

October 31, 2023



CULTURAL HERITAGE IMPACT ASSESSMENT (CHIA)

for

**0, 12245 Torbram Road and 12542 Airport
Road**

'Tullamore Lands' - Part of Lots 17 to 20, Concession 6.
Town of Caledon, Ontario

GBCA Project No: 21012

prepared for:

Tullamore Industrial GP Limited
5762 Mayfield Road
Caledon, ON

prepared by:

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Second date of issue: Monday, October 30, 2023

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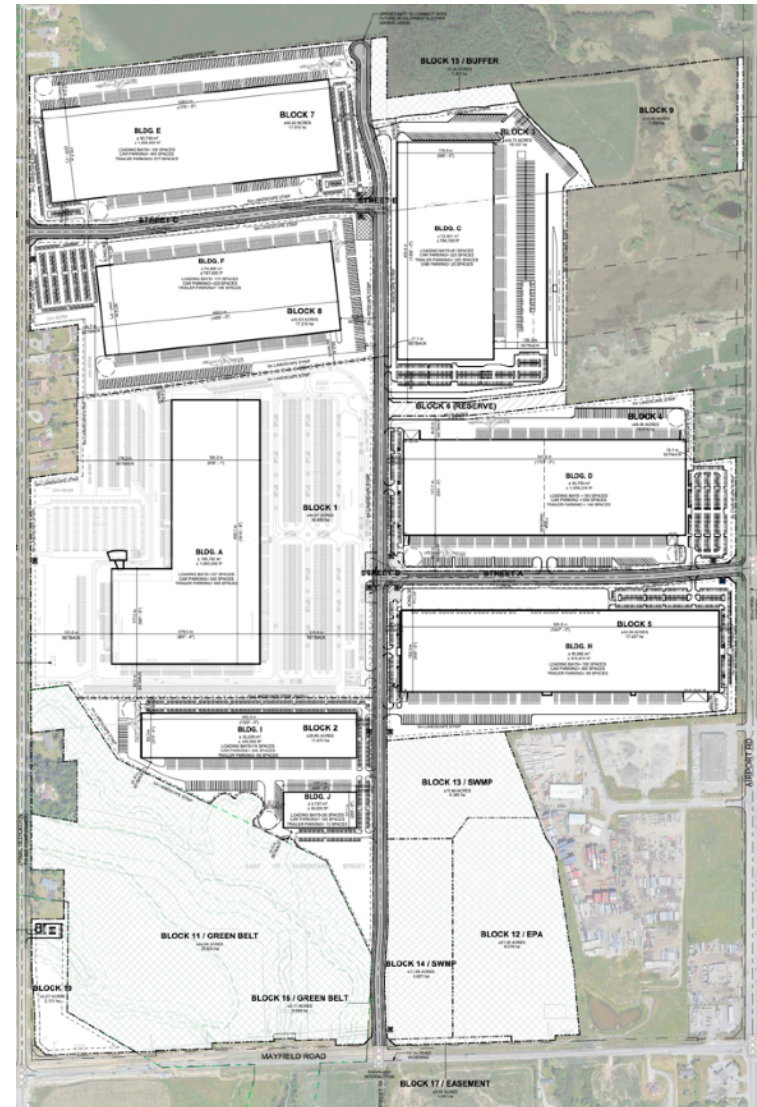


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EXECUTIVE SUMMARY

Goldsmith Borgal & Company Ltd. Architects (GBCA) was retained by The Rice Group to prepare a Cultural Heritage Impact Assessment (CHIA) for the purposes of an Official Plan and Zoning By-law Amendment application.

The subject property comprises part of Lots 17, 18, 19, 20, Concession 6 East of Hurontario Street. The environs feature a mix of early 19th-century farm houses, newer residential and commercial development, as well as open land historically associated with agricultural uses. Early settlers realised the agricultural potential of the rich soils located here in the Peel Plain. The resulting rural character of the area is a result of the concession/lot system dating back to 1819 which created hundreds of 200-acre lots.

Located on the western portion of the large assembly is a small farmhouse at 12245 Torbram Road, estimated to date from c1850-1874. The house, with medium-pitched side gable roof and dichromatic brick patterning, is similar to several other farmhouses in the area. At the time of this report, the listed property is in very poor repair and requires stabilisation (see Appendix III). The proposed conservation strategy will involve the preservation, rehabilitation and restoration of the house. Details regarding repair, stabilisation, preservation and long term conservation would be explored in a detailed Conservation Plan at a later date. Council approval has been received for the demolition of outbuildings located on the property and is also included for reference in Appendix IV.

This revised HIA follows our previous submission from July 16, 2021. Since this time, a Minister's Zoning Order (MZO) was approved on September 09, 2022 as part of Ontario Regulation 438/22. In principle, the revised development proposal will maintain a similar relationship between the massing of the new development, and the proposed location and strategy for dealing with the existing heritage structure.

As part of this, the site has been expanded and rezoned for a proposed industrial park development. Additional lands to the north also include an extension of the previous scheme with industrial buildings, new roads, and open spaces. Notable changes in addition to this are listed below:

- The addition of Robbins Lands at 97.306 acres, and Gino Cilio Lands at 34.80 acres.
- The expanded site includes 12 facilities (larger than previous submission) with associated parking and road network.
- The site has been expanded and rezoned for a proposed industrial park development including warehouse, and distribution space with altered road network and associated parking.
- Lands have been extended north-east to include 12542 Airport Road which is a non-designated property featuring a neoclassical style farmhouse. The development will terminate to the east of this property, and will be screened by existing trees and plantings.

The SPA drawings provided by Turner Fleischer Architects have been revised to address comments from Staff, in order to accommodate a number of new requirements and reconfigurations of the plan and structures as a result of the extension of the previous scheme.

The farmhouse is located on a parcel comprising a small portion of the subject site or 'Tullamore lands'. A commercial/warehousing centre is proposed here, including 12-warehouse facilities of new built area are proposed within a new road network extending from Torbram Road to the west, Airport Road to the east, and Mayfield Road to the south.

As a consequence of the proposed industrial park, the necessary relocation of the small farmhouse at 12245 Torbram Road is required. The subject property, with a current lot frontage of approx. 43 metres from the limit of Torbram Road, is proposed to be permanently relocated to the south-east of this onto lands on the northern part of Block 10, close to the intersection of Torbram Road and Mayfield Road. Here, the farmhouse is proposed to be reused, incorporated within the existing property and located closer to Torbram Road with additional parking.

The development site is adjacent to the Tullamore Secondary Plan Industrial/Commercial Centre which has been viewed (in section 5.1.5.4 of the Official Plan) as *"transitioning toward a more urban development pattern... with a wider range of retail and service uses permitted in order to accommodate contemporary retail formats."* The Official Plan encourages the retention and conservation of heritage resources and the integration of these resources into the new development proposals.

The proposed land assembly and relocation will have impacts on the heritage attributes of the existing farmhouse at 12245 Torbram Road. Although relocation is not typically favoured, in this case relocation can be seen as a mitigation strategy for the building's long-term preservation. These elements along with mitigation strategies are discussed in more detail throughout the report. While the proposed revisions to the subdivision plan extend to 12542 Airport Road, a non-designated property featuring a neoclassical style farmhouse, there will be no development here. Therefore, impacts heritage properties adjacent to the proposed industrial park development will be minimal.

GBCA has reviewed the proposed development with respect to its adjacency to heritage resources (as defined in the Town's Official Plan) and the overall "fit" of the development into the existing site and context, based on the HIA Terms of Reference and include the requirements set out in the Town of Caledon Official Plan, policy 3.3.3.1.5.

In our view, and in light of consideration of strategies to reduce impacts on heritage properties, this proposal balances demands for intensification with those of heritage preservation in a manner that allows both objectives to be appreciated as a part of a complex and changing environment.

1. INTRODUCTION

1.1 Description of the property

The site has been expanded and rezoned for a proposed industrial park development. Lands have been extended north-east to include 12542 Airport Road which is a non-designated property featuring a neoclassical style farmhouse (Block 9).

The site includes also includes a more prominent a 1-1/2 storey neo-classical style farmhouse, built c.1850-1875 (Block 1). The property is noted in the Town of Caledon's listing under Section 27 (1.2) of the Ontario Heritage Act.

The property at 12245 Torbram Road is a farmhouse is located on a parcel of land comprising the Tullamore lands, which totals approximately 363 acres. A proposed commercial/warehousing development, bound irregularly by Torbram Road to the north, Airport Road to the east, and Mayfield Road to the south.

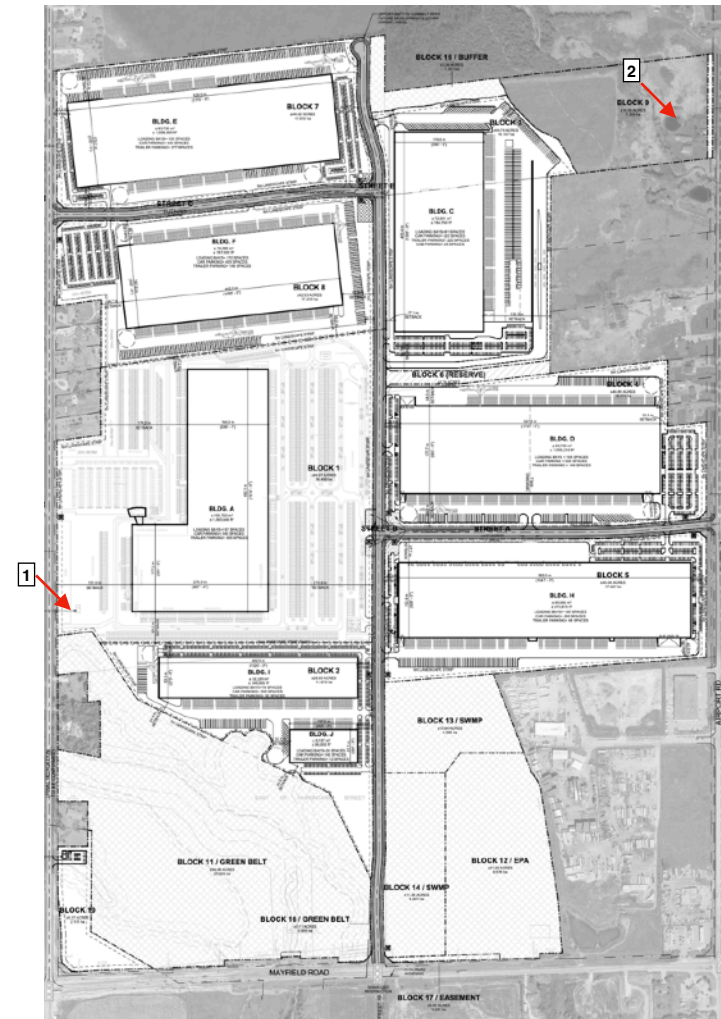
The south-east corner of the site is occupied by a 64.77 acre lot designated as a greenbelt/swamp area. The proposed development site is located within an area that is characterised by pasture land, farm houses and its largely rural character dating to the concession/lot system that created hundreds of 200-acre lots in the early 1800s.

1.2 Present Owner and Contact Information

Owner: Tullamore Industrial GP Limited
5762 Mayfield Road
Caledon, ON

Contact: Aarthi Thaya
Development Manager
aarthi.thaya@ricegroup.ca

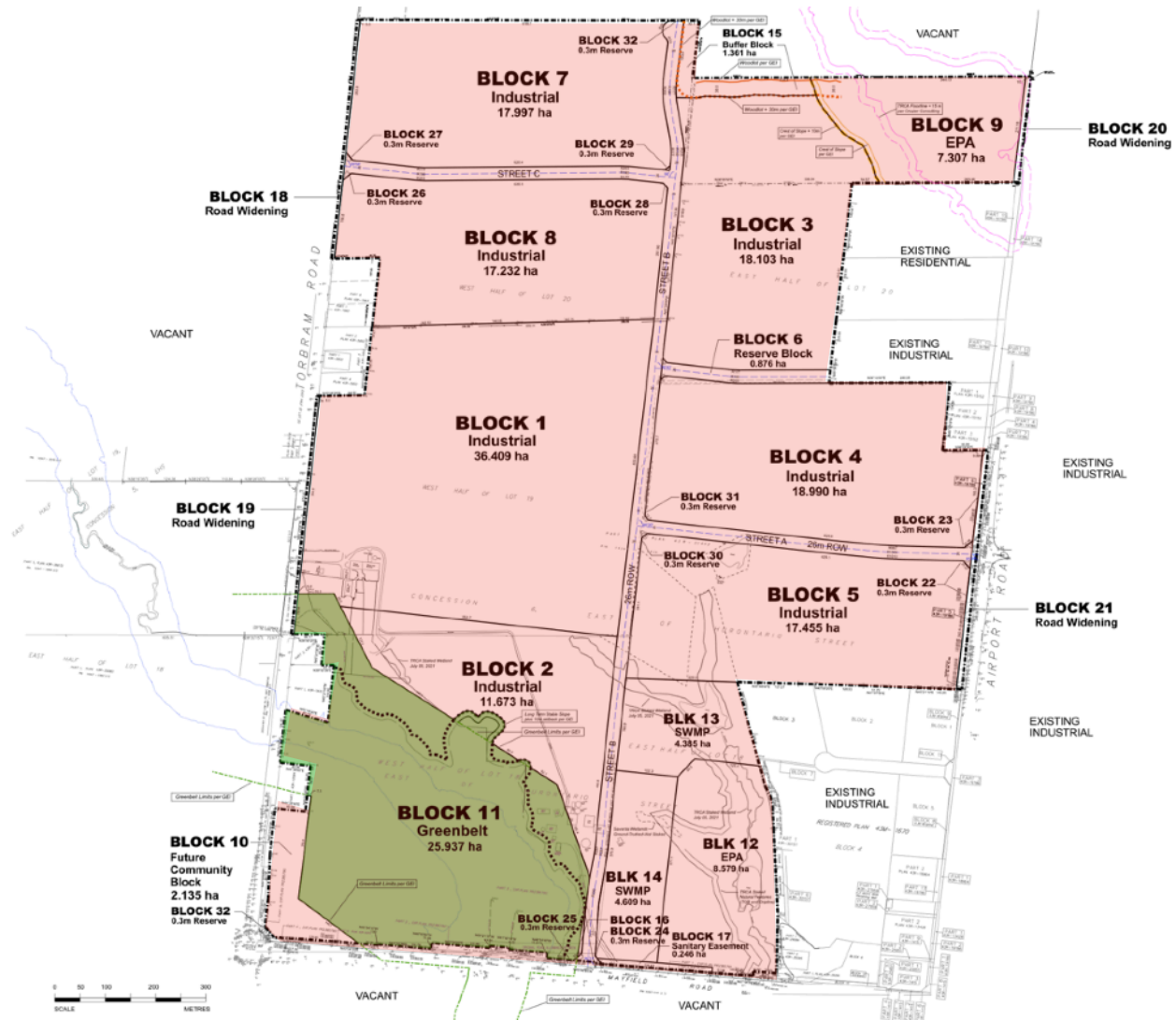
1.3 Location Plan



The location of the heritage resource is noted above in red:

1. **12245 Torbram Road** (1850-1875) Listed in the Town of Caledon's heritage register.
2. **12542 Airport Road** - Listed in the Town of Caledon's heritage register.

1.4 Property Survey



Topographic Survey (Weston Consultants)

1.5 Site Photographs



A view looking towards the front elevation of 12245 Torbram Road from the west side of the road. A portion of the west elevation is visible here, although a majority of the site is obscured from the public realm by a heavy cover of older coniferous and deciduous plantings.



From the northern boundary of 12245 Torbram Road looking to an adjacent field. Building 'A' is proposed to be located in this general area.



Looking east from the rear yard of the subject site towards the actively used barn on the property. The barn is proposed to be removed to accommodate Building 'A'.



View looking along the tree-lined entrance of 12245 Torbram Road looking west towards Torbram Road.



View of the principle (west) elevation of 12245 Torbram Road with the main house located to the right, with a central opening and a pair of six-over-six sash windows on either side. The frieze beneath the roof cornice is visible here, and a dentil band above.



View of the east elevation, with the lower addition visible and the rear elevation of the main house obscured by vegetation. The top windows of the east elevation have been covered.



View of a portion of the development site, looking south from Old School Road.



View from Torbram Road looking east across the development site to Airport Road. Visible along Airport Road are other logistics centres of a similar nature to the proposed development.



View of a portion of the development site, looking north from Mayfield Road. The lands to the west are proposed for the relocation of the farmhouse at 12245 Torbram Road.



View of a portion of the development site, looking west from Airport Road. To the immediate right is 12542 Airport Road which is a Non-designated property listed under section 27 (1.2) of the Heritage Act. Currently, the lands comprising this property is located outside of the proposed development site plan.



View looking along the tree-lined entrance of 12542 Airport Road looking west from the entrance at Airport Road with a variety of trees and plantings surrounding the property.



View of out-buildings located on the property of 12542 Airport Road, on the eastern side of the site.



A view of the neoclassical style farmhouse with a newer addition on the west side, and coniferous tree cover in the front.



View of 12542 Airport Road which is a Non-designated property listed under section 27 (1.2) of the Heritage Act. The Caledon Heritage Register includes this property for the above pictured neoclassical style farmhouse.

2. BACKGROUND RESEARCH

The land now encompassed by the Town of Caledon and Peel Region has a cultural history that begins approximately 10,000 years ago (the Peel Plain once being an ancient lakebed). During most of the 1600s, the Iroquois Confederacy or Five Nations controlled vast portions of Southern Ontario, including the area that is now Peel. Around 1700, the Iroquois, weakened by disease and warfare with the French, were pushed out of the area by the Anishinabeg, a group then migrating southward. The Anishinabeg were given a different name by the European settlers: The Mississaugas.

In addition to their three small reserves located on the Lake Ontario shoreline, the Mississaugas of the Credit held 648,000 acres of land north of the Head of the Lake Purchase lands. The Deputy Superintendent of the Indian Department met with the Mississaugas in October 1818 and proposed that the Mississaugas sell their land in exchange for an annual amount of goods. Chief Ajetance, on behalf of the assembled people, agreed to the sale of lands due to the weakened traditional economy and rapidly declining population following the continuous inflow of settlers. European settlement followed the Ajetance Treaty, No. 19 (1818).

Historically, the subject properties are located in the Former Township of Chinguacousy, County of Peel. The County of Peel was an upper-tier municipality created in 1852, named after Sir Robert Peel, Home Secretary and Prime Minister of Britain. When formed, the County of Peel comprised five townships - Toronto, Toronto Gore, Chinguacousy, Caledon and Albion – and was bounded on the south by Lake Ontario, on the west by the Counties of Halton and Wellington, on the north by the Counties of Wellington and Simcoe, and on the east by the County of York. The Township of Chinguacousy (said to have been named by Sir Peregrine Maitland after the Mississauga word for the Credit River meaning “young pine”) was the largest township in Peel County. It was bounded on the north by Caledon, on the east by Albion and Toronto Gore, on the south by Toronto Township, and on the west by the County of Halton.



The Crown Patentees for Lot 19, Concession 6 East of Hurontario were Peter McIntyre (east half) and Dugald Mackenzie (west half).

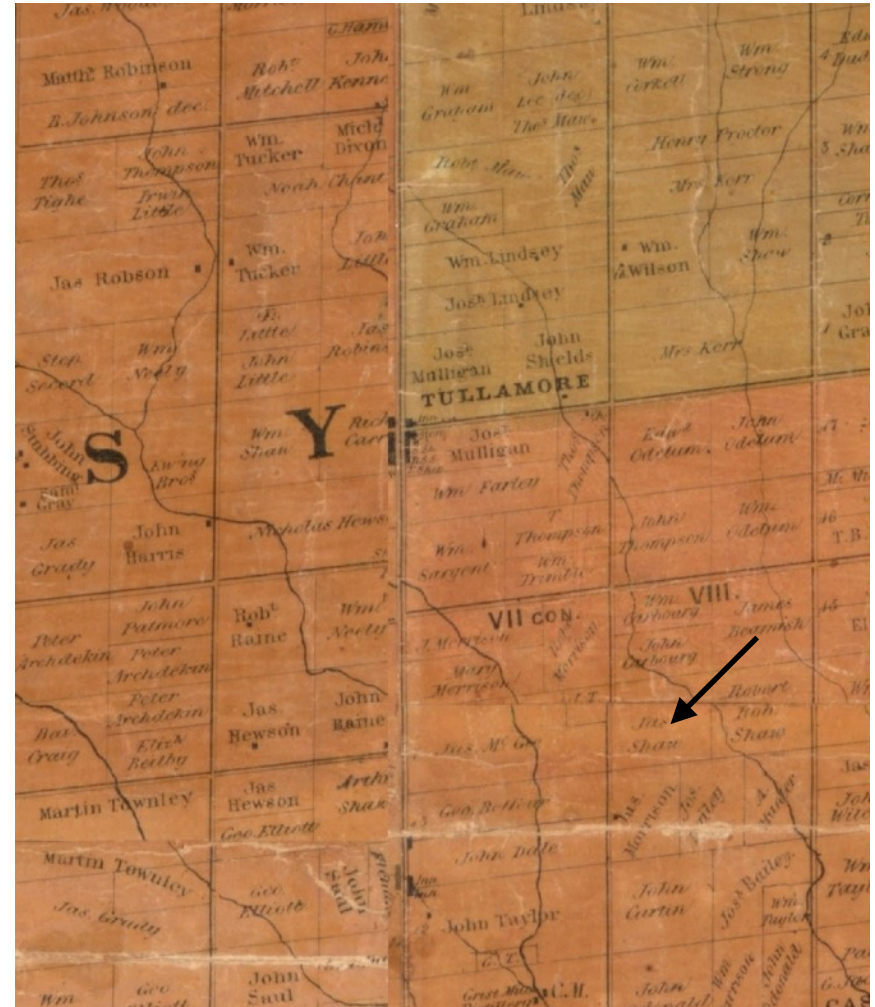
Chinguacousy Township was formally surveyed in 1819 by the surveyor Richard Bristol and the first legal settlers took up their lands later that same year. It was recorded that the first landowners in Chinguacousy included settlers from New Brunswick, the United States, and parts of Upper Canada. A large portion of them were the children of the United Empire Loyalists who came to Canada at the close of the American war and settled near Niagara.

The Township was one of the first surveys to use the “double-front system.” In this system the common unit of concession is the half-lot of 100 acres, with each half of the 200 acre lot fronting on a different concession line road. These half lots are almost square. Concessions run essentially north-south and at every five lots there was an allowance for a side road. Chinguacousy is divided by Hurontario street running through its centre, the concessions numbering east and west from it. The subject property consists of part of Lots 18, 19, 20, Concession 6 East of Hurontario Street.

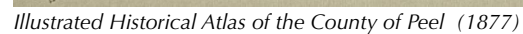
The conditions for land grants included that a cabin, at least 16 x 20, had to be built “in the clear” and occupied for one year. Five acres of land had to be cleared, fenced and planted and half of the road allowance along the front of the lot had to be cleared. The land could not be resold for three years.

The 1827 Assessment Roll shows a total of 27,211 acres of land taken up, with 3,702 acres cleared and the remaining 23,509 acres uncleared. This total of over 27,000 acres, occupied by 1827, represented almost a third of the Township. Only six settlers had by that time managed to build what might be termed a respectable house. Three were assessed for a one storey, square timber house and three for a frame house of less than two storeys. One saw mill and two stores were located in the Township in that year.

These lots have historically been used for agricultural purposes as the rich soils of the Peel Plain were quickly recognised for their agricultural potential and greatly coveted by early pioneers. Once the land had been cleared, farmers benefited from the plain-like flatness and rich soil. According to the 1866 Directory, the Township was second to none in the County as a wheat producer.



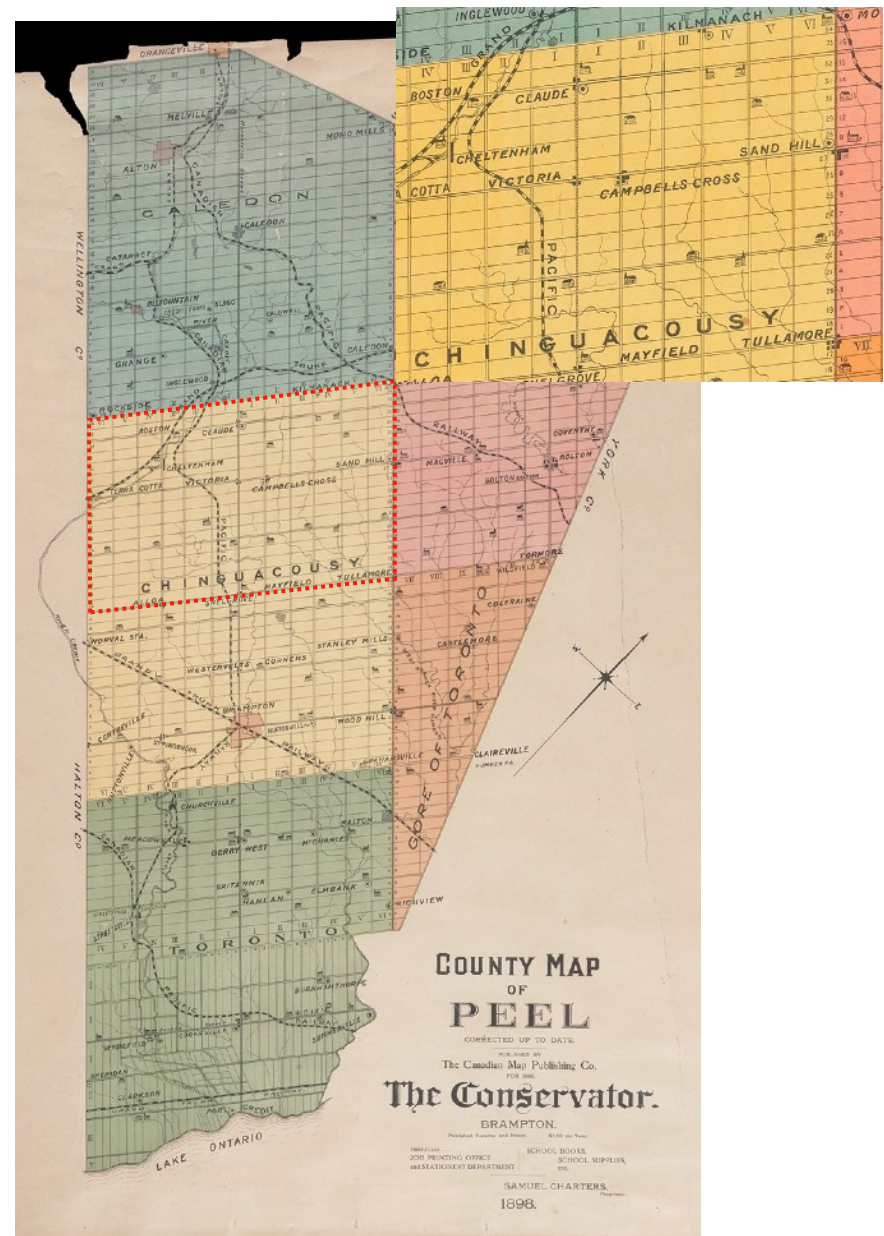
By 1859, the Tremaine Map of Peel denotes the west half of Lot 19 as the property of William Shaw. By 1877 (the date of the Illustrated County Atlas), the property was occupied by Thomas Shaw. Thomas Shaw can be found in the Directory of the County of Peel on Concession 6, Lot 19 as early as 1873-74.



Wheat prices skyrocketed in the mid-nineteenth century, pushed by a chain of events which began with the gold rush of 1849. Prices peaked in 1854-1855. Original log and frame houses were quickly replaced with brick farmhouses – some of which hinted at the wealth the land had brought to the Chinguacousy farmers. By 1877 the township was noted for its beautiful and substantial farm residences and barns. It is significant to note that from 1861 to 1881, the population of Chinguacousy was greater than that of the neighbouring Toronto Township. With its better land and the preeminence of the agricultural economy in those days, the township prospered.

Some lots were subsequently subdivided over the years - interspersed throughout this rural area are the historic settlements of Mayfield, Tullamore, Wildfield, Bolton, Sandhill, Campbell's Cross, Victoria, Alloa, Columbia, Edmonton, Glasgow and Macville. These historic settlements/villages/hamlets were situated at intersections of major early thoroughfares (giving them the name “crossroad” communities) and were important in supporting the surrounding agricultural areas.

The villages closest to the subject property were Tullamore (Toronto Gore Township) and Mayfield (Chinguacousay Township). Situated on the town line between Chinguacousy and Albion (now the intersection of Airport Road and Mayfield Road), the village of Tullamore was ten miles from Brampton and seven miles from Mono Road station. Initially anticipated to become a larger settlement, the village remained quite modest. It contained a large school house, one Church of England, a cabinet factory, several stores and one hotel. The other nearby village of Mayfield was a smaller settlement on the third line, seven miles from Brampton. There was once a brick school house, a general store and post office, a blacksmith shop and hotel.



County of Peel (1898)- Canadian Map Publishing Company

Concession 6 East of Hurontario Street, part of Lots 18, 19, and 20

The township has historically been rural in character. The rural character was created by the concession/lot system which created hundreds of 200-acre lots. The subject property consists of part of Lots 18, 19, 20, Concession 6 East of Hurontario Street.

The Crown Patentees for Lot 19, Concession 6 East of Hurontario were Peter McIntyre (east half) and Dugald Mackenzie (west half).

By 1859, the Tremaine Map of Peel denotes the west half of Lot 19 as the property of William Shaw. By 1877 (the date of the Illustrated County Atlas), the property was occupied by Thomas Shaw. Thomas Shaw can be found in the Directory of the County of Peel on Concession 6, Lot 19 as early as 1873-74. The Collectors Roll for the Township of Chinguacousy for 1879 records Thomas Shaw as the Freehold owner of Concession 6, Lot 19, west half, with 100 acres of land cleared and a value of \$3,800.

The eastern half of Lot 19 was owned by Richard Carr (1830s-1850s) and later Thomas Corberry (1860s-1870s).

The farmhouse on the former Shaw property (west half of Lot 19, Concession 6 East of Hurontario Street) is a good example of mid-nineteenth century Ontario rural vernacular architecture. Conforming to the typical rural pattern, the house fronts onto the concession line (originally 5th Line East).



Caesar-Ravenscroft House.



James McNeece House.

Other nearby listed properties of similar vintage include 13470 Torbram Road, 14324 Torbram Road, 14524 Torbram Road, 14676 Torbram Road, 14966 Torbram Road and 14681 Torbram Road.

The farmhouse at 12245 Torbram Road has a medium-pitched side gable roof with a dentilated wood cornice. Architectural features include a centre entrance flanked by large multi-pane, six-over-six sash windows. Decorative dichromatic brick patterning accentuates the window heads, corner quoins and the frieze beneath the roof cornice. In this manner, the building is similar to several other farmhouses in the area, notably the nearby Kennedy-Breen House (5962 Old School Road/Part Lot 23, Concession 6 East of Hurontario Street), which is a fine example of an early dichromatic brick house, a style of brick that was popular in Peel County in the early 1860s to late 1870s.

The farmhouse at 12245 Torbram Road is estimated to date c1850-1874 as noted in the Town of Caledon's listing of the building under Section 27 (1.2) of the Ontario Heritage Act. The approximate date is based on stylistic characteristics of the extant building, being similar to the one and one-half storey, red brick, medium gabled Caesar-Ravenscroft House, which dates c1850s. It is also stylistically similar to the nearby James McNeece house (14905 Bramalea Road/Part of West half of Lot 32, Concession 5 East of Hurontario Street), which dates c1860. And, as it is likely the building denoted on the 1877 County Map, the farmhouse at 12245 Torbram Road most probably post-dates 1877.



Kennedy-Breen House

3. HERITAGE STATUS

The site includes two non-designated properties that are listed on the Town of Caledon, under section 27 (1.2) of the Heritage Act. On the western portion of the site, a 1-1/2 storey neo-classical style farmhouse, built c.1850-1875 at 12245 Torbram Road. Located on the north-eastern part of the site is 12542 Airport Road and is noted as a neoclassical style farmhouse with a newer addition on the west side. This latter property is not proposed for development in the subject subdivision plan.

The properties were added to the Heritage Register on May 4th 2020 through Council Resolution #2020-91. This was undertaken through a batch listing of the remaining 509 properties from the Town's Built Heritage Resource Inventory (BHRI).

This Register is an administrative and documentation tool used to assist in the identification and on-going preservation of significant heritage resources within the land use planning and permit application process.

Before being added to this Register, the cultural heritage value of a given property is assessed, using a set of provincially regulated criteria, that determines overall merit and significance. Based on the criteria, the heritage attributes and overall cultural heritage value of each resource is evaluated and assigned a score. The Caledon Heritage Board must then formally recommend each new addition to the Register, pending confirmation by the Planning, Design and Development Committee and Caledon Town Council.

3.1 Adjacencies

'Adjacency' in this context refers to lands that are directly across from and near to a property on the Heritage register.

While there are no heritage resources located directly adjacent to the property at 12245 Torbram Road and 12542 Airport Road, given that these are located on the development site - there are a number of nearby structures also included on the register.

Nearby:

- 12380 Torbram Road (1875-1899) - Late Victorian Gothic style farmhouse with a red-and-buff brick exterior.
- 12729 Torbram Road (1875-1899) - High Victorian Gothic style farmhouse with a red-and-buff brick exterior
- 13470 Torbram Road (1850-1874) - Neoclassical style farmhouse with a red-and-buff brick exterior.
- 13523 Torbram Road (1875-1899) - high Victorian gothic style farmhouse with a synthetic exterior

4. CONDITION REVIEW

4.1 12245 Torbram Road

GBCA visited the site in June 2021 on to conduct a high level visual review of the building's exterior. This visual review was limited to the exterior walls of the building. Any alterations, damage or improvements occurring after the date of assessment have not been updated as part of this report, however with subsequent visits in July 2021, February 2022 and June 2023 to monitor the condition.

GBCA obtained only limited access to all exterior parts of the building facades (north, east, south and west elevations). Assessment was limited from the ground level only as no boom lift was utilised for higher areas such as the gable, second-floor windows, soffit and roof. Furthermore, approximately one half of the building's elevation is obscured by vegetation overgrowth.

The visual review of the building revealed progressing structural deterioration throughout the building. This is seen most notably through numerous instances of pronounced step cracks along the north elevation, along with other localised cracks in other portions of the east and west walls. Furthermore, there is a gap in excess of 1" at the gable, and sagging/deformation of lateral support members of the roof. The presence of these issues, viewed together, suggests that the structure of the exterior walls is severely compromised.

Side wall cracks typically correspond to the "bending" or rotation of the structure due to soil conditions, compounded by settlement cracking due to the deformation of the structure, will continue to progress if the structure is not stabilised.

Based on these initial impressions and evidence of immediate structural concerns, GBCA recommended that a Structural Engineer provide a more fulsome review. These findings are available in Appendix III.



Numerous instances of pronounced step cracks ($\pm 1"$ and up to 2" or more) along the north elevation, along with other localized cracks in other portions of the east and west walls



Parging applied to the masonry on the east elevation next to open joints. Ingress water damage and deterioration is likely present in these areas.



12245 Torbram Road, 2021 (GBCA). West Elevation. Top left window added at a later date. The central door opening is inaccessible, and side window of the lower addition is closed in.



12245 Torbram Road, 2021 (GBCA). North Elevation. Blank wall with closed window openings and buttress to the left. Open joints in the masonry are noticeable from a distance. The buff-brick quoins are clearly visible on this elevation.



12245 Torbram Road, 2021 (GBCA). South Elevation. Inaccessible elevation due to tree and foliage overgrowth.



12245 Torbram Road, 2021 (GBCA). East Elevation (rear), showing current entrance to house with a brick buttress extending from the elevation, and structural deflection noticeable at gable ridge.

4.2 12542 Airport Road.

GBCA visited the site in June 2023 to conduct a high level visual review of the building's exterior. This visual review was limited to the exterior walls of the building. Any alterations, damage or improvements occurring after the date of assessment have not been updated as part of this report.

Assessment was limited from the ground level only as no boom lift was utilised for higher areas such as the gable, second-floor windows, soffit and roof.

The visible north, south and east brick elevations have been whitewashed, with a two-storey garage addition on the west clad in wood. The front elevation features a symmetrical 3-bay plan with central plan, with 6-over-6 windows on either side with flat voussoirs and matching chimneys above. The entranceway features side lights, transom. The gable above, likely a later addition, is asymmetrical in relation to the neoclassical composition.

Overall, the structure is in good repair with no major deficiencies noted.



Non-original storm door with raised entrance. Side lights, transom flanked by 6-over-6 windows in good condition.



A view from the south elevation of the neoclassical style farmhouse with central plan, and a newer two-storey garage addition to the west.



A view from the east elevation with randomly arranged windows. Evidence of potential ingress water at foundation is present given loss of white-wash.



At the random rubble foundation, efflorescence is present.



A view of the 6-over-6 ground floor windows. Evidence of potential ingress water at foundation is present given loss of white-wash.



A view of the north elevation eaves, in good repair.

5. ASSESSMENT OF PROPOSED DEVELOPMENT

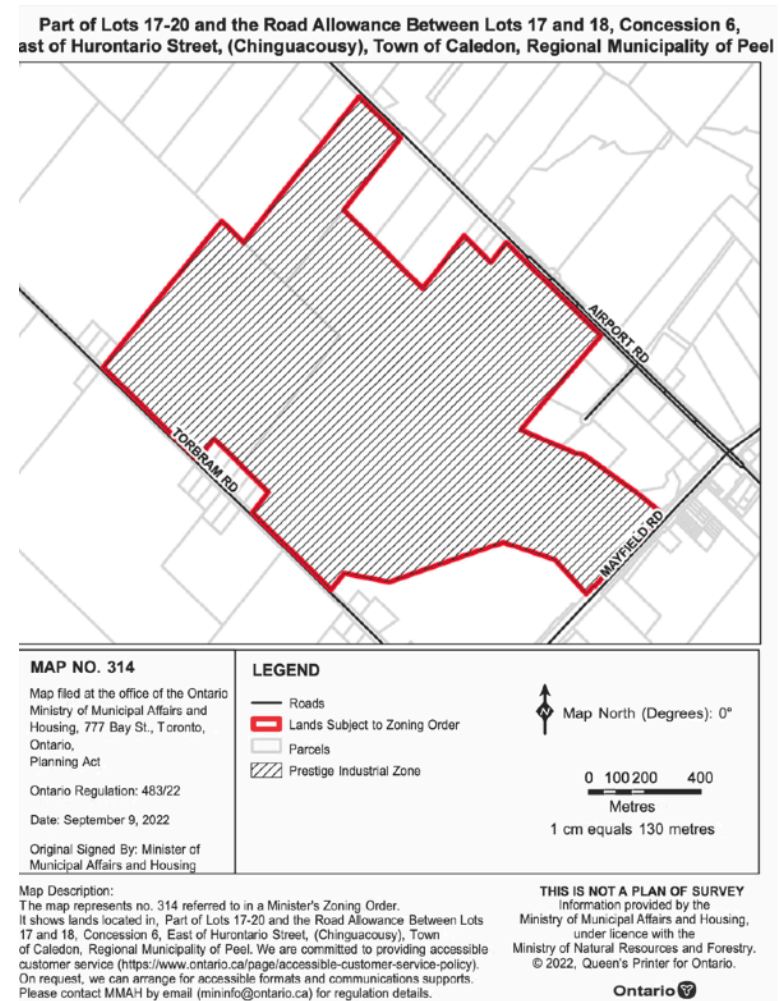
5.1 Description of Proposed Strategy

The owner is proposing to develop a new commercial/warehousing centre on lands that are currently and have been historically used for agriculture purposes. The proposal seeks to rezone and reconfigure the existing farmland to construct a warehouse complex featuring 12-buildings. Designed by Turner Fleischer Architects, at 12-metres in height.

This extensive campus will include a new subdivision with new road network extending from Torbram Road to the north, Airport Road to the east, and Mayfield Road to the south. The development is flanked on either side along Mayfield road by two swamp areas that are part of the Greenbelt lands.

Development of the west side of the site will be the first phase (Phase 1 of 4) for the development, the heritage resource at 12245 Torbram Road is currently located here. This area is immediately adjacent to the 63.66 acre area designated as greenbelt/swamp land to the south-east. Industrial uses will terminate to the west of 12542 Airport Road which is a Non-designated property listed under section 27 (1.2) of the Heritage Act. The extensive tree plantings will screen the industrial uses from the property that includes several outbuildings and an altered neoclassical style farmhouse.

As it relates to 12245 Torbram Road, although it is acknowledged that relocation is not typically favoured, in this particular case, relocation can be seen as a mitigation strategy for the building's long-term preservation. Relocation to a properly designed foundation will ensure the conservation and preservations of the building. Prior to relocation, the building will require restoration and stabilisation. The relocation is proposed to occur in a single stage. The site (to be confirmed) receiving the house will have to be partially excavated and new foundations provided. Once this new site is prepared with its new foundations, the farmhouse perimeter will be partly excavated to install necessary moving supports and wheeled bogies. Final restoration work on the heritage building would be carried out after it is moved to its final location.



A Minister's Zoning Order (MZO) was approved on September 09, 2022. The MZO regulates the type of industrial uses permitted on-site and zoning requirements concerning the lot size, building area, building setbacks, building height, landscaping requirements, and specific requirements for the open storage of goods and materials

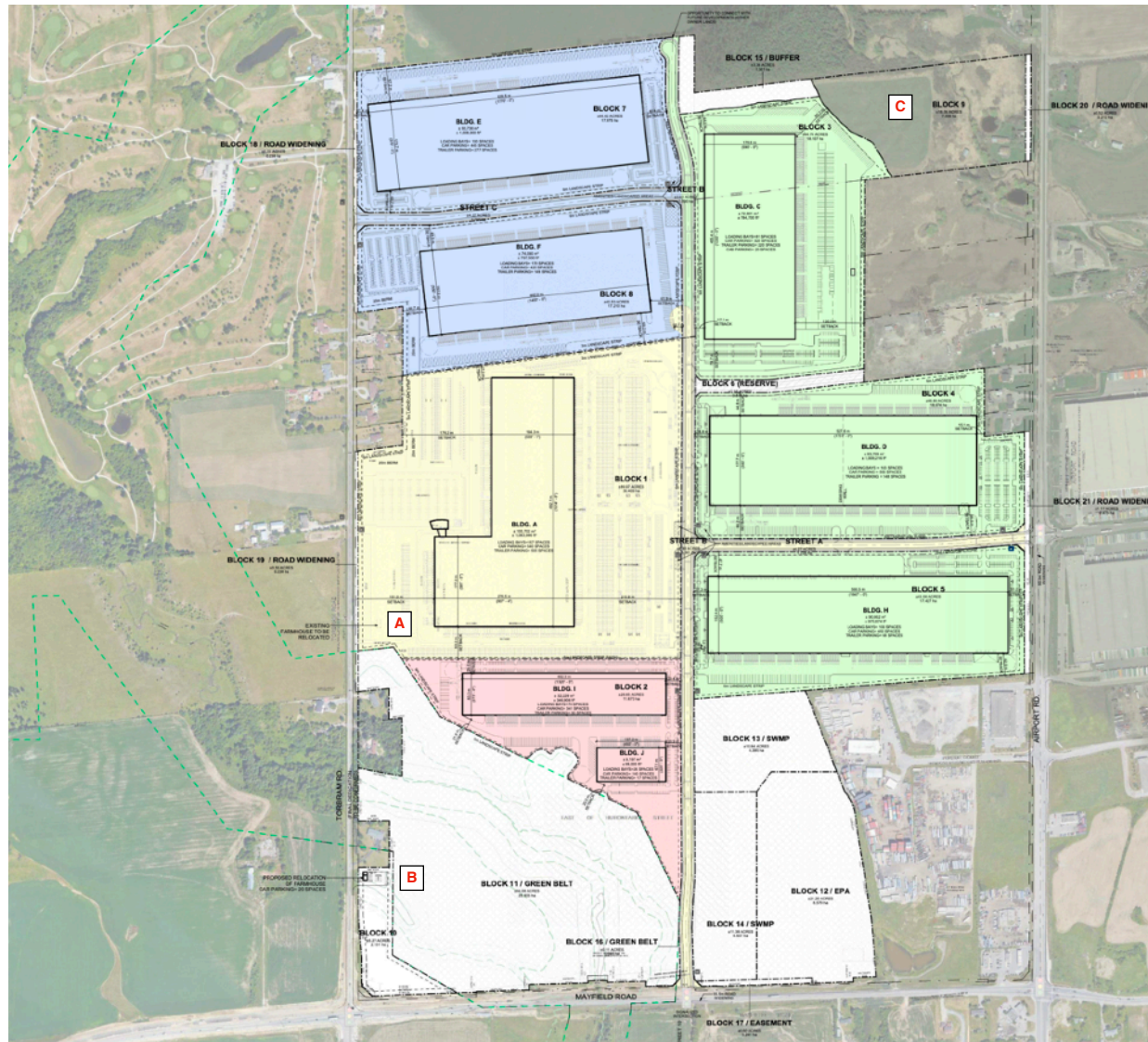
5.1.2 Updates since July 2021 proposal.

As part of Ontario Regulation 438/22, the lands have been subject to the Minister's Zoning Order (MZO) that was approved on September 09, 2022. The MZO regulates the type of industrial uses permitted on-site and various specific zoning requirements about the lot size, building area, building setbacks, building height, landscaping requirements, and specific requirements for the open storage of goods and materials. The general location of the development remains the same along Airport Road, Torbram Road and Mayfield Road in the Town of Caledon, beside the Tullamore Settlement Area. The revised boundaries of the subject lands are illustrated described in more detail below.

The original cruciform shaped road network bisecting the site has been adjusted, with notable changes to the building types north of Block 4, on the west half of lot 18 and north of Building H on the east half of the lot. The previous north-south road is extended from Mayfield Road to the northern extent of the subject lands, with east and west connections from Airport Road and Torbram Road respectively.

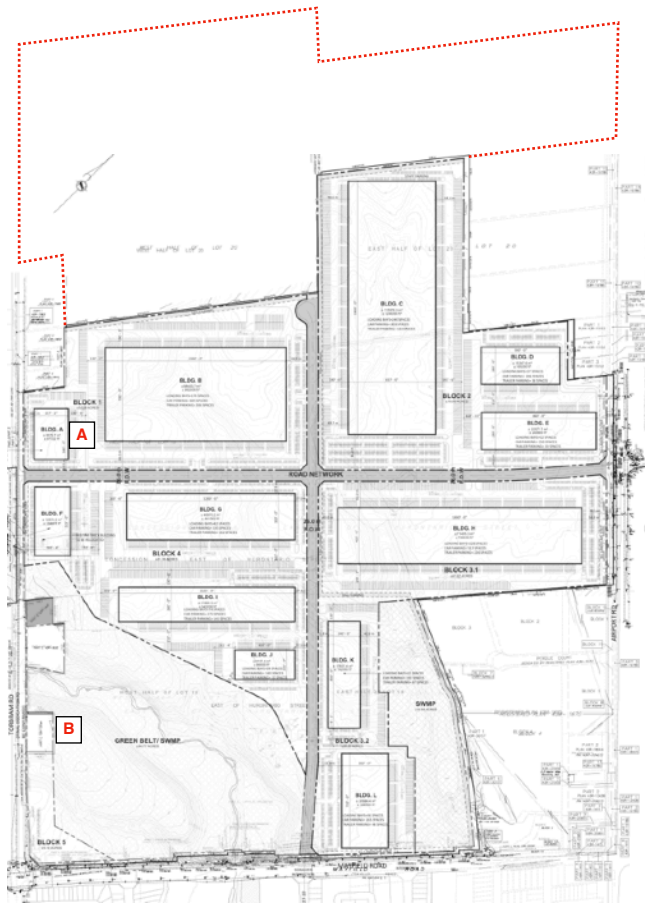
Block 4 and 3.1 have remained unchanged. Two smaller structures in lands designated as swamp (Buildings K and L) have been removed in the revised scheme.

A total of approximately 549,079 square metres of industrial, warehouse, and distribution space will be contained within these buildings, and storm water management blocks have been established at the southwest part of the subject lands. Adjacent to this, at Block 10, is where the farmhouse at 12245 Torbram Road will be relocated.



- A) 12245 Torbram Road.
- B) Relocation site for 12245 Torbram Road.
- C) 12542 Airport Road.

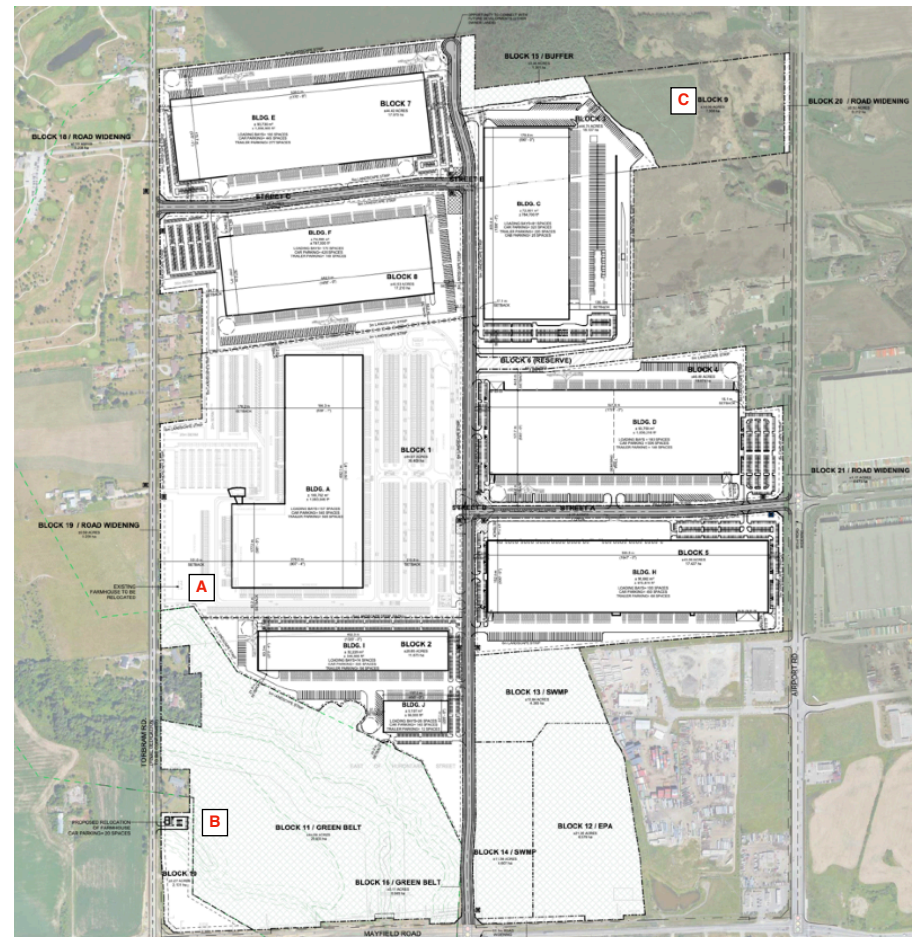
Extract of the proposed development drawings, showing the overall site plan (by Turner Fleischer Architects). Highlighted in green is the western part of the site above the Greenbelt/swamp where the subject heritage resource is located. The first phase of the project will be located at Building A, which is in the vicinity of 12245 Torbram Road.



2021

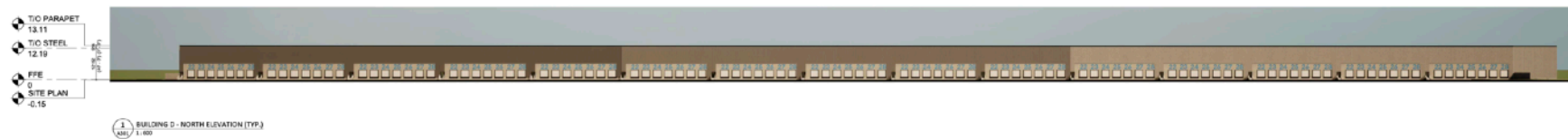
Overlay of property survey with proposed development. The two images show the change in the size of the development, with the addition noted in a dashed red line above.

The site has been expanded and rezoned for a proposed industrial park development. Additional lands to the north also include an extension of the previous scheme with industrial buildings, new roads, open spaces and reconfiguration of building structures and associated parking spaces.



2023

- A) 12245 Torbram Road.
- B) Relocation site for 12245 Torbram Road.
- C) 12542 Airport Road.



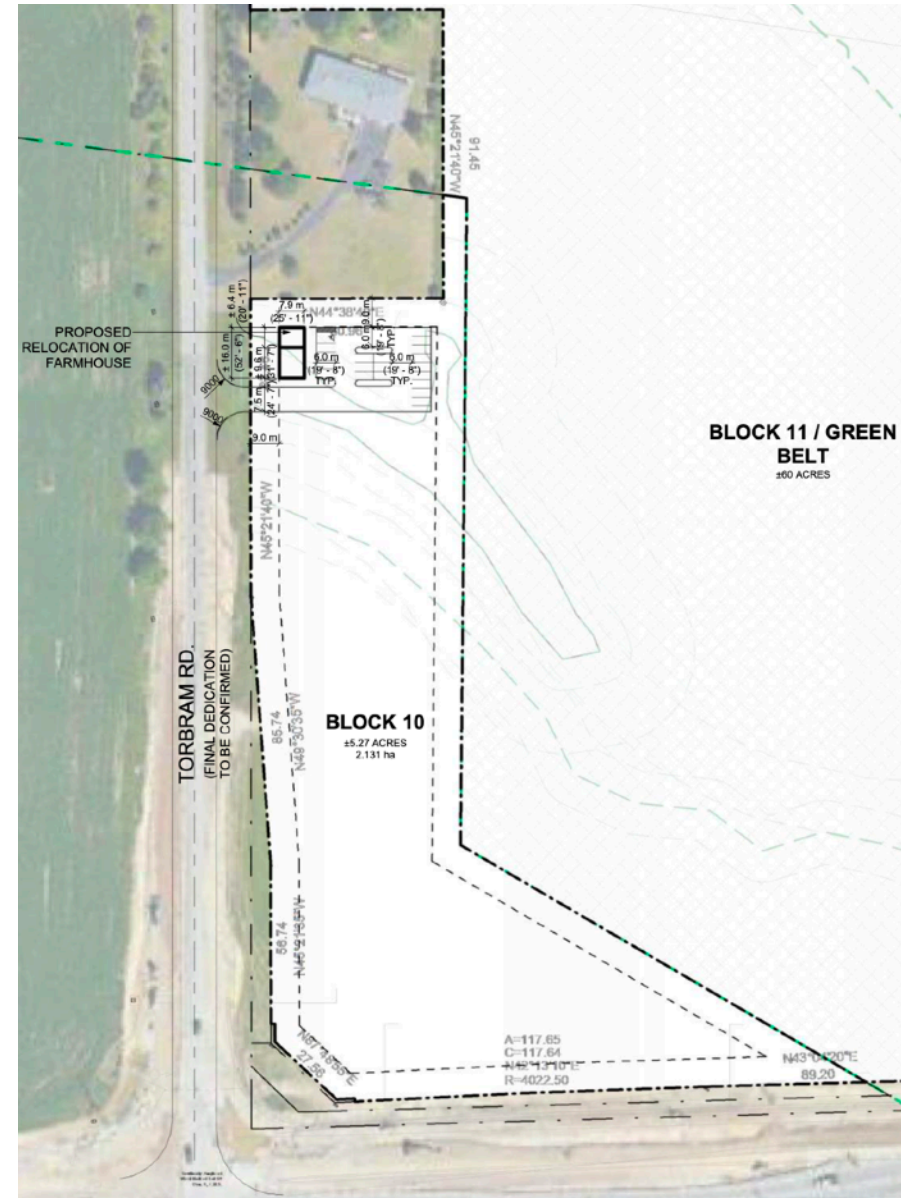
Elevations for Building D, a 12m high logistics facility located on Block 4. This structure will be constructed as part of Phase 2 of the overall development. The design of adjacent structures have been conceived in a similar manner. (Turner Fleischer)

5.1.3 Proposed relocation site for 12245 Torbram Road

The farmhouse at 12245 Torbram Road has been proposed for location to the north part of Block 10, as noted in the image on the right. The relocated structure will maintain its original orientation with Torbram Road, including a set-back of 9-meters from the northern and western property lines. To the rear (east) of the approx. 16m x 7.9m structure, parking will be located to support a range of uses. Block 10 is a boot shaped stretch of land that is bound by Torbram Road to west, and Mayfield Road to the south. To the west it is adjacent to Block 11, which has been identified as Greenbelt lands.

At this time, the client has proposed to adaptively re-use the farmhouse for a purpose that is compatible with existing zoning limitations on the site (residential). The proposed concept study to the right shows the potential range of uses, despite the constrained site. Specific amendments may be required at a later date to address adjustments to the proposed use and in light of provincial policies.

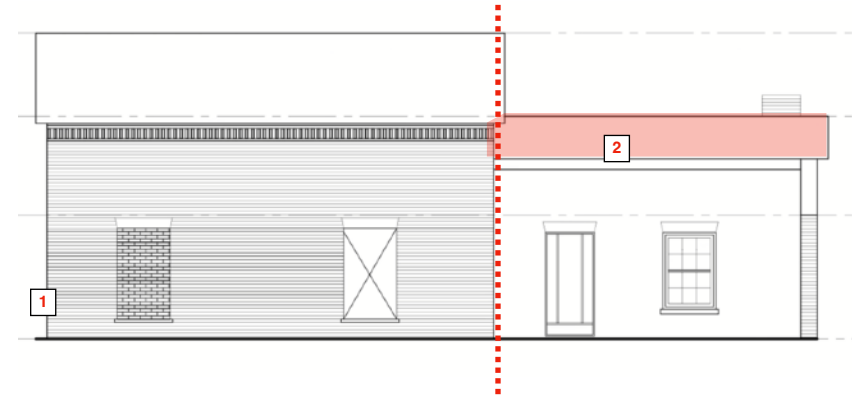
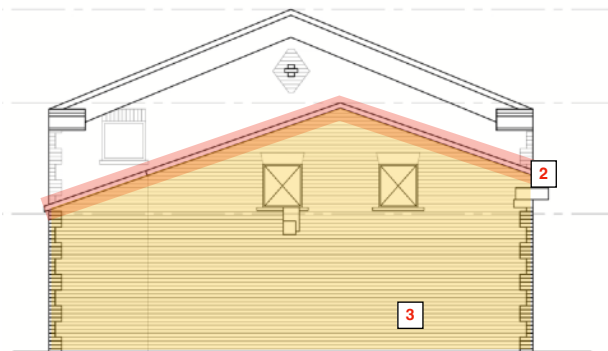
The relocation is proposed to occur in a single stage. The site receiving the farmhouse will have to be partially excavated and new slab provided. Once this new site is prepared with its new foundations, the House will be partly excavated so to install necessary moving supports and wheeled bogies. The building will require substantial stabilization before such a move. Restoration work on the heritage building would be carried out after it is moved to its final location on Block 10.



5.1.4 Heritage Protection for 12245 Torbram Road

As noted in the structural report prepared by Ojdrovic Engineering, dated July 19 2021, a full assessment of repairs to cultural heritage resources is required before a relocation strategy is approved and the recipient site prepared. Details of this will be described in a forthcoming Conservation Plan, along with strategies to deal with repairs and consolidation prior to construction works.

The above noted structural report (included in Appendix III) requests that the proponent shall prepare a maintenance schedule to ensure, among other items, that the bushes and vines covered the farmhouse are cleared away, and repairs made to the western portion of the roof.



- 1) Clear overgrowth from all elevations, and stabilize structure;
- 2) Repairs required to roof;
- 3) Extensive masonry repairs required;

5.1.5 General Procedures for Relocation

While every building is different and each project must be accessed on a case-by-case basis, there are general procedures that are followed in the moving process. The engineer working for the moving company will factor into their calculations the building dimensions, weights and materials (and their characteristics).

As noted above, the building will require substantial preliminary restoration and stabilisation prior to preparation for the move. Once this is done, the area around the structure would be excavated and a designed crib, consisting of lengthwise and cross beams, would be installed in the basement of the house to support the structure. Then, the area at grade will be cut through to the separate the house from its foundation. Extensions to the carrying frame will be inserted below the walls and secured to the exterior. Windows will be framed with solid panels to prevent “racking”. Additional cross bracing may be installed in the interiors to reduce potential for structural deformation. The engineer would factor in the building’s dimensions, weight, and materials. The frame carrying 12245 Torbram Road would then be raised by hydraulic lifts or jacks and lowered onto hydraulically self-levelling wheels or “bogies”.

While the above work is under way, a supporting bed of engineered will be created between the original and final locations. At the same time, a new foundation will be created for the house then filled to allow the house to roll over it into position. The steel support/crib would be used as the base of the building during the move until the perimeter of the house is lowered onto the foundation, then removed with the bogies. From then , gaps in the foundation required for the crib members will be filled and final restoration work will commence.

The best route to the new location – in this case to the south-east of its present site, onto greenbelt lands – would be determined by the building mover who would be responsible for analysing telephone cables, wires, utility poles and topography. The proper authorities would be consulted in relation to permits for the removals or reinforcement of any elements.

5.1.6 Technical Issues of relocating 12245 Torbram Road

Moving heritage buildings, if not well considered, could result in physical endangerment of the resource. In any project that proposes relocation of heritage buildings, a detailed feasibility study would be undertaken and approved by a structural engineer and the best method for moving determined. The engineer would be employed by a qualified moving contractor.

GBCA is able to provide examples of successful relocations of heritage buildings. In 2008, GBCA worked with Tridel Developments for the relocation of the Cooper Mansion, a 950 ton, three-storey Second Empire brick building. Laurie McCulloch undertook this move. In 2009, GBCA worked with Pinnacle Developments for the relocation of a two-storey Georgian brick building, undertaken by Danco. Both of these relocations took place in urban environments, in downtown Toronto.



Richard West House relocation in downtown Toronto (2009)



James Cooper Mansion relocation in Toronto (2008)

5.2 Impacts on Heritage Resources

An assessment of possible effects from the proposed development on the heritage properties is presented on the following page which lists possible effects. In developing new buildings that introduce new scale, typology and massing that will attenuate the existing landscape, and contextual relationships.

The inclusion of the properties in the Town of Caledon's Municipal Register of Cultural Heritage Resources is a clear statement that the City would like to see the heritage attributes of these properties preserved. No evaluation of heritage value was deemed necessary for this HIA as it is understood it has been assessed through listing, with the conclusion that it meets Ontario Regulation 9/06.

The primary changes to the cultural heritage value arising from this development is the development of a large industrial campus in formerly agricultural lands, which will result in the loss of a large portion of the context in this area. Part of this loss is mitigated by appropriate lotting patterns and setbacks of relocated structures, vegetation buffers and plantings that retain enough of the existing lands to retain the heritage attributes of both 12245 Torbram Road and 12542 Airport Road.

A proposed building relocation will result in the following impacts on the property.

- New lot pattern and change of use to adjacent lands;
- Relocation of 12245 Torbram Road to a new site

An assessment of possible effects of the proposed development on the property is presented opposite. The table lists possible effects based on the Ontario Heritage Tool Kit - Heritage Resources in the Land Use Process.

While there is no means of reducing the impact from moving the house in order to prepare the lands for development, the preservation of 12245 Torbram Road and the conservation of the majority of the heritage attributes is important to acknowledge this existing heritage resource on this site. The relocation will ensure that the setting of the house is

consistent with its current situation. The sequencing of events will be discussed in more detail through a Conservation Plan.

The building will require substantial stabilisation before relocation, which is also an opportunity to provide necessary intervention and provide long-term conservation of the heritage resource. Restoration work on the heritage building would be carried out after it is moved to its final site.

5.3 Mitigation Strategies

5.3.1 Physical relocation from current Torbram Road location

The current location of 12245 Torbram Road resulted from early settlement patterns of the concession/lot system dating back to 1819, together with farming activities that would occur in this rural setting.

It is acknowledged that relocation is not typically favoured. However, in this particular case, permanent relocation can be seen as a mitigation strategy for the building's long-term preservation, and for the remediation of the site. While every building is different and each project must be assessed on a case-by-case basis, there are general procedures to be followed in the moving process. Details of the relocation will be provided under a separate Conservation Plan.

Mitigation strategies/Alternate Strategies:

- Leaving the brick building for a long period of time during construction poses risks to the stability of this fragile structure. Therefore, a permanent move is seen as the best course of action.
- Although integrating the existing farmhouse into the new development is the preferred option, this poses the risk of further isolating the house from its setting given the scale of the warehouse buildings and corresponding vehicular activities.

5.3.2 New lot pattern and location

The proposed assembly of properties will see a change in the lot pattern and the proposed relocation of the existing building footprint to the south of its present location. The proposed lot will front Torbram Road, which will allow the character defining attributes of this listed property to be viewed from the public realm. In addition to this, the benefits of its subsequent conservation are clear.

The *Standards and Guidelines for the Conservation of Historic Places in Canada*, heritage conservation requires the retention of an appropriate visual setting and other relationships that contribute to the cultural

significance of the place. New construction, demolition, intrusions or other changes that would affect the setting or relationships are not encouraged.

In the current relocation scenario, the front elevation of the farmhouse will be oriented to the public realm.

Mitigation strategies/Alternate Strategies:

- The visual setting of Torbram Road relates to the cultural significance of the farmhouse. The proposed reuse of the house at 12245 Torbram Road does not necessarily need to be associated with the development to the east. However, locating the house closer to the road and maintaining a use that locates parking and services to the rear of the building will allow for the full three-dimensional view of the building to be perceived in an unobstructed manner from the public realm.

Possible Effect - (from Ontario Heritage Tool Kit-Heritage Resources in the Land Use Process) Assessment (Considered Alternative Development and Mitigation Measures)
<p>Destruction of any, or part of any, significant heritage attributes or features</p> <p><i>No character defining elements will be removed as part of the proposed alterations, and development strategy.</i></p>
<p>Alteration not sympathetic, or incompatible, with the historic fabric and appearance</p> <p><i>The land assembly and proposed industrial development will move the extant farmhouse in order to prepare lands for a new development. The house will maintain its original quality and contextual fit with the present site as a result of being moved to a new location along Torbram Road.</i></p>
<p>Shadows created that alter the appearance of a heritage attribute or change the viability of an associated natural feature or plantings, such as a garden</p> <p><i>Given the scale of the development and the 12m building heights, Shadow studies are not relevant to the proposed building relocation strategy.</i></p>
<p>Isolation of a heritage attribute from its surrounding environment, context or a significant relationship</p> <p><i>The visual setting of Torbram Road is considered part of the character, and relates to the cultural significance of the house at 12245 Torbram Road. The building relocation will not isolate the existing farmhouse from its former context. It has been proposed to be moved to an area immediately to the south within the greenbelt, and will maintain its original orientation with Torbram Road. Mitigation strategies are listed in Section 5 of this report. Furthermore, it is recognized that the construction of the industrial facility will result in the loss of open agricultural space, however the proposal has been sensitive to the heritage resources on the site including 12245 Torbram Road and 12542 Airport Road.</i></p>
<p>Direct or indirect obstruction of significant views or vistas within, from, or of built and natural features.</p> <p><i>12245 Torbram Road will be relocated a site along Torbram Road, and will not impact any identified views or vistas. No existing vistas at 12542 Airport Road have been defined, however the retention of existing plantings and trees will retain the existing quality of the property.</i></p>
<p>A change in land use (such as rezoning a church to a multi-unit residence) where the change in use negates the property's cultural heritage value</p> <p><i>The land assembly will change through merging three large lots into one subdivision, complete with 12 new industrial buildings and a newly defined road network.</i></p>
<p>Land disturbances such as a change in grade that alters soils, and drainage patterns that adversely affect a cultural heritage resource, including archaeological resources</p> <p><i>Land disturbances will not occur in any of the designated greenbelt/swamp areas that occupy the subject site and associated heritage resources.</i></p>

6. CONSERVATION STRATEGIES

A variety of options are typically available for the redevelopment of sites with buildings of heritage value due to proposed developments, and options must be weighed with other decisive factors. The most common are time and costs. In the case of 12245 Torbram Road, a number of preliminary options have been considered and are discussed.

In this instance, 12245 Torbram Road, which is of heritage value, is at risk if left in its current location. Therefore, the house will be preserved, rehabilitated, and restored. Details regarding repair, stabilisation, preservation and long term conservation will be explored in a detailed Conservation Plan at a later date.

Preservation involves the protection, maintenance and stabilisation of the existing form, material and integrity of a historic place or of an individual component, while its heritage value is protected.

Rehabilitation is the sensitive adaptation of an historic place or of an individual component for a continuing or compatible contemporary use, while protecting its heritage value. In this instance, the heritage building will be rehabilitated and with a new use related to the proposed development.

Restoration is the revealing, recovering or representing the state of a historic place or of an individual component as it appeared at a particular period in its history, as accurately as possible, while its heritage value is protected. In this instance, 12245 Torbram Road will have reinstated its original exterior finishes by removing any non-original cladding, restoring masonry, and reinstating original window designs and details.

6.1 International Standards for Heritage Conservation

Beginning in 1931, various heritage charters have been drafted and subsequently recognised internationally as part of a consistent and objective approach to issues being faced by all countries in addressing heritage conservation.

The concept of setting was addressed in the 1960s in the Venice Charter, the seminal text that underpinned the foundation of ICOMOS (the International Council of Monuments and Sites). Many national charters, including Canada's Appleton Charter (1983), further developed the importance of setting.

The **Venice Charter** is the first of the modern charters (1964) to tackle heritage conservation, codifying acceptable universal principles and practices for the conservation of historic monuments. Of particular relevance for this assessment are the following Articles:

ARTICLE 1: *The concept of a historic resource embraces not only the single architectural work, but also the urban or rural setting in which is found the evidence of a particular history.*

ARTICLE 6: *The conservation of a monument implies preserving a setting which is not out of scale. Wherever the traditional setting exists, it must be kept. No new construction, demolition or modification which would alter the relations of mass and colour must be allowed.*

ARTICLE 13: *Additions cannot be allowed except in so far as they do not detract from the interesting parts of the building, its traditional setting, the balance of its composition and its relation with its surroundings.*

The **Appleton Charter** was established in the Ottawa area in 1983 and adapted the principles of the Venice Charter to a Canadian context. This charter recognises that the sound management of the built environment is an important cultural activity.

C. Principles (Protection): *Any element of the built environment is inseparable from the history to which it bears witness, and from the setting in which it occurs. Consequently, all interventions must deal with the whole as well as with the parts.*

While the primary option for the building is its relocation, the relocation will be to a setting within a greenbelt area that remains part of the property. There will be minimal impact on the relative setting of the house if relocated with the new location in tune with the original setting of the house vs. the proposed new development context.

6.1 Exterior conservation and stabilisation

The building will require substantial reconstruction and stabilisation before it is repurposed. Anticipated actions once the house at 12245 Torbram Road is stabilised include:

- Repair, replacement and painting of windows, and sills where required;
- Masonry repairs and cleaning;
- Reinstall blocked in window openings;
- Repairs to exterior stone masonry;
- Repairs to exterior woodwork – fascia, soffits, etc.;
- Removal of unsympathetic cladding;
- Repairs or replacement to - metal doors, eaves and troughs;
- Replacement of visible roof area with new high quality shingles.

As the farmhouse is proposed to be relocated, specifications and scope of work for moving the building will be prepared by a qualified building mover in conjunction with a structural engineer. Refer to the International Association of Structural Movers and Ontario Structural Movers Association for qualified companies. Since the practice of professional engineering is regulated by provincial laws, qualified engineering advice must be obtained from a licensed professional engineer with specific experience in the relocation of masonry buildings.

6.2 Interior conservation

The mitigative options for the interior spaces and features will need to be worked out in conjunction with the project architect as the plans are developed in more detail.

At this time, it is possible to assert that the proposed re-use of the house at 12245 Torbram Road does not necessarily need to be associated with the development to the east. Attention will need to be paid to the required program needs, with the plans being adjusted according to existing building, if need be. Consideration of new use should begin with respect for existing and traditional patterns of the layout.

7. CONCLUSION

The proposed land assembly, and site preparation for a future development will result in a number of changes to the context value, particularly to the north and east of the listed property at 12245 Torbram Road. The heritage resource on this property will be relocated. Relocation proposes to keep the building in a greenbelt area on the existing property, with views of the primary elevations from the public realm unobstructed. Integrating the existing farmhouse into the new development poses the risk of further isolating the house from its setting given the extensive scale of the warehouse buildings and corresponding vehicular activities.

The proposed land assembly and relocation will have impacts on the heritage attributes of the existing farmhouse at 12245 Torbram Road. Although relocation is not typically favoured, in this case relocation can be seen as a mitigation strategy for the building's long-term preservation, re-use and new visibility from the public realm. These elements along with mitigation strategies are discussed in more detail throughout the report. While the proposed revisions to the subdivision plan extend to 12542 Airport Road, a non-designated property featuring a neoclassical style farmhouse, there will be no development here. Therefore, impacts heritage properties adjacent to the proposed industrial park development will be minimal.

Due to existing structural concerns, the stabilisation of the house is a necessary intervention for immediate and long-term conservation. The proposed conservation strategy will involve the preservation, rehabilitation and restoration of the house. Details regarding repair, stabilisation, preservation and long term conservation would be explored in a detailed Conservation Plan at a later date.

By considering the recommendations contained in this report, the proposed development on the Tullamore development lands and the associated heritage resource at 12245 Torbram Road can be accomplished in accordance with accepted conservation principles.

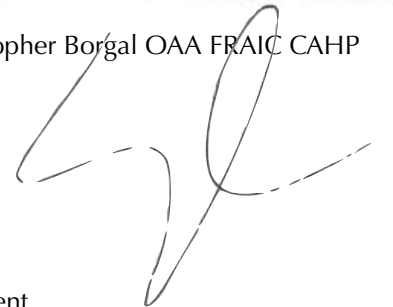
In our view, and in light of mitigating strategies to reduce impacts to heritage properties, this proposal balances demands for *intensification* with

those of *heritage preservation* in a manner that allows both objectives to be appreciated as a part of a complex and changing urban environment.

8. CLOSURE

The information and data contained herein represents GBCA's best professional judgment in light of the knowledge and information available to GBCA at the time of preparation. GBCA denies any liability whatsoever to other parties who may obtain access to this report for any injury, loss or damage suffered by such parties arising from their use of, or reliance upon, this report or any of its contents without the express written consent of GBCA and the client.

Christopher Borgal OAA FRAIC CAHP

A handwritten signature in black ink, appearing to be 'CB', written over a faint, circular stamp.

President
Goldsmith Borgal & Company Ltd. Architects

9. SOURCES

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APPENDIX I

Standards and Guidelines for the Conservation of Historic Places in Canada

THE STANDARDS

The Standards are not presented in a hierarchical order. All standards for any given type of treatment must be considered, and applied where appropriate, to any conservation project.

General Standards for Preservation, Rehabilitation and Restoration

1. Conserve the *heritage value* of an *historic place*. Do not remove, replace or substantially alter its intact or repairable *character-defining elements*. Do not move a part of an historic place if its current location is a character-defining element.
2. Conserve changes to an *historic place* that, over time, have become *character-defining elements* in their own right.
3. Conserve *heritage value* by adopting an approach calling for *minimal intervention*.
4. Recognize each *historic place* as a physical record of its time, place and use. Do not create a false sense of historical development by adding elements from other historic places or other properties, or by combining features of the same property that never coexisted.
5. Find a use for an *historic place* that requires minimal or no change to its *character-defining elements*.
6. Protect and, if necessary, stabilize an *historic place* until any subsequent *intervention* is undertaken. Protect and preserve archaeological resources in place. Where there is potential for disturbing archaeological resources, take mitigation measures to limit damage and loss of information.
7. Evaluate the existing condition of *character-defining elements* to determine the appropriate *intervention* needed. Use the gentlest means possible for any intervention. Respect *heritage value* when undertaking an intervention.
8. Maintain *character-defining elements* on an ongoing basis. Repair character-defining elements by reinforcing their materials using recognized conservation methods. Replace in kind any extensively deteriorated or missing parts of character-defining elements, where there are surviving *prototypes*.
9. Make any *intervention* needed to preserve *character-defining elements* physically and visually compatible with the *historic place* and identifiable on close inspection. Document any intervention for future reference.

Additional Standards Relating to Rehabilitation

10. Repair rather than replace *character-defining elements*. Where character-defining elements are too severely deteriorated to repair, and where sufficient physical evidence exists, replace them with new elements that match the forms, materials and detailing of sound versions of the same elements. Where there is insufficient physical evidence, make the form, material and detailing of the new elements compatible with the character of the *historic place*.
11. Conserve the *heritage value* and *character-defining elements* when creating any new additions to an *historic place* or any related new construction. Make the new work physically and visually compatible with, subordinate to and distinguishable from the historic place.
12. Create any new additions or related new construction so that the essential form and integrity of an *historic place* will not be impaired if the new work is removed in the future.

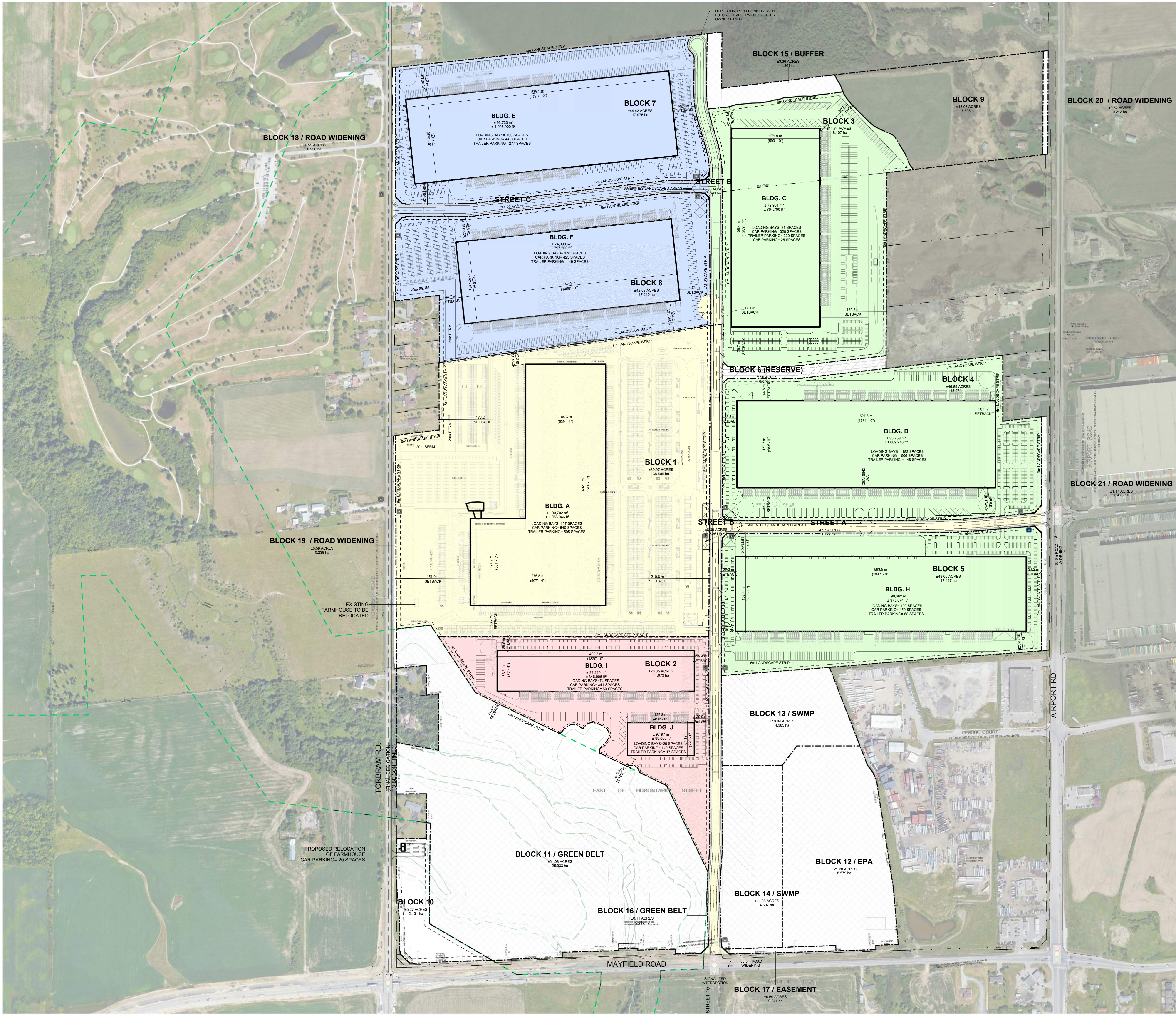
Additional Standards Relating to Restoration

13. Repair rather than replace *character-defining elements* from the *restoration* period. Where character-defining elements are too severely deteriorated to repair and where sufficient physical evidence exists, replace them with new elements that match the forms, materials and detailing of sound versions of the same elements.
14. Replace missing features from the *restoration* period with new features whose forms, materials and detailing are based on sufficient physical, documentary and/or oral evidence.

APPENDIX II

Development Drawings (Turner Fleischer Architects)

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This drawing, as an instrument of service, is provided by and is the property of Turner Fleischer Architects Inc. The contractor must verify and accept responsibility for all dimensions and conditions on site and must notify Turner Fleischer Architects Inc. of any variations from the supplied information. This drawing is not to be scaled. The architect is not responsible for the accuracy of survey, structural, mechanical, electrical, etc. information shown on this drawing. Refer to the appropriate consultant's drawings before proceeding with the work. Contractor must conform to all applicable codes and requirements of authorities having jurisdiction. The contractor working from drawings not specifically marked "For Construction" must assume full responsibility and bear costs for any corrections or damages resulting from his work.

66	2023-10-26	ISSUED FOR COORDINATION (A1-113)	RLA
65	2023-10-24	ISSUED FOR COORDINATION (A1-112)	RLA
#	DATE	DESCRIPTION	BY



PROJECT
TULLAMORE LANDS
CALEDON, ON

DRAWING
STAGING AND SEQUENCING PLAN

PROJECT NO. 20.302SD	<div>DRAWING NO. A101</div>
PROJECT DATE 2023-10-26	
DRAWN BY RLA	
CHECKED BY JJJ	
SCALE As indicated	

- STAGING/SEQUENCING LEGEND
- PHASE 1
 - PHASE 1 - TEMPORARY
 - PHASE 2
 - PHASE 3
 - PHASE 4



1 BUILDING D - NORTH ELEVATION (TYP.)
A301 1 : 600



2 BUILDING D - WEST ELEVATION (TYP.)
A301 1 : 250



3 BUILDING D - EAST ELEVATION (TYP.)
A301 1 : 250

S	2023-06-24	ISSUED FOR COORDINATION (A1-104-SPA)	RLA
I	2023-06-15	ISSUED FOR COORDINATION (A1-103-SPA)	RLA
#	DATE	DESCRIPTION	BY



PROJECT
TULLAMORE LANDS
CALEDON, ON

DRAWING
BUILDING D - ELEVATIONS

PROJECT NO. 20-302	
PROJECT DATE 2022-12-05	
DRAWN BY RLA	
CHECKED BY JJI	
SCALE As indicated	

DRAWING NO.
A301

APPENDIX III
Structural Memo
as prepared by Ojdrovic Engineering

July 19, 2021

File: F005-017

Carlos Morell
OAA Intern, Ms. Conservation of Cultural Heritage, CAHP
Goldsmith Borgal & Company Ltd. Architects
362 Davenport Road W, Suite 200
Toronto, ON M5R 1K6

Dear Mr. Morell:

Re: Farmhouse at 12245 Torbram Rd., Caledon East – Structural Opinion

Thank you for inviting us to participate in this project. The following report discusses our observations and offers professional opinion on condition of the structure of the house at 12245 Torbram Road in Caledon East.

Scope of Work

Ojdrovic Engineering was retained as a structural consultant by Rice Group for the 12245 Torbram Rd. project. The mandate for the project is to review the building and provide professional opinion on its condition from structural engineer's point of view.

The farmhouse at 12245 Torbram Rd. in Caledon East (Figure 1) is listed as a heritage property on historic and contextual grounds. It was built approximately around 1850. The house is located on property which is about to be developed. The intent is to relocate the house to a nearby property.

The scope of work for this project is to prepare a structural assessment of the house to determine if it is deemed to be safe for living at this time and recommendations for remedial work, as needed.

Limitations

This report is based on visual inspection from ground only. We did not perform any non-destructive or destructive investigations in order to ascertain actual characteristics of the house construction. We did not have access to higher levels such as roofs. No materials were tested for mechanical characteristics. No previous reports or drawings were available for review. In addition to visual inspection, the professional opinions expressed in this report are also based on experience in working on similar structures.

This report should be read together with the letter prepared by Goldsmith Borgal & Company Ltd. Architects.

General Description of Building Structure



Figure 1: Aerial View of Property

The farmhouse consists of two parts. The west, smaller part is a single storey red brick house without a basement. The east part is larger one and a half storey red brick house with full basement. Both roofs are medium gabled, wood framed. The foundation walls appear to be made using randomly coursed rubble stone masonry. The simple wood floor framing could be observed only in the basement, but it is expected to be similar everywhere.

Almost whole east storey and a half part of the house was completely covered with vine at the time of our visit and our observations are very limited in this area.

Exterior Foundation Walls

Exterior brick building walls are supported by rubble stone foundation walls that extend around the perimeter of the building.

The height of the exterior foundation walls varies. At the west single storey part without the basement, it is not clear how deep the foundation walls are. In the east part, rubble stone foundation walls extend to the bottom of the basement. The foundation walls do

not appear to have a footing: the base of the foundation wall appears to be approximately the same width as the top of the foundation wall.

The exterior rubble stone foundation walls are in fair to poor condition. Some of the mortar joints are cracked. In general, lime mortar is friable and is slowly turning into sand. Figure 2 below illustrates the typical condition of foundation wall. It should be noted that no remedial work on foundation walls will be required if the house is moved to different location.



Figure 2: Typical Condition of Stone Foundation Wall

Exterior Walls Above Grade

The exterior walls of the farmhouse are made of three wythes of brick. During the construction, an effort was clearly made to create a visual statement by adding yellow brick to predominantly red brick walls. Yellow brick accents can be seen around the whole house. Please see Figures 3 and 4.



Figure 3: North Elevation



Figure 4: South Elevation

Before making any definitive restoration plans for the brick walls, all vine and organic growth around the house must be removed to expose the walls completely. The following paragraphs describe some of the observed deficiencies of the brick walls.

The west gable wall of the single storey house appears to be in the worst shape. Several diagonal cracks appear to be caused by uneven settlement at the north and south ends of the wall. Attempts were made in the past to repoint the cracks, but at this moment, the crack that starts above the south attic window and propagates down to within a few feet above grade is open up to an inch. The flat arches that form window lintels have failed

and the brick is resting on window wood frames. The flat arches must be rebuilt now. Please see Figure 5.

The cracks could not be observed from within the house because there is plaster on wood lath on all walls. It is reasonable to assume that the cracks propagate through the full wall thickness.

While the cracks do not pose imminent threat to the occupants of the house at this moment, strong wind could potentially dislodge some of the bricks, in particular, above the window. This wall would have to be repaired before moving the house to the new location.

Some of the bricks show frost damage, but this is not something that causes safety concern at this moment or is critical for the house relocation.



Figure 5: West Wall Damage

The brick wall of the main – south elevation was built using stretcher bond, probably for aesthetic reasons. It is not known if “clip” bond was used to tie the outer brick wythe with interior wythes. Stretcher bond without ties to the inner wythes results in inferior wall and there is a possibility of delamination, especially during the relocation of the house. We recommend that a test opening is performed before the move to determine whether some additional bracing of the wall is needed for the move.

The west end of the south wall has a diagonal crack and apparent movement towards west of the top corner (Figure 6). Both flat arch brick lintels on this part of the wall have failed and have to be rebuilt before the move.

A sample of hidden deterioration of brick wall behind the vine is shown in Figure 7. The wall corner seems to be bulging out. The wide open cracks allow for water infiltration and damage to the wall below. It was not possible to inspect the damage closer.



Figure 6: West End of South Wall

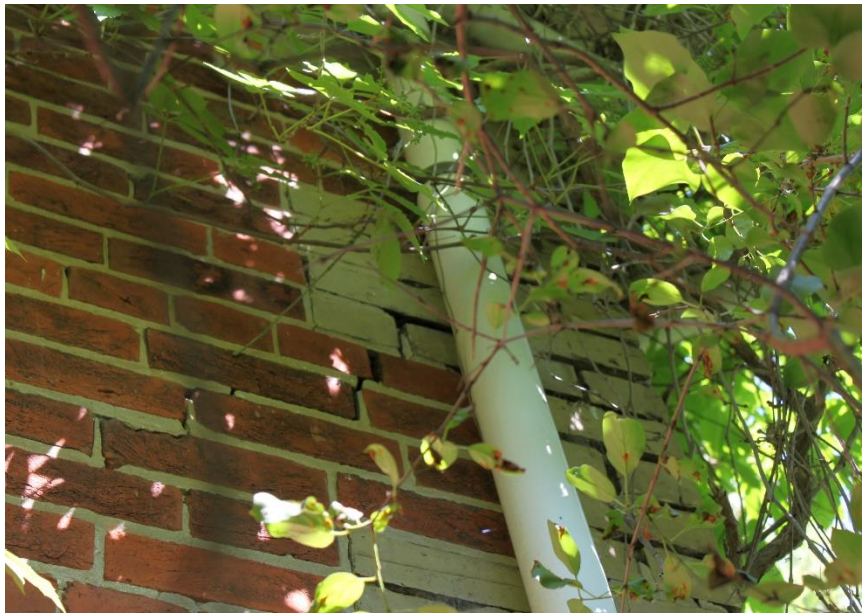


Figure 7: Hidden Deterioration Behind the Vine

The bricked-in opening at the north elevation (Figure 8) appears to have open cracks and some movement. It is not known if the infill is only one brick wythe thick or it is filled for the full wall thickness.

The condition of the wall at the base must be inspected closely before the move and necessary repairs performed. The soil around the house has crept up since the time the house was built in 1850s. Please see Figure 9. The brick that was once above grade, on top of the stone foundation, has been partially buried and exposed to moisture for many

years. This condition does not pose imminent threat to the occupants but may be critical for the house relocation.



Figure 8: Bricked-in Opening



Figure 9: Base of Wall Deterioration

Wood inserts are installed instead of every fifth brick for the full length of the taller building on the south side. It is likely that they served as anchor blocks for a wood porch roof when the house was built. Please see Figure 10.



Figure 10: Wood Inserts in South Facade

In general, the exterior brick walls, or the parts that are not covered by vine, show typical signs of aging. Parts of the south wall appear to be in fair to good condition with solid mortar joints. In some locations, cracks vary from localized to very pronounced, as in west wall. Loose bricks pose some threat to the occupants and should be at least temporarily stabilized now.

All damage to the masonry walls should be repaired before the move.-As long as cracks are open, the water will keep entering the walls and causing further damage. Ideally, the vine and organic growth around the base of the walls would be removed immediately, walls inspected in detail, and program of repairs specified, with repairs to follow. We do not expect any major failure if the exterior wall repairs are postponed for the year 2022, with caveat that the extent of damage will be larger, and the repairs will cost more.

Floor Framing

It is not clear what constitutes the floor of the west part of the house. The linoleum tiles appear to be glued to something solid, but it is not clear if the substrate is some kind of slab on grade installed at any time in the past, or something else. It does not seem that a crawls space exists in this area.

The ground floor framing of the east – taller part of the house consists of sawn wood floor joists on masonry walls. Please see Figure 11. The joists appear to be in good condition, with exception that we could not verify if the joist ends are rotting inside exterior walls.

It is assumed that the second floor framing consists of similar joists. This could not be verified because of plaster on wood lath ceiling. It was not possible to verify if the sill plates under the joists are anchored into the walls and whether the joists are nailed to the sill plates.



Figure 11: Ground Floor Framing

This review did not include load capacity analysis of the ground or second floor framing. Such analysis should be performed in case the house is meant to become a part of some heritage complex open to public, or even, if it is going to be occupied by tenants in new location.

Roof Framing

This review did not include access to heights and detailed investigation of the roof framing was not possible. The most obvious condition is substantial sagging of the roof ridge of the west part of the house. Please see Figure 12.



Figure 12: Sagging Roof

The red line shows how much the roof ridge has sagged. The yellow line shows slight curvature upwards of the fascia which means that there is some lifting of joist ends as a result of opening of the rafters. Fascia is also bowing outwards. Some of the rafters on the south side are rotten at the ends, and fascia and soffit have fallen apart.

Although the ceiling inside the west part of the house does not show distress, based on deformations of the roof alone, the roof framing should be considered as failed. **The roof of the west part of the house is unsafe** and must be repaired immediately. In its current condition, it can fall under the heavy snow. The fact that it survived the last few winters does not mean that it is safe.

The roof of the east part of the house is completely covered in vine and could not be reviewed.

Conclusions and Recommendations

The visual review of the house at 12245 Torbram Road in Caledon East was performed on June 29th, 2021. The purpose of the review was to assess current condition of the house from the structural point of view and provide professional opinion on necessary remedial work having in mind that the house is occupied at the moment and will be moved to a different location in the near future, most likely in 2022.

The following are tasks that must be performed as soon as possible as they pose threat to the occupants.

- Remove all vine from the house and all organic growth around the base of walls to fully expose for review all walls and roofs. There may be loose bricks that are not visible now.
- As a minimum, reset in mortar all loose bricks, rebuild failed lintels, and fill large cracks to reduce water ingress into the walls. This is based on assumption that comprehensive masonry walls repairs will happen before the house relocation some time in the future.
- Rebuild the roof framing of the west single storey part of the house. **The roof of the west part of the house is unsafe** and must be repaired immediately. In its current condition, it can fall under the heavy snow next winter.

The house relocation should be performed by a company experienced in such work. Before the relocation, masonry walls must be completely restored by partially rebuilding walls around cracks and bowing parts, repointing and other masonry techniques. We recommend tying of all walls across the ground floor, second floor and attics and bracing in both directions. Additionally, we recommend sandwiching the south wall between two steel frames to prevent any potential delamination of the exterior brick wythe.

APPENDIX IV

Assessment of outbuildings at 12245 Torbram Road- GBCA Architects

16 March 2023

Planning and Development Services
6311 Old Church Road
Caledon ON L7C 1J6

Attention: Cassandra Jasinski, Heritage Planner

RE: 12245 Torbram Road 'Tullamore Lands'. Town of Caledon, Ontario.
Addendum to CHIA including out buildings on property.
GBCA Project No. 21012

Goldsmith Borgal & Company Ltd. Architects (GBCA) was initially retained by Tullamore Industrial GP Limited for the purposes of a Official Plan and Zoning By-law Amendment application to assess the impact on heritage resources arising from a development proposal on a large assembly at 12245 Torbram Road.

We provided a Cultural Heritage Impact Assessment for this application (dated 16 July 2021), which assessed a development proposal including warehouse and office facilities totalling 4,934,987 square-feet of new built area are proposed along side a new road network extending from Torbram Road to the north, Airport Road to the east, and Mayfield Road to the south. The report concluded that the proposal balanced demands for intensification with those of heritage preservation in a manner that allows both objectives to be achieved as a part of a complex and changing environment.

This Addendum, at the request of the municipality, assesses the significance and impact to out buildings located on the subject site. The property will become a part of a commercial development and a study has been deemed to be required to determine the heritage importance and condition of these structures and to their recommended disposition.

The subject property comprises part of Lots 18, 19, 20, Concession 6 East of Hurontario Street. The environs feature a mix of early 19th-century farm houses, newer residential and commercial development, as well as open land historically associated with agricultural uses.

A- Torbram Road

- 1-** Small farmhouse at 12245 Torbram Road, estimated to date from c1850-1874 (to be relocated);
- 2-** Main barn (c. 1860-1870) (to be demolished/materials salvaged) ;
- 3-** Hay barn connected to main barn (to be demolished);
- 4-** Storage shed (to be demolished);
- 5-** 20th century silo and storage building (to be demolished);
- 6-** Hay barn (to be demolished);
- 7-** Utility structure constructed c. 2010 (to be demolished).



Figure 1: Aerial photograph showing 12245 Torbram Road and associated buildings.

1.0 Assumptions for the work:

Investigations were limited to visual inspection. We did not conduct any destructive testing nor was the examination done in detail. No building elements were moved during the inspection in deference to health and safety concerns. The site was visited in January 2022 and February 2023.

2.0 Site Background:

The subject site history is detailed in Cultural Heritage Impact Assessment (dated 16 July 2021) for this site, with specific reference to the farm house at 12245 Torbram Road. Please see Appendix II for additional background information related to this property.

2.1 Location and buildings:

The subject buildings are located on the east side of Torbram Road (A) north of Mayfield Road (B) and south of Old School Road (C), in Caledon, Ontario. The site is illustrated below and includes 8-structures including a small brick house (subject of GBCA's earlier CHIA) (1) constructed c1850-1874; the main barn (2); a utility shed (3); a hay barn addition to the main barn (4); a 20th century silo and storage shed (5); a hay barn/stable (6), and a utility building constructed c. 2010 (7). The principal focus of this addendum will focus on the main barn.



Figure 2: West (front) elevation of the farmhouse at 12245 Torbram Road (Building #1).



Figure 3: East elevation of Building #2 with south elevation of Building #3.



Figure 4: North elevation of storage building and 20th century silo. To the rear of this, is the north elevation of Building #2.



Figure 5: North west view of the main barn.



Figure 6: Interior view of newer 2x beams in the hay barn (Building #3) viewed from within the main barn.



Figure 7: East elevation of Building #6. Building #7 (c. 2010) is visible to the south of this.



Figure 8: Utility shed (Building #4) associated with the farmhouse at 12245 Torbram Road.



Figure 9: Interior view of the hay barn (Building #3) showing a mix of new timbers, and older hewn beams. Foundation is a combination of cinder block and wood construction.

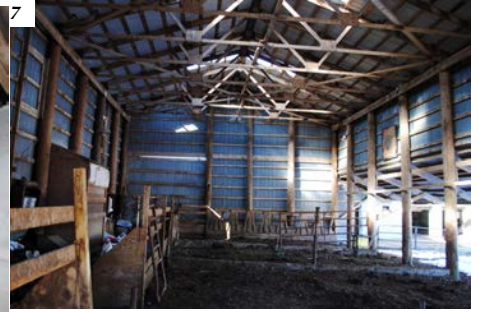


Figure 10: Interior view of Building #6 with metal framing, and corrugated metal siding.

2.3 The main Barn:

The Barn is a modified Bank or Pennsylvania style barn with gambrel roof and simple yet robust detailing. The basement foundation is set into the side of a small hill. The basement was used for animal husbandry. The hillside entrance would have provided easy access to wagons bearing wheat or hay, with the main floor divided into three sections: mows to the east and west, and a threshing floor in the centre. The simple rectangular plan has been retained, and, abutting the north-end of the building is a 7-bay hay barn which is a later addition with a structure independent from the main barn.

There is significant evidence of alterations to what may have been the original structure including displaced braces, open mortices and missing pegs struts that are visible from the interior structure. This strongly suggests that the current gambrel roofed barn was likely rebuilt using salvaged timbers sourced from an earlier barn on the same site. As there is no evidence of fire damage, we can conjecture that the original barn, placed on the stone portion of the foundations, may have collapsed, or have been blown down after which its components were re-cycled to erect the barn currently found on the site. Use of concrete block piers along one long elevation tends to confirm this along with the amount of weathering on the wood siding and the gambrel design - typically, mid-nineteenth century barns in Ontario had gable roofs rather than gambrel (a style which was imported from the U.S. in the mid-20th century). The concrete blocks are imperial measure which were used in Ontario from the 1920's until the conversion to metric c1974. Therefore, we estimate the current barn with gambrel roof to have been constructed sometime between the 1920s and the 1970s using salvaged timbers from an earlier barn on the same site. Based on the degree of weathering on the siding and the type and deterioration of concrete patches on the original stone foundation, a reasonable estimate would be in the period just after the second world war and possibly as late as the 1960's.

The structure has a building area of approximately 50 feet (running east/west) x 70 feet (running north/south) and is a single storey with over a 2/3 forebay below. Given the presence of the adjacent farmhouse (c1850-1874), a barn would have been constructed around 5-10 years following this depending on harvest yields (the earlier house would have served in the interim as a small barn pending construction of a new one).

The primary entrance to the main floor is accessed via a large double wagon door at the top of bank, located mid-point of the west elevation leading to the central threshing floor. The lower stabling floor and northern addition would have been used to shelter animals from inclement weather.

A portion of the foundation, visible on only the east, north and south elevations, is formed of field stone rubble set in mortar. The structure supporting the forebay is of cinder block construction, as are reconstructed portions of the south foundation.

The exterior walls are sheathed in approximately 1-inch thick by 12-inch wide rough-sawn pine boards (barn board) of variable length. Gaps of approximately 1-inch in width are typical between each plank and this served the purpose of providing air flow through the structure to allow stored hay to maintain an appropriate moisture content and avoid the risks of spontaneous combustion and/or rot. The siding is greyed but not blackened as would be typical of a barn built in the mid-19th century which is further confirmation of the proposed date of construction.

The gambrel roof is also sheathed with galvanized metal panels which appear to date to the early part of the 20th century.

The northern side of the building has a silo erected with corrugated metal. We suggest it was constructed during the second half of the 20th century. An assessment of this structure is not included in the current report, nor surrounding structures not connected to the barn, which are utilitarian structures added several decades following the barn.

The main barn building is in poor condition with damage present in the floor structure, in addition to displacement and empty mortices in several locations associated with structural alterations overtime that have compromised the overall structural integrity of the building.

Exterior elevation and interior images are included on the following pages, with commentary and observations related to condition. Detailed images are included in Appendix I.



Figure 11: West end of the barn showing gambrel roof. It is likely that materials were salvaged from an earlier barn with the majority of the gambrel roof barn constructed sometime between the 1920's and 1970s.

Note the missing boards to the left and right of the conveyor. To the right of this is a hay barn clad with corrugated metal, which would have been an addition in the mid-20th century.



Figure 12: North wall of the main barn – the silo is located to the left. Note the foundation at the west, and north where blocks have been used for repairs to the stone. Boards are missing on the sliding wagon doors that are located at the mid-section of the barn on a rise of land. Foundations have been patched with a hard concrete surface -the concrete is typical of the post WWII era rather than the interwar period which would have had a higher lime content.



Figure 13: South wall of the main barn - the hay barn addition and silo are visible to the right. In this case, the forebay uses cinder blocks suggesting that the footprint and size of the barn was changed with renovations in the 20th century. The hay barn incorporates heavy framing timbers salvaged from an earlier structure so may have been constructed from framing members salvaged from an earlier barn on site.

The roof is sheathed with galvanized metal panels which, from the ground, present in fair condition.

2.3.1 Interior

The interior of the building is unfinished with the structural components and backside of the exterior walls visible.

The upper portion of the building is framed in hand hewn heavy timber with mortice and tenon connections fastened with wood “trunnels” (originally derived from “tree nails”). The wood appears to be old growth pine for all components.

The roof rafters are carried on purlins framed into the main structural bents. These in turn support approximately 1” x 12” rough sawn pine planks (also commonly known as “barn board”). The roof is sheathed with galvanized steel which may be original to the building (as galvanized steel was used in Ontario from the 1850’s)



Figure 14: Detailed view of roof framing – north end at west side showing roof planking, roof rafters, roof purlins, diagonal bracing, and horizontal members.



Figure 15: Detailed view of tie beam showing splitting that has not been restrained with iron strapping. Note the empty mortices indicating that this and several other elements have been re-assembled.



Figure 16: General interior view looking west from main entrance showing three bents, with ladder access along the post.



Figure 17: Straining beam shows evidence of double bracing due to empty mortise. These would have originally carried a double diagonal brace.

This construction system sees the Queen post directly supports the longitudinal purlin plate from the ground, which is not typical. Throughout the post there are missing pegs.

Lack of sway compression bracing would suggest that the structure is sensitive to wind pressure.



Figure 18: Detailed view of roof framing – east side showing roof planking, roof rafters, roof purlins, diagonal bracing, and horizontal members.

Note the 2x4 braces with metal brackets that have replaced smaller mortise and tenon braces.

These alterations indicate that a considerable amount of structural revision was done to this building by the removal of beams and braces. This has compromised the overall structural integrity of the structure.



Figure 19: View of south wall, showing lack of significant bracing to the wall structure and empty mortises along the bent. The gaps between the planks provides considerable relief from wind loads.

Note open mortises in posts where diagonal struts are missing.



Figure 20: Detail of the hewn tie beam morticed and tenoned into a post with pegs.



Figure 21: General interior view looking west from main threshing floor in the centre. Note the left and right tie beams have displaced braces.

2.3.2 Foundation:

A majority of the building foundation is constructed of field stone (stones excavated from fields during clearing operations). As these stones are glacial in origin, they represent a variety of types of material. Such stone walls are typically constructed in a massive form as it is difficult (due to the rounded shape of the stone) to provide a cohesive and solid wall. Cracking was noted, particularly on the narrow-end east wall, due to its height and inherent weakness resulting from the type of stone used and the window openings. The structure supporting the forebay is of cinder block/masonry construction, as are reconstructed portions of the south foundation.

The core of the stone walls are typically full of voids due to wash-out of the original lime/sand mortar as a result of ground water and rain water flowing down the exterior walls. Field-stone walls of the type found in this foundation are not very resistant to movement as the stones are not locked together in the construction as they are essentially round elements rather than square. It is noted that the south, east and west corners of the original barn were rebuilt with cinder blocks, which might have been done in order to provide appropriate stability.



Figure 22: This type of repair is due to rot of the sill beam at this location due to its proximity to wet earth. The quality of the cinder suggests it was batched on site and was probably done in the early to mid twentieth century.



Figure 23: The forebay terminates in a plate, which is supported by cinder blocks, allowing for a much deeper space.

The 8" x 16" blocks used cinder as an aggregate prior to the mid 1960's, thus putting the alterations sometime before this.



Figure 24: The forebay terminates in a plate, which is supported by cinder blocks, allowing for a much deeper space.

3. Heritage Impact

3.1 Physical

See above description and assessment of main barn in section 2.3.

3.2 Contextual

The barn and house form a part of the context of the original Tullamore area and are consistent with early settlement of the area. The later hay barn and shed are not consistent with the early heritage of the area or the general context of the community. As the barn appears to have been reconstructed, only its massing and location would be consistent with the early heritage of the area.

3.3 Historical

The barn complex serves as an historical reference to the continuing evolution of the community in which it is located.

Based on the construction of the building, it is probable that the original barn was constructed about ten to twenty years after the initial settlement of this property in the late 1860's. A barn of the scale of the main barn would require several years of land clearing and successful crops to finance, although it is known that the original owners migrated from other successful farms of an earlier period to this area. However, the current barn appears to be a reconstruction of the original, with only some portions of the foundation and recycled materials referencing the original.

The east addition is certainly a later addition and incorporates heavy framing timbers salvaged from an earlier structure so, like the main barn, may have been constructed from framing members salvaged from an earlier barn on site.

The large timber in the main barn is a candidate for dendrochronological survey work which would conclusively determine the date of its felling and provide a date +/- 5 years of the construction of the original barn but not the reconstruction.

As a preliminary assessment, the original barn on the site was probably constructed somewhere between 1865 and 1875 with the beams being reused for the current gambrel roof structure that was built approximately 50 to 80 years later.

3.4 Current heritage status

The property is noted in the Town of Caledon's listing of the building under listed under Section 27 (1.2) of the Ontario Heritage Act. The site includes a 1-1/2 storey neo-classical style farmhouse, built c.1850-1875 at 12245 Torbram Road.

This Register is an administrative and documentation tool used to assist in the identification and on-going preservation of significant heritage resources within the land use planning and permit application process.

3.5 Evaluation

The current report has been prepared to determine the importance of the outbuildings on site and to provide an impact assessment related to its removal for a proposed new development. As part of this report, an evaluation of the main barn at 12245 Torbram Road, under Ontario Regulation 9/06 was conducted. GBCA concluded that the structure, meets the only the minimum criteria for determining cultural heritage value. This evaluation is provided on the following pages.

s. 27 (3) (b) of the Act (O.Reg.9/06), Criteria for Determining Cultural Heritage Value or Interest:	Assessment of Heritage Value or Interest of 12245 Torbram Road (Main Barn)	Meets Criteria:
1. The property has design value or physical value because it is a rare, unique, representative or early example of a style, type, expression, material or construction method.	The barn structure at 12245 Torbram Road is the reconstructed version of an original barn that was likely first built between 1865 and 1875. The original beams were reused for the current gambrel roof structure that was reconstructed approximately 50 to 80 years later. The Barn is a modified Bank or Pennsylvania style barn with gambrel roof and simple yet robust detailing. This is not particularly rare for the Town of Caledon.	No
2. The property has design or physical value because it displays a high degree of craftsmanship or artistic merit.	There is significant evidence of alterations to what may have been the original structure including displaced braces, open mortices and missing pegs struts that are visible from the interior structure. The craftsmanship is representative of the period of construction, however it is not particularly unique or rare.	No
3. The property has design or physical value because it demonstrates a high degree of technical or scientific achievement.	Based on the visual inspection observed on site, the construction method, while common for the time - does not appear to demonstrate a high degree of technical or scientific achievement.	No
4. The property has historical value or associative value because it has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community.	No significant associations to an event, person, organization or institution that is significant to the community could be found for the barn located at 12245 Torbram road.	No
5. The property has historical value or associative value because it yields, or has the potential to yield, information that contributes to an understanding of a community or culture.	The building has the potential to yield information that contributes to the understanding of the mid-19th century settlement of the area, but this attribution is minor due to design changes.	No
6. The property has historical value or associative value because it demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.	The designer or builder has not been identified and the integrity of the original design has been lost through numerous alterations, in any event.	No
7. The property has contextual value because it is important in defining, maintaining, or supporting the character of an area.	The structure has been heavily altered and while it illustrates a portion of the original character of the area, it does not define or maintain that character.	No
8. The property has contextual value because it is physically, functionally, visually or historically linked to its surroundings.	The structure is physically connected to the early settlement of Caledon and farming activities; however, it is no longer functionally linked to farming activities which has considerably reduced these associations.	No
9. The property has contextual value because it is a landmark. O. Reg. 569/22. s. 1.	While the barn is visually linked with the farmhouse located at 12245 Torbram road, it was constructed at a different time period and does not share a connection to the public road. The barn is not considered a landmark.	No

3.6 Heritage Impact Statement

Barn structures are increasingly at risk as development spreads across the region. However, based on our assessment, the subject structure is not original and has lost its integrity as a result of reconstruction. Furthermore, it is in poor condition and several areas of the building are at risk of collapse.

While the loss of barn structures is regrettable, not every structure can be retained and this is particularly the case when the condition of the structure is such that it poses a hazard to the public, nor is an original structure given the dismantling and reconstruction from an earlier structure.

While the loss of a structure of this type in the Town of Caledon can be mitigated by recording the building by drawings in accordance with known documentation standards, photographs keyed to the drawings, and salvage of important structural components for display and interpretation in the community, we question whether this is reasonable given the reconstruction of a lost earlier structure. However, recording can be completed on request. Refer to the recommendations below.

The other outbuildings on the site, namely the hay barn connected to the main barn, the utility shed, the 20th century silo and storage building, the later hay barn and the utility structure (c.2010) are of little heritage value in terms of the early settlement of this area and may be removed with no heritage impact.

4.0 Recommendations

The barn structure at 12245 Torbram Road is not an immediate candidate for record taking but, should the municipality request, can be recorded in detail in a manner which is outside of the scope of this current report. A set of measured and dimensioned drawings could be prepared with keyed photographs to document the structure for posterity. Only trained professionals competent in the evaluation of structures of this type should be permitted on the site of the main barn due to the condition of the foundation and its superstructure.

Once recorded, the barn should be carefully dismantled by a company familiar with the salvage of old growth timbers so that these elements can be appropriately recycled. Some of the timbers and/or some of the largest field stones could also be recycled for inclusion at, potentially, the proposed relocation site for the farmhouse.

We trust this Addendum satisfies the municipality's requirements for this resubmission, and remain available should you require any additional information.

Christopher Borgal OAA FRAIC CAHP



President
Goldsmith Borgal & Company Ltd. Architects

The information and data contained herein represents GBCA's best professional judgment in light of the knowledge and information available to GBCA at the time of preparation. GBCA denies any liability whatsoever to other parties who may obtain access to this report for any injury, loss or damage suffered by such parties arising from their use of, or reliance upon, this report or any of its contents without the express written consent of the GBCA and the client.

APPENDIX I

Site photographs taken on January 2022 and February 22, 2023.



Figure A: Building #2. Main barn- west elevation.



Figure B: Building #2. Main barn - north elevation.



Figure C: Building #2. Main barn- north and west elevation, showing adjacency to Building #6, a hay barn clad with corrugated metal dating to the mid-20th century.



Figure D: Building #2. Main barn - south elevation.



Figure E: East elevation of Building #2 with south elevation of Building #3. The east elevation of Building #6 is visible to the far left of this image.



Figure F: Partial view of the south elevation of the main barn. The south elevation of Building #3, a later hay barn, is visible to the right.



Figure G: Detailed view of the south elevation at Building #2.



Figure H: East elevation of the main barn is visible here, with the silo and east elevation of Building #3 visible beyond this.



Figure I: North elevation of the small storage building (#5) and adjacent 20th century silo. To the rear of this, is the north elevation of the main barn.



Figure J: The east elevation of Building #6, constructed of corrugated metal. Beyond this is Building #7, a utility structure (quonset) that was added to the site c. 2010.



Figure K: Interior view of Building #6, a utilitarian structure clad with corrugated metal.



Figure L: Building #4 located on the rear lot of the farmhouse, clad with plywood and faux half-timbers.