peel Official Plan (RPOP) is a long-term planning document and applies to the combined areas of the City of Brampton, the City of Mississauga, and the Town of Caledon, and outlines strategies to guide and manage growth and development in Peel Region for the period between the years 2005 and 2031. The RPOP provides a holistic approach to planning establishes a long-term policy

framework to guide growth and development, while recognizing the different contexts of the local municipalities. The general goals of the RPOP are to create healthy and sustainable communities, recognize, respect, restore, and enhance the importance of the environment, recognize the importance of a vibrant economy, and to support growth and development in a sustainable manner.

The subject lands are designated Rural Service Centre by Schedule D- Regional Structure to the RPOP. Rural Service Centres, as described in policy 5.4.3, are intended to serve as the primary growth areas within the rural system. The relevant objectives of Rural Service Centres are:

Policy 5.4.3.1.1 and **5.4.4.1.2** clarifies that the objectives of RPOP is to: *“promote safe and secure communities and improvement in the quality of life through proper design and effective use of the built environment”* and *“to preserve and enhance the distinct character, cultural attributes, village atmosphere and historical heritage of Bolton and Caledon East”*. It is noted that RPOP directs the Town of Caledon to prepare

growth management and phasing strategies specifically for the Rural Service Centres (Section 5.4.3.2.5) Growth is to be prioritized within the built-up areas, and the Bolton Rural Service Centre is expected to accommodate the majority of the Town’s growth. Growth as proposed will defer the necessity of Future Settlement Area Expansion into the Regionally designated Rural Areas of the Town.

A range and mix of community type uses are permitted to be developed in accordance with the development policies of the RPOP.

The mixed-use development as proposed is considered to conform to the Rural Service Centre land use policies of the RPOP.

The RPOP recognizes Growth Plan policies and the guiding principle that decision making of the land development process, resource management and investment is to “build compact, vibrant and complete communities” to be balanced with sustainability objectives.

Section 5.5.1.1 *“To optimize the use of the existing land supply of the Region by directing a significant portion of growth to the built-up areas through intensification, particularly the urban growth centres, intensification corridors and major transit service areas.”*

Section 5.5.1.4: *“To achieve the intensification targets while providing for sufficient greenfield growth to satisfy the land need to accommodate the population and employment forecasts in this Plan.”*

Section 5.5.1.5: *“To optimize the use of the existing and planned infrastructure and services.”*

Section 5.5.1.6: *“To support planning for complete communities in Peel that are compact, well-designed, transit-oriented, offer transportation choices, include a diverse mix of land uses, accommodate people at all stages of life and have an appropriate mix of housing, a good range of jobs, high quality open space, and easy*

access to retail and services to meet daily needs”.

Section 5.5.2 *“It is the policy of Regional Council to direct a significant portion of new growth to the built-up areas of the community through intensification, and 5.5.2.4 Prohibit the establishment of new settlement areas.”*

Section 5.5.2.1: *“Direct the area municipalities to incorporate official plan policies to develop complete communities that are compact, well-designed, transit-oriented, offer transportation choices, include a diverse mix of land uses, accommodate people at all stages of life and have an appropriate mix of housing, a good range of jobs, high quality public open space and easy access to retail and services.”*

Section 5.5.2.2 *“Direct a significant portion of new growth to the built-up areas of the community through intensification.”*

Section 5.5.3 of the RPOP outlines objectives and policies related to intensification in particular and directs new growth to built up areas promoting compact urban form on underutilized lots and identifies that “this Plan recognizes the importance and advantages of intensification in Peel and implements the intensification policies of the Growth Plan” (Section 5.5.3), specifically Section 5.5.3.2.2 asserts that it is the policy of Regional Council to, among other things, “facilitate and promote intensification”.

The lands surrounding the subject property have been developed with a general mix of commercial, employment and residential uses. Notwithstanding the need for a local Official Plan Amendment, in this context the mixed-use development conforms to the Regional intensification policy framework and the Rural Service Centre designation. The proposed redevelopment contemplates a compact, urban built form with transit supportive density within the Rural Service Area, on a Major Road frontage which includes an existing transit route. The proposed development could be considered to reflect an infill and intensification development.

Sections 5.8.2 and 5.8.3 of the RPOP provides that a full range of housing needs is to be provided for, and commits to achieving a supply of accessible, affordable and adequate housing types, sizes, densities and tenures to meet existing and future housing market requirements.

The proposed development will ultimately provide 2229 residential apartment units in the form of one, two and three-bedroom units. This form of housing will facilitate housing for all income groups to address Regional affordable housing policies. The density of the development will create a compact built form and help to diversify the housing mix within the community. Commercial uses will also be provided on site maintaining a commercial presence on the lands and will service the residents of the proposed development and general community within the Town of Caledon. The built form will introduce a mixed-use format with direct pedestrian

connections to the public realm from active uses at grade. The proposed development will aid in establishing a pedestrian friendly environment along Hwy 50/ Queen Street. Upper-level dwellings will include overlook to the street and public realm, thus aiding in the sense of community and security for pedestrians at grade level.

The subject lands front onto Highway 50 which is identified as a Major Road within the RPOP, with a planned road width of 45 metres (Schedules E and F). A road widening conveyance sufficient to provide an ultimate width of 45 metres (22.5 metres from centreline) has been reflected on the proposed development plans.

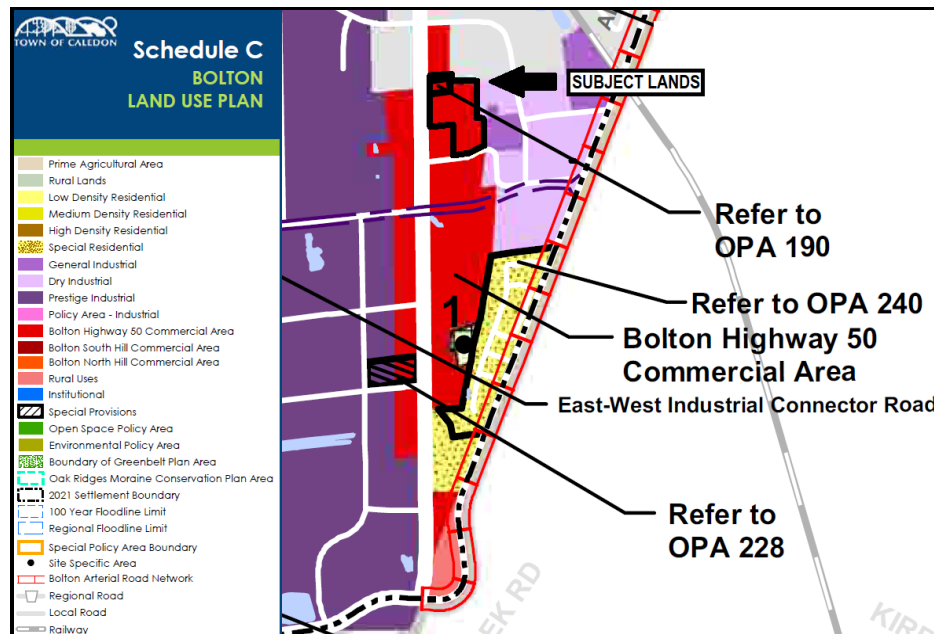
1.5.4 Town of Caledon Official Plan

The Town of Caledon Official Plan is a statement of principles, goals, objectives and policies to guide future land use, physical

development and change, and the effects on the social, economic, and natural environment within the Town of Caledon. It provides the detailed local basis upon which the Town and the Region will provide services within the municipality; provides a basis for preparing zoning and other by-laws which will implement the land use policies of the Plan.

The subject lands are designated *Bolton Hwy 50 Commercial Area*. The property at 12599 Highway 50 is also subject to the policies of Official Plan Amendment 190 (OPA 190) which allowed for the development of the existing

commercial plaza. Highway 50 is identified as a *Major Arterial Road* and the site is within the *Designated Built Up Area* of the Town.



The purpose of the *Bolton Hwy 50 Commercial Area* designation is to generally function as the principal centre for automotive-related uses and commercially related recreational uses in the Bolton trade area. All highway commercial uses,

large-scale business and professional office uses, and high-quality prestige industrial type uses are permitted in the Bolton Highway 50 Commercial Area.

An Official Plan Amendment will be required to permit the subject development. It is proposed that the site be redesignated to a site-specific mixed-use land use designation under the Town of Caledon Official Plan.

Section 3.5 of the Town of Caledon Official Plan promotes diverse housing types in order to meet the needs of current and future residents of the Town of Caledon, including the needs of different income groups, people with special needs and accessibility challenges and the needs of people through all stages of their lives.

The Housing objectives of the Town are to:

Section 3.5.2.2 *To encourage all forms of residential intensification in parts of built-up areas that have sufficient existing or planned infrastructure and community services.*

Section 3.5.3.1 *In conformity with the Strategic Direction of the Plan, the majority of new housing shall be located in settlements where full water, sewer, and community support services can be provided in an effective and efficient manner.*

Section 3.5.3.5.1 *A full range of housing types and densities will be planned as an integral part of each Rural Service Centre.*

Section 3.5.3.5.3 *The Town will consider innovative housing types which are in keeping with the Principles, Strategic Direction, Goals, and Objectives of this Plan.*

Section 3.5.3.5.4 *The Town will consider establishing and implementing housing targets for affordable rental and ownership housing and for market housing based on local supply and demand.*

The Town's structure policies identify areas where growth should and should not occur and how much growth Caledon should plan for and contain direction regarding the character of this growth. These Plans also establish a policy framework for managing renewable and non-renewable resources and planning for infrastructure investments. The hierarchy of settlements directs the primary growth to the Rural Service Centres of Bolton, Mayfield and Caledon East.

Section 4.1.1.3.1 *Development of settlements will take place within the following hierarchy:*

a) Rural Service Centres – compact, well-integrated rural towns on full piped water and sewer services.

Rural Service Centres are designated as the primary growth areas for the planning period. In order to provide services in an efficient manner to the large geographical area that comprises the Town, the Rural Service Centres will be the focus for the majority of new residential and employment growth as well as the focus for the provision of a wide range of goods and services for residents of the Town. Given this role, the Rural Service Centres are emerging urban communities within the Town and their character will evolve accordingly.

In accordance with Provincial policy, the Town of Caledon Official Plan identifies policies for Intensification:

Section 4.2.1.2.1 *sets an objective of the Plan to achieve compact and efficient urban forms, optimize the use of existing infrastructure and services, revitalize and/or enhance developed areas, increase the availability and diversity of housing and business opportunities and create mixed-use, transit-supportive, pedestrian-friendly urban environments through intensification.*

Section 4.2.1.1 *specifies that intensification potential is greatest within the Bolton Rural Service Centre: Opportunities for intensification within the built-up area exist primarily in the Bolton Rural Service Centre and, to some degree in the rural settlements identified as un-delineated built-up area.*

Section 4.2.1.2.1 *includes the following intensification objective of the Town of Caledon Official Plan: To achieve compact and efficient urban forms, optimize the use of existing infrastructure and services, revitalize and/or enhance developed areas, increase the availability and diversity of housing and business opportunities and create mixed-use, transit-supportive, pedestrian-friendly urban environments through intensification.*

Policy 4.2.1.3.1 *Caledon will encourage intensification within the built-up area and un-delineated built-up areas shown on Figure 1 of this Plan and will work to overcome barriers to intensification, where consistent with Section 3 of the Provincial Policy Statement.*

The proposed development represents redevelopment intensification of an existing underutilized commercial site. The development is considered to be consistent with the Town of Caledon housing and intensification policies which encourage intensification within the built-up area. Specifically, the proposed development is consistent with Town of Caledon policy which recognizes that opportunities for intensification exist primarily in the Bolton Rural Service Centre.

1.5.5 Comprehensive Town-Wide Urban Design Guidelines (2017)

The Town of Caledon's Comprehensive Town-Wide Urban Design Guidelines will guide the urban design and landscape of the proposed development. The purpose of the Guidelines is to "offer a unique 'Made in Caledon' approach to development that provides comprehensive town-wide and area specific urban design, landscape and architectural solutions. These Guidelines

deliver creative and sustainable recommendations for rural and urban areas of the Town that protect and enhance the natural environment, while accommodating future development and design trends.”

The Town of Caledon identifies these design principles:

1. *Sustainable Design & Compact Development*
2. *Accessibility & Universal Design*
3. *Community Safety & Security*
4. *Complete Streets & Active Transportation*
5. *Cultural Heritage Conservation*

The success of the TWDG is dependent on its effective application through a design-oriented development approvals process that is applied consistently over time. This section will present the role of supporting design briefs, outline the review process for the TWDG, and provide an overview of related studies that may be required in support of development applications, as identified through the DART process.

The Role of Design Brief.

As mentioned in Section 2.1 (Part 1) of these guidelines, a design brief is required to accompany a draft plan of subdivision, Official Plan Amendment application, Zoning By-law Amendment application and/or site plan application. Requirement for the design brief has been identified through the DART Process. The exact scope and content of the design brief has been determined through discussions with Town staff. The brief will help facilitate the Town’s review of these more complex applications. In general, the design brief will be required to:

6. *Be consistent with the TWDG.*
7. *Be prepared by a qualified Landscape Architect, Architect, or Registered Professional Planner (RPP) with demonstrated*

urban design experience (to the satisfaction of the Town).

8. *Introduce the development proposal, establishing the vision, objectives and character of the proposed development.*
9. *Demonstrate compliance with applicable Official Plan policies and guidelines.*
10. *Demonstrate the application of context-sensitive design solutions that address potential concerns relating to compatibility. Where the development application varies from the TWDG, the brief provides the designer with an opportunity to offer a rationale for such deviations.*

Architectural Review

A complete architectural review process in the Site Plan Approval stage is used to support the development of high-quality communities in the Town of Caledon. This Urban Design Report is in support of an application for Official Plan Amendment and Zoning By-Law Amendment.

The Town of Caledon’s Comprehensive Town-Wide Urban Design Guidelines do not formally address the High-Rise nature of the proposed development however they do apply to the Mid-Rise element.

Section 8.1.7 of the TWDG outlines the following design standards that have been addressed in the development of the proposal.

Mid-rise Design Standards (from TWDG)

- *Orient buildings to face and address public streets and locate them close to the street to maintain a strong street edge.*

- *Articulate all publicly exposed façades to provide relief and visual definition through the expression of cornices and other architectural elements and details*
- *Clearly identify main entrances and orient them to face the street or significant corners*
- *Incorporate vents and exhaust elements into the design of building façades so as not to be visually disturbing*
- *Screen rooftop mechanical equipment from public view by setting them back from the building edge and incorporating parapet walls.*
- *Provide an adequate buffer zone between waste facilities and adjacent developments and public streets*
- *Lighting for outdoor areas should be designed and located to provide safe outdoor space for users at night, and to facilitate crime prevention (CPTED)*
- *Lighting for outdoor areas, including signage lighting, should be located to minimize light spillage onto adjacent properties and the sky, and should be cast downward, where possible.*
- *On-street parking is encouraged, where possible, to support mid-rise developments.*
- *Enhanced landscaping and detailed fencing should be provided to soften the views to parking areas.*
- *Shade trees are encouraged on parking islands, and should be planted along street edges, where feasible.*
- *Provide secure bicycle parking and storage near entrances with protection from the elements.*
- *Provide consolidated driveways and parking garage entrance from the side street or from the rear of buildings.*
- *Provide outdoor drop off and waiting areas, with weather protection.*
- *Integrate signage and wayfinding into the design of the structure.*
- *Provide a clear hierarchy of circulation routes for pedestrians and vehicles.*

Mid-rise Design Requirements also include;

- *Provide clear and unobstructed pedestrian routes using pavement markings and signage to ensure intuitive wayfinding for all users of the site (including pedestrians, cyclists and drivers).*
- *Where feasible, underground parking garages are preferred, allowing for landscaped and enhanced open spaces and amenity areas in place of surface parking areas. Where this is not possible, provide surface parking at the rear of buildings, screened from public view.*

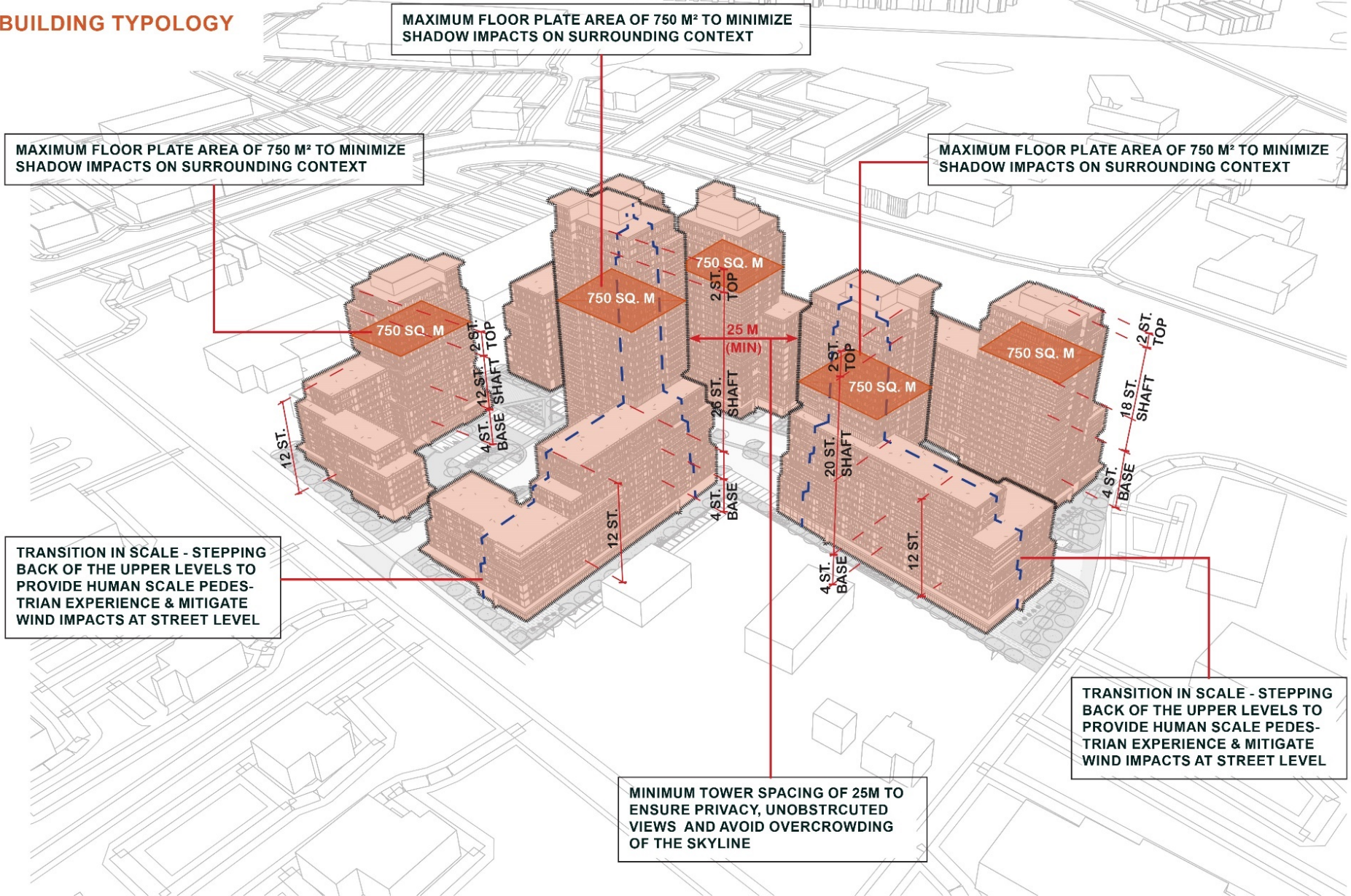
High-rise Design Guidelines (Self-directed)

- *Massing: In large Mixed-Use developments with multiple buildings, a variety of heights, wall planes and architectural elements should be employed to provide interest and articulation to the public realm.*
- *Built Form; The Built form of High-Rise buildings should employ the architectural design principle of Base, Shaft and Top.*
- *Setbacks: Adequate and appropriate Setbacks should be provided to all buildings, parking areas and landscape elements from all Property Lines.*

- *Step backs: Step backs in building wall planes and architectural elements should be used to mitigate the visual impact of large buildings, both slab and tower.*
- *750sqm floor plate for point-towers: Tower floorplates should be limited to 750sqm to avoid the monolithic look of slab buildings and reduce the shadow impact on adjacent properties.*
- *Distance between buildings: Adequate and appropriate distance should be provided between all separate building in the development. A minimum of 25m should be provided between point-towers.*
- *12.5m setback from the property-line or/and the public roads centerline for all high-rise point towers.*
- *1.5m minimum Step-Back after the 4th floor to create the High-rise base.*

These High-Rise Guidelines are generally regarded as good design principles for large, mixed use developments and have been self generated and directed in the creation of the Site Plan and Architecture of the proposed development.

BUILDING TYPOLOGY



MAXIMUM FLOOR PLATE AREA OF 750 M² TO MINIMIZE SHADOW IMPACTS ON SURROUNDING CONTEXT

MAXIMUM FLOOR PLATE AREA OF 750 M² TO MINIMIZE SHADOW IMPACTS ON SURROUNDING CONTEXT

MAXIMUM FLOOR PLATE AREA OF 750 M² TO MINIMIZE SHADOW IMPACTS ON SURROUNDING CONTEXT

TRANSITION IN SCALE - STEPPING BACK OF THE UPPER LEVELS TO PROVIDE HUMAN SCALE PEDESTRIAN EXPERIENCE & MITIGATE WIND IMPACTS AT STREET LEVEL

TRANSITION IN SCALE - STEPPING BACK OF THE UPPER LEVELS TO PROVIDE HUMAN SCALE PEDESTRIAN EXPERIENCE & MITIGATE WIND IMPACTS AT STREET LEVEL

MINIMUM TOWER SPACING OF 25M TO ENSURE PRIVACY, UNOBSTRUCTED VIEWS AND AVOID OVERCROWDING OF THE SKYLINE

High-Rise Design Principles

2.0 SITE PLAN

2.1 Development Proposal

The proposed development is designed as a campus of mid-rise and high-rise buildings arranged around a new spine road connecting Queen Street (Hwy 50) and Industrial Road. The towers range from 18 to 32 storeys and emerge from 12 storey mid-rise blocks.

There is provision for ground floor commercial in the podium level with some surface visitor parking to service the future tenants. The majority of the residential parking is in 3 levels of underground structure with elevator cores to serve each building and tower.

Amenity areas and terraces are arranged on the rooftops of podium or mid-rise blocks as well as at grade level between the blocks.

2.2 Building Relationship to the street and neighbouring buildings

The proposed development is located in a predominantly commercial and industrial area. There will be substantial landscape buffers with connecting walkways between the development and the surrounding uses.

The ground floor of the various blocks will provide additional small-scale commercial development aimed at servicing the new residents. Vehicle drop areas will service each of the lobbies of the blocks and there will be provision for refuse pick up at various locations in the development.

2.3 Parking & Loading (including accessibility)

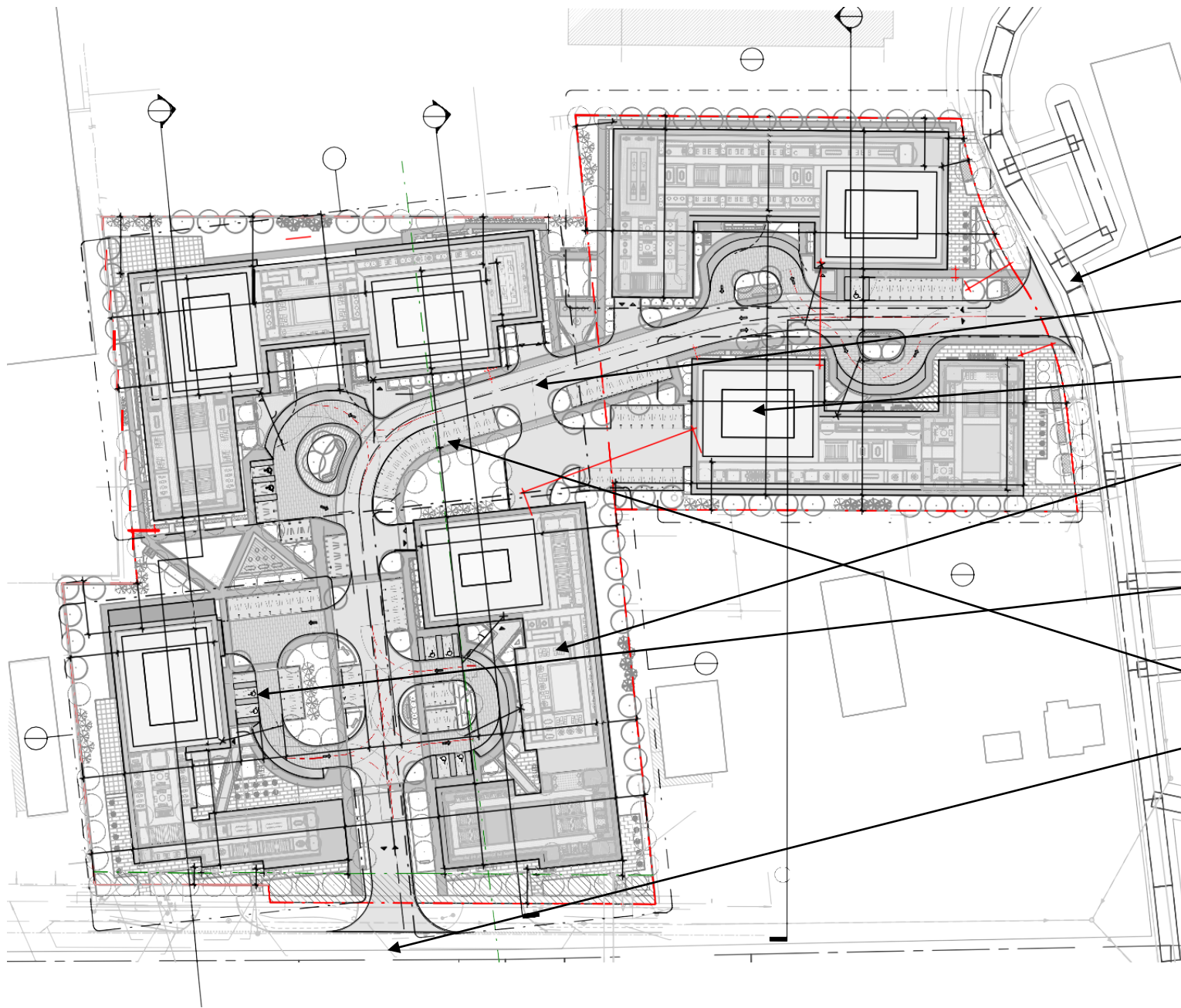
As stated previously, the majority of the residential and visitor parking is located on 3 levels of underground structure.

Proposed parking ratios are as follows;

- Residential parking at 0.85 per unit,
- Visitor parking at 0.20 per unit,
- Commercial parking at 1 space per 20m²

Handicap parking spaces to the required ratio will be provided, both surface and underground.

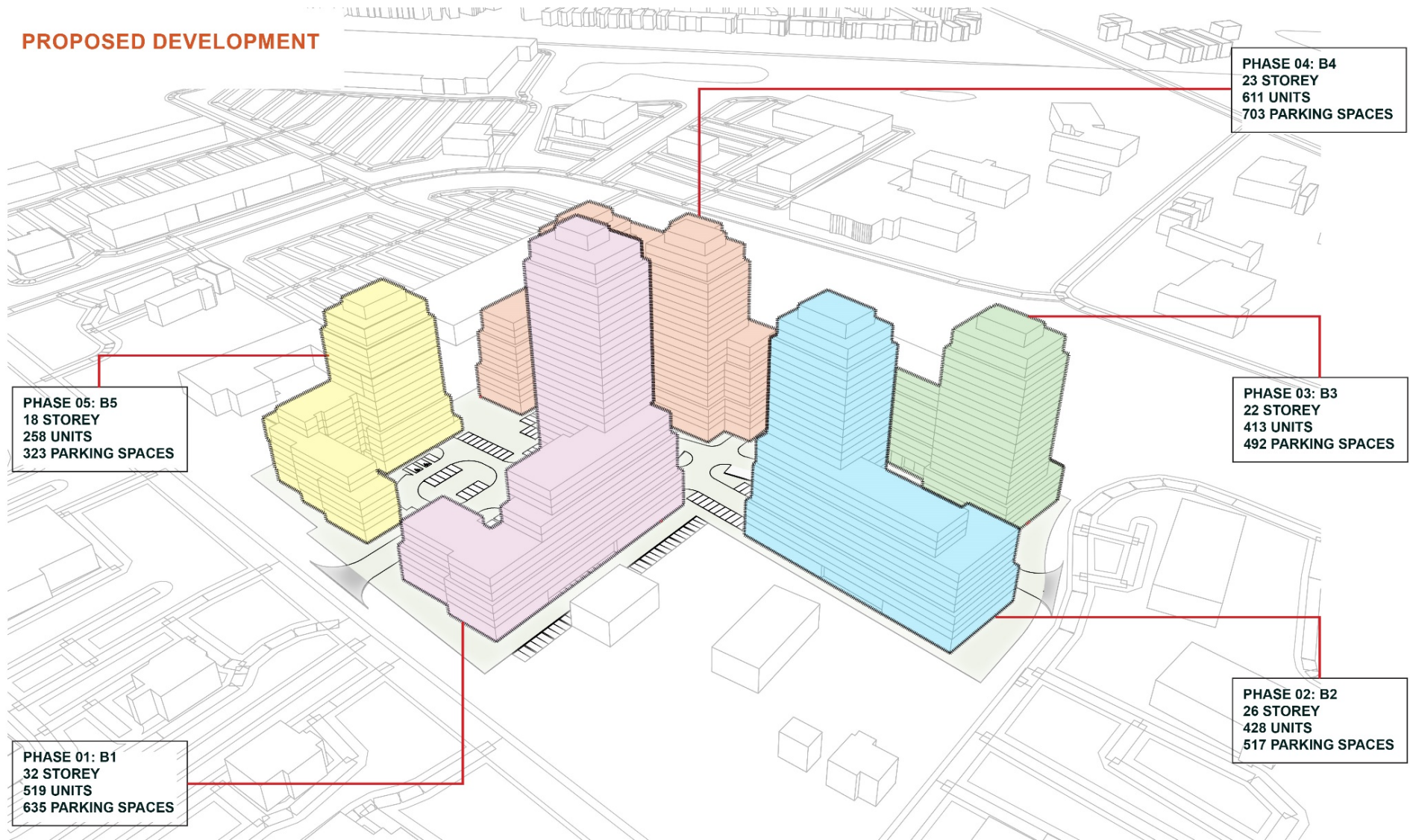
Refuse loading and moving truck areas, internal and external, will be provided in each of the phased development blocks.



- Industrial Road
- New Spine Road
- Typical 750m2 Tower
- Rooftop Amenity Areas
- Typical drop-off Area
- Commercial Parking Area
- Queen Street (Hwy 50)

Site Plan

PROPOSED DEVELOPMENT

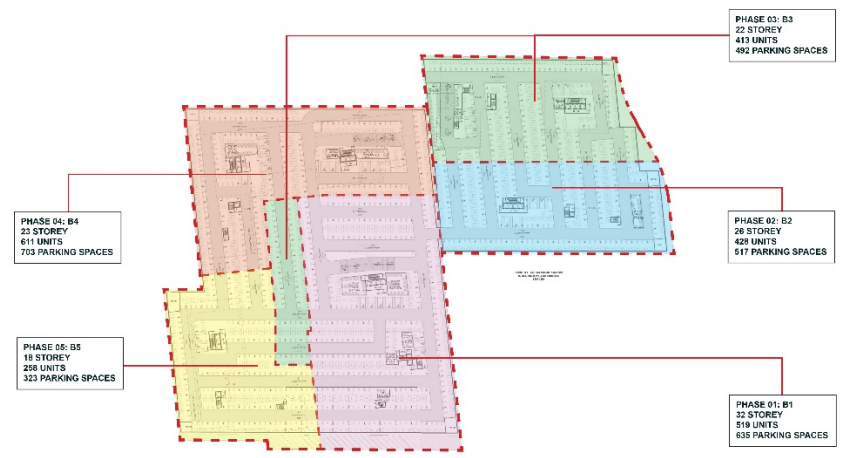


Phasing Plan (Aerial)

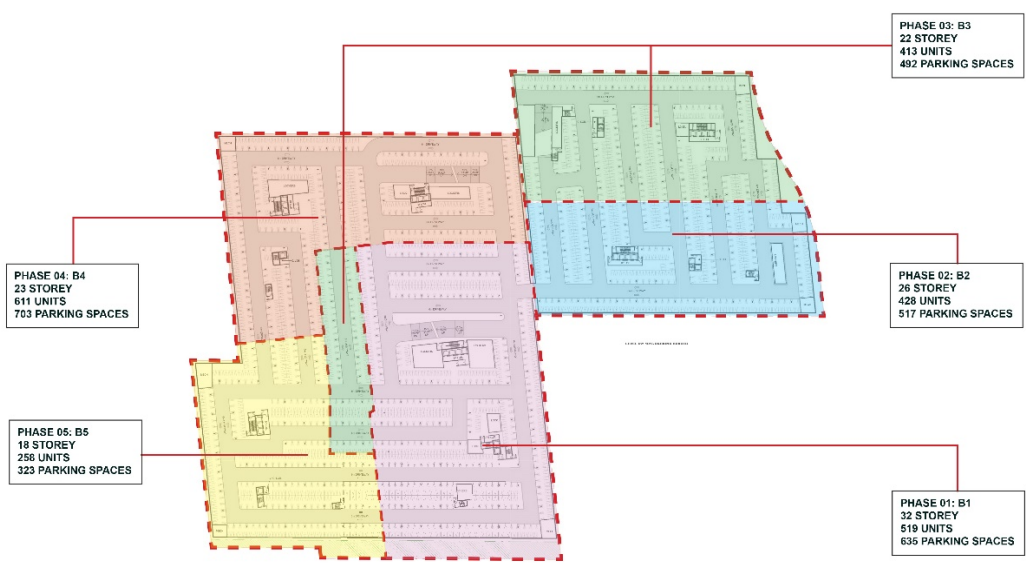
PHASING PLAN- SITE PLAN



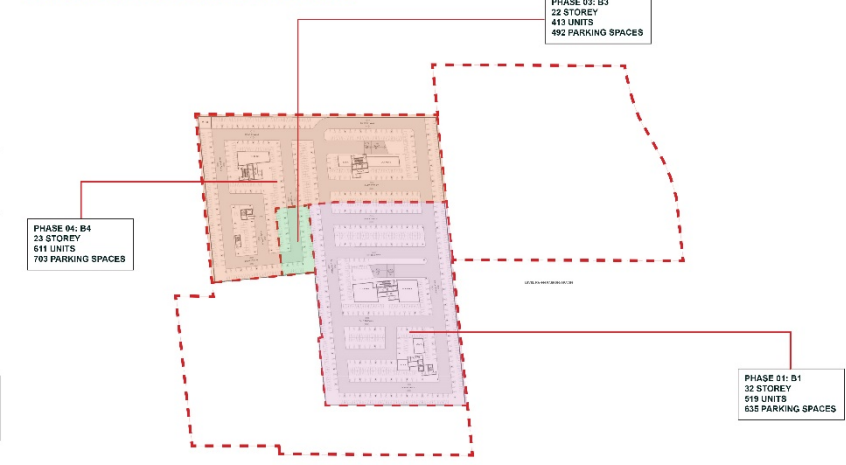
PHASING PLAN- LEVEL P1 (994 PARKING SPACES)



PHASING PLAN- LEVEL P2 (1015 PARKING SPACES)



PHASING PLAN- LEVEL P3 (446 PARKING SPACES)



Phasing Plans including Underground

SITE ANALYSIS

PHASE 01: B4
 23 STOREY
 611 UNITS
 703 PARKING SPACES

OUTDOOR AMENITY AREA

SURFACE PARKING AREAS
 10 PARKING SPACES

PHASE 01: B3
 22 STOREY
 413 UNITS
 492 PARKING SPACES

SURFACE PARKING AREAS
 11 PARKING SPACES

OUTDOOR AMENITY AREA

SURFACE PARKING AREAS
 17 PARKING SPACES

SURFACE PARKING AREAS
 40 PARKING SPACES

PHASE 01: B2
 26 STOREY
 428 UNITS
 517 PARKING SPACES

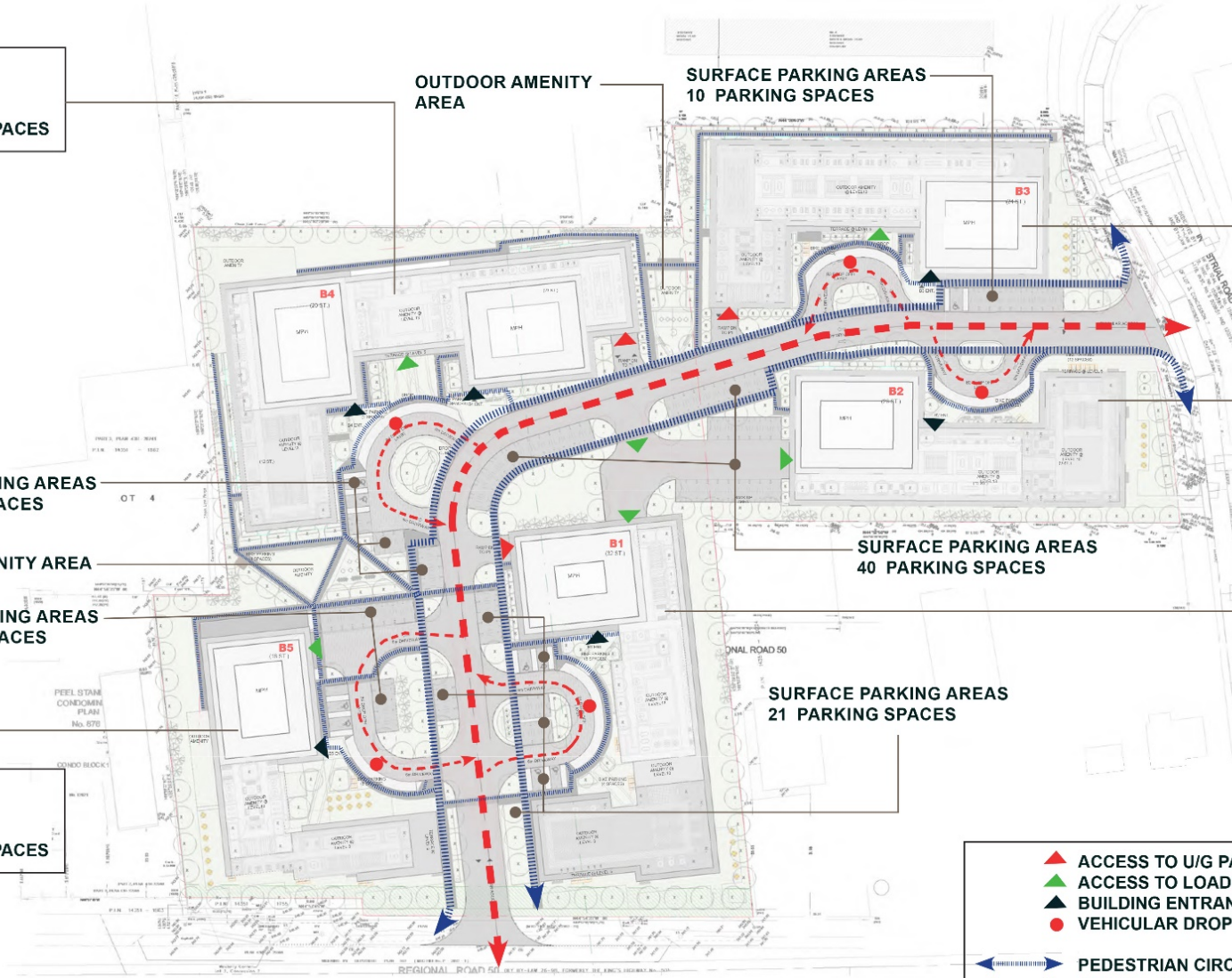
PHASE 01: B5
 18 STOREY
 258 UNITS
 323 PARKING SPACES

SURFACE PARKING AREAS
 21 PARKING SPACES

PHASE 01: B1
 32 STOREY
 519 UNITS
 635 PARKING SPACES

- ▲ ACCESS TO U/G PARKING
- ▲ ACCESS TO LOADING
- ▲ BUILDING ENTRANCE
- VEHICULAR DROP-OFF
- ↔ PEDESTRIAN CIRCULATION THROUGH THE SITE
- ↔ VEHICULAR CIRCULATION THROUGH THE SITE

Site Circulation, Access and Movement



2.4 Site Circulation – Site Access and Movement

As stated previously, the development is accessed and serviced by a central, private, 7.5m wide, two-way road that connects Queen Street and Industrial Road. 2.0m wide sidewalks are provided on both sides of the road, providing pedestrian access to every building and to the surrounding properties. The private road is designed and engineered as a Fire Route and to Municipal Refuse truck access standards.

2.5 Sustainability/Site Management

A number of Sustainability provisions have been and will be employed in the design and construction of the proposed development. These are in accordance with current Provincial standards and will include, but not be limited to the following;

- provide a high level of efficiency in energy consumption;
- maximize solar gains and be constructed in a manner that facilitates future solar energy installations;
- include or facilitate future on-site renewable energy systems;
- provide a high level of efficiency in water consumption including rainwater harvesting and recirculation for irrigation purposes;
- enhance indoor air quality;
- contain or facilitate the future of plug-ins for electric vehicles;
- use environmentally preferable building materials, high-renewable and recycled content building products and certified sustainably harvested lumber;

- provide water efficient and drought resistant landscaping, which should include the use of native plants and xeriscaping;
- maximize permeable surfaces, including the provision of permeable driveways;
- incorporate green roofs into building design;
- reduce construction waste and divert construction waste from landfill; and,
- promote Energy Star qualified development.

3.0 BUILT FORM – ARCHITECTURAL DESIGN

3.1 Proposed Built Form (Massing and Design)

As started in the previous Section 1.1.5, the proposed built form of the development is a campus of podiums, mid-rise block buildings and high-rise tower buildings. They are positioned around the spine road in a non-uniform, non-repetitive arrangement to present a modern, distinct mixed-use development.

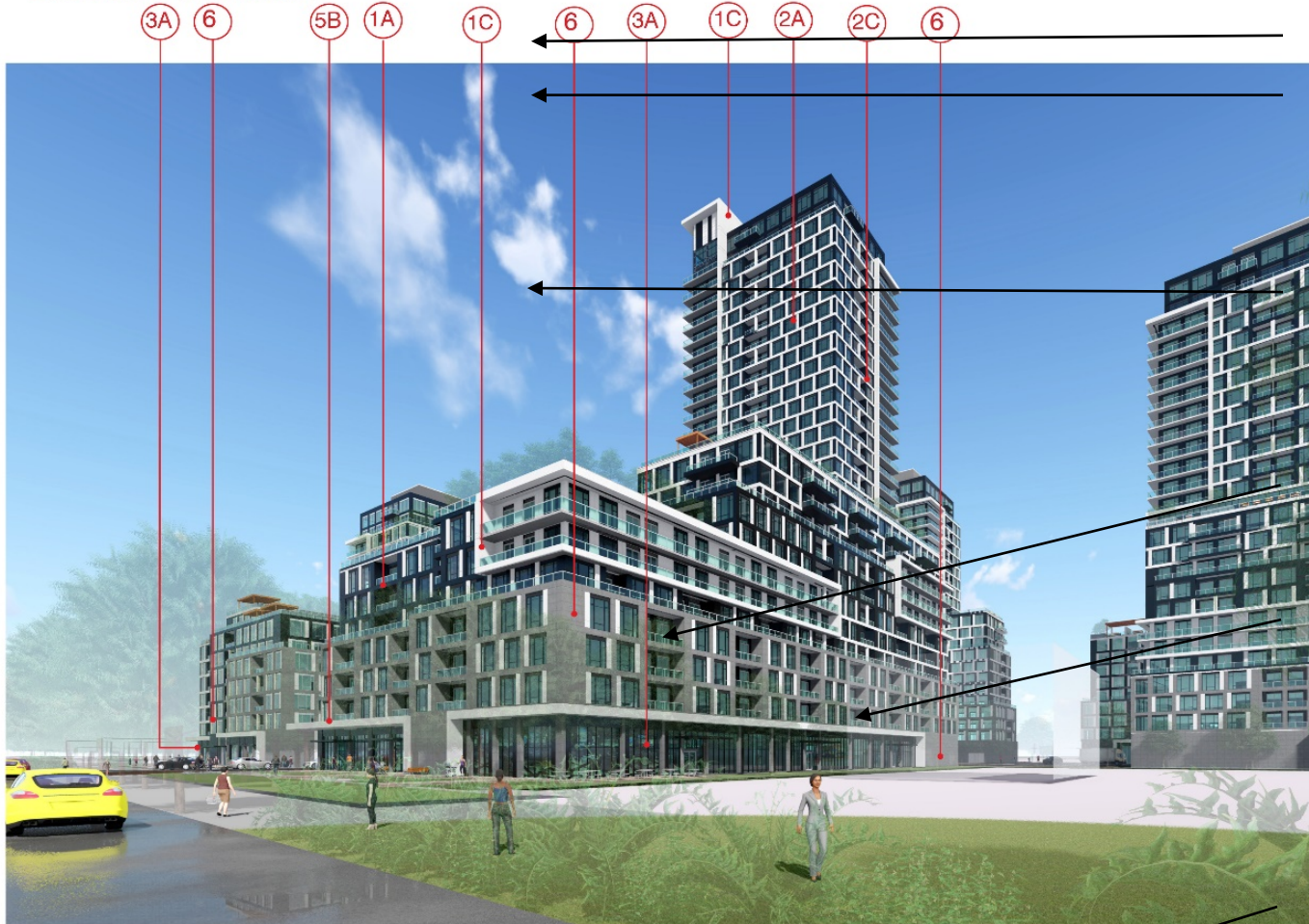
Contemporary building elements, balconies, large curtain wall glazing, spandrel panels, canopies, solid material planes and surfaces are used appropriately to create the proposed developments' design composition.



3.2 Materials and Colours

A complete Material and Colour Schedule is attached to the Building Elevations in the Architectural Drawing Package and indicates a high-quality, contemporary range of building materials.

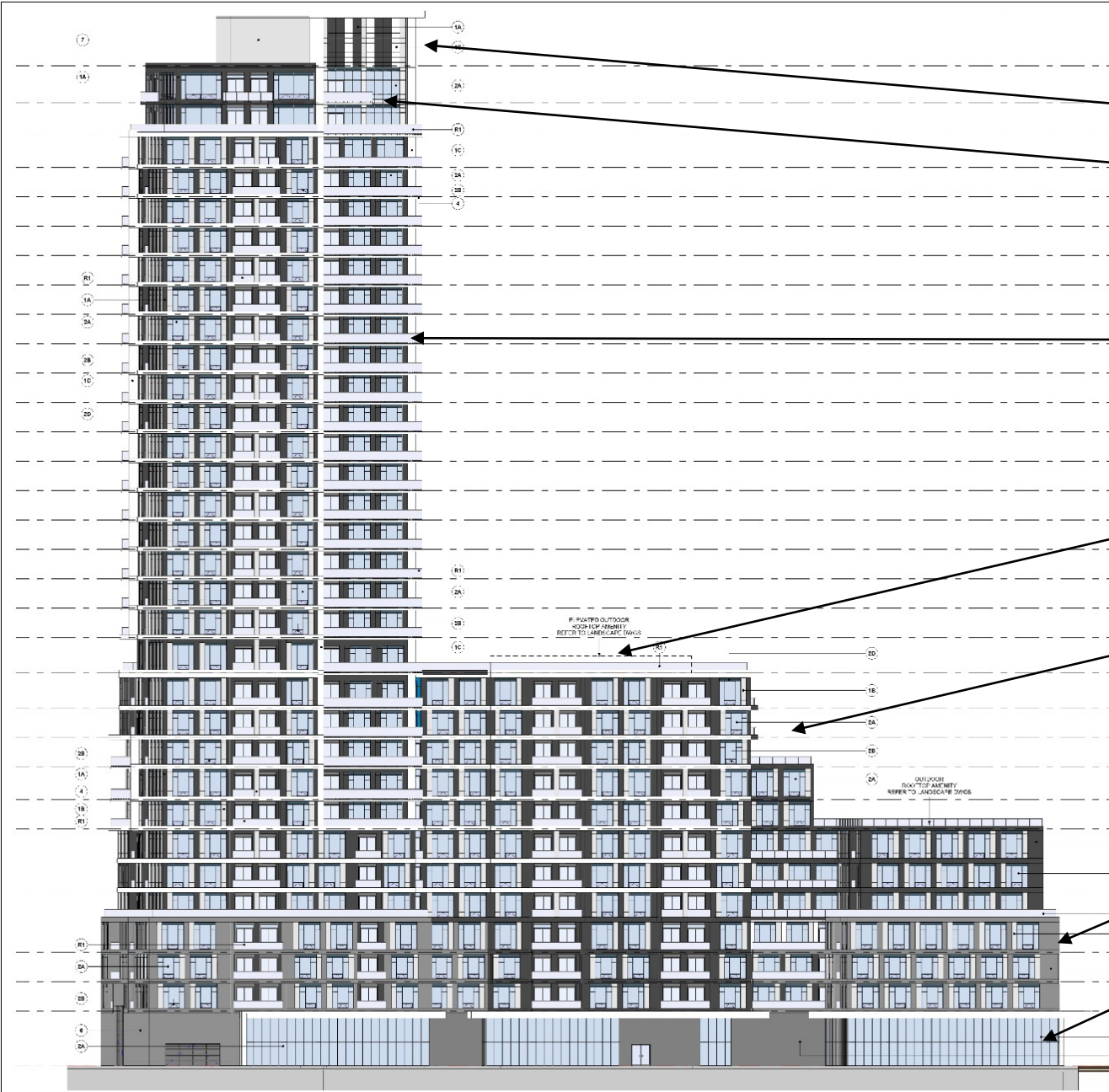
EXTERIOR MATERIAL



EXTERIOR FINISH LEGEND

- 1A** PRE-CAST CONCRETE - BLACK HIGH SANDBLAST
COLOUR : BLACK
MANUFACTURER : TBD
STYLE : TBD
- 1B** PRE-CAST CONCRETE - GREY HIGH SANDBLAST
COLOUR : GREY
MANUFACTURER : TBD
SIZE/STYLE : TBD
- 1C** PRE-CAST CONCRETE - WHITE HIGH SANDBLAST
COLOUR : WHITE
MANUFACTURER : TBD
SIZE/STYLE : TBD
- 1D** PRE-CAST CONCRETE - GREY HIGH DEEPER SANDBLAST
COLOUR : GREY
MANUFACTURER : TBD
SIZE/STYLE : TBD
- 2A** PREFINISHED WINDOW WALL SYSTEM - VISION PANEL
EXT. COLOUR : TBD
INT. COLOUR : TBD
MANUFACTURER : TBD
GLASS : TBD
PAINT COLOUR : TBD
- 2B** PREFINISHED WINDOW WALL SYSTEM - OPERABLE AWNING
EXT. COLOUR : TBD
INT. COLOUR : TBD
MANUFACTURER : TBD
GLASS : TBD
PAINT COLOUR : TBD
- 2C** PREFINISHED WINDOW WALL - GLASS SPANDREL PANEL
EXT. COLOUR : TBD
INT. COLOUR : TBD
MANUFACTURER : TBD
GLASS : TBD
PAINT COLOUR : TBD
- 2D** PREFINISHED WINDOW WALL SYSTEM - METAL LOUVRES
EXT. COLOUR : TBD
INT. COLOUR : TBD
MANUFACTURER : TBD
LOUVRES : TBD
- 3A** PREFINISHED CURTAIN WALL SYSTEM - VISION PANEL
EXT. COLOUR : TBD
INT. COLOUR : TBD
MANUFACTURER : TBD
GLASS : TBD
PAINT COLOUR : TBD
- 3B** PREFINISHED CURTAIN WALL SYSTEM - GLASS SPANDREL PANEL
EXT. COLOUR : TBD
INT. COLOUR : TBD
MANUFACTURER : TBD
GLASS : TBD
- 3C** PREFINISHED CURTAIN WALL - METAL LOUVRES
EXT. COLOUR : TBD
INT. COLOUR : TBD
MANUFACTURER : TBD
METAL PANEL : TBD
- 4** ARCHITECTURALLY FINISHED CONCRETE SLAB COATING
EXT. COLOUR : OFF-WHITE
INT. COLOUR : TBD
MANUFACTURER : TBD
METAL PANEL : TBD
- 5A** PREFINISHED ALUMINUM COMPOSITE PANEL
EXT. COLOUR : DARK GRAY
MANUFACTURER : TBD
SIZE/STYLE : TBD
- 5B** PREFINISHED ALUMINUM COMPOSITE PANEL
EXT. COLOUR : WHITE
MANUFACTURER : TBD
SIZE/STYLE : TBD
- 6** MASONRY - BRICK VENEER / OR STONE FINISH
EXT. COLOUR : WHITE
MANUFACTURER : TBD
SIZE/STYLE : TBD
- 7** EIFS - STUCCO
EXT. COLOUR : CLEAR WITH COATED PATTERN
MANUFACTURER : TBD
SIZE/STYLE : TBD
- R1** BIRD FRIENDLY GLASS BALCONY RAIL
EXT. COLOUR : CLEAR WITH COATED PATTERN
MANUFACTURER : TBD
SIZE/STYLE : TBD





3.3 Wall Articulation and Openings

The various wall planes comprise a mixture of precast concrete panels, brick masonry panels, glazed curtain wall panels and pre-finished metal panels. Balconies are either fully projected with glazed guards, semi-recessed with glazed or precast parapet guards. Private terraces occur at each building step back and there are glass balcony rails at the rooftop amenity areas. Refer to the Architectural Design Drawings for details of the above.

3.4 Exterior Equipment (Rooftop Mechanical, etc.)

Given the nature of the various buildings, all mechanical service equipment will be housed in rooftop Mechanical Penthouses. Service meters, both Commercial and Residential will be located away from Public view.

4.0 LANDSCAPE

The purpose of this chapter of the Design Brief is to describe and illustrate the design intent of the proposed landscape treatment for the proposed mixed-use residential development at 12563 & 12599 HWY 50, Bolton.

The landscape design indicated within this design brief is conceptual in nature and photographs are provided to show the intended character of the proposed landscape development. Final design solutions are to be resolved during Site Plan Approval and the detailed design stage of the project in consultation with the Owner, the Town of Caledon, and the team consultants. Site landscaping will be in accordance with the Town of Caledon's Site Plan requirements.





4.1 Landscape Plan

4.1.1 General Landscape Elements

Landscape Elements consist of:

- Boulevard trees
- Planting
- Fencing
- Paving
- Lighting
- Wayfinding Signage
- Site Furnishings

Landscape Elements Serve to:

- Provide unification to the community
- Provide visual interest
- Contribute to the character of the community.
- Provide shade and create comfortable, safe environment for residents.
- Provide sense of identity

4.1.2 Boulevard Trees

- Boulevard trees are a significant feature of the streetscape, softening and defining the edges of the street creating natural character of the community. Boulevard trees are to be planted in areas with sufficient space, along private roads, landscape spaces and around parking areas.
- All boulevard trees in this development are to be high branching deciduous trees. Trees are to be planted in groups of 3 to 9 of the same species.

- Boulevard tree planting and species to be in accordance with Town of Caledon and Region of Peel requirements.
- Native tree species or their cultivars are preferred, but non-invasive exotics may be used in limited cases, as necessitated by site conditions.

4.1.3 Planting

- Deciduous trees are a significant feature of the landscape and shall be provided along the public and internal road frontages to provide shade for pedestrian sidewalks, create visual interest, and provide softening of the architectural built form.
- Narrow form trees shall be required next to street light standards or where spatial constraints exist.
- Ornamental planting beds including hardy, drought resistant shrubs, ornamental grasses and perennials will provide visual interest, seasonal colour, and visual softening of the hardscape components
- Drought tolerant plant species should be considered. These species require reduced amounts of supplemental irrigation.
- Plant species hardy to site conditions will be selected for this site. Hardy species require less maintenance and are less susceptible to disease and damage caused by insect infestations.

4.1.4 Fencing

Depending on the need for sound abatement, privacy, or property delineation, the following types of fencing may be required: wood acoustic, wood privacy, and decorative metal. All fencing types will be in accordance with the Town of Caledon Standards.

The site is subject to a detailed acoustic study. Noise attenuation provisions may be necessary. The design of the wood privacy and wood acoustic fences should reflect the style of the built form.

Decorative Metal Fence:

- Decorative metal fencing will be provided to delineate the limits of ground floor amenity patios.

Wood Privacy Fence:

- Wood privacy fencing will be used along the north-west & south-east property lines that abut the surrounding industrial properties.
- The wood privacy fence will be 1.8m (6') high, providing effective visual screening from the neighbouring industrial properties.
- The wood privacy fence will be constructed with superior materials and carpentry, consistent with the quality of other built elements within the development.

4.1.5 Paving

- Paving treatments shall be of high-quality materials that harmonize with the building's exterior architectural treatments.
- Front walkways leading to the building entrances shall be treated with a decorative paving treatment.
- Provide high-albedo paving for pedestrian walkways, rooftop amenity area, private patios, and on-grade parking spaces to reflect light and thereby mitigating the Urban Heat Island Effect.
- Heavy-duty traffic-calming paving shall be provided in entry court areas.

4.1.6 Lighting

Decorative light standards will be used within the Community in accordance with Hydro One Standards. The light standards will be superior, upscale models, coordinated with the style of other design elements.

- Landscape-level lighting should be provided within amenity areas and along pedestrian walkways.

4.1.7 Entry Feature & Wayfinding Signage



The entrance feature signage conveys the first impression of the new development/ communities' image and character. Entrance features are clear, recognizable and attractive entry points into the community which provide visual interest and identity for the community. The following general design considerations shall be met for all entry and wayfinding signage:

- Entry features create gateways which convey the first impression of a community's image and character and are recognizable and attractive entry points.

- Ground signage should be in a highly visible location adjacent to the main vehicular entrance as well as at the entry courts.
- High quality durable materials shall be used as the primary material for the signage wall.
- Any ornamental planting should be low growing or be easily maintained/ trimmed to ensure that the signage is visible and legible.
- High quality durable materials shall be used as the primary material for the signage wall. Material palette shall be harmonious with the main building while reflecting the style of similar features located throughout the community.

4.1.8 Site Furnishings





- Site furnishing, including benches, tables, chairs, trash receptacles and bike parking will be located at the lobby entrances, commercial frontages and amenity areas. These site furnishings will provide opportunities for seating and social gatherings for residents and visitors.
- Site furnishings shall be selected to harmonize with the architectural style, materiality, and character of the building.

4.2 Amenity Space & Open Space



Amenity areas provide central common green space which contribute to defining the character of the neighbourhood.

These open spaces provide opportunities for active and passive recreation, children to play, and residents to socialize. The amenity areas provide opportunities for both active and passive recreation opportunities.

The following guidelines apply to the outdoor amenity areas:

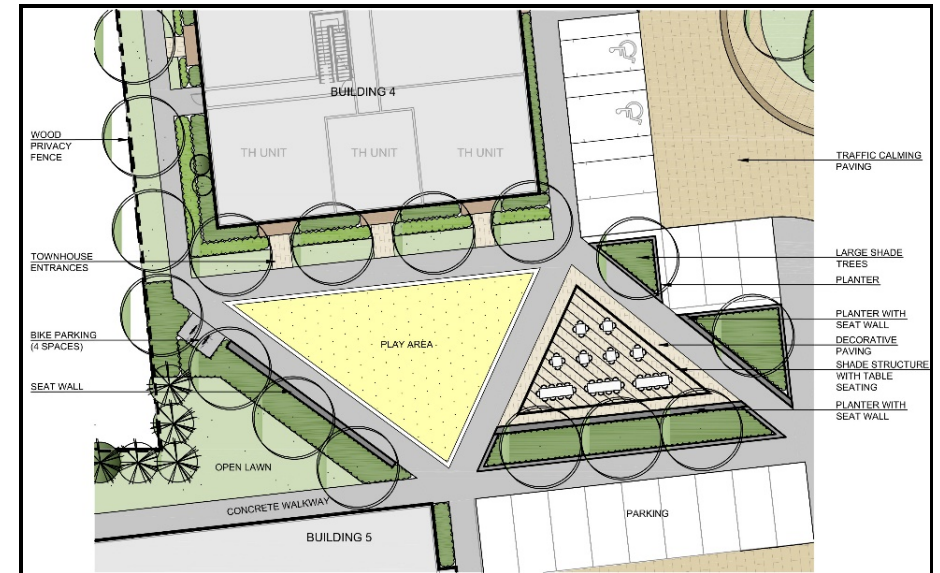
- Play areas should be centrally located and highly visible.
- Pedestrian access to the play areas should be convenient and easily defined.

- An open lawn should be considered beside the play areas to provide opportunities for free play and various passive recreational uses.
- trees and ornamental shrubs should frame the amenity areas, provide shade, and buffer views of parking. All plant material will be selected to provide seasonal colour while exhibiting hardiness to site conditions.
- The design of hard and soft landscape elements and features, including points of entry, should be consistent with and complementary to the overall community.
- Landscape features located within the amenity area will complement the workmanship and architectural styling of the surrounding buildings.
- Individual or cluster groupings of trees should be shaded passive use areas.
- Hard and soft landscape elements and features will be designed to identify areas of activity, circulation, entry points, seating and gathering areas.
- Pedestrian-scale light standards should be provided

4.2.1 Ground Floor Amenity Space

The ground floor allows for a large number of facilities for the residents. Facilities located on the ground floor consist of two amenity areas, open lawn space, private amenity patios and commercial amenity patios.

4.2.2 Amenity Area A



Amenity area A is located between Buildings 4 and 5. The centrally-located amenity space is accessible from the sidewalk along the internal road.

Amenity area A shall contain the following elements:

- Accessible children’s play area
- Shade structure
- Paved pedestrian walkways
- Decorative paving
- A variety of site furnishings: table seating, seat walls, trash receptacles
- Ornamental planting & large shade trees.
- Bike parking
- Small open lawn
- Planting buffer from parking areas

4.2.3 Amenity Area B

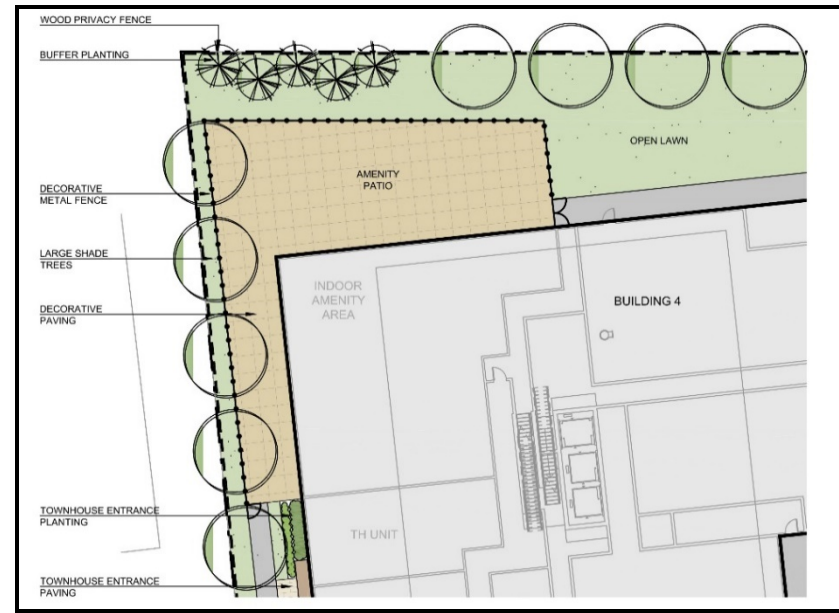


Amenity area B is located between Buildings 3 and 4. The centrally-located amenity space is accessible from the sidewalk along the internal road.

Amenity area B shall contain the following elements:

- Accessible children's play area
- Open lawn
- Shade structure
- Paved pedestrian walkways
- Decorative paving
- A variety of site furnishings: table seating, seat walls, benches, trash receptacles
- Ornamental and large shade trees
- Focal feature: art installation/ statue

4.2.4 Private Amenity Patio



Residents will have access to two private amenity patios that will serve as key social gathering spaces for residents. These private spill out patios are located on the north side of Building 4 and the west side of Building 5. These patios serve as an exterior extension of the ground floor amenity rooms.

The private spill out patios will consist of the following:

- Decorative paving
- Decorative metal fencing & gates to define the space
- Large shade & coniferous trees
- Potential area for lounge, dining and shared workspace

4.2.5 Open Lawn



A centrally-located open lawn is provided on the north side of Building 1. The centrally-located lawn is accessible from the sidewalk along the internal road. An open lawn area provides opportunity for free play and various passive recreational uses and gatherings. Large canopy trees around the perimeter shall provide shade. Benches along the sidewalk edge shall provide seating opportunities.

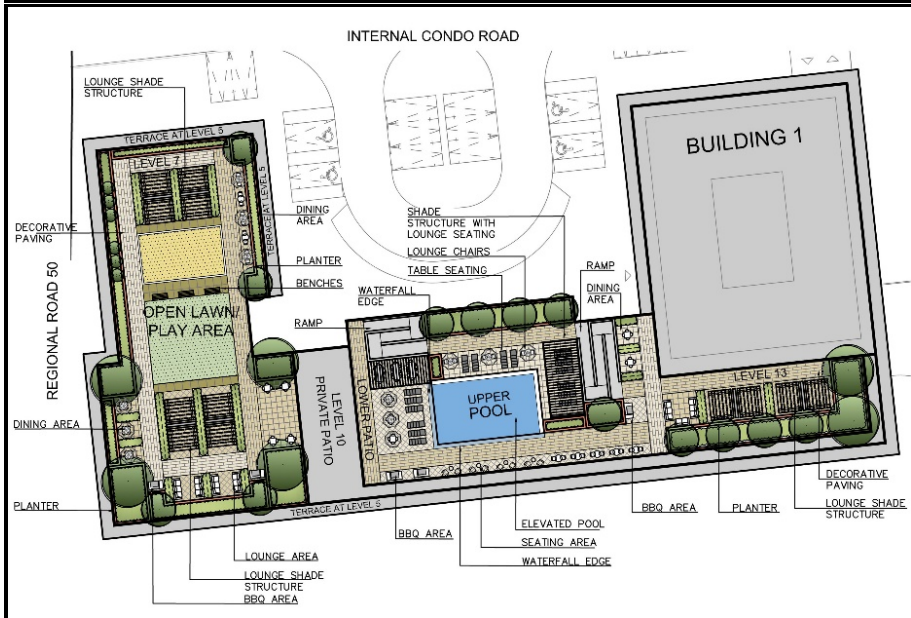
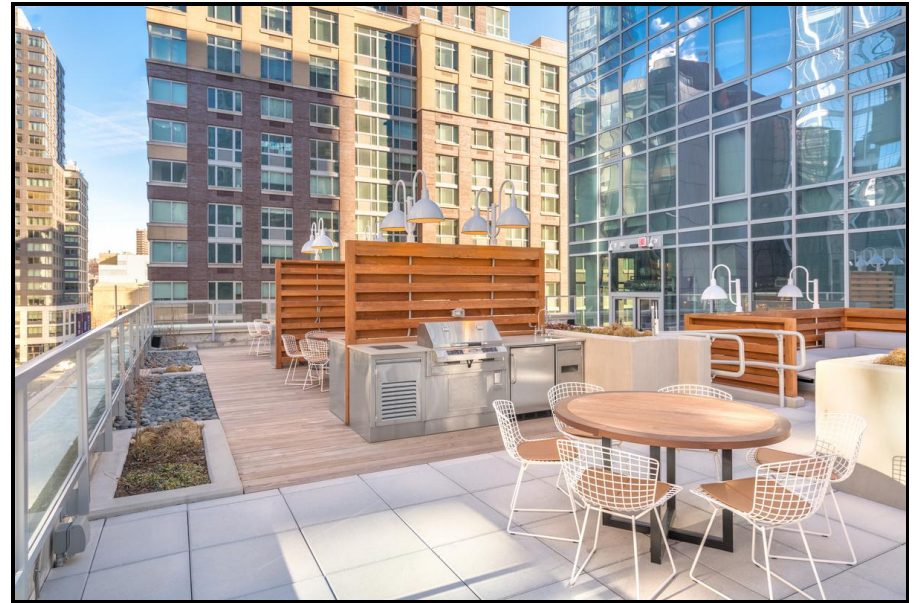
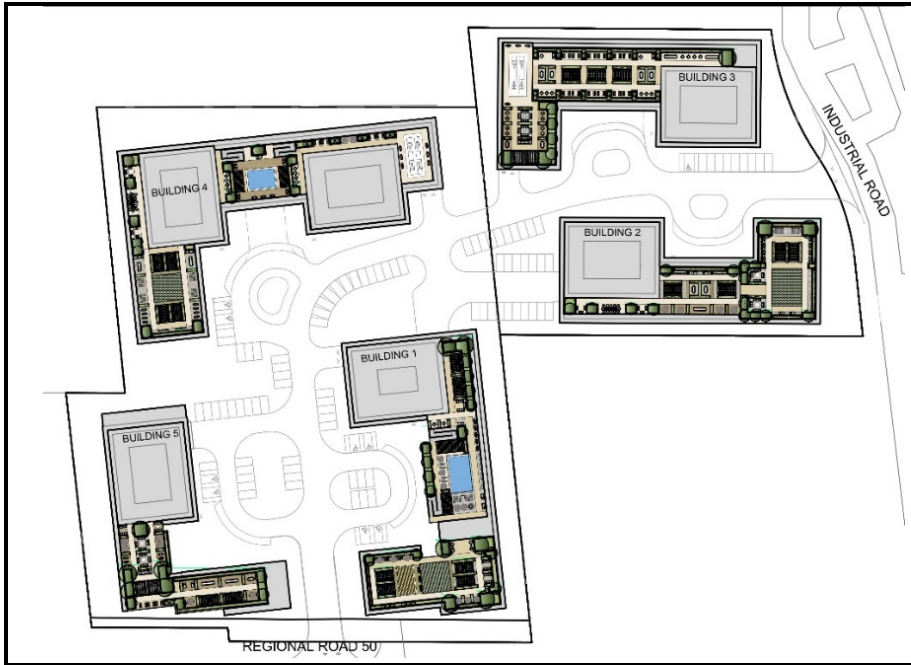
4.2.6 Commercial Amenity Patios

Potential commercial patios have been identified on the masterplan. These commercial patios are located at Buildings 1, 2, 3, and 5.

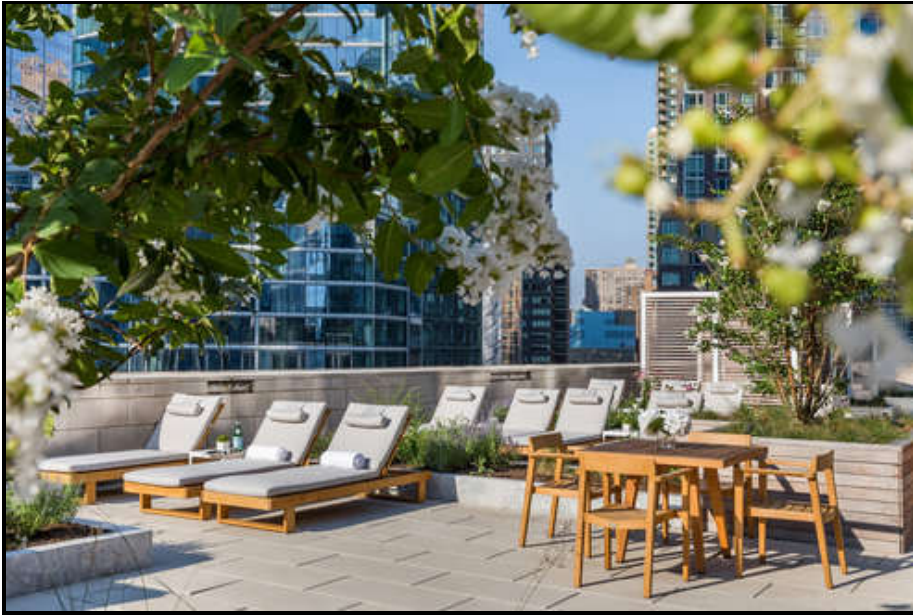
- Decorative paving
- Feature planting consisting of ornamental planting that acts as a buffer from ROW
- Large shade trees
- Bench seating
- Table seating
- Bicycle parking



The commercial amenity patios will consist of the following:



Rooftop Amenity Space



4.2.7 Rooftop Amenity Space

Each building has outdoor amenity terraces that provide a variety of amenity features for the residents. The following general design considerations shall be met for rooftop amenity spaces:

- Decorative paving provides definition of areas and circulation routes within the amenity terrace.
- Shade structures and tree planting will provide shade, contributing to a comfortable environment with the amenity area.
- Raised planters along the perimeter of the terrace provides screening and prevents overlook to the private terraces below.

The roof terraces serve a variety of functions and is composed of several distinct spaces:

- **Pool amenity spaces:** Outdoor pool terraces are located on the 13th level of Buildings 1 and 4. The pools are elevated on an upper terrace to accommodate the water depth and are easily accessible by ramps. A combination of lounge and table seating are provided to facilitate a variety of gatherings. Shade structures and deciduous trees provide protection from winds and hot summer sun.
- **Dining areas:** Dining areas are provided for each building. The dining areas range in sizes and configurations to allow small or large gatherings. BBQs / outdoor kitchen spaces are located throughout the terraces.
- **Lounge areas:** The lounge areas range in sizes and configurations to allow for small or large gatherings. A combination of low planting and privacy screens shall be used to create separate zones.
- **Shared work space:** A combination of table seating and lounge areas can be used to create a flexible shared workspace to allow residents the opportunity to work outside. These spaces will be broken up with planting and screens to create private zones that vary in size and in site furniture that will allow for different uses.
- **Open lawn/ Flex amenity space:** Buildings 1,2,3, and 4 have open lawn or flexible amenity spaces that can be defined based on the needs of the community/

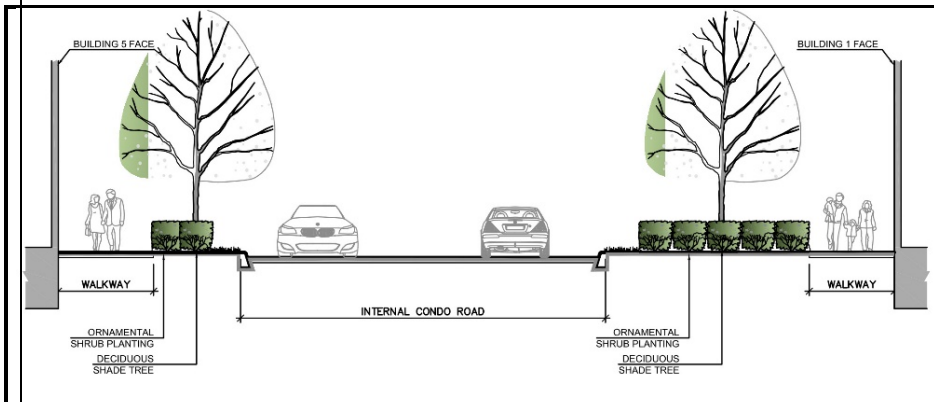
building ideal demographic. These flexible amenity spaces can be used as potential play areas, dog relief areas, open lawn, games areas and/or fitness areas.

4.3 Landscape Buffers – Street Front and Building Entrance

4.3.1 Streetscape

Streetscape involves the combination of landscape elements including boulevard trees, fencing, lighting, paving, and built-form elements. These elements provide unification to the community, create visual interest, and provide shade, resulting in a comfortable atmosphere within a community with a defined character.

4.3.2 Highway 50 Streetscape Treatment



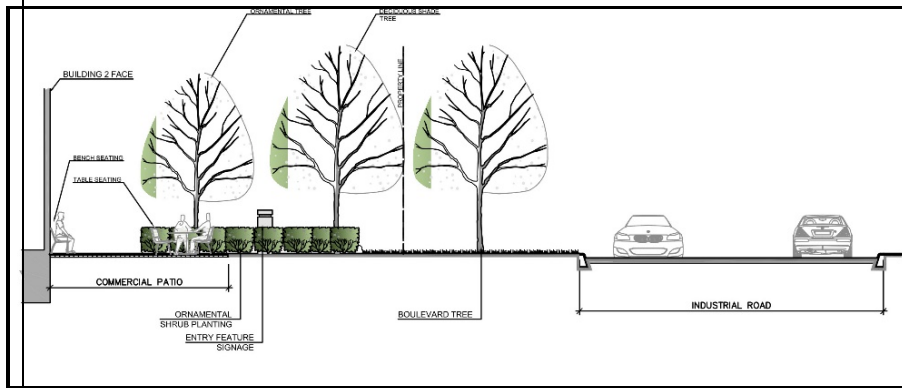
Regional road Highway 50 forms the south-western boundary of the site development and is one of the main vehicular entrances into the proposed development. The proposed streetscape frontage will provide an upscale and attractive interface along the arterial roads and enhance the character of the neighbourhood. The landscape elements shall create visual

interest, provide shade and contribute to the pedestrian scale, resulting in a comfortable pedestrian atmosphere with defined character.

- The vehicular entrance located on Regional Road 50 is clearly defined with ornamental planting beds flanking both sides of the driveway. The future multi-use trail which crosses the driveway shall be clearly defined with curb cuts and contrasting paving, ensuring safe vehicular and pedestrian interactions.
- Convenient pedestrian connections are provided from the subject site to the multi-use trail along Highway 50.
- The design of hard and soft landscape elements and built form features should be consistent with and complementary to the overall development's themes.
- Site furnishings (benches, bike parking and trash receptacles) shall complement the style and character of the proposed building and built form. Strategically located benches and site furnishings will provide opportunities for a passive pedestrian experience and will contribute to an enhanced pedestrian environment along the streetscape.
- Entrances to the commercial units should be clearly defined with feature paving.
- Potential commercial patio areas are identified at the south-eastern side of Building 1 and the north-western side of Building 5.

- A row of large canopy deciduous trees within private property shall be provided for shade, creating a comfortable pedestrian realm.
- The streetscape design shall be in conformity with the Town of Caledon and Region of Peel streetscape guidelines.

4.3.3 Industrial Road Streetscape Treatment



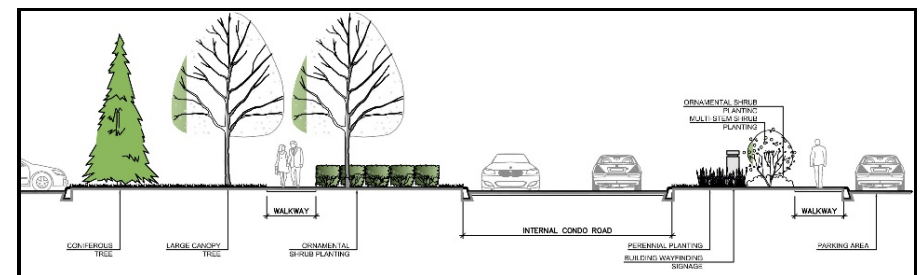
Industrial Road forms the eastern boundary of the site development and is one of the main vehicular entrances into the proposed development.

- The vehicular entrance located on Industrial road is clearly defined with ornamental planting beds and entry signage.
- A row of large canopy shade trees will be provided within the ROW, with a second row provided within private property.
- The streetscape shall include a combination trees, shrubs plantings; details to be resolved at the design/site plan stage.

- The proposed streetscape treatment shall enhance the public realm while delineating boundaries between public and private property and also help unify the community and create visual interest.
- Site furnishings (benches, bike parking and trash receptacles) shall complement the style and character of the proposed building and built form. Strategically located benches and site furnishings will provide opportunities for a passive pedestrian experience and will contribute to an enhanced pedestrian environment along the streetscape.
- Entrances to the commercial units should be clearly defined with feature paving.
- Commercial patio areas shall be screened from the public realm.
- The streetscape design shall be in conformity with the Town of Caledon streetscape guidelines.

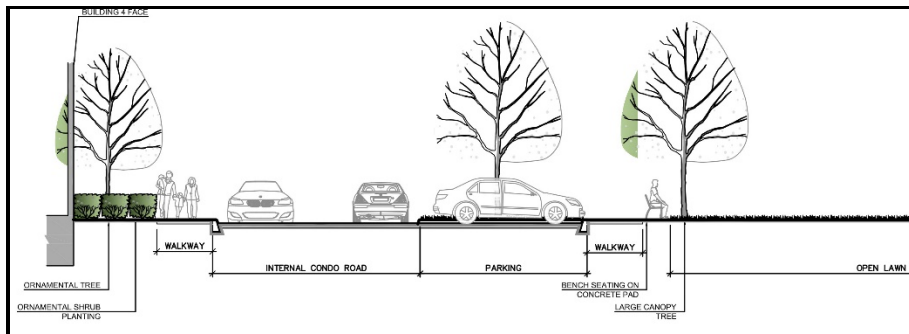
4.4 Internal Streetscapes

4.4.1 Internal Road Streetscape Treatment



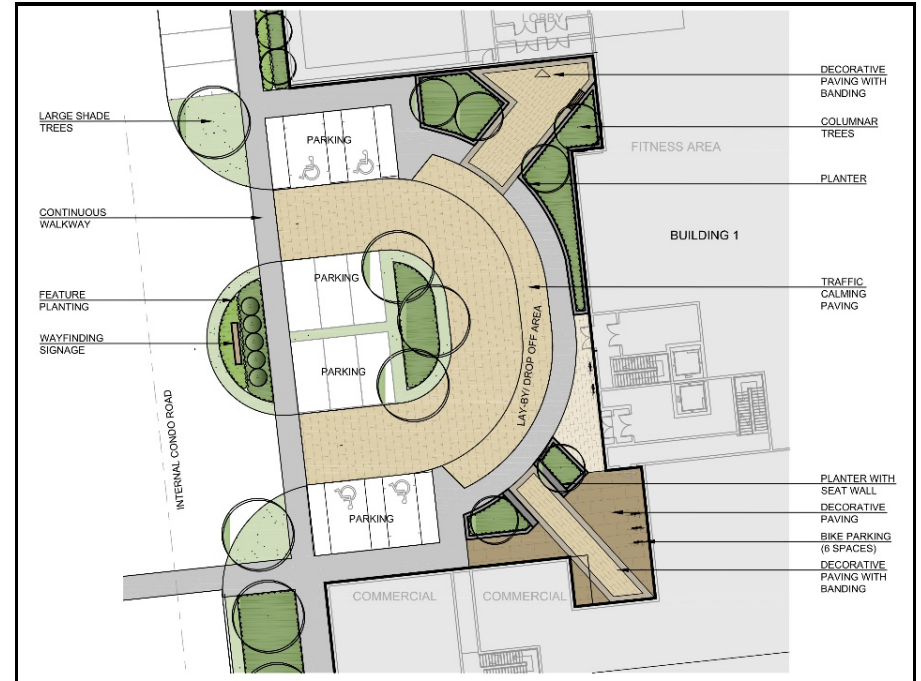
The following guidelines apply:

- The streetscape treatment includes deciduous trees, planting beds and continuous pedestrian sidewalks on both sides of the internal road.
- Where sidewalks cross paved vehicular areas they shall be constructed with contrasting materials such as decorative pavers or concrete paving to visually alert motorists that pedestrians may be crossing in these locations.



- Plantings of deciduous canopy trees and ornamental shrubs provide shade along pedestrian walkways and also soften the surrounding hardscape, enhancing the pedestrian experience.

4.4.2 Building Entrance Court



The following guidelines apply to the building entranceways:

- The entrance court areas provide access to the building entrances, as well as the loading areas.
- The entrance court shall be paved with a material that contrasts from the internal asphalt road. Decorative pavers may be used to encourage traffic-calming and help create a sense of arrival.
- The design of hard and soft landscape elements should be consistent with and complementary to the overall development's themes.
- A feature paving design shall be used to define the main lobby entrances.

- Pedestrian entrances should be highly visible and accessible from sidewalks.
- Outdoor bicycle parking should be provided next to the building entrances for convenient access.
- Conveniently located bench seating shall be positioned at building entrances.
- Raised planter curbs and planting beds provide an enhanced planting treatment. These planting beds, composed of ornamental shrubs, perennials and grasses soften the court area while providing privacy and screening for the at-grade units (for Buildings 2 and 4).

4.4.3 Commercial Street Frontage



Commercial areas are located at the ground floor of Buildings 1,2,3 and 5. Commercial areas shall front onto the public roads (Regional Road 50 and Industrial Road), while also having access from the internal site.

The following guidelines apply to the commercial street frontages:

- Pedestrian walkway areas along the commercial frontages shall include decorative paving which enhances and harmonizes with the character of the proposed building.

- Entrances to the commercial units should be clearly defined with feature paving pattern.
- Site furnishings (benches, bike parking and trash receptacles) shall complement the style and character of the proposed building and built form. Strategically located benches and site furnishings will provide opportunities for a passive pedestrian experience and will contribute to an enhanced pedestrian environment along the streetscape.

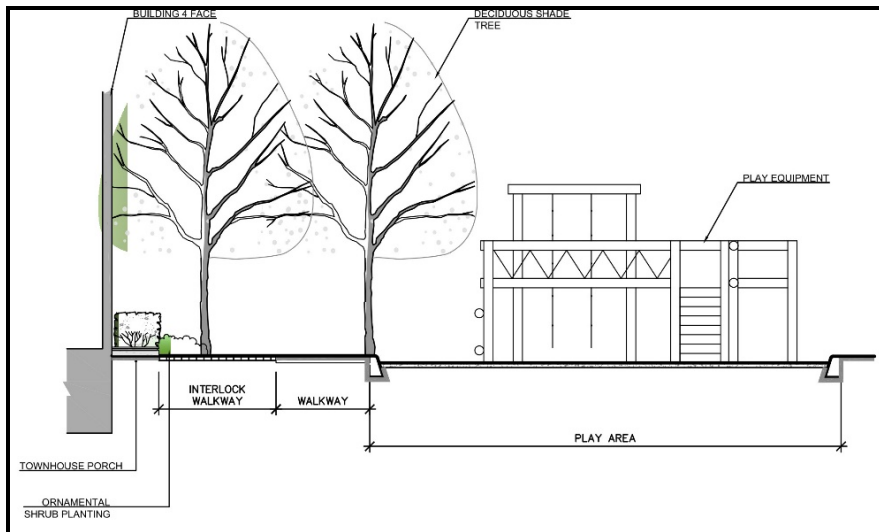
4.4.4 Townhouse Entrances



Ground floor townhouse units are proposed for Building 2, 3 and 4. Where possible, townhouse units shall front onto the internal road, or amenity space.

The following guidelines apply to townhome entrances:

- Decorative paving shall be used for private walkway entrances.
- Visual screening of the private entrances is provided with ornamental plantings.



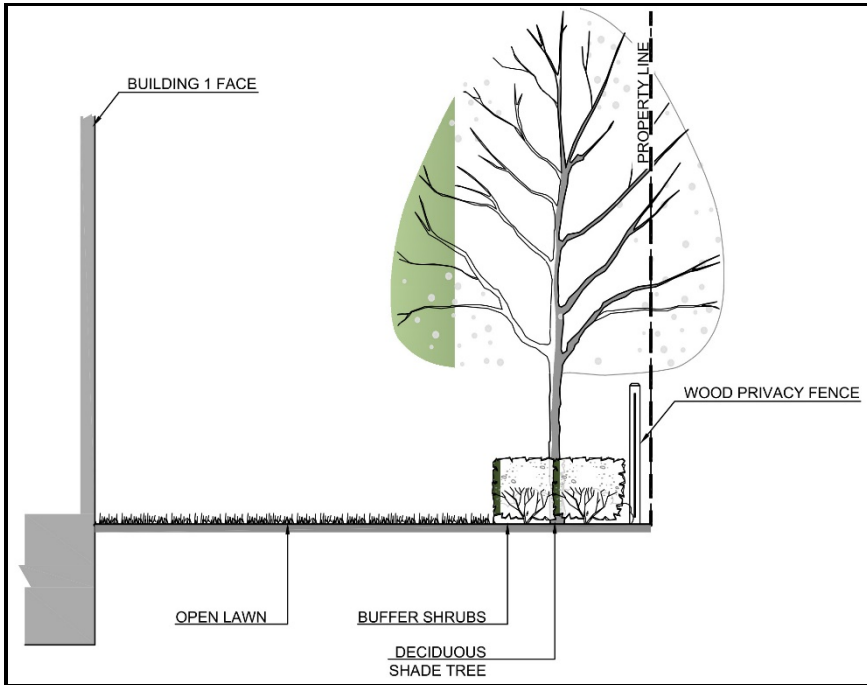
4.4.5 Parking

- Underground parking is provided for building visitors, employees, and residents.
- Vehicular access to the underground parking is provided at Building 1,3 and 4.
- A mixture of at-grade parking will be located along the internal road and adjacent to building drop-off areas.

- Large uninterrupted expanses of paving will be avoided. Parking will be broken up into smaller blocks and defined by buffer plantings, consisting of shade trees, ornamental deciduous trees, and ornamental shrubs and perennials.
- Outdoor bicycle parking has been provided by building entrances and at key amenity areas.



4.4.6 Landscape Buffer



Landscape buffers frame the edge of the development, providing screening from adjacent land uses, as well as providing privacy for the residents.

The following buffer treatments shall be provided:

- 1.8m High wood privacy fence shall be proposed along the will be placed along the property line.
- A combination of large canopy deciduous, coniferous trees, and hedges will be planted along the fence line.

4.5 Foundation Landscape

4.5.1 Entry landscaping



- Entry features create gateways which convey the first impression of a community's image and character and are recognizable and attractive entry points.
- The entry feature and signage wall shall incorporate a masonry signage walls and ornamental planting.
- High quality durable materials shall be used as the primary material for the signage wall. Material palette shall be harmonious with the main building while reflecting the style of similar features located throughout the community.

4.5.2 Building Foundation Landscaping

- Foundation plantings will be used along the building to break up the hard surface and soften the landscaping around the building.
- Plantings may consist of a variety of trees (both deciduous and coniferous), shrubs, perennials. Emphasis should be placed on specifying native plant material and on plants with showy colour, texture and form that will provide interest throughout the year.
- Plantings may consist of a variety of trees (both deciduous and coniferous), shrubs, perennials. Emphasis should be placed on specifying native plant material and on plants with showy colour, texture and form that will provide interest throughout the year.
- Provide drought tolerant, hardy, disease and pest resistant plant species that also contribute to an increased urban tree canopy.

4.6 Sustainability

The design, including architecture, engineering and landscaping shall conform to the requirements to prepare for the impacts of a changing climate through the effective management of stormwater, including the use of green infrastructure and mitigating risks to human health, safety, property and the environment.

4.6.1 Building Materials

- Landscape building materials which include sustainability features and/or qualities i.e., recycled materials etc. will be specified where possible.
- Locally sourced materials will be specified where applicable.
- Paving materials with lower albedo rates which contribute to lower levels of solar gain will be preferred.

4.6.2 Plant Materials

- A variety of tree species and plant material will mitigate the potential impact of the spread of disease and insect infestations.
- Drought tolerant plant species which require reduced amounts of supplemental irrigation to thrive shall be selected.
- Plant species hardy to the site conditions shall be selected. Hardy species require less maintenance and are less susceptible to disease and damage caused by insect infestations.
- Adequate soil conditions shall be provided, including the re-use of the existing soil and provision of sufficient space to ensure optimal root growth.

5.0 CONCLUSION

With its development at 12563 & 12599 Queen Street, HWY 50 in Bolton, Township of Caledon, 12599 Hwy 50 Ltd. has proposed a significant addition to the residential market in the western GTA.

It is a bold vision by any measure however, it reflects the great success of the regions surrounding the City of Toronto in attracting new residents, many from the busy downtown and others arriving from all around the world.

By celebrating contemporary architecture in the design of the residences, and building to meet the Towns vision, this project proposes to be a worthy addition to the fabric of the Highway 50 (Queen Street) Mixed-use Development Area.





Street view Images of the Proposed Development

