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URBAN DESIGN BRIEF

13656 AND 13668 EMIL KOLB PARKWAY

FILE #: RZ 2021-0015

DECEMBER 10, 2025



PREPARED FOR:
CAMCOS (BOLTON VILLAGE INC.)

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Town of Caledon
6311 Old Church Rd,
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December 10, 2025

c.c.
Camcos (Bolton Village Inc.)
(Owner)
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13656 and 13668 Emil Kolb Parkway

0.0 Introduction

The following document provides an analysis of the architectural and urban design impact of the proposed development located on 13656 and 13668 Emil Kolb Parkway, in Bolton, Town of Caledon, Ontario. This document has been created in support of a rezoning application, reference number RZ 2021-0015 and on behalf of Camcos (Bolton Village Inc.) (owner).

This report has considered compliance with Zoning By-law 2006-50, Town of Caledon Official Plan (March 2024 Consolidation), Town of Caledon Comprehensive Town-wide design guidelines – March 2025.

The report aims to provide analysis of the proposed development and its compliance with the guidelines named above. Also offering design considerations to enhance the interaction of the development with the street and its presence within the neighbourhood.

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1.0 PROPOSED DEVELOPMENT

1.1 Location

The subject land is located on 13656 and 13668 Emil Kolb Parkway at the Northwest corner of Emil Kolb Parkway and Harvest Moon Drive.



Image 1

The immediate surroundings of the site have consistent low rise residential typologies, with a variety of building sizes.

Directly south, on the southwest corner of the intersection is a storm water management pond. The corner will never be developed, and the height of our building or shadow casted will never impact any future uses here.

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1.2 Site Context

A single-family house currently sits on the north side of the subject land, with most of the southern/corner area currently empty.

To the North and West of the site there are blocks of two-storey townhouses and a few semi-detached houses.

See *image 2* for reference of overall area.



Image 2

The site is bordered by a major intersection to the southeast and is situated across from, and adjacent to, green spaces and residential neighbourhoods.

It should be noted that the design of the intersection will be changing when Capital Project #24-4825 is implemented. At which point, a roundabout will be introduced, changing the character of the road and its surrounding areas.

At present, the site offers limited contribution to the intersection or the neighbourhood's urban fabric. The corner is missing a prominent marker that would help define the street edges and establish a notable presence within the streetscape. Existing vegetation is present around the house, as illustrated in Views 01 and 02 below.

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View 01 – Southeast corner



View 02 – Northeast corner

1.3 Project Summary

The newly proposed development introduces a range of housing typologies to help address current housing needs, providing multiple options within a higher-density, mixed-use project. The proposal includes an 8-storey midrise building with 102 units of varying sizes, plus 2 blocks of 3-storey townhouses, for a total of 124 units on the site. Additionally, the development will offer communal spaces to promote walkability and energize the intersection, creating spaces that benefit the public realm and enhance the streetscape and urban fabric.

1.4 Policy Compliance

This Architectural and Urban Design Guidelines report will base its analysis on town documents that aim to shape the development of the Town of Caledon. Such documents include Zoning By-law 2006-50, and the Town of Caledon Comprehensive Town-wide design guidelines – March 2025.

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These documents aim to encourage more urban-oriented, pedestrian-friendly neighbourhoods. The project incorporates these urban design principles to ensure an appropriate built form and to support the continued improvement of Caledon's urban fabric. As part of the designated existing urban area, the proposed development encourages the intensification of residential density contributing to the evolution and transformation of Caledon's established communities.

By increasing population density, the proposed buildings will activate the surrounding community with newly introduced pedestrian connections, while providing alternative housing options, defining street edges and spawning urban growth.

In accordance with the Town's Urban System Guidelines, new developments define and reinforce the relationship between streets and building forms. A well-designed, pedestrian-friendly streetscape with site-wide connections will encourage active use, shape growth patterns, and uphold design quality.

1.4.1 Zoning Analysis

The subject land is currently zoned as A1 – Agricultural zone, under the Zoning By-law 2006-50 from the Town of Caledon. Due to the restrictive nature of the zone, the owner has proposed a revision to the zoning by law to allow for the development.

The following chart outlines the requirements of the zoning, and it compares to the information being proposed under revised submission RZ 2021-0015.

ZONE STANDARDS	ZONE RM	PROPOSED SITE PLAN
Lot Area (min)	925 m ²	8,363.7 m ²
Lot Frontage (min)	30 m	52.55 m along Harvest Moon Drive 83.21m along Emil Kolb Parkway
Building Area (Max)	N/A	28%
Front Yard (Harvest Moon Drive)	9 m	0m – 5m
Exterior Side Yard (Emil Kolb Parkway)	9 m	0 m
Rear Yard	7.5 m	6 m (Townhouses)
Interior Side Yard	7.5 m	3 m (Townhouses)
Building Height (maximum)	12.2 m	27.28m to roof surface, 28.88m to top of parapet, 31.3m to top of mechanical penthouse (Apartment building)

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		13.02 m (Townhouses)
Landscape Area (Min)	45%	24%
Max # of dwelling units per townhouse	12	2 Blocks (12 & 10)
PARKING		
Parking ratio	<ul style="list-style-type: none"> Dwelling, Apartment 1.5 parking spaces per dwelling unit + 0.25 parking spaces/ unit for visitor parking. 	<ul style="list-style-type: none"> 1.0 Parking spaces/unit + 0.17 Parking space/unit for Visitors. Residential Provided: 102 Visitor Provided: 18 Total Provided: 120
	<ul style="list-style-type: none"> Dwelling, Back-to-Back Townhouse 2 parking spaces per dwelling unit + 0.25 visitor parking spaces for each dwelling unit. 	<ul style="list-style-type: none"> 2.0 Parking spaces/unit + 0.17 Parking space/unit for visitors. Residential Provided: 44 Visitor Provided: 4 Total Provided: 48
Parking Space Setback	6 m	3.0 m (Townhouse) 5.0 m (Apartment)
Barrier – Free Parking	3% + 1= 6 spaces	7 spaces

The proposed zoning addresses shortfalls, and increases the height and density of the lot, bringing a bolder urban approach to the area.

1.5 Contextual Compatibility

The proposed development promotes connectivity and integration with the existing neighbourhood, by offering clear pedestrian pathways around the site, encouraging connectivity and allowing the public to interact with strategically designed green areas within the site, while interfacing with the existing and future surrounding context to create a unique sense of place and built form.

1.6 Development Site Plan

The proposed site plan has been designed to increase density while maintaining and respecting the neighbouring urban fabric. The proposal includes one 8-storey residential midrise building at the southeast corner and two blocks of 3-storey townhouses, along the northern edge.

The proposed building has been positioned strategically to provide appropriate separation from the lower-density housing on the neighbouring lots and acting as corner marker for the intersection. This configuration establishes a gradual transition in scale, supporting the residential character of the surrounding area. The siting of the townhouses also responds to privacy considerations, with their design ensuring a sensitive and compatible interface with neighbouring properties and providing front-door housing alternatives. In addition, the

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site layout accommodates efficient circulation patterns, allowing the entrance, waste collection, and fire route to operate within a continuous loop.

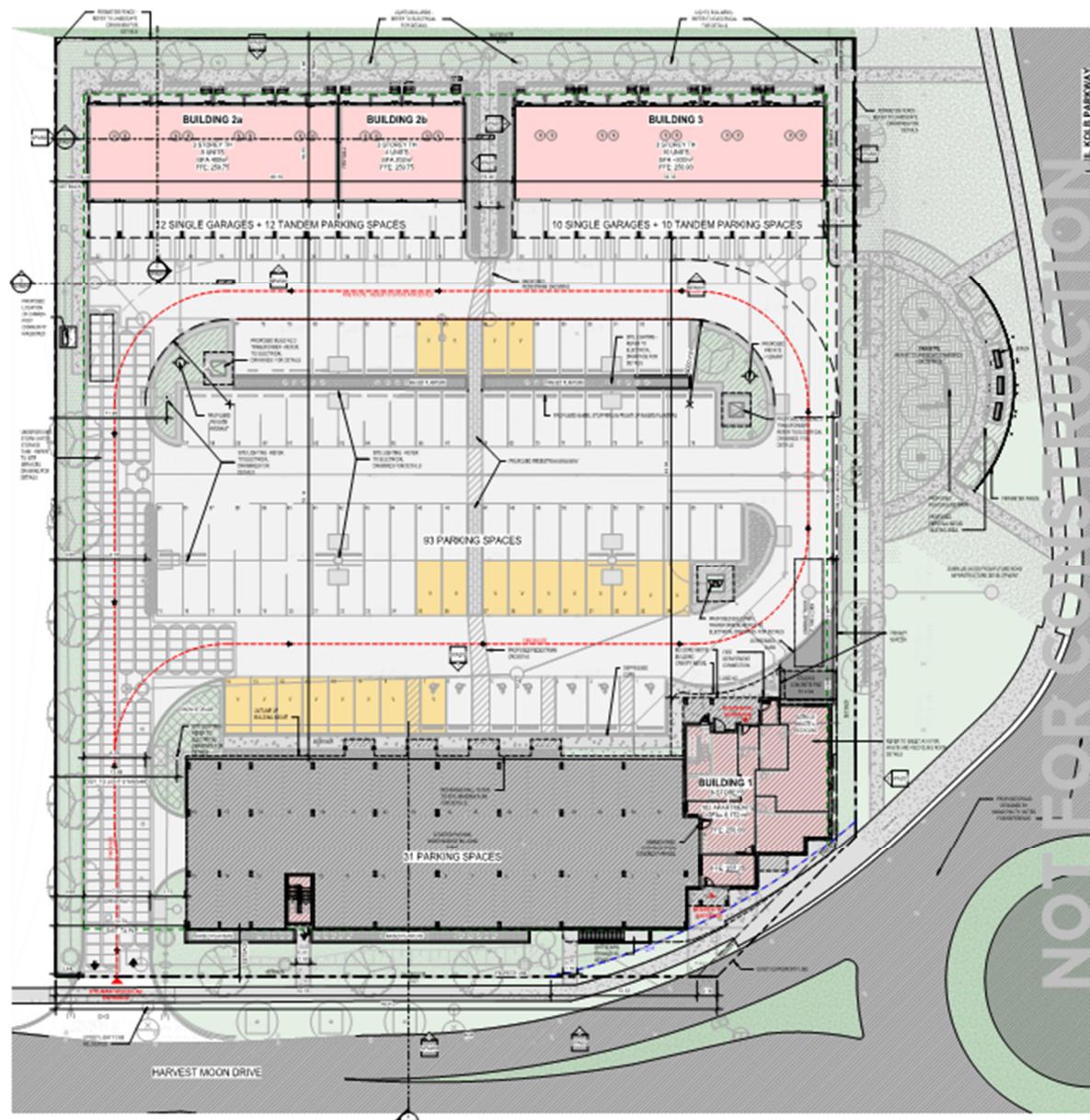


Image 3

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2.0 PUBLIC/PRIVATE REALM

2.1 Linkage, Connections and Circulation

2.1.1 Pedestrian Circulation within the Site

The proposed site plan offers pedestrians the opportunity to walk the site safely, with pathways and dedicated areas for residents, while promoting connections to the public realm facing the two major streets. The townhouses to the north of the site are interconnected via a pathway that runs along them. The same path reaches the public sidewalk on the east of the property and the newly proposed sitting area in the surplus lands along Emil Kolb Parkway.

As well, the pedestrian path is demarcated throughout the parking lot by changes on the surface material, providing a clear path of connection from the townhouses to the midrise building amenities, avoiding connections in between closely parked cars.

2.1.2. Vehicular Circulation

The site features an efficient vehicular circulation system, forming a continuous loop that supports parking and smooth local traffic flow. Access to the rear of the site, where the townhouses are located, is designed so that residents can drive directly to their homes without passing the midrise building entrances.

2.1.3. Parking and Servicing Circulation

Parking spaces on the site have been organized and distributed to serve the residents and visitors.

The midrise building comprises a parking ratio of 1 parking space for every apartment unit, for a total of 102 parking spaces available for residents. The townhouses have a parking ratio of 2 parking spaces for every unit. These include the tandem spaces allocated in front of each unit. The visitor parking ratio for all residential use, is 0.18 parking spaces per unit. The 22 parking spaces are distributed around the site for proximity and convenience.

The main driveway is also used by service vehicles such as waste, recycling, mail delivery, loading and emergency vehicles. Site circulation remains safe and unobstructed, even when service vehicles are parked for deliveries or pick-ups.

2.1.4. Connections to Adjacent Sites and Neighbourhoods

Pedestrian connections with the adjacent neighbourhoods are maintained and enhanced with green areas and surrounding landscaping.

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2.2 Landscape Plan

The landscape design of the site encompasses a variety of native species that surround the structures, bringing privacy and physical separation from the neighbouring sites and public roads.

Throughout the site, vegetation is used to enhance the resident's experience, as it provides a relief from the parking area. The planters, arranged perpendicular to the parking spaces, introduce greenery that gently screens the parking area and enhances the overall visual experience. Among other features, there are pathways throughout the parking area, with different paving materials to delimit and distinguish the pedestrian paths.

A sitting, playground and amenity area are being proposed in the surplus land to the east of the development, providing the community a recreational area.

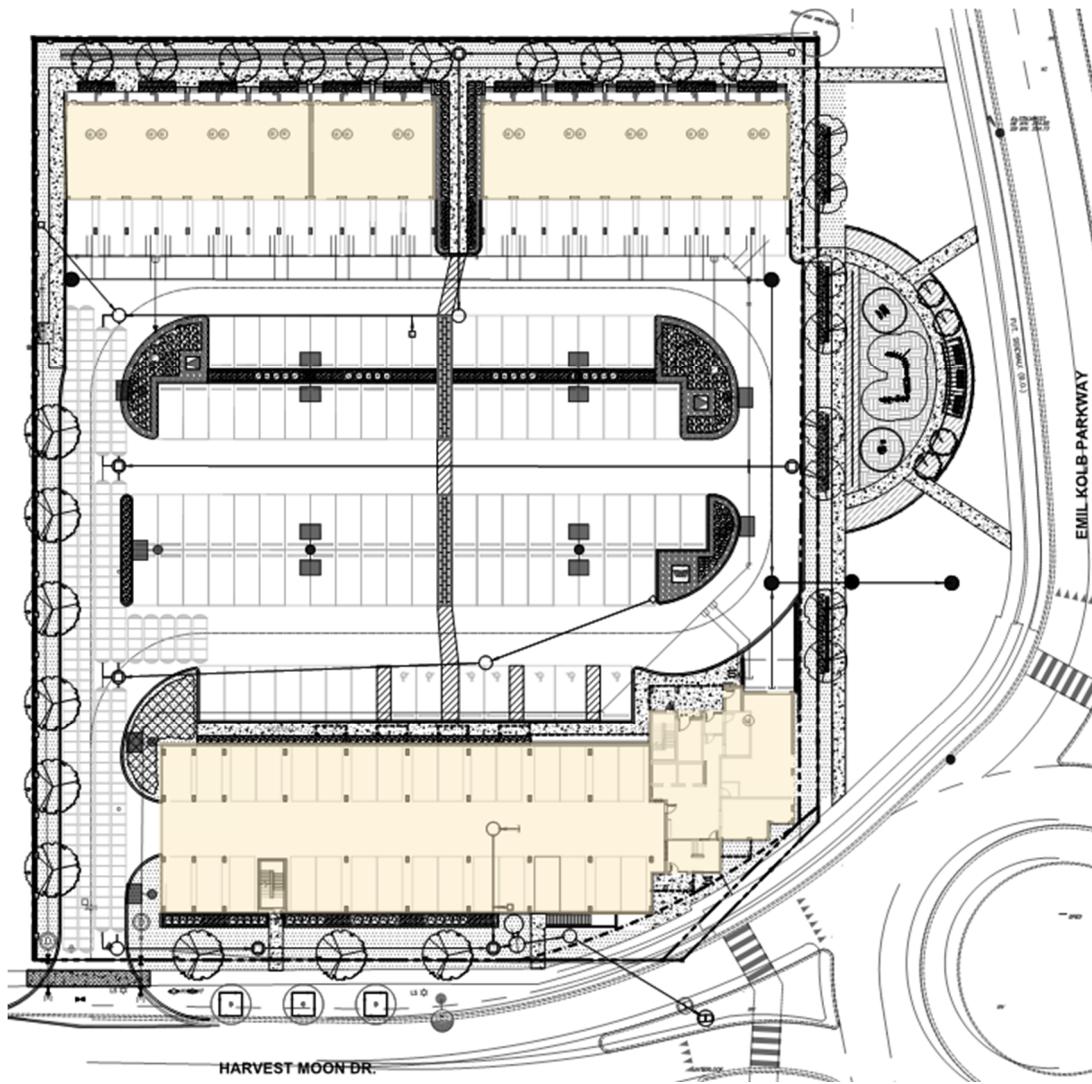


Image 4

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2.2.1 Corner Design

The building has been placed at the corner of a major intersection to activate the streetscape it will emerge as a defining landmark for the neighbourhood, establishing a clear and memorable identify for the site, and supporting the area's continued urban growth. Corner proximity and Harvest Moon Drive frontage limit shadow impacts on nearby homes and establish defensible space within the site.

The midrise building entrance and corner treatment follow the lines of the new property line and future street design. The addition of the curvilinear canopy replicates the motion of the corner while the exterior walls of the building maintain its straight angles along the street line.

2.2.2 Screening of Service Areas

Screening is provided at the waste and recycling collection point along Emil Kolb Parkway to conceal daily activities associated with the waste room and waste truck during collection. The open parking level beneath the building is also screened using a series of linear raised planter boxes, which create a visual barrier facing the public realm. A combination of ornamental reed grass and deciduous shrubs provides a buffer from Harvest Moon Drive. As illustrated in Image 5 below, ornamental grass can serve as an effective screen, offering a softer transition while requiring minimal maintenance.



Image 5

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2.3 Signage

Signage is proposed above the main entrance of the building, facing Harvest Moon Drive. Directional signage and way finding will be strategically designed and included.

2.4 Lighting

Lighting is provided throughout the site, including parking areas, around buildings, and along all pedestrian pathways and spaces. The light fixtures are designed to complement the architecture and landscaping of the development.

2.5 Community Design Safety

Through lighting, open pedestrian pathways, balconies and a variety of fenestration throughout, the site provides a safe environment for residents and visitors. The different residential typology within the site promotes public connectivity and spontaneous visual supervision by residents. Private residence entrances are strategically located to be separate and away from public access.

3.0 BUILT FORM

The introduction of a higher density development in this Bolton neighbourhood, will be the starting point for future developments in the area. Allowing for higher density, especially in areas such major intersections can bring opportunities for growth and ensuing interconnectivity and will create a landmark while providing more housing options for the community.

The 8-storey residential midrise building design involves three major massing components, starting with a somewhat rational block that houses most of the units in the project. The development offers a variety of sizes, from 1 to 3-bedroom units, including barrier-free units of all sizes. This section of the building (shown in green below) has a consistent and repetitive structure, allowing for uniformity in the interior design of the units. However, it also has variations on the façade, both in materials and recesses which articulate the elevations, providing a sense of address and visual interest, while mitigating the impact on the street and surroundings.

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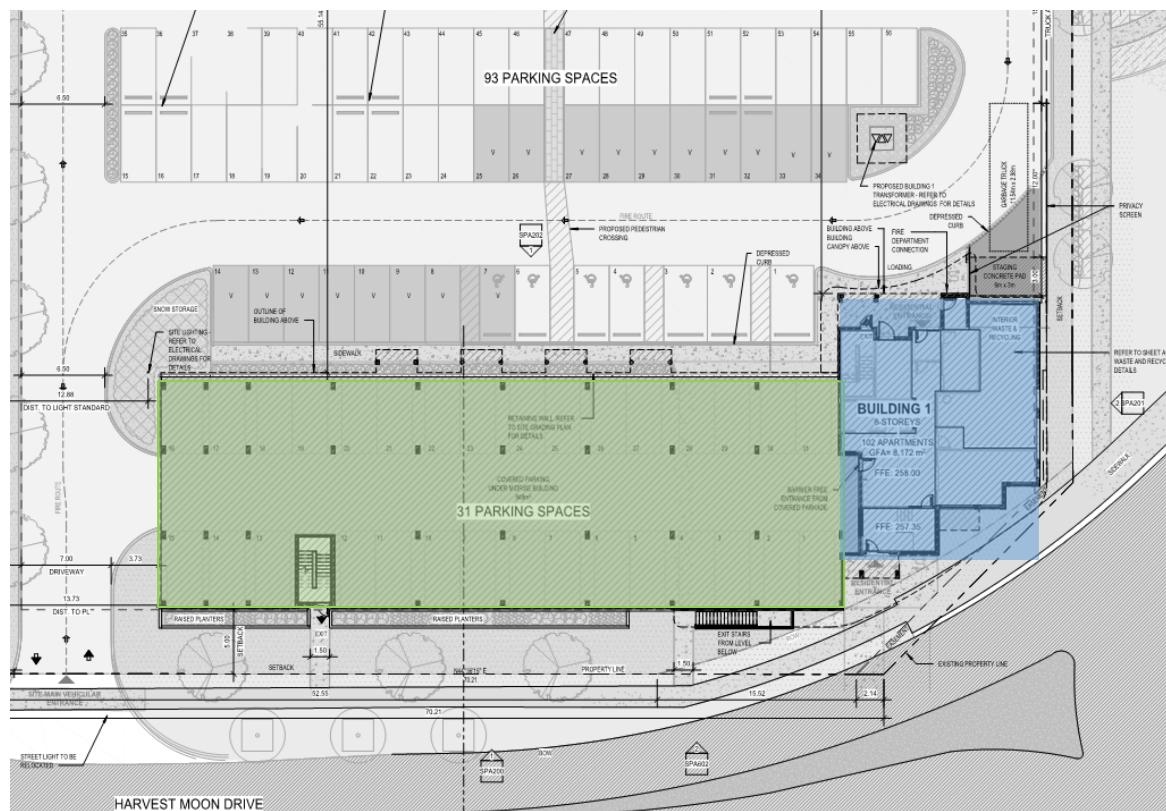


Image 6

The next section of the building (shown in blue) accommodates a combination of services and residential spaces that complement and serve the rest of the building. At the basement level, one can find storage and mechanical spaces. On the ground floor, common spaces take most of the floor area. These include, residents' lounge, moving room and waste room.

These sections translate to the elevations and overall massing of the building, as well. The corner treatment has a strong yet welcoming approach, mimicking the stepping back of the north elevation vertically, as it steps back turning the corner, following the line of the road. In the residential block the emphasis of the vertical elements is achieved by changes in the material and slight massing adjustment that give depth to the elevation. Refer to *image 10*, south elevation for reference.

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Image 7

Horizontally, the project shows two layers on the street side: ground floor on stilts supporting a seven-storey volume with a regular façade, configured as a grid-like pattern. The corner intersects the horizontal tiering with a strong vertical beacon that turns the intersection of the two roads and engages the curvilinear limit between building and public realm. There is more horizontal layering in the rear, where the project faces small scale residential townhouses and the play of projecting balconies and top floor recesses break down the courtyard scale more appropriately. Refer to *image 11*, below, to see the relation of the north elevation with the townhouses and neighbouring structures.

3.1 Built Form Principles

3.2 Compliance with Relevant Sections Of TWDG

The development proposes a mix of residential and mixed-use built forms, including: Ground Townhouses: Oriented to activate the public realm with direct pedestrian access and minimal front yard setbacks.

Mid-Rise residential building: Located strategically near transit nodes and mixed-use areas, with active ground-floor uses to support street-level animation.

3.3 Plans, Elevations and Perspective Renderings/3D Views

The concept that inspired the elevations of the midrise and subsequently the townhouses, comes from the Italian word, Arpeggio. Which in music, refers to a chord whose notes are sounded separately instead of at the same time. It can be played in an ascending or descending order. *Image 8* below, illustrates the concept.

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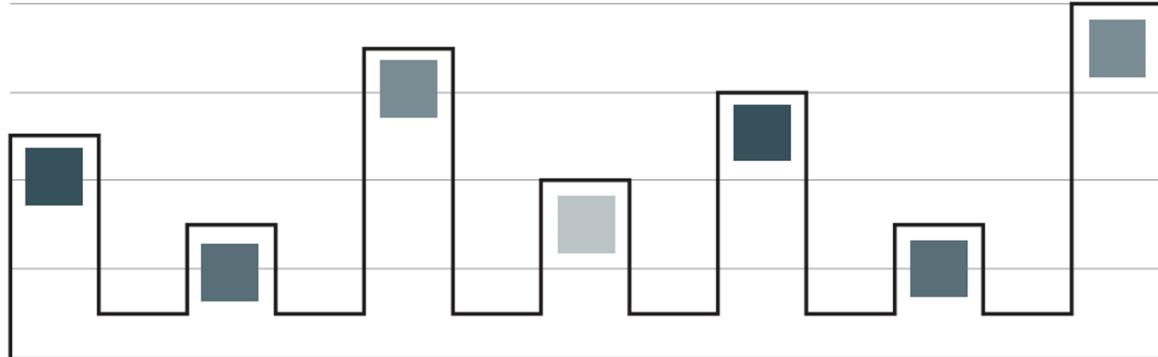


Image 8

The variations of colours and heights can be seen in the elevations in the form of materials, voids and solids. By articulating the elevations in this dynamic matter, the design brings a unique approach, which shows accent colours appear intermittently, like sharp notes in a musical phrase.

Balconies and recessed terraces punctuate the façade in a rhythmic pattern, mimicking the spacing of notes, creating a type of melody. Complimenting the pattern throughout the building, materials and colours create an additional layer in the composition. Below is the proposed material palette, which is used consistently throughout all the buildings.



Image 9

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Image 10



Image 11

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4.0 SUSTAINABLE DESIGN

Sustainability design options are being considered and will be submitted at a later date.

5.0 ACCESSIBILITY AND UNIVERSAL DESIGN

As part of the typical floors plans of the midrise building, 15% or 15 units meet barrier-free design parameters, offering occupants different unit types to choose from. As well, interior and exterior public spaces have been designed to provide barrier-free paths of travel and to ensure residents and the public have access to all areas of the site.

All entrances to the midrise building are barrier-free as per building code requirements. Barrier-free pathways have been positioned around the main entrances, to ensure uninterrupted access.

4% of the total of parking spaces have been designated as accessible parking. Therefore, 7 accessible parking spaces have been included in the overall parking count.

6.0 SUN AND SHADOW STUDY ANALYSIS

In support of the re-zoning submission RZ 2021-0015, and Site Application #SPA 2021-0077, and as requested by the Town of Caledon, sun and shadow study visual models can be found in the drawing package under sheets SPA600 & SPA601. The study has been completed following the guidelines of the Town of Caledon found in the document titled "Terms of Reference: Sun and Shadow Study". The dates recorded as per this document, are March 21, June 21, September 21 and December 21.

The conclusions of the study are:

In early spring, the surrounding areas to the south of the mixed use 8 story building have more than 7 hours of uninterrupted daylight, as the building doesn't cast shadows on Harvest Moon Drive and area, until after 5pm.

The proposed townhouses cast some shadows on the backyards of the neighbouring residences only in the early hours of the morning. By 11am there are no shadows cast on these properties, allowing for more than 7 hours of sunlight.

Late spring: In June, there is no change to the impact of the building casting shadows on the south side of the property. Shadows can be seen crossing Harvest Moon Drive only in the evening, sometime after 5 pm. The areas affected in the evening hours include a portion of the intersection, part of the opposite sidewalk and the corner unused land which corresponds to less than 50% of the neighbouring site.

Proposed townhouses have no impact during this time on the adjacent properties.

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In early fall, late September, the midrise mixed used building cast shadows on the site across from Harvest Moon Drive after 5pm and only to the sidewalk at the corner of the intersection. Allowing for sunshine and no shadows on the neighbouring site for more than 8 hours a day.

Same as in early spring, the proposed townhouses cast some shadows in the early morning hours, on the backyards of the adjacent properties, dissipating by 11am and allowing for more than 7 hours of sunlight.

Towards the end of December, in early winter, the mixed used building and the proposed townhouses, cast some shadows on Emil Kolb Parkway, reaching the sidewalk across the parkway only after 3pm.

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7.0 CONCLUSION

The proposed development introduces a transformative urban intervention at a key intersection in Bolton, addressing both functional and aesthetic gaps in the existing context. Currently, the site lacks a defining presence and fails to contribute to the public realm. By introducing an 8-storey midrise building, complemented by townhouses, the project establishes a corner landmark that activates the streetscape and sets a precedent for future growth.

The design draws inspiration from the musical concept of Arpeggio, expressed through rhythmic variations of materials, colors, voids, and solids across the elevations. Balconies and recessed terraces punctuate the façade in a dynamic rhythm, creating a visual melody that reinforces the building's identity. This articulation, combined with a curated material palette, ensures a distinctive yet contextually sensitive presence.

Public and private realms are seamlessly integrated through pedestrian pathways, landscaped green spaces, and communal amenities, promoting walkability and connectivity. Vehicular circulation and parking are efficiently organized, while service areas are discreetly screened to maintain visual quality. The inclusion of barrier-free design principles, accessible entrances, and designated parking spaces ensures universal access and inclusivity.

Aligned with the Town of Caledon's Urban Design Guidelines, the development supports intensification and pedestrian-oriented environments. Its layered massing strategy provides a gradual transition to adjacent low-rise housing, while the active ground floor animates the street edge. Ultimately, this project delivers a bold yet harmonious built form, enhancing the urban fabric, fostering community interaction, and creating a memorable landmark that anticipates future growth and design excellence.