

TOWN OF CALEDON
PLANNING
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March 18, 2022

URBAN DESIGN BRIEF

Commercial/Industrial Development

13846 and 13940 Airport Road
Sandhill, Town of Caledon, Ontario



PREPARED BY:
RG CONSULTING INC
2201 Finch Ave W
Suite 27 Toronto, ON M9M 2Y9

May 2020

R G C
rgcdesigngroup.com

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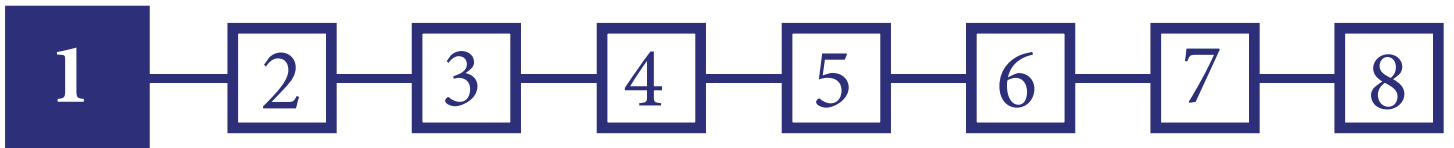
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INTRODUCTION



1.1 INTRODUCTION/EXECUTIVE SUMMARY

This Urban Design Brief (UDB) has been prepared by RG Consulting Inc. (RGC) as an attachment to the Site plan approval process as outlined by the Town of Caledon Planning department. The UDB shall address the design principles and guidelines as they pertain to the proposed development to ensure compatibility of the subject property within the context of Governing Planning Policy and uses as previously approved by the Town for the subject sites.

UDBs are used to evaluate the various elements of development applications. These documents provide an understanding of how and why projects are designed in a particular manner, and give Caledon Planning staff the opportunity to work co-operatively with proponents.

The main purpose of the UDB is to illustrate the design solution for a new development project in the Town of Caledon and how the solution was devised. It provides clear guidance on how a site should be developed, and details the design principles for a site, as well as the implementing policies to achieve these principles. Hence, this report mainly contains architectural, landscape and site design aspects for the future development.

The primary goal of this document is to ensure the subject property's development fits with, and contributes to, the outline requirements of a mixed use commercial and industrial development.

The UDB describes the principles and standards on which the subject property's development will be based through the following criteria:

1. Provide a high standard of site plan design particularly due to the location of the development along Airport Road, south of King Street in the hamlet of Sandhill, On.
2. Help to create an attractive, successful, and economically viable industrial and commercial development. Furthermore, the design will provide for future Master Planning within the Sandhill Industrial Commercial Centre expansion. The efficient use of this prominent land parcel will create an increased range of employment uses.
3. Conform with the general intent of the Region of Peel and Town of Caledon policy for new developments, the Industrial Commercial Design Guidelines (ICDG), and the Comprehensive Town-Wide Design Guidelines (2017)
4. Implement a vision of success for places of employment and create a model of the developer's provincial marketing concept for the project – Art + Work;
5. Create safe and usable community spaces, and attractive focal areas within the development for art-exhibits, seasonal celebrations, crafts and industrial arts;
6. Preserve, protect, and exemplify the heritage house (13940 Airport Road), within the guidelines of Caledon heritage Act and provide community spaces around the house, in order to enhance the northwest corner of the site. Independent use within a greater community use.

This Urban design brief should allow the Town planning and officials to better evaluate the design philosophy of the project.

RGC APPROACH

In response to this design vision, RGC on behalf of the Client has prepared this UDB to illustrate how the proposed development has met the goals as set out in the Town's Official Plan and criteria of the Urban Design Guidelines.

Should you have any questions or wish to discuss the brief in further detail, please do not hesitate to contact us.

1.2 CONSULTING TEAM

RGC has retained the following consultants for reports and studies that should substantiate the purpose of this application:

Client:

Airport King Inc., Airport Caledon Inc.
4 Abacus Road Unit 3, Brampton, ON L6T 5J6

Planners and Architectural Design

RG Consulting Inc. - RGC Design Group
Contact: Ralph Grittani - ralph@rgcdesigngroup.com

Archaeological Assessment

Detritus Consulting Ltd.
Contact: Garth Grimes - garth@golden.net

Environmental Site Assessment

HLV2K Engineering Ltd.
Contact: Kimberly Dubarry - kimberly.dubarry@hlv2k.com

Geotechnical and Hydrogeological

HLV2K Engineering Ltd.
Contact: Kourosh Mohammadi - kourosh.mohammadi@hlv2k.com

Site Survey

Mandarin Surveyors Limited
Contact: Steven Zeng - mandarinsurveyor@gmail.com

Noise Study

Trans-Plan Transportation Inc.
Contact: Anil Seegobin - anil.seegobin@trans-plan.com

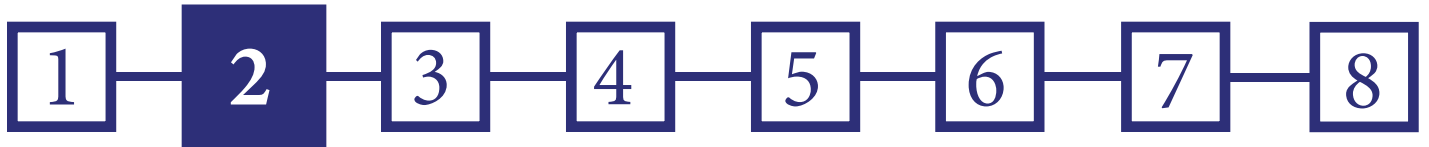
Transportation Impact Study

Trans-Plan Transportation Inc.
Contact: Anil Seegobin - anil.seegobin@trans-plan.com

Landscape Design

STUDIO tla
Contact: Paul Marsala - pmarsala@studiotla.ca

CONTEXT ANALYSIS



2.1 SUBJECT SITE

The subject site is located on the easterly sections of 13846 and 13940 Airport Road in the Hamlet of Sandhill, Town of Caledon, legally known as Part of Lot 27, Concession 6, EHS, Former Township of Chinguacousy, County of Peel now in the Town of Caledon, Regional Municipality of Peel. The portions of these properties that will be developed are fully contained within the Settlement Boundary Area of Sandhill.

13846 Airport Road, the southern portion of the site, previously had a farmhouse located on the property, which, through aerial photography, appears to have been removed between 2006-2009. Salt Creek meanders through the westerly portion of the property. Currently, the property is used for agricultural purposes.

13940 Airport Road, the northern portion of the site, contains a late 1800s farmhouse with a number of out buildings. Also present on the property are two buildings that appear to have an industrial nature. Salt Creek meanders through the westerly portion of the property. Currently, the property is predominantly used for agricultural purposes.

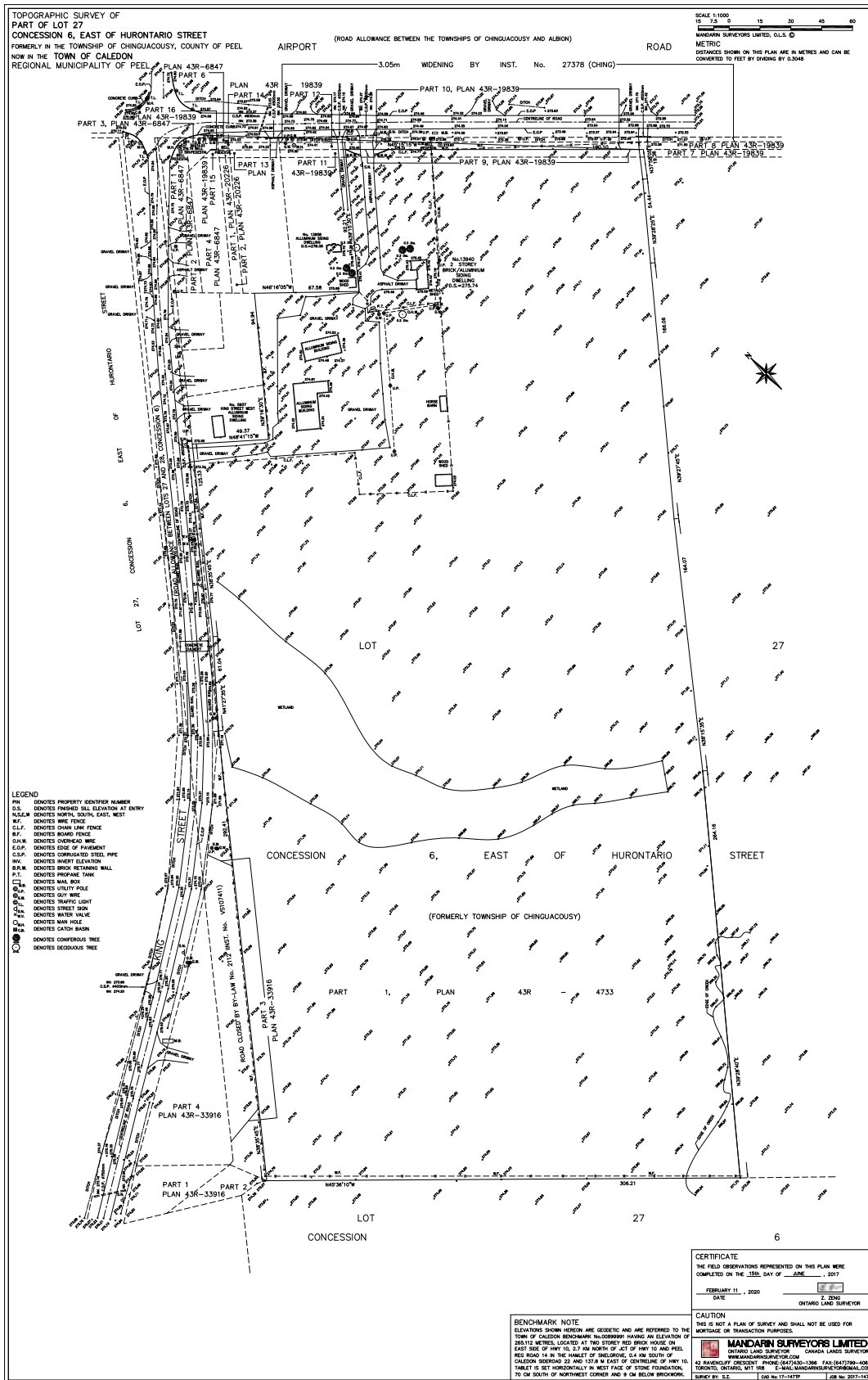


Aerial Image of Sandhill showing subject site.

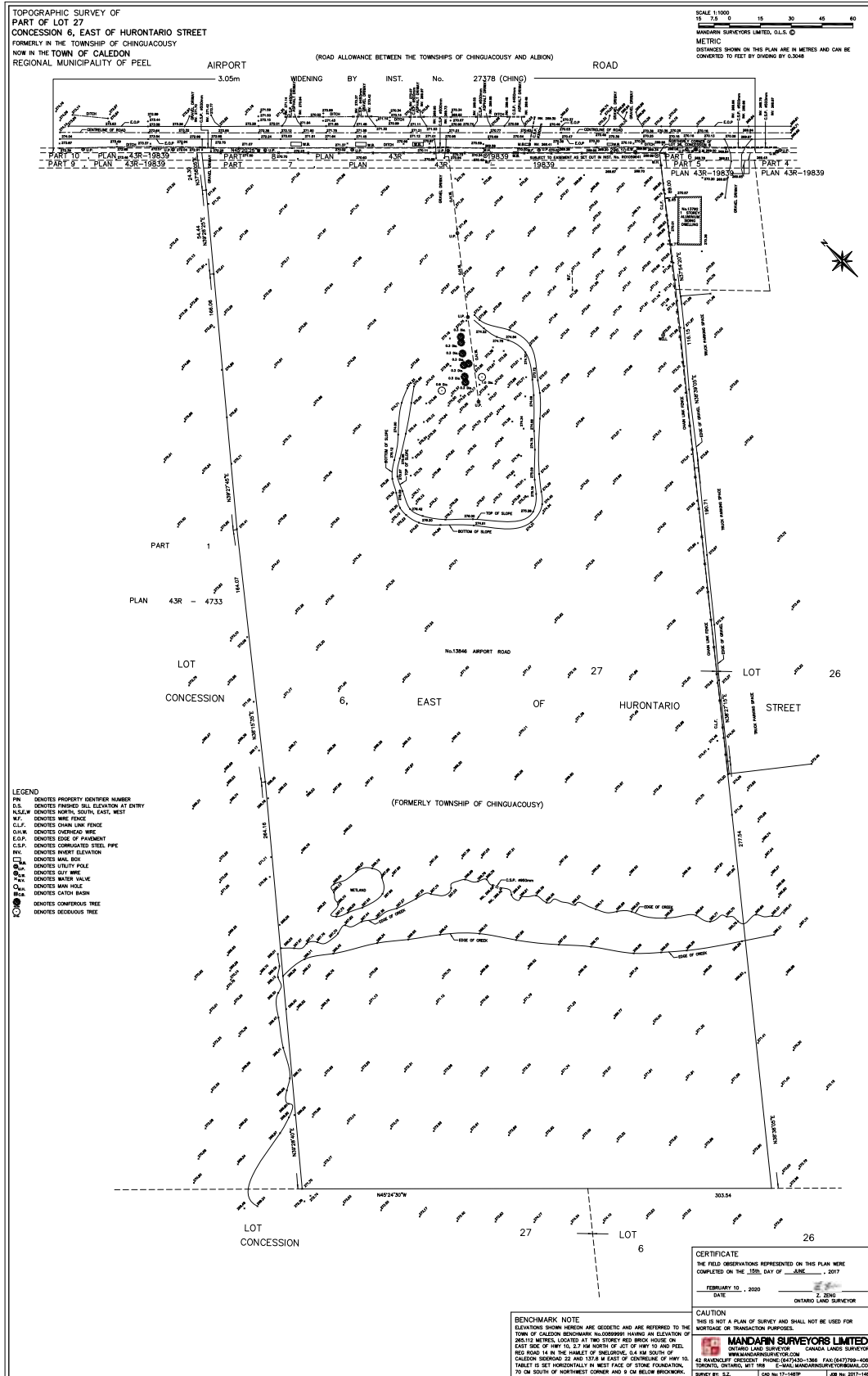
1 Surrounding Context key Section 2.3

The subject site is approximately 673.4 metres deep, with a frontage of approximately 486 metres along Airport Road. The proposed development of the site is to occur within the Settlement Area of Sandhill, with an approximate depth of 97.34 metres. This provides a development area of approximately 4.23 hectares or 10.44 acres. A buffer area of approximately 0.4 hectares or 0.96 acres along the Airport Road frontage is designated on the Site Plan for future Region of Peel widenings and improvements along the Airport Road corridor. Topographical Surveys of 13846 Airport Road and 13940 Airport Road are provided on the following pages showing a gentle slope in the developable area, with the northern portion having an elevation approximately 5m greater than that of the southern portions. Full size surveys of both properties will be submitted as part of the application package.

2.2SITESURVEYS



Survey of 13940 Airport Road



2.3 SURROUNDING CONTEXT



1

To the north of the subject site are existing residential properties with a small auto market located on the south-west corner of Airport Road and King Street. The other corners are mostly vacant, other than a vacant garage on the north-east corner.



2

To the south of the subject site lies a truck haulage company. This use borders the entire southern portion of the subject site. Beyond this use are agricultural lands with associated barns, farm houses, and other agricultural structures.



3

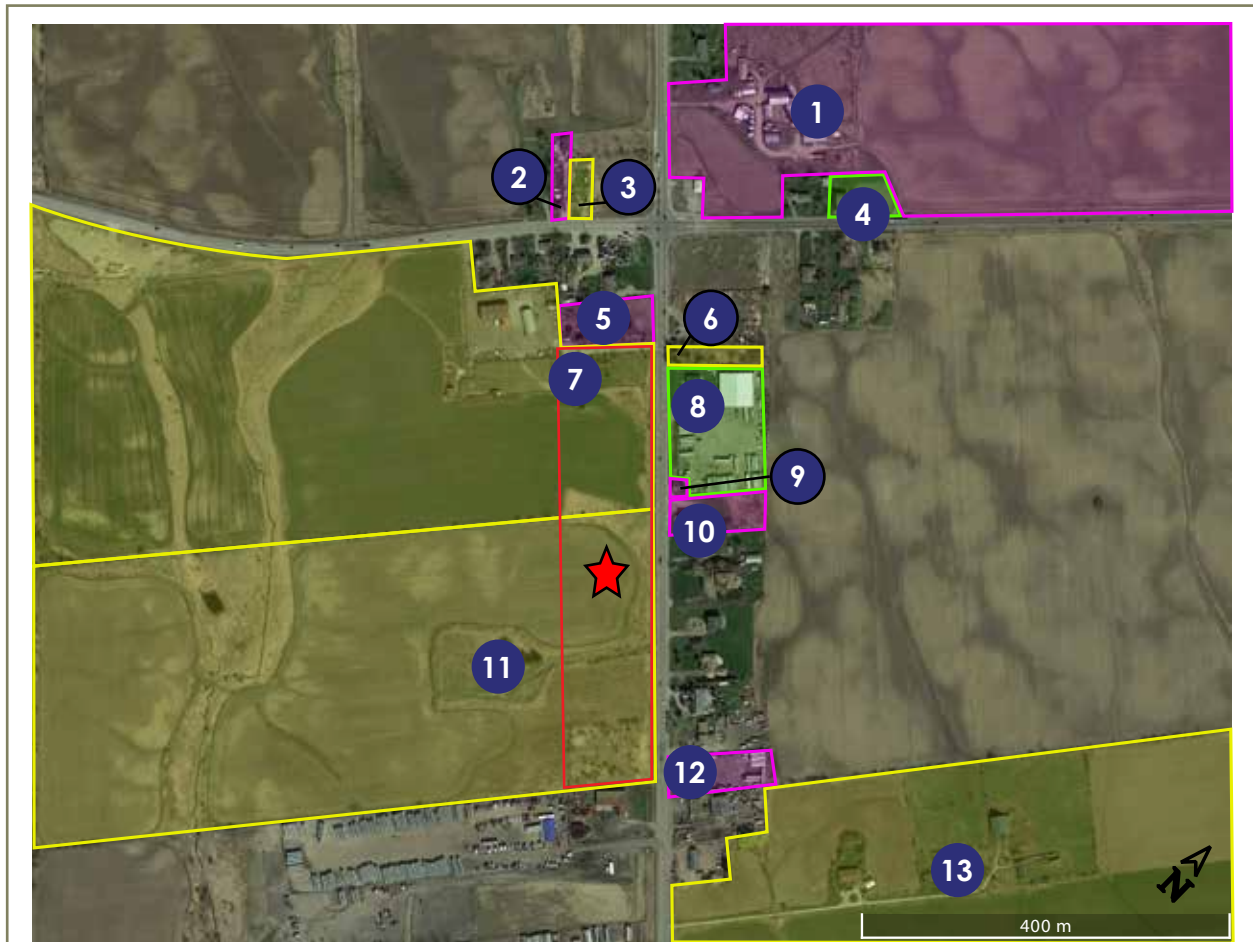
To the west of the subject site, including the portions of 13846 and 13940 Airport Road which are not being developed at this time, are agricultural uses with the associated barns, farm houses, and other agricultural structures. Salt Creek also meanders through this area.



4

To the east of the subject site are a number of residential uses, a logistics company, a truck parking lot, Sandhill United Church, and the Sandhill Pipes and Drums, housed in the last Orange Hall of the Town. Beyond these uses which line Airport Road in the Sandhill Settlement Area lie agricultural lands.

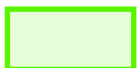
2.4 CULTURAL HERITAGE CONTEXT



Cultural Heritage Context Map



Subject Sites



Designated under Part IV of the Ontario Heritage Act



Listed under Section 27 of the Ontario Heritage Act
















Identified on Caledon Built Heritage Resource Inventory



Heritage Property Image Key (following page)



	14045 Airport Road Gothic Revival Listed		13941 Airport Road Regency Masters House Designated
	5964 King Street Log House Listed		13899 Airport Road Frame Construction Orange Hall Listed
	5968 King Street Identified		13889 Airport Road Gothic Sandhill United Church Listed
	6060 King Street Gothic elements St. Mark's Anglican Church Designated		13846 Airport Road Demolished Neoclassical farmhouse Identified
	13958 Airport Road Neoclassical Listed		13803 Airport Road Neoclassical Listed
	13949 Airport Road Neoclassical Listed		13755 Airport Road Identified
	13940 Airport Road Identified		

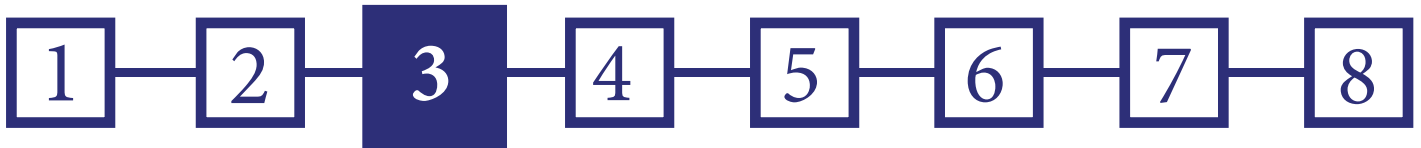
Sandhill is located on the boarder of the historic townships of Chinguacousy and Albion, at the intersection of the now named King Street and Airport Road, with Airport Road being the boarder between the two Townships. The subject site is located on the western side of Airport Road, in the Township of Chinguacousy. The area was originally surveyed in 1818-1819 on land purchased from the Mississaugas. Settlers, including the children of Loyalists, War of 1812 veterans, and immigrants from England, Ireland, and Scotland, started to move into the area that had been surveyed the same year. The good quality farmland of the area supported a strong agricultural base, with settlements growing up around sources of water power for milling and intersections of major roads, the latter being the case of Sandhill. Originally named Newton Hewitt after its earliest settler, John Hewitt, Sandhill was renamed to its present name in 1844 with the arrival of the post office. The settlement had a number of amenities to serve the local population, including hotels, churches, stores, a school, and a variety of services. The population of the community reached approximately 200 in 1873, and also serviced the surrounding rural population. It has since declined down to a population of approximately 55, but is still designated by the Town Official Plan as an Industrial/ Commercial Centre to service the surrounding community. It also retains a number of heritage assets, eluding to its historic purpose of a service centre for the local community.



The heritage home located on the subject site (13940 Airport Road) demonstrates typical architectural styles and massing found in Sandhill.

Typical characteristics of historic development in Sandhill are small scale, 1.5 storey houses, primarily in the neoclassical style, set relatively close to the Airport Road and King Street corridors. There are also a number of farms within and around Sandhill set further back from Airport Road and King Street, but of a similar scale to the other dwellings. The two churches in Sandhill are of a larger massing, in the Gothic style, but remaining in close proximity to the road edges. Key elements to compliment in the hamlet are the gable style roofs prominent in the hamlet (noticeable exception being the designated Masters House with a hip style roof), the small setback development form the road corridors, and the relatively low massing of development.

POLICY CONTEXT



3.1 TOWN OF CALEDON OFFICIAL PLAN

The subject site is located within the Settlement Boundary Area of Sandhill, in the Town of Caledon. The Town of Caledon Official Plan designates Sandhill has an Industrial/Commercial Centre on “Schedule A1: Town Structure” with the subject sites further being designated as Highway Commercial uses under “Schedule T: Sandhill Land Use Plan.

5.4.9 General Design Policies

5.4.9.1 All commercial development shall be subject to the following General Design Policies to ensure that the development will have a high standard of site and building architectural design, appropriate buffering from possible adjacent residential uses, a safe and efficient design for internal and external vehicular/pedestrian circulation, and an adequate supply of parking/loading facilities on-site including:

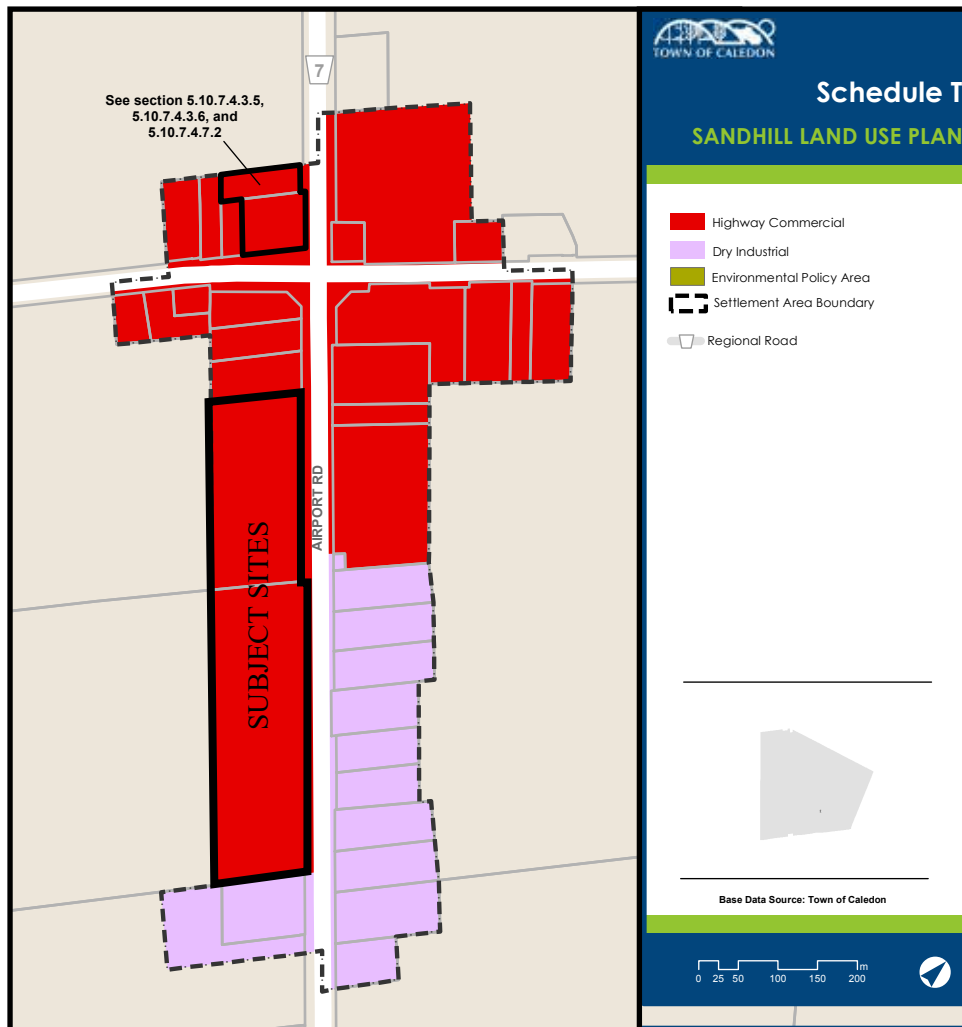
- a) The architectural design and development of commercial districts shall encourage a variety of massings of commercial buildings, which shall be compatible with the scale of the surrounding community and shall discourage linear commercial strip development along roadways in commercial areas;*
- b) The development shall have, wherever possible, consolidated access/egress on the site and with adjacent development(s), and also an integrated parking and vehicular/pedestrian circulation. Vehicular access and egress points to and from commercial parking areas shall be limited in number, as specified in the implementing Zoning By-law, and shall be designed to minimize danger to pedestrian and vehicular traffic, and conflict with adjacent uses;*
- c) A high standard of landscape and streetscape features shall be provided for all commercial uses; and,*
- d) Adequate off-street parking and loading spaces are to be provided for all commercial uses.*

5.10.7 Industrial/Commercial Centres

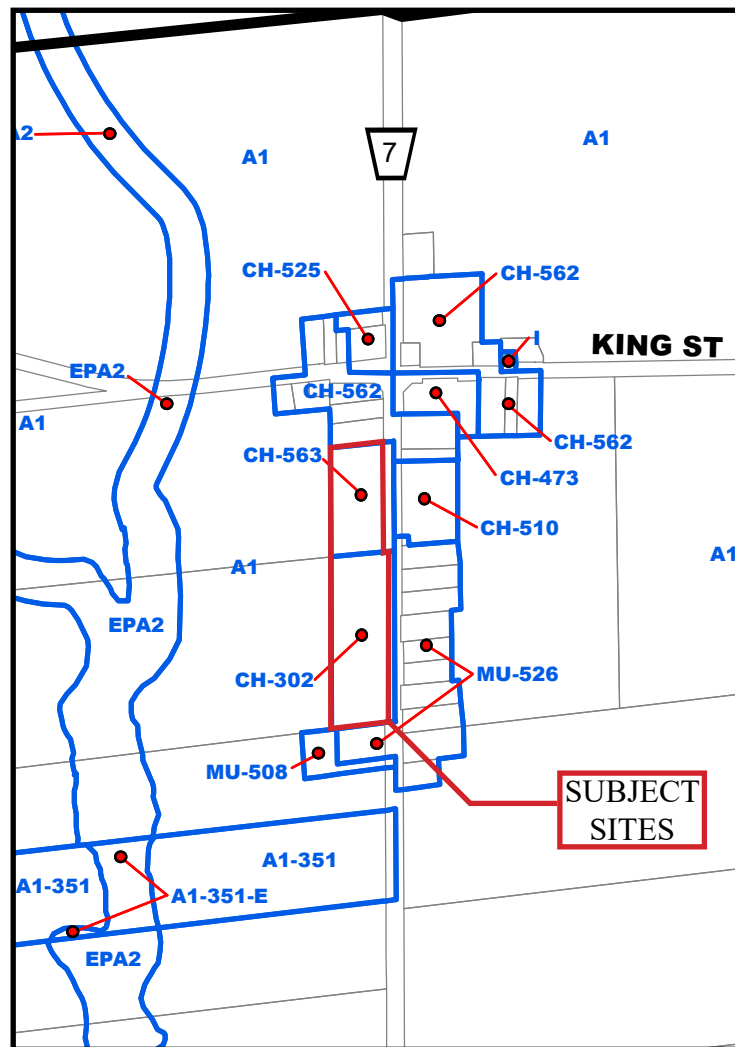
5.10.7.4.6.3 In order to address visual impacts, aesthetics, and compatibility of uses, development adjacent to Airport Road and King Street shall, in addition to conforming to the General Design policies of Sections 5.4.9 and 5.5.7 and the Industrial/Commercial Design Guidelines referenced in Section 5.10.7.4.6.2 be guided by the following general site design principles:

- Sensitive residential land uses must be buffered from the loading, service, and parking areas associated with the Dry Industrial and Highway Commercial uses;*
- Transport truck and transport trailer parking and storage, waste enclosures, overhead doors, accessory buildings, and loading/unloading bays shall be oriented away from Airport Road and King Street, unless fully screened by a solid wall or a building;*
- High quality building facades are encouraged and building elevation drawings may be required as part of the development approvals process. This shall include screening/enclosure of rooftop mechanical units, and limiting the height of buildings;*
- Signage shall be coordinated and integrated into the landscaped areas, specific restrictions may be placed on signage in order to reduce visual impacts;*
- Larger minimum lot sizes and wider frontages shall be required.*

The proposed development is consistent with the Design policies outlined in the Town of Caledon Official Plan sections 5.4.9 and 5.10.7. Note that Section 5.5.7 of the Town of Caledon Official Plan refers to General Industrial Design Policies, more fitting for the dry industrial uses outlined in Schedule T as opposed to the proposed highway commercial uses. An in depth look at how the development follows the outlined policies follows in Section 4 - Site Plan Design, Section 5 - Architectural Design, and Section 6 - Landscape Design.



3.2 ZONING BY-LAW



Section 13 Zone Map Town of Caledon Zoning By-law

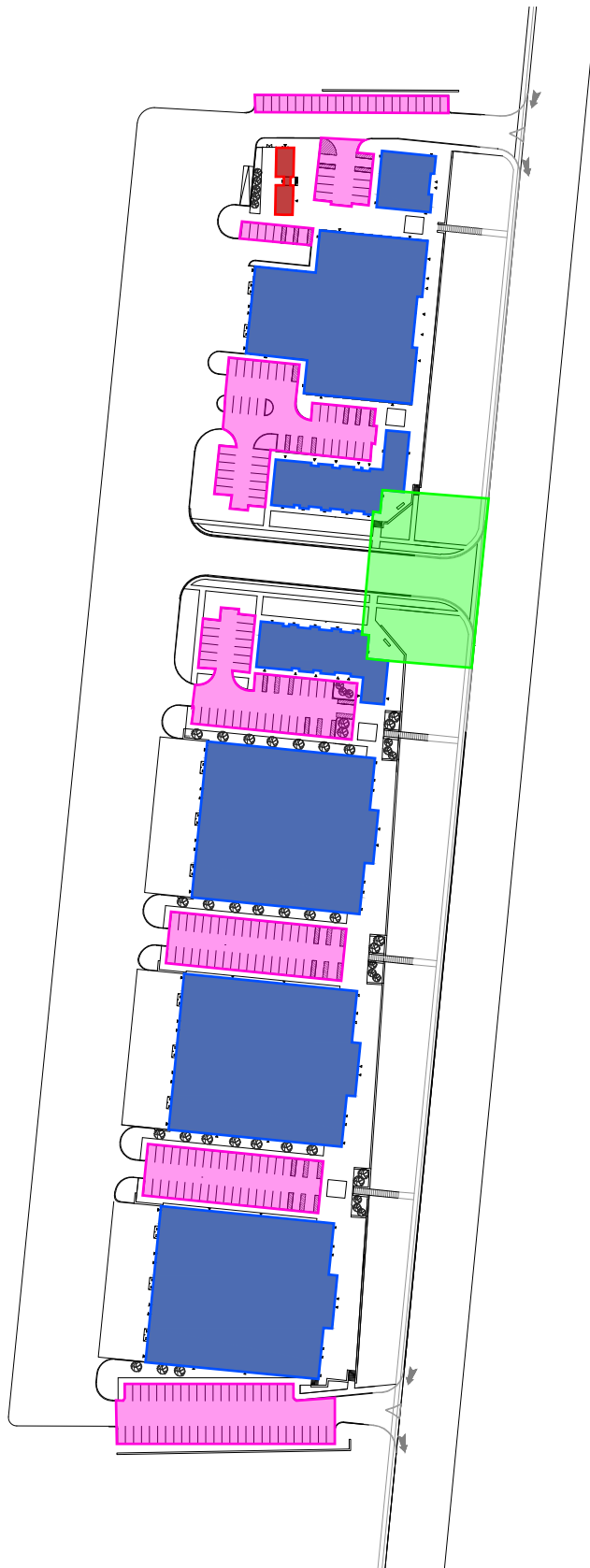
The Town approved Zoning By-law amendments to permit a series of commercial uses along the Airport Road frontage within our lands. The northern development parcel was zoned as Highway Commercial - CH-563 while the southern development parcel was zoned as Highway Commercial - CH-302. The balance of the lands are zoned as the Agricultural - A1 zone and Environmental Protection Area - EPA2 zone. No development is proposed on these lands at this time.

Refer to By-law 2006-50 for the full permitted uses and zone requirements for these lands.

SITE PLAN DESIGN



4.1 SITE PLANNING



f

Reuse of existing heritage resource identified by the Town of Caledon to provide an indoor/outdoor amenity space, as well as a plaque outlining the significance of the property.

m

Snow storage is to be located in the rear portions of the site, in the “future access” areas. Snow storage will be accommodated for in future development requiring these accesses.

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b

j

k

l

Emphasis is put on the main access of the site through the built form of the buildings, addressing both Airport Road and the primary access, as well as primary site signage at this location. Significant pedestrian accesses are also located in this area, with overhangs from the buildings and landscaping providing a comfortable pedestrian realm.

a

d

e

h

i

k

l

n

Commercial buildings are aligned to the street edge, creating a visually appealing streetscape and pedestrian realm. Main entrances are oriented towards Airport Road and accessible at grade to activate the streetscape. Art pods are located between commercial buildings for pedestrian access and usage as outdoor amenity space.

g

Parking areas are located behind and to the sides of the commercial/industrial frontage, with landscaping screening views from Airport Road. Loading areas are also located to the rear of both commercial and industrial uses.

	Key	TWDG Policy	Policy Met?
Design Standards	a	Locate buildings close to the street edge, orienting the building massing and height to architecturally address the street and key intersections.	✓
	b	Provide enhanced architectural features at corners closest to major intersections.	✓
	c	Promote seamless transitions between different densities and heights; where relevant, use stepbacks to minimize the impact of larger buildings on adjacent low-rise residential neighbourhoods	✓
	d	Support commercial activities at grade along main streets to activate the streetscape, providing vibrant storefronts along the public realm to add interest and promote pedestrian activity	✓
	e	All major commercial entrances shall be accessible at grade thresholds.	✓
	f	Integrate cultural and natural heritage assets through sensitive site planning and opportunities for adaptive reuse	✓
Design Requirements	g	Locate parking, servicing, and loading facilities to the rear of the building, screening them through the use of architectural elements and landscaping from public view.	✓
	h	Front commercial development onto the public realm to activate the streetscape, courtyard, or open space. A secondary entrance may be provided from surface parking at the rear of the building.	✓
	i	Building frontages should occupy at least 50% of the street frontage. Building frontages should be increased to at least 70% in more compact areas.	✓
	j	Commercial Buildings should address the street and exhibit a high degree of visual appeal on all exposed frontages. At corner lot locations, corner specific details such as corner entrances or corner glazing should be incorporated.	✓
	k	Accentuate all public entries by integrating intuitive signage, effective architectural features, and hard and soft landscaping elements.	✓
	l	Clearly articulate building entrances through signage and pavement markings, making visible pedestrian walkway connections to the street and designated vehicular drop-off areas. Provide weather protection at all public entry locations	✓
	m	Provide an adequate room for snow storage.	✓
Encouraged Practices	n	Building entrances are encouraged to open onto an exterior area suitable for gathering and waiting.	✓
	o	The use of LID techniques in commercial site planning is encouraged.	✓

4.2 BUILDING ELEVATIONS



a c e f h j k

Building frontages have articulation along the frontage to break continuity in the form, with gable roofed structures at the entrances, pulling in the character of the existing built form in Sandhill. These frontages are primarily windowed, providing clear sight lines into and out of the units. Additionally, overhangs are present along a number of frontages to provide shelter and interest in form.

b h i k

Roof lines will have variability to them, with gabled portions around the entrances, and the remainder as a flat roof and parapet to screen mechanical units. Articulations on the facade will further break the continuity of the facade. Buildings adjacent to the primary entrance of the development are shaped to address both street frontages and have greater diversity in architectural form to create greater interest at this significant location on the site.

a b j k l

Retaining walls along the Airport Road frontage of the site will have bump outs in the areas of the art pods, with different materials used in these bump outs, breaking up monotony of the wall. Walkways will run along the top and bottom of the retaining wall.

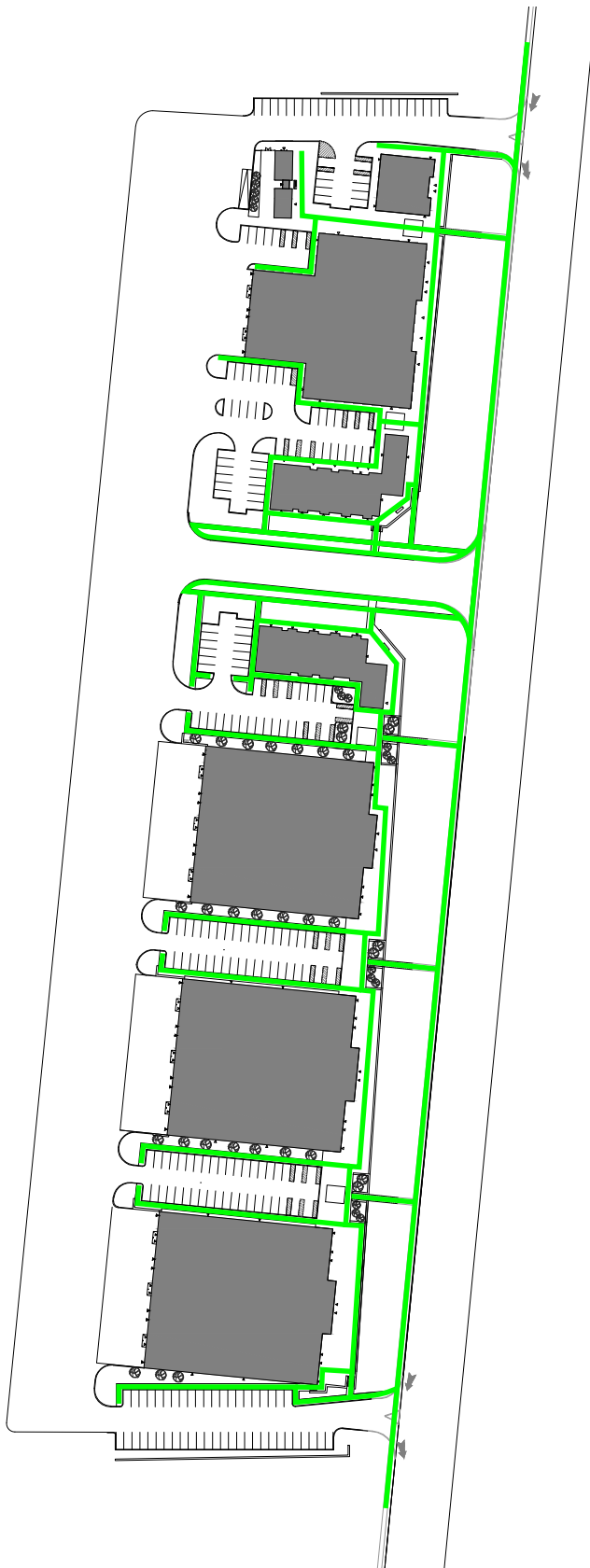
LANDSCAPING??

d l m

Materials have been chosen to compliment each other, as well as the existing context of built form in Sandhill and the heritage home present on the site. Materials will vary from building to building to prevent monotony in colour and texture, but will compliment each other. Green hued glass with bird-friendly design will be consistent across the development, uniting the buildings.

	Key	TWDG Policy	Policy Met?
Design Standards	a	Building elevations shall provide visual interest through design, articulation, and fenestration. Large unarticulated wall surfaces are not permitted.	✓
	b	Integrate and screen all rooftop mechanical units by setting them back from the building edge and incorporation parapet walls. Mechanical units shall not be visible from the public street line.	✓
	c	Vents and exhaust elements will be incorporated into the design of the facades, so as not to adversely impact the aesthetic of the public realm.	✓
Design Requirements	d	All design elevations should be clad with the same prominent materials. Where relevant, there should be purposeful termination of building materials.	✓
	e	Elevations should be pedestrian friendly, providing appropriate setbacks and human scaled articulation, detailing and fenestration.	✓
	f	Elevations should use awnings, canopies, and other overhangs to create sheltered pedestrian routes and to add depth to the appearance of facades.	✓
	g	Elevations should not be designed to appear as front facades when they are not.	✓
	h	Where the commercial site is proposed with more than one building, the collective architectural composition of the buildings should be considered in terms of massing, roof lines, street relationship, and visual impact on adjacent buildings. Stand alone buildings should be allocated additional height and massing at intersection and focal points.	✓
	i	Long continuous roofscapes should be divided and varied to provide visual interest and variety. Rooflines and parapets should be designed to facilitate the integration and screening of all roof top mechanical units.	✓
	j	Increased fenestration should be provided along the street. Spandrel glass is subject to design merit and may only be located in limited areas.	✓
Encouraged Practices	k	Elevations may contain changes in plane and relief to break up long, continuous stretches.	✓
	l	The architectural elements, colour, and material treatment of individual buildings is encouraged to be compatible with adjacent buildings and the overall streetscape.	✓
	m	Incorporate architectural elements, such as visual markers or muted reflections, into the design of the facades for bird-friendly elevations.	✓

4.3 PEDESTRIAN CIRCULATION



a b d f

Multiple pedestrian only accesses are present through the “art pods” into the site, in addition to the typical sidewalks along primary site accesses. These landscaped art pods create a clear and attractive entrance for pedestrians apart from the vehicular access routes.

d g i

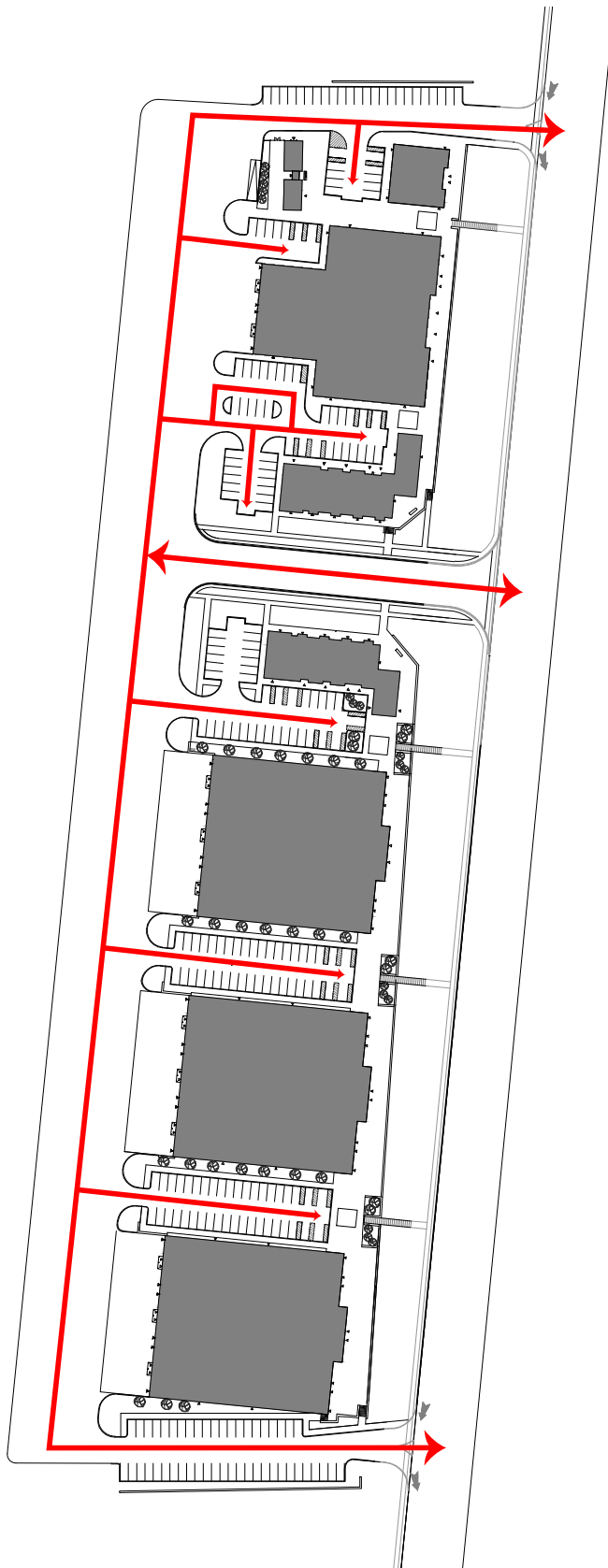
Street fronting uses have an appropriate setback to accommodate landscaping and pedestrian access, lending themselves to an attractive and activated pedestrian realm. These sidewalks currently lack access to a broader network due to the lack of sidewalks in Sandhill, but will provide a precedent for future development in the area.

c j

The art pods located along primary pedestrian accesses are focal points of the development, providing amenities to pedestrians such as seating areas, landscaping, public art of local artists, and trash receptacles.

	Key	TWDG Policy	Policy Met?
Design Standards	a	Pedestrian walkways shall be designed to ensure a safe, comfortable, and attractive environment for walking, accommodating movement from and through parking areas to building entrances. Walkways should be designed in concert with parking areas and drive aisles for pedestrian safety, and shall comply with the Accessibility of Ontarians with Disabilities Act.	✓
	b	Major pedestrian access points and routes shall be clearly marked and identified using pavement markings and signage, ground oriented and upright hard and soft elements.	✓
	c	An enhanced pedestrian realm is required along the street and at focal point throughout the development to accommodate multiple users. Enhanced pedestrian areas should be designed to facilitate meeting and gathering by incorporating street furniture, seating areas, displays, trash receptacles, public art, and landscaping.	✓
	d	Entrances used by the public must be fully accessible and should not take their access from steps or other condition which would create a barrier to accessibility.	✓
Design Requirements	e	On-site pedestrian walkways should have direct and easy connections to the streets and sidewalks of adjacent neighbourhoods wherever possible.	
	f	Pedestrian connections should be designed to accommodate high volumes of unencumbered movement at peak times, minimizing the incidence of short-cutting across drive aisles and landscaped area. Incorporate sidewalks on both sides of the street, with a minimum width of 2 metres.	✓
	g	Commercial building fronts that abut the street should have hard surface paving along their frontages, and should orient their main entrances towards the public realm.	✓
	h	Pedestrian connections should facilitate access to existing and planned transit stops.	
	i	Provide a greater building setback along major arterials to accommodate landscaping and a pedestrian sidewalk, and orient major public entrances to the flankage elevation, where possible.	✓
Encouraged Practices	j	Provide street furniture, such as benches, waste receptacles, lighting, and bicycle parking, on all major pedestrian routes and in areas with wider walkways.	✓

4.4 VEHICULAR ACCESS, PARKING & SERVICING



b f g h i m

Parking areas are primarily positioned between and behind the proposed buildings, with a landscape screen along Airport Road. Large areas of parking have been avoided, with access provided by walkways along the length of parking areas. Bicycle parking will be located close to building entrances.

e p

Three vehicular access points are located off of Airport Road, marked by landscaping and signage. The primary, full access entrance is also clearly marked through architectural emphasis in the built form. The remaining two accesses are right-in, right-out, with less emphasis placed on them. Interior circulation is emphasized through usage of built form, signage, and landscaping features.

c j k l o

Loading areas are set behind the commercial and industrial uses of the site, screened from view by the built form. Landscaping along neighbouring lots and the setback of loading from lot lines provides a buffer to neighbouring uses. Utilities and waste storage are contained internally.

	Key	TWDG Policy	Policy Met?
Design Standards	a	Lighting for buildings and parking will be designed and sited to minimize light spillage and distribution onto adjacent residential properties by incorporating dark sky compliant lighting.	✓
	b	Avoid surface parking areas between the building and the street. Where permitted, sufficiently screen from public view through a coordinated combination of berms, fences, and landscaping.	✓
	c	Provide a sufficient separation distance between residential uses and commercial waste and loading areas to avoid adverse impacts.	✓
Design Requirements	d	Vehicular and service access points should be provided from adjacent side street, away from major streets, wherever possible.	
	e	Vehicular access points and routes should be clearly identified using both ground oriented and upright hard and soft elements.	✓
	f	Large parking areas should be broken up with landscaped parking islands with a minimum width of 3 metres to sustain adequate tree growth and retention.	✓
	g	Parking islands should be curbed, landscaped, and located at the ends of all rows of parking stalls. Parking islands should include walkways where required to support a system of pedestrian routes.	✓
	h	Parking areas should be screened from view using plantings and high quality fencing.	✓
	i	All parking areas should be paved in hard surface material, and should be able to accommodate snow removal and storage.	✓
	j	Loading and servicing areas should be screened from public view through placement of buildings, screen walls, and landscaping.	✓
	k	Planting and fencing should be used to buffer residential lots from service areas, waste storage, and loading service areas. These elements should be integrated into the building envelope, where possible, and should be screened from view.	✓
	l	Utility structures should be integrated into the design of commercial buildings wherever feasible. Where not feasible, these structures should be screened from view from surrounding areas by landscaping, screen walls, and/or other architectural features, in consultation with utility providers to ensure operational access is maintained.	✓
	m	Bicycle storage racks should be provided adjacent to main building entrances.	✓
Encouraged Practices	n	On-street parking is encouraged, where possible, to support commercial uses.	
	o	Internal waste areas are encouraged.	✓
	p	Where possible, provide clearly marked circulation routes.	✓

ARCHITECTURAL DESIGN



5.1 ACCENTUATING HERITAGE CHARACTER



The existing farm home located at 13940 Airport Road will be a primary feature of the site, inspiring the built form and materials of the proposed development. The purpose of the proposal is to link the character and history of Sandhill to the present through the use of art and architecture, and so draws on the characteristics of the heritage properties in Sandhill. It is the vision of RGC that, when these two worlds of old and new come together, the result can be awe-inspiring.

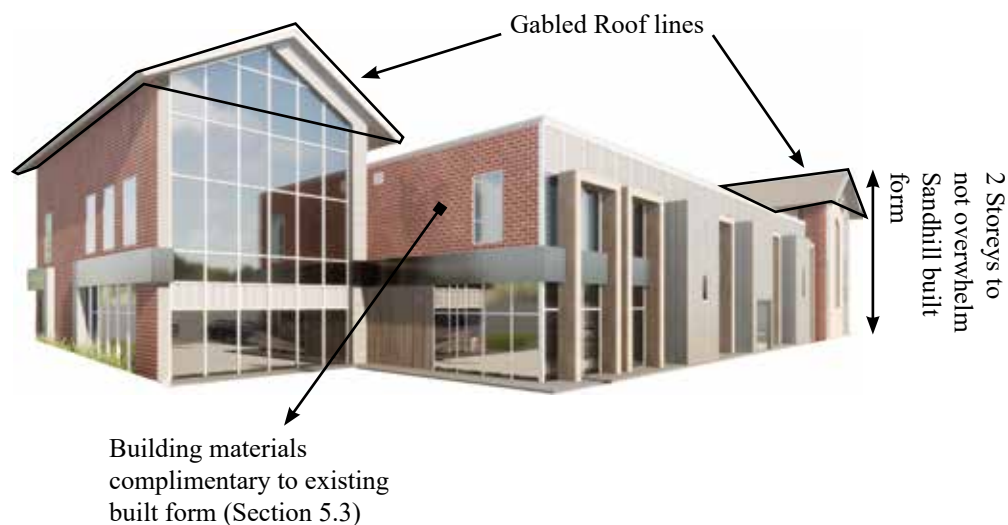
Thus far the heritage home was partially collapsed due to a house fire. Rather than demolishing the home, both RGC and the Town agreed to protect the main body of the house and secure it from collapse. The modern rear addition and attached garages were not regarded as adding to the heritage value of the home, and were removed in a professional manner. The demolition was completed in May, 2020. The home is now better protected from the elements and vandalism through measures proposed by Town staff. The developer will co-operate with the Town of Caledon officials, Heritage boards, under the supervision of RGC, to restore the home with a new proposed interior use as it relates to the attached new addition. Two buildings of different vintages will provide one new purpose and function. The heritage property will be juxtaposed with a modern community space, including harmonious indoor and outdoor public spaces. Modern construction materials shall compliment and emphasize the materials of the original home in a continuous extension of its former use. Landscape and pedestrian amenities will also be used in the area of the heritage home to create a comfortable, outdoor amenity space for public use. RGC and the Town of Caledon Heritage officials, propose to install a heritage plaque sharing the history of Sandhill, as it relates to the significance of 13940 Airport Road.

The architectural features drawn from the heritage home shall be shadowed across the new rooflines for proposed buildings within the new site development. Gabled roof lines are present and used in a deliberate commercial context of mixed use commercial and industrial buildings for Phase 1 of the master plan commercial buildings.

The building exterior designs are also limited to two storeys in overall height so as to not overwhelm the heritage building on site. This will provide an interesting back drop and new architectural horizon for the rest of Sandhill. The backdrop of the new building located south of the heritage home will provide a unique opportunity for the Urban design element. RGC together with the developer have agreed to use the north wall of the proposed building to create a white canvas for an artist's impression of background view of the historic agricultural landscape. This wall art will frame the new public interest buildings in a more intimate multi-dimensional scene. This work will also support RGC's Urban design concept for the subdivision of "Art + Work" and, the corporate image for the expanded master development plan.

Finally, natural colours and materials are used throughout the site. Brick and limestone will continue to be tastefully included into the heritage plan, in addition to the tinted green glass on the majority of surfaces, producing a modern elevation that is transparent with the outdoor-scape to the west. The existing heritage home and proposed sister addition will be visible from Airport road although setback against the western limit of the Phase 1 development. Once again, creating a point of interest for the public realm and business visitors to the site.

Through the months of April and May 2020, RGC met on site with Planning And heritage municipal officials to discuss our proposed site plan and potential impact on the heritage home. RGC presented initial ideas and renderings which led to the current submission included within this Urban Design Brief.



Late 1800s, Home
Constructed



2020, Current State



2022, Proposed
Adaptive Reuse to
Existing Heritage
Home at 13940
Airport Road



5.2 COMMERCIAL SIGNAGE AND OUTDOOR DISPLAYS



a e j

Commercial signage will be integrated into the front face of buildings fronting onto Airport Road. Lighting will be used where appropriate to accentuate signage, while being conscious to not have light spill into unintended areas. Wayfinding signage will also be present at key pedestrian areas to help guide pedestrians throughout the site.



b c d h i

Ground related signage will be located along the main vehicular entrances to the site, using materials complimentary to those used in the construction of the buildings. Planting will be used to accentuate and incorporate ground signage into the site design.

f g

Commercial display areas and windows will be used to address street frontage and pedestrian routes along Airport Road, bringing the development closer to the street and providing a comfortable and attractive user experience to all forms of traffic moving past the site.



	Key	TWDG Policy	Policy Met?
Design Standards	a	Signage shall be aesthetically pleasing, and shall be unobtrusive to residential neighbours.	✓
Design Requirements	b	Ground-related signage should be integrated into the site plan, landscaping, and contribute to the overall wayfinding strategy of the site.	✓
	c	Ground-related signage located within core areas of existing communities shall be no taller than it is wide, and should be used at key vehicular access points to direct vehicular traffic into the site. Horizontal signage is preferred.	✓
	d	Ground-related commercial signage should be used at key vehicular access points to direct vehicular traffic into the site.	✓
	e	Signage should be compatible in scale and design with design, colour, and material of the building and is designed as an integral element of the building's facade.	✓
	f	Display windows, at-grade glass doors, accent lighting, and business signage should be integrated into the front face of the building along the commercial street edge.	✓
	g	Display areas should be set back 4.5m from the street line and defined (using pavers, etc). These areas should be integrated into the front yard landscape and illustrated on the landscaping plan.	✓
	h	Tall, freestanding pylon signs are discouraged. Freestanding signs should be ground-related with a horizontal form and consist of materials complimentary to the building design. They should be integrated with a landscape treatment.	✓
	i	Accent planting is required for ground signage.	✓
Encouraged Practices	j	Commercial signage that is integrated in the overall design may be provided directly above the storefront glazing.	✓

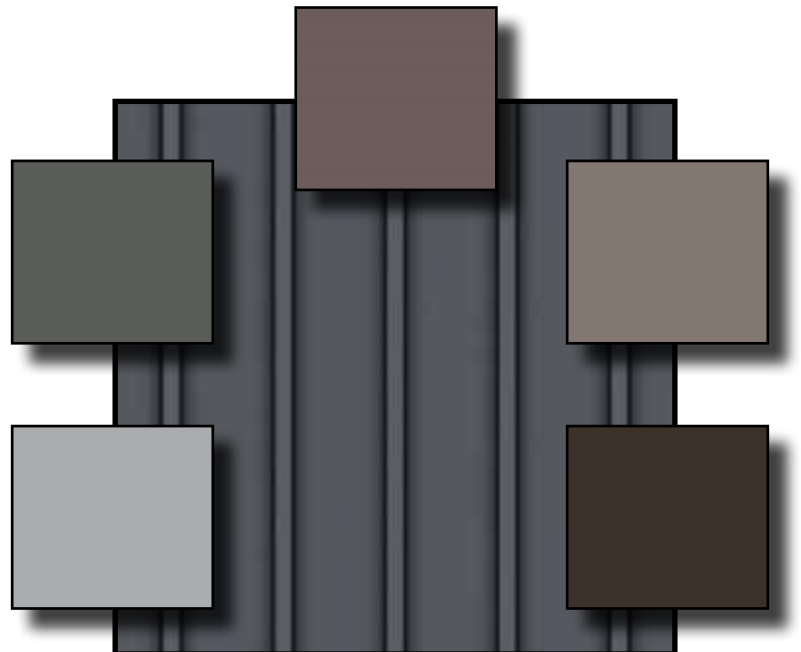
5.3 MATERIALS AND COLOURS

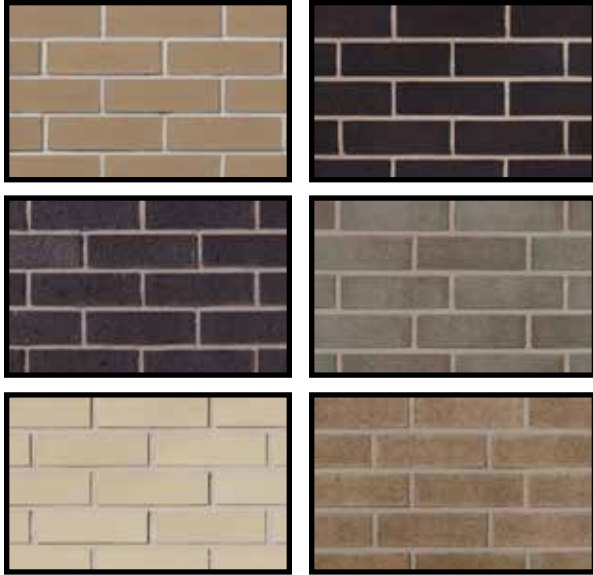
The development will make use of earthy toned materials; greys, blacks, browns, greens, and other neutral tones to create an attractive development that will compliment the existing built form of the sandhill area. These colours will also allow emphasis to be drawn to the proposed landscaped art pods that will be used to display industrial themed art from local artists. Another place of emphasis will be around the existing heritage building, which will be made into a community space with adjacent outdoor amenity space. This area will provide additional landscaping, seating, architectural emphasis, and art space. The project will make use of materials which are sustainable and have long life cycles.



Limestone will be used as a primary building material, bringing a natural material to the proposal to link the modern form to the rest of Sandhill. A variety of colours of limestone will be used across the development to provide some variety in the buildings, while still connecting them through the common building material.

Metal roofing is the proposed roofing material for the development. This roofing will come in a variety of earth tone colours. The roofing material will only be utilized on the gabled portions of the buildings, with the remainder having a flat roof with storm water storage areas built in.





Brick will be used throughout the site, complimenting the existing brick heritage home present on the site. This material will help in uniting the modern development with the history of the site. Similarly to the limestone, a variety of colours of brick will be used across the development to provide diversity, while still uniting the separate buildings in material.

Corten steel planters are to be used throughout the development. The aesthetic of these planters will compliment the industrial character of the rear portion of the site, while providing an aesthetic amenity to pedestrian spaces among the commercial portions of the site, uniting the character of the site.



The curtain wall system of windows will utilize 6” Aluminum mullions with green tinted, high performance glass. The glass will contain UV stripe coating for visibility and safety of birds. This glass will be used throughout the site.

5.4 LIGHTING

	Key	TWDG Policy	Policy Met?
Design Standards	a	Exterior lighting shall be unobtrusive to residential neighbours, minimize light spillage and distribution onto adjacent residential properties.	✓
	b	Lighting shall be dark sky compliant, positioned to minimize glare, and improve visibility, whilst providing an efficient source of light.	✓
Design Requirements	c	Lighting for outdoor areas should be designed and located to ensure safety for users at night, and to facilitate crime prevention.	✓
	d	Lighting should reflect architectural styles of the community in scale and profile. For example, heritage light fixtures may be incorporated within a HCD or village setting.	✓

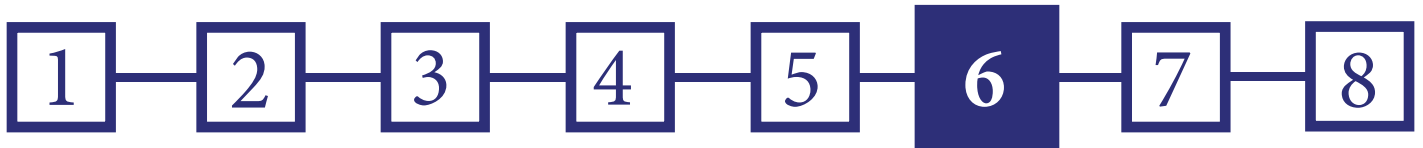


a b c d

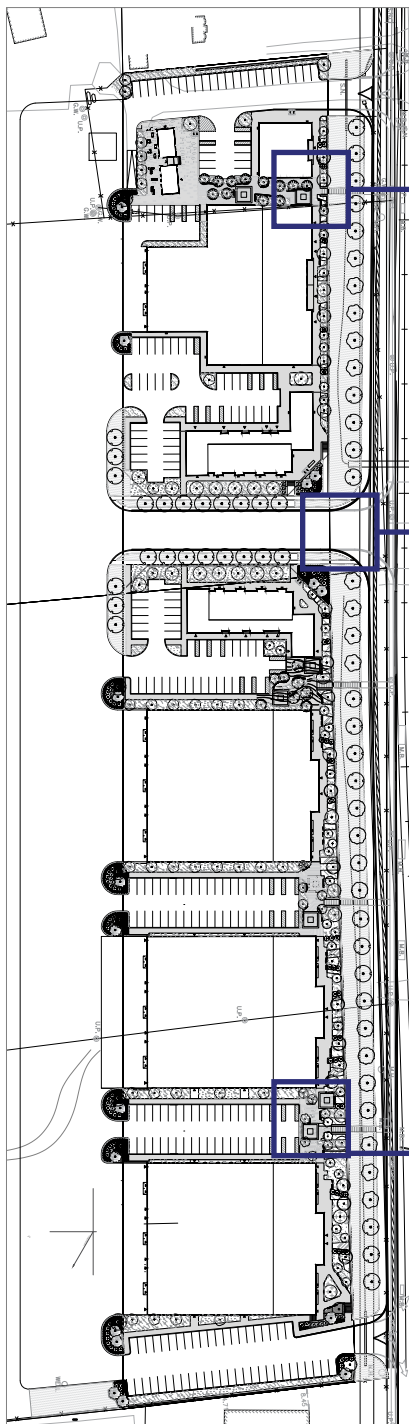
Light fixtures are to be dark sky compliant, while also providing adequate lighting of the property to discourage crime and ensure safety. Lights will be shielded to prevent glare on to neighbouring properties. The fixtures will be selected to enhance the character of the development through materials and style, with special attention around the heritage home to ensure its features are maximized. Pictures demonstrate possible fixture styles.



LANDSCAPE DESIGN



6.1 LANDSCAPE DESIGN



a d e h i

The focus of the development is around the four art pods located along the Airport Road frontage of the site. These art pods are themed around the four seasons, from north to south these are winter, fall, summer, and spring. These planting areas will also contain corten steel planters with brick pavers, tying into the industrial theme of the site. Benches and informal seating will be present, as well as canopies and trees for shade. These will be presented as “outdoor rooms.”

a d e h i

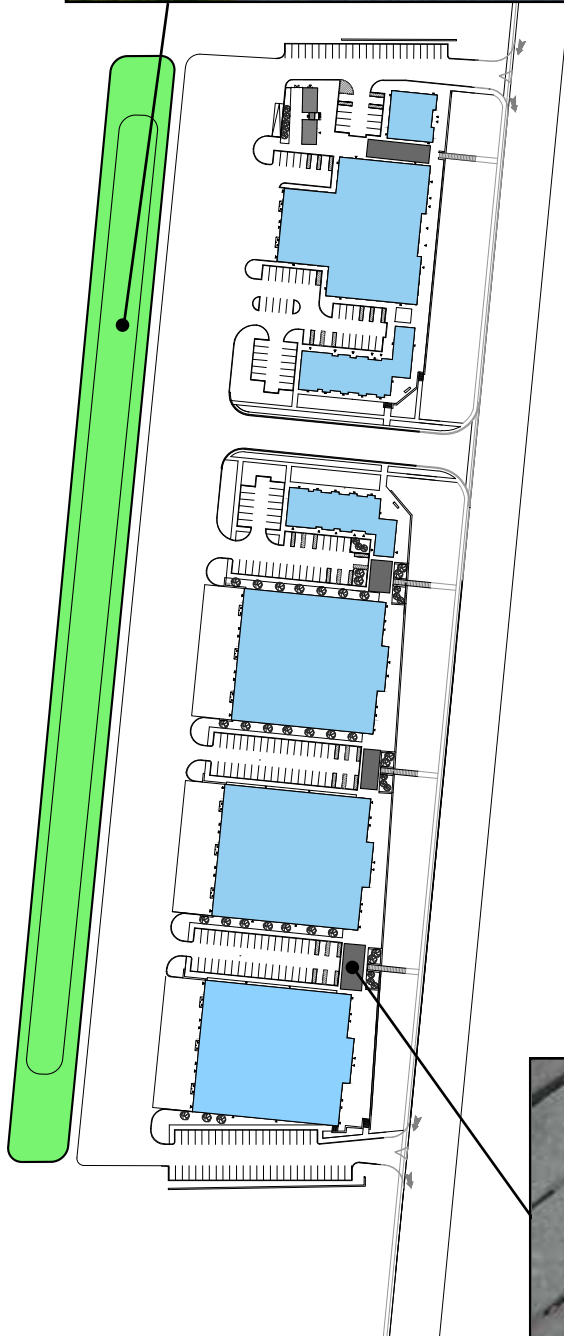
The main access to the site is lined with trees, as well as ground oriented signage with landscaping accents, clearly emphasizing the primary access and inviting users into the site. Pedestrian routes through the site are also lined with trees and landscaped islands, but to a lesser extent so as to not overwhelm the pedestrian experience and provide clear site lines to encourage CPTED principles.

a d e h i

Landscaped buffers and built form will be utilized to screen the development from residential uses to the north, as well as to screen loading and parking from the Airport Road frontage. Additionally, landscaped islands will be used to break up long stretches of parking between buildings.

	Key	TWDG Policy	Policy Met?
Design Requirements	a	Landscaping should identify, accentuate, complement, and unify key areas including site and building entrances, pedestrian and vehicular access points, circulation systems, signage, parking areas, and the public realm. These prominent locations may be augmented by hard and soft landscaping and special paving to establish a sense of arrival and create a sense of place.	✓
	b	Incorporate on-lot landscaping along the property edge to sensitively integrate commercial developments with the public realm and provide for an inviting pedestrian experience by providing enhanced landscaping and high quality fencing to soften views of parking areas.	✓
	c	Permanent site furnishings, including tree grates, guards, lighting, bollards, benches, bus shelters, trash and recycling receptacles, and street signage should be designed or specified to contribute to a consistent and compatible community style.	✓
	d	Plant material should incorporate native, non-invasive and drought tolerant perennials with seasonal colour variation and winter interest.	✓
	e	Landscaping design should be formal, rather than free-form or informal, on commercial sites. Avoid a naturalized or overgrown appearance and incorporate raised planters, where appropriate.	✓
	f	Hard and soft landscaping elements should allow for clear sight lines and reinforce the principles of CPTED, eliminating places to hide. Building fronts and entrances, and pedestrian walkways should not be obscured by landscaping elements.	✓
	g	All site areas not specifically landscaped or paved for pedestrian or vehicular use should be sodded.	✓
	h	Street furniture, or other hardscaping intended to provide seating, is strongly encouraged adjacent to building entrances.	✓
	i	Shade trees should be planted on parking islands, along street edges, and at other locations, wherever feasible.	✓
	j	Incorporate enhanced landscaping, using both fencing and planting, to buffer commercial uses from residential areas. Noise attenuating fencing should be provided between commercial and residential uses, where required.	✓
	k	A landscape strip of a minimum of 3 metres should be provided between parking areas and residential areas.	✓
	l	Utilities, servicing, storage, and loading areas should be screened using a mix of architectural elements and enhanced landscaping, using evergreen plant material, in consultation with utility providers to ensure operational access is maintained.	✓
	m	Buffer incompatible land uses through densely planted landscaping, where they cannot be separated by other means.	✓
Encouraged Practices	n	Incorporate LID measures and effective stormwater treatment as part of the overall landscaping strategy for commercial sites.	✓

6.2 STORM WATER MANAGEMENT



a b c d e f g i

A storm water management pond will be constructed on the western portion of the site, outside of the portion proposed to be developed in this application. The storm water management pond will be planted with native species as outlined by the landscape architect for the project.

j k l n o p

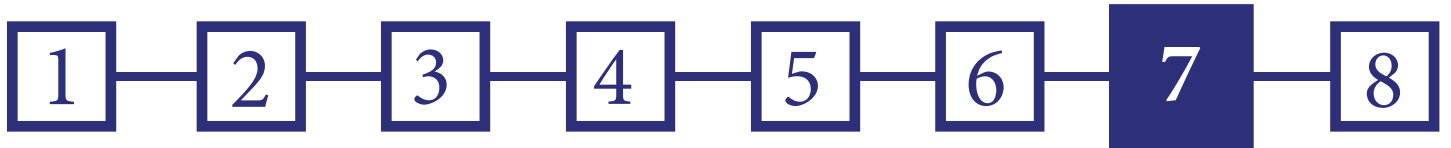
LIDs will be incorporated into the site design to support the SWM pond. The Art Pods will make use of permeable pavers, as well as rain gardens to decrease run off and provide some storm water storage. Permeable pavers will also be used in selected low volume parking areas. Oil/grit separators will provide pre-treatment of road drainage prior to discharge to bioretention areas. Additionally, roof top storage will be utilized to retain water on site and limit release rates.



	Key	TWDG Policy	Policy Met?
Design Standards	a	Locate, design, and implement stormwater management ponds to support and enhance the larger open space network. Stormwater management ponds should be located outside of natural heritage features and natural hazards.	✓
	b	Ensure that all species planted within the stormwater management pond are native, in consultation with the relevant conservation authority. Pond planting will follow the governing conservation authorities storm pond planting guide for species and densities.	✓
	c	Provide life preserver rings or throw lines at strategic locations for water rescue.	✓
Design Requirements	d	Provide grading and vegetation that are controlled to ensure natural transitions that sensitively integrate them into the existing natural environment.	✓
	e	Provide generous tree planting around the upper perimeter of all ponds in order to harmonize the pond landscape with adjacent natural features and provide a pleasant transitional buffer to the adjacent housing and public streets.	✓
	f	Plant fast growing wetland species of trees and shrubs along the pond edge to encourage rapid naturalization.	✓
	g	Arrange tree and shrub planting in significant groups to frame views of the stormwater pond from amenity areas.	✓
	h	Provide buffer planting to screen views of engineering structure, headwalls, spillways, etc.	✓
	i	Incorporate trails within the stormwater management pond and where possible, integrate the trails into the wider pedestrian network of sidewalks and trails.	✓
	j	Integrate stormwater management practices early in the design stage to ensure that they are taken into account prior to the detailed planning stages.	✓
	k	Incorporate infiltration facilities located adjacent to natural heritage features without requiring the use of the buffer, and ensuring not to compromise the function and ecological integrity of the feature and slope	✓
	l	Integrate stormwater management techniques, including LIDs in naturalized open space areas and into the landscaping plan of parks and public open spaces in locations where they do not interfere with the intended function of the park.	✓
Encouraged Practices	m	Include seating and lookout areas, providing them where views of natural areas are available, at a distance from arterial road edges. Provide benches, waste and recycling receptacles, and plantings for shade as components of the lookout area and to terminate trails where possible.	✓
	n	Incorporate LIDs within road ROWs and at the lot level.	✓
	o	Incorporate LIDs such as bioretention areas, soakways, or permeable pavement into parking areas, cul-de-sac enclaves, roundabouts, and sidewalks.	✓
	p	Encourage private on-lot LIDs, and ensure outlet control structures are placed on municipal property, beyond the private property line, to allow for easy maintenance and inspection.	✓

ART + WORK

**A UNIQUE DESIGN
OPPORTUNITY MERGING
PEOPLE WITH ART AND
ARCHITECTURE**



7.1 ART + WORK



The design of the site hinges around the four art pods positioned along the Airport Road frontage, breaking up the facade of the commercial buildings. The art pods serve as focal points and outdoor amenity spaces for the proposal, meant to convey the sense of an “outdoor room” to the users.

The art pods will provide local artists an opportunity to display significant art pieces, exploring the themes of industry, heritage, Sandhill, and Caledon as a whole.

With the art pods are to be framed by the commercial buildings to the sides, with a view corridor along the landscaped pedestrian access to the rear portion of the site. The buildings to the rear of the site will have their primary entrances at the termination points of these

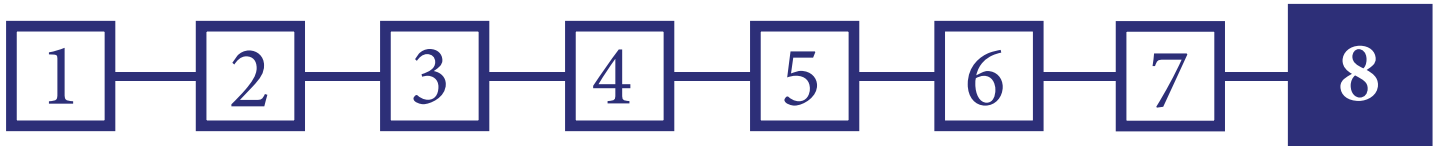


view corridors. Enhanced architectural features will be used in these primary entrances, including the use of canopies, peaked roof lines, and generous placement of windows and doors. This will create an inviting entrance to the rear of the site, while also creating a backdrop for the art pod view corridors.



An enhanced number of pedestrian amenities will be provided in the vicinity of the art pods, including formal and informal seating, trash recepticals, and canopies and trees to provide shaded areas.

CONCLUSION & IMPLEMENTATION



8.1 CONCLUSION AND IMPLEMENTATION

From an urban design perspective, the proposed project complies in all regards with the urban design policies as defined by the Town of Caledon.

The project has the potential to demonstrate positive intensification strategies at a key highly visible location within the emerging Employment Zone expansion along major Regional Roads like Airport Road.

It should be strongly supported through the approval processes to advance the vision set out for Sandhill, On and the continuance of well thought out Places of Employment within a Hamlet community.

Through the full Site Plan Review process, the details of the project design will be formalized and secured ensuring the full implementation of these urban design controls and recommendations.

RGC has incorporated site plan modifications based upon initial reviews presented by Staff at formal DART and Pre-app meetings completed in December, 2019.

This report was prepared by RG Consulting Inc (RGC), with the team of consultants listed within the Content of the urban design brief. The proposed site plan design and architectural building concepts are in keeping with the local character , heritage and charm of the hamlet of Sandhill, ON. This project will enhance and improve the experience for the extended community. The proposed buildings and respected uses will not negatively impact existing view corridors, but will improve the local fabric and site Airport Road

In conclusion it is our submission that this project and design illustrates the positive urban design principles enforced by the town, and, as such, could endeavor to provide a supporting vision guideline for future Employment zone growth characteristics for others to follow.

We respectfully make this submission if support of Caledon Staff members and City council.