

# **FERNBROOK SUBDIVISION - SNELL'S HOLLOW**

Town of Caledon



## **Architectural Vision & Design Guidelines**

**TOWN FILE NO.: 21T-95058C**

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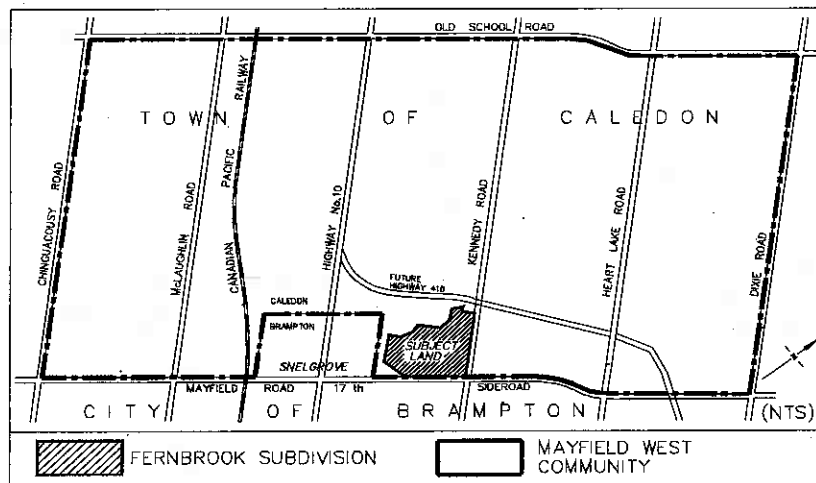


## 1.0 INTRODUCTION

The Fernbrook Subdivision (21T-95058) is a proposed residential neighbourhood located within the Mayfield West Community in the Town of Caledon. The subject lands are bounded by Mayfield Road to the south, proposed Highway 410 extension to the north, Kennedy Road to the east and the Etobicoke Creek to the west as shown on the Key Map below and the Neighbourhood Design Plan on the following page.

These guidelines have been prepared as required by the Town of Caledon. The purpose of this document is to provide an architectural vision for built form within the private realm and to establish objectives for appropriate relationships between the private and public realms within the Fernbrook Subdivision. The "Fernbrook Subdivision - Snell's Hollow Architectural Vision & Design Guidelines" provide a framework of design criteria addressing such matters as materials, colours, architectural style, detailing, siting, and massing to assist the homebuilder in providing dwelling designs which will help promote an attractive, high quality community. These guidelines will be implemented through a privately administered architectural control process to ensure proposed built form within Fernbrook Subdivision complies with the design objectives stated herein.

This document should be read in conjunction with the "Fernbrook Subdivision - Snell's Hollow Community Design Guidelines" prepared by NAK Design Group which establish neighbourhood design principles addressing public realm design items.



**Fernbrook Subdivision Key Map**

## 1.1 Objectives

The objectives of these Guidelines are:

- To establish a positive visual character for the neighbourhood by requiring a high standard of architectural design quality for all new dwellings.
- To provide attractive, harmonious and pedestrian-scaled streetscapes incorporating tradition-based architecture which will give Snell's Hollow a distinct character.
- To promote compatible variety among new dwelling designs within an established vocabulary of architectural styles, materials and details.
- To establish design criteria for buildings on focal lots (lots with a higher degree of public visibility) and recognize their landmark qualities within the neighbourhood as well as the community.
- To ensure appropriate relationships and connections between buildings within the private realm and adjacent public spaces.
- To diminishing the visual intrusion of garages and parking areas within the streetscape.
- To promote safe, pedestrian-friendly streetscapes by applying the principles of CPTED (Crime Prevention Through Environmental Design).

## 1.2 Architectural Design Vision

The architectural design vision for the Fernbrook Subdivision is:

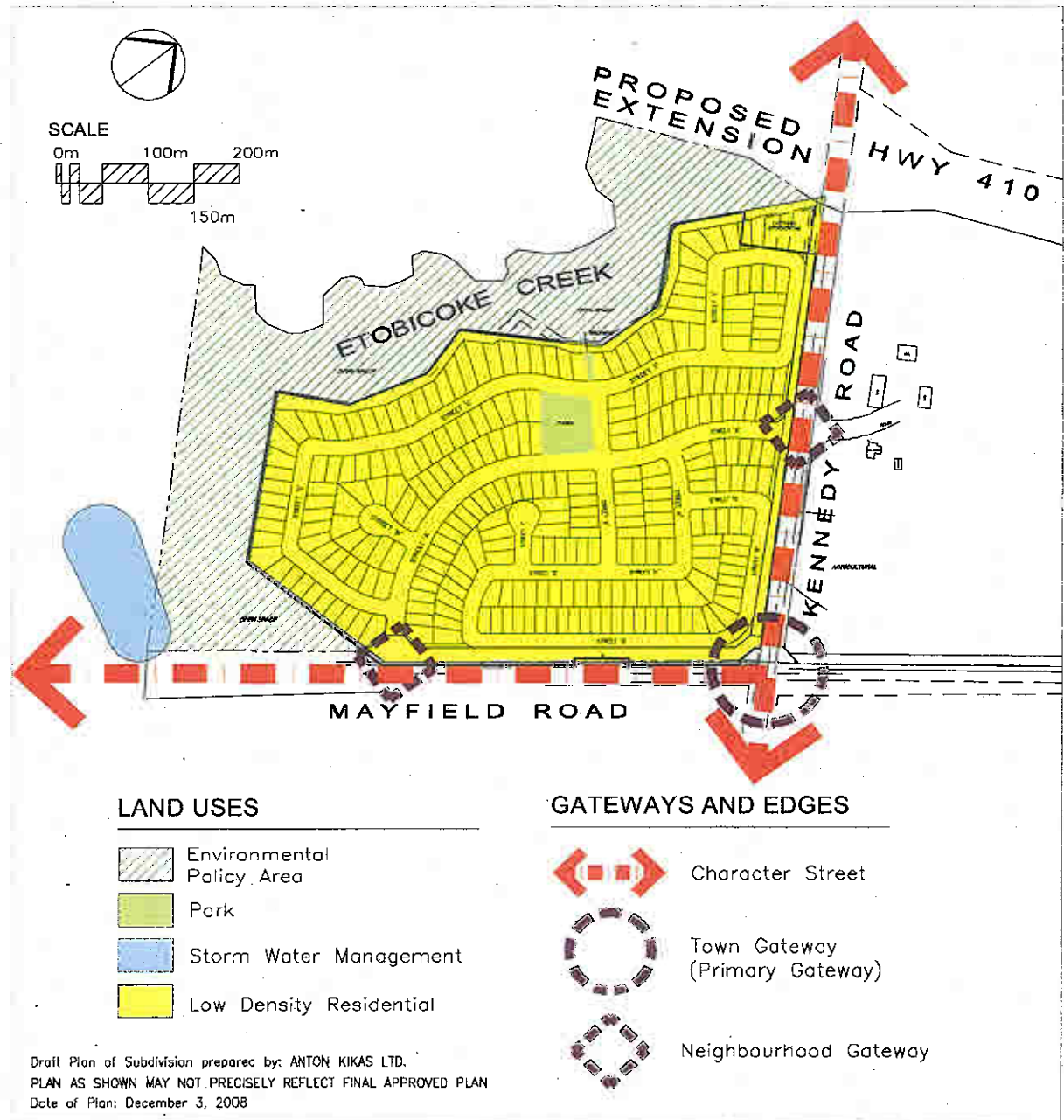
*To be an attractive residential neighbourhood, designed to a human scale, with a distinct identity where well-designed streetscapes and public spaces encourage pedestrian activity. Public realm design elements and residential architecture should reinforce and maintain a high degree of quality and distinguish the Fernbrook Subdivision from neighbouring development.*

1.3 Community Context

Fernbrook Subdivision is located prominently along the southern edge of the Mayfield West Community and acts as a gateway to the Town of Caledon from the City of Brampton to the south. The neighbourhood will largely be defined by its compact form set amongst ample green spaces, including valleylands with trails, neighbourhood park and landscaped stormwater management facilities.

Built form development within the Fernbrook Subdivision will consist of low density residential uses in the form of detached dwellings with lot frontages ranging from 12m to 15m.

Residential architecture within the Fernbrook Subdivision is intended to establish a high quality traditional neighbourhood character, distinct from neighbouring Brampton, and provide a sense of arrival into the Town of Caledon. The use of attractive buildings designed to suit their location, combined with distinctive streetscape elements such as tree-lined streets, community fencing and entrance features will be incorporated to implement the architectural design vision and create a unique and harmonious neighbourhood.



*Fernbrook Subdivision -  
Neighbourhood Design Plan*

## 2.0 RESIDENTIAL BUILT FORM AND SITE PLANNING

The design of residential built form and treatment of site planning within the Fernbrook Subdivision will play a major role in establishing the visual character of the neighbourhood. A well-defined street edge contributes to the pedestrian-oriented goals of this neighbourhood. Attractive streetscapes typically consist of a landscaped boulevard adjacent to a defining edge of private front yards and carefully placed, well-designed dwellings. The closer the dwelling is to the sidewalk, the more it promotes a pedestrian-friendly sense of scale and provides enclosure to the public space of the street.

### 2.1 Design Quality

- Publicly exposed elevations shall incorporate adequate massing, proportions and wall openings to avoid large blank façades.
- The use of high quality, low-maintenance building materials is required to contribute to a positive visual character for the neighbourhood.
- All dwellings within the neighbourhood shall provide street-facing façades designed to minimize the prominence of the garage.



*High Quality Dwelling Designs Are Required*

## 2.2 Neighbourhood Safety

To promote a safe, pedestrian-friendly neighbourhood, the design and siting of dwellings will incorporate principles of CPTED (Crime Prevention Through Environmental Design), including the following:

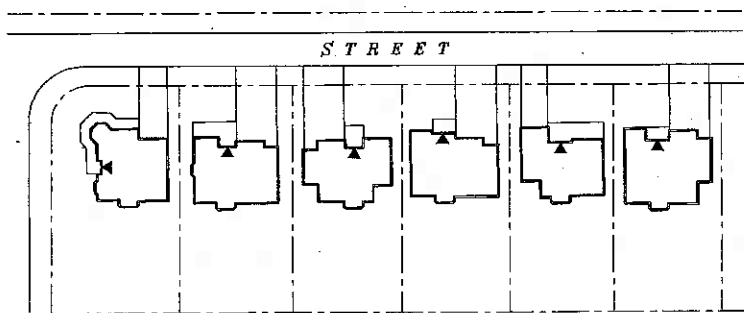
- Dwellings will be designed and sited to provide maximum observation of public areas (streets, open spaces and recreation areas).
- Ample fenestration facing public areas will be provided to foster natural surveillance (eyes on the street).
- Dwelling designs that promote interaction among neighbours and within the neighbourhood shall be utilized. Large, usable front porches help to promote interactive outdoor spaces and act as an interface between private and public realms and shall be incorporated in dwelling design to the extent possible.
- Garage protrusion within the streetscape shall be minimized.
- Dwelling designs as well as the location and design of fencing and landscaping should not create blind spots or hiding areas.
- All entries to the dwelling shall be appropriately well lit.
- Walkways will be located in such a way as to provide clear and direct pedestrian routes.



*Dwellings Should Be Designed To Encourage "Eyes On The Street"*

**2.3 Street & Building Relationships**

- Housing shall be ground-related and designed at a human scale to reinforce a pedestrian oriented neighbourhood and contribute to the neighbourhood's design vision.
- The front face of the dwelling should generally be located close to the minimum building setback to establish a defined street edge.
- Dwellings should be sited with due regard for the front yard setbacks of adjacent dwellings in order to provide consistency in the appearance of the streetscape. Notwithstanding this, some variety in setback is desirable to avoid a sterile, uniform streetscape.



*Desirable Siting Relationships Between Adjacent Dwellings*



- The front façade of the dwelling shall directly relate to the street and shall visually dominate the garage. Garages shall be fully or partially enclosed within the dwelling to minimize their visual impact and contribute to a comfortable and attractive environment.
- Large, usable covered front porches are encouraged on the majority of dwellings within a street block, where appropriate to the style of the home. Wraparound porches are encouraged on corner lots. Porch encroachments into front and exterior side yards should be permitted to encourage these features.
- All elevations of the house which are highly visible within the public realm shall be well-detailed and have attractive façades reflective of their architectural style.
- Variety of architectural expression among publicly exposed façades should occur within each street block.
- Main entrances shall face the street and be connected to the street or driveway by a walkway.
- Building scale, height, massing and within a street block should be compatible with the importance of the adjacent street while retaining a human scale to encourage pedestrian activity.
- For corner units, the flanking and visible rear elevations shall be given the same level of architectural detailing as the front elevation. Provision of a porch on the flanking facade helps create a human scale interface between public and private realms.
- The Kennedy Road and Mayfield Road street edges shall be carefully designed to address their unique conditions arising from the approved plan of subdivision, which includes window street lotting, flankage lotting and interfacing with existing residential dwellings. Visible rear or side elevations for dwellings flanking onto Kennedy Road and Mayfield Road shall be designed and detailed in a manner consistent with their front elevations.
- For lots abutting highly visible public areas, such as parks and open space areas (except where heavily treed), the exposed facades shall address the public realm with a well-detailed treatment consistent with the front elevations.

**2.4 Streetscape Variety and Model Repetition**

The exterior appearance of new housing will have the greatest impact on the perceived quality of the Fernbrook Subdivision development. The visual appeal of the streetscape is enhanced when the arrangement of the dwellings is ordered with respect to model variety, massing, height and repetition within the group. Variety of architectural expression among publicly exposed façades should occur within each side of a street block to ensure harmonious diversity.

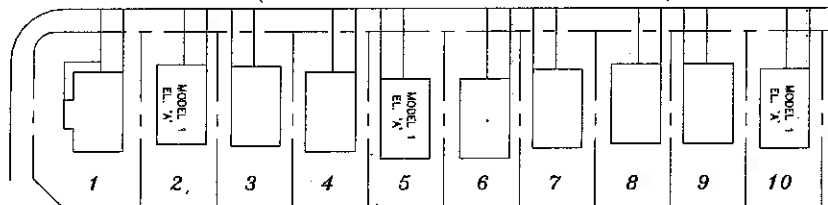
- Each model should have at least two distinctly different elevations. Popular models may require more than two elevations to avoid repetition and monotony within the streetscape.



*Alternative Elevations of the Same Model*

- Identical dwelling elevations shall not be permitted directly adjacent to one another or directly across the street from one another.
- Identical elevations shall not comprise more than 30% on one side of a street block. To further promote visual diversity along each street, a minimum of 2 dwellings must occur between identical elevations of the same model.

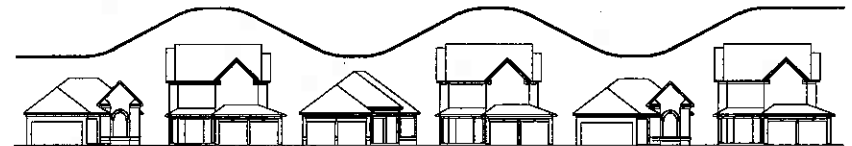
SAME MODEL ELEVATION PERMITTED 30% MAX. IN STREET BLOCK  
(MUST BE SEPARATED BY MIN. 2 DWELLINGS)



*Model Repetition Criteria*

**2.5 Massing Within the Streetscape**

- Dwellings adjacent or opposite one another should be compatible in massing and height. Extreme variation in massing shall be avoided.
- The apparent variation in height and massing between adjacent one and two storey dwelling types should be minimized in the following ways :
  - Where two storey dwellings are located adjacent bungalows, they should occur in groupings of at least two adjacent dwellings.
  - Single bungalows may be sited on corner lots provided they are appropriately designed corner models.
  - Bungalows shall not be sited on priority corner lots.
  - Where bungalows or 1 1/2 storey dwellings are located adjacent to two storey dwellings, they should occur in groupings of at least two dwellings and their design should include enhancements such as taller roofs, dormers, side gables or raised front elevations, for an effective visual transition between dwelling types.



UNACCEPTABLE BUNGALOW MASSING IN STREETScape

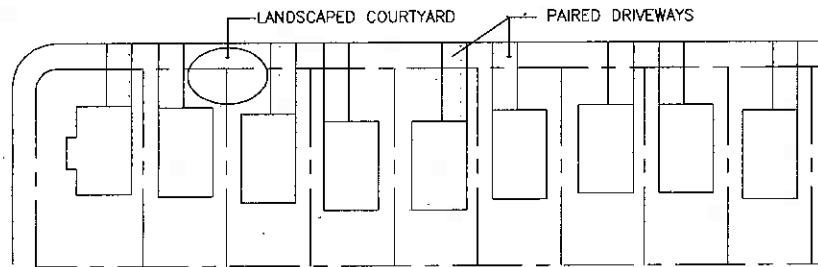


ACCEPTABLE BUNGALOW MASSING IN STREETScape

*Extreme Variation in Massing is Not Permitted*

**2.6 Driveways**

- Generally, the pairing of driveways is desirable in order to maximize the green space between garages (landscaped courtyard) and maximize on-street parking. Exceptions will be allowed when it can be demonstrated that street furniture, service facilities or adverse grade conditions prevent the pairing of driveways.
- Driveway locations shall be predetermined on the site servicing plans and approved by the municipality.
- The frequency and width of curb cuts should be kept to a minimum.
- Driveway widths shall be no greater than the garages they serve.
- Adjacent driveways at cul-de-sac and street elbow locations should be designed to eliminate overlap between the property line and the curb.
- Driveways for dwellings adjacent intersections, transit stops, public walkways, open space and other non-residential land uses shall be located as far from the adjacent use as possible.
- Driveways located at the top of T-Intersections shall be located to the outside of the pair of dwellings which terminate the view.



*Example of Paired Driveways*

**2.7 Adverse Grade Conditions**

- Where severely sloping grade conditions occur, the builder shall provide dwelling models which are adapted to suit the site. The following are suggested design approaches for reducing the impact of elevated front entries:
  - Integrate groups of steps into the front walkway over the length of the front yard.
  - Turn steps toward the driveway.
  - Inset 1 or 2 risers into the porch.
  - Provide a dwelling design having a lowered foyer and internal steps up to the main living level.

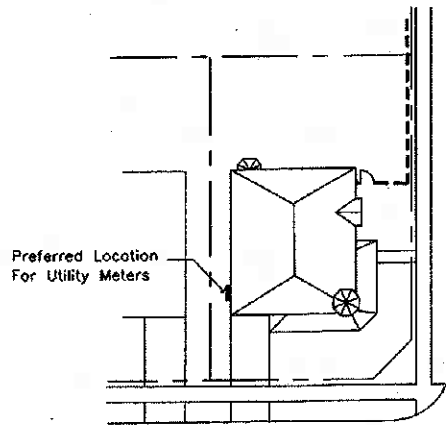
**2.8 Streetscape Elements**

- Streetscape elements occur within the R.O.W. and include but are not limited to street trees, light standards, hydrants, street signs, community mailboxes, transformers and other street furniture. The location of these elements shall be predetermined and approved by the municipality.
- The builder shall coordinate dwelling site plans with all streetscape elements located within the street R.O.W., to ensure there are no conflicts between dwelling, driveway, walkway or other dwelling site plan component and streetscape elements. This requirement is solely the builder's responsibility.

**2.9 Utility and Service Elements**

- The location of utilities within the public rights of way as well as on private property shall be located discreetly. Utilities shall be clustered or grouped in a manner so as to not visually detract from the streetscape. Utility providers will be encouraged to apply innovative methods of containing utility services on or within streetscape features such as gateways, lamp posts, transit shelters, etc., where feasible.

- To reduce their visual impact, utility meters or service connections for hydro, water, natural gas, telephone and satellite should be located out of direct view from any street or other public area such as parks, open space, etc. preferably within the interior side yard on dwelling wall faces perpendicular to the street.
- For corner lot dwellings, utility meters should be located on the interior side wall. Where utility meters must be located on flanking walls exposed to public view, they shall be located to reduce their visibility from the street or architecturally screened or recessed.
- The placement and location of utility meters on the dwelling shall at all times conform with the local utility company's criteria.



*Meter Location for  
Corner Dwellings*

## 2.10 Municipal Address Signage

- A co-ordinated approach to municipal address numbers should be provided by the builders. For example, a similar style throughout the neighbourhood provides a unifying urban design element which helps define neighbourhood character.
- It is critical that the municipal address is legible from the street, particularly in emergency situations. For this reason the following criteria shall apply:
  - It shall be located prominently on the front façade of the dwelling or garage.
  - It shall be in a well-lit area.
  - Numbering shall be a minimum of 100mm (4") tall and in a simple, legible font face.
  - Numbering shall be dark and placed on a light coloured background for maximum contrast.
- The design of the address plaque shall be complementary to the character of the dwelling and reflect the image of the neighbourhood. Acceptable designs include:
  - Etched masonry plaques set into the wall cladding;
  - Pre-finished ceramic or plastic plaques set in a bezel.



*Examples of Municipal Address Plaques*

### 3.0 ARCHITECTURAL DESIGN CRITERIA

#### 3.1 Architectural Styles

In order to develop a traditional architectural theme for the neighbourhood appropriate styles may include, but are not limited to: French Country, English Country, Victorian, Georgian and Colonial. Mixing discordant architectural styles together within a single building is not permitted. A meeting between the developer, builders and control architect shall take place at the outset of the project to review builders' proposed architectural styles for design compatibility within the neighbourhood.



*Conceptual Images of Proposed House Styles to Suit the Architectural Design Vision Theme for Fernbrook Subdivision (concepts prepared by Hunt Design Associates for Fernbrook Homes)*

### 3.2 Building Projections

- Visual interest of the dwelling from the street can be enhanced through the use of projecting elements consistent with the architectural style of the dwelling, including: roof extensions, dormers, porticos, chimney projections, bay windows or other projecting elements.



*Building Projections Help to Visually Enhance the Streetscape*

### 3.3 Main Entrances

The main entrance to the dwelling should convey its importance as both a focal point of the façade and the interface between the private realm of the dwelling and the public realm of the street.

- Main entries to the dwelling shall be directly visible from the street.
- Weather protection at entries shall be provided through the use of covered porches, porticos, overhangs or recesses.
- The front entry design and detail shall be consistent with the architectural style of the dwelling.
- Houses should be designed so that front entries do not require more than a few steps up. A relationship of no more than approximately 1.0m between the main floor and finished grade is desirable to maintain a pedestrian scale.



*The Main Entrance Should Be Designed as a Focal Point of the Dwelling*

### 3.4 Porches / Porticos

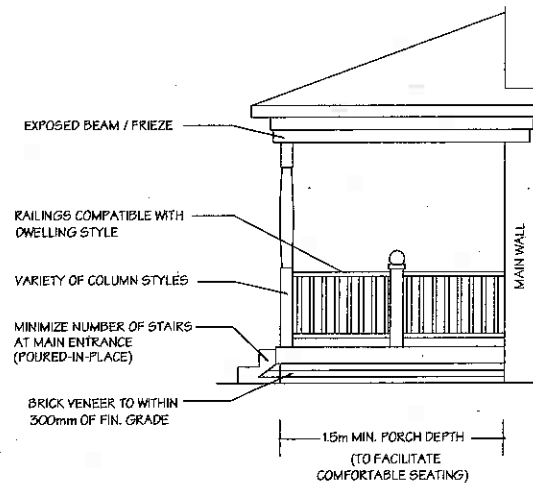
The generous use of front porches, verandahs and porticos is required to provide a front yard outdoor amenity space, opportunities for 'eyes on the street' as well as social interaction among neighbours. Generally, porches should be located closer to the sidewalk / street than the adjacent attached garage. This has the effect of diminishing the importance of the garage and creates a comfortable relationship between the private and public realm for pedestrians.

- Porches or porticos are required on at least 50% of dwellings within a street block.
- Wraparound porches are encouraged on corner lots where appropriate to the dwelling style.
- The design of a porch or portico shall be consistent with the architectural style of the dwelling.



*Porches Provide an Outdoor Amenity Area*

- Porch dimensions should be adequate to comfortably accommodate seating. Depths in the order of 1.5m to 2.4m are sufficient for this purpose.
- The size of the porch/portico and its components (columns, piers, brackets or moldings) shall be proportional to the scale of the dwelling. Porch/portico columns should generally be no less than 200 mm square or diameter.
- Porch/portico roofs shall generally be supported on a continuous frieze resting on columns.
- Ground-level wood porch decking is prohibited on front or flanking elevations.
- A maximum of 3 precast risers/steps will be permitted to access the front or flankage porch. Where more than 3 risers are necessary they shall be poured-in-place concrete with the exposed sides finished to match the front façade cladding.
- Where railings are required, they shall be of design appropriate to the style of the dwelling with pickets between top and bottom rails. The use of pre-finished aluminum or wrought iron is preferred; unpainted, pressure-treated wood railings on elevations visible from the public realm are prohibited.



*Typical Porch Detail*

### 3.5 Wall Cladding

The use of high quality exterior building materials which are reflective of the architectural style of the building will be required to contribute to the character of the neighbourhood. The following requirements for wall cladding will apply:

- The predominant main wall cladding materials within the neighbourhood will be a mix of clay brick, stone (with precast accents) and stucco (with appropriate trim detailing).
- The use of vinyl or aluminum siding as a main cladding material is not permitted and will only be permitted as a minor accent material or in areas of low public visibility where structural support of masonry is not feasible. The use of high quality fibre-cement siding (such as Hardi-Board) is permitted. Where siding is used, it shall be encased with 100mm (4") wide mouldings.
- The choice of exterior cladding material shall be compatible with the architectural style of the house.
- Material changes which help to define the transition between the base, middle and top of the building are appropriate. Where changes in materials occur they should happen at logical locations such as a change in plane, wall opening or downspout.
- The use of secondary accent materials, such as stone or stucco, is encouraged provided it blends harmoniously with the primary cladding material.
- Decorative architectural detailing, either wood or molded composite, is encouraged where appropriate to the design style of the building.
- Stone plinths are encouraged and should return along the side walls a minimum of 1200mm (4') from the front of the dwelling.

**3.6 Exterior Materials & Colours**

Colour schemes and material selections should be carefully coordinated for visual harmony and for consistency with the architectural style of the dwelling, to the satisfaction of the Control Architect.

- Dwellings adjacent or directly opposite one another shall not have main wall cladding of the same colour. Identical colours shall be separated by a minimum of 2 dwellings.
- Identical front elevations which occur within a group of dwellings shall use a different colour package.
- Street blocks should have no more than 30% of the dwellings sharing the same colour package.

- The use of an accent colour for brick detailing such as lintels, bands or quoins, should be used sparingly and should be subtly different from and complementary to the colour of the main façade brick.
- Paint colours should be selected from the manufacturer's 'Heritage Collection'.
- The roof shingle colour should complement the colour of the primary wall cladding. The use of light coloured shingles, such as white or light grey, shall be prohibited.
- Aluminum soffits, eavetroughs, frieze boards and fascias should be a single colour for each dwelling.
- All flashings shall be prefinished or painted to match adjacent wall cladding colour or roof.

**Typical Exterior Material and Colour Schedule**

| Item   | Package #1            | Package #2            | Package #3            | Package #4                | Package #5            | Package #6               | Package #7                | Package #8               |
|--|-----------------------|-----------------------|-----------------------|---------------------------|-----------------------|--------------------------|---------------------------|--------------------------|
| Brick  | Cortes                | Heritage Athens       | Heritage Beige        | Antique Grey              | Heritage Antique      | Devon Cream              | St. James                 | Tremaine                 |
| Stone  | Topaz-Legacy Cut      | Graphite-Legacy Cut   | Graphite-Legacy Cut   | Stone Mahogany-Legacy Cut | Driftwood-Citadel Cut | Desert Brown-Citadel Cut | Antique White-Citadel Cut | Desert Brown-Citadel Cut |
| Accent Precast/Sills                                 | Standard Grey         | Standard Grey         | Standard Grey         | Standard Grey             | Standard Grey         | Standard Grey            | Standard Grey             | Standard Grey            |
| Aluminum   | Beige                 | White                 | Linen                 | Sand                      | Linen                 | Linen                    | Linen                     | Linen                    |
| Shingles   | Colonial Blend        | Fulltone Black        | Driftwood             | Driftwood                 | Fulltone Black        | Fulltone Brown           | Fulltone Black            | Fulltone Brown           |
| Metal Flashing                                       | To match roof         | To match roof         | To match roof         | To match roof             | To match roof         | To match roof            | To match roof             | To match roof            |
| Stucco - -   | Marble Coat #8475     | Marble Coat #8470     | Marble Coat #8470     | Marble Coat #8470         | Marble Coat #8463     | Marble Coat #8161        | Marble Coat #8161         | Marble Coat #8161        |
| Hardie Board   | Navajo White          | Arctic White          | Arctic White          | Khaki Brown               | Khaki Brown           | Navajo White             | Navajo White              | Navajo White             |
| Miscellaneous Wood Detail (To include porch columns) | ICI Paint 20YY 58/082 | ICI Paint 00YY 65/060 | ICI Paint 00YY 65/060 | ICI Paint 00YY 65/060     | ICI Paint 20YY 49/071 | ICI Paint 20YY 68/102    | ICI Paint 20YY 68/102     | ICI Paint 20YY 68/102    |
| Windows  | White Vinyl           | White Vinyl           | White Vinyl           | White Vinyl               | White Vinyl           | White Vinyl              | White Vinyl               | White Vinyl              |
| Garage Brick Moulds                                  | White                 | White                 | White                 | White                     | White                 | White                    | White                     | White                    |
| Front Door   | ICI Paint 20YY 49/071 | ICI Paint 10YY 17/140 | ICI Paint 70YY 46/053 | ICI Paint 20YY 22/129     | ICI Paint 10YY 18/074 | ICI Paint 10YY 17/140    | ICI Paint 00NN 13/000     | ICI Paint 70YR 15/073    |
| Garage Door  | ICI Paint 20YY 58/082 | ICI Paint 20YY 49/071 | ICI Paint 70YY 63/045 | ICI Paint 20YY 31/106     | ICI Paint 10YY 33/054 | ICI Paint 20YY 39/130    | ICI Paint 20YY 58/082     | ICI Paint 20YY 39/130    |

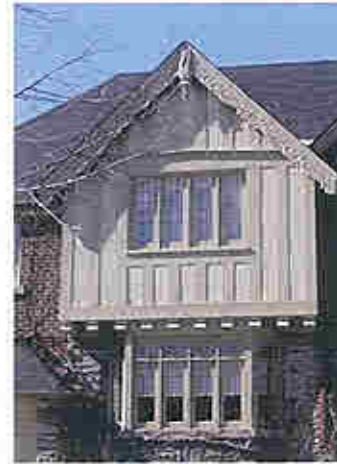
General Notes:

1. This chart is for illustrative purposes only and indicates the typical exterior materials and colours which shall be identified by the Builder.
2. All exterior colour selections are subject to Architectural Control Approval in compliance with the Architectural Vision & Design Guidelines.
3. All roof vents and flashings to be prefinished or painted to match roof colour.

**3.7 Architectural Detailing**

The use of architectural details which are characteristic to the style of the dwelling help to enhance its appearance. Where a dwelling elevation has reduced visibility from the public realm, the level of building detail may be simplified. Architectural detailing shall display the following design criteria:

- A variety of maintenance-free trim detailing is encouraged where architecturally appropriate, including:
  - louvers, brackets, dentils, pilasters, etc.
- A variety of brick detailing is encouraged where architecturally appropriate, including:
  - quoining, window/door headers, pilasters, banding, soldier coursing, base corbelling, etc.
- A variety of precast stone detailing is encouraged where architecturally appropriate, including:
  - sills, lintels, keystones and surrounds for windows and doorways.
- A variety of stucco detailing is encouraged where architecturally appropriate, including:
  - window/door surrounds, cornices, sills, accents, etc.
- All masonry detailing should be accentuated by projecting about 12mm (1/2") from the wall face.
- A maintenance-free frieze board (or brick soldier course cornice) is required on all exposed elevations and should run along the entire courtyard side of any projecting garage returning a minimum of 1200mm (4'-0") along elevations facing the interior sideyard.
- Where masonry banding is used on the front elevation it shall return a minimum of 1200mm (4'-0") along elevations facing the interior sideyard.



*Trim Detailing*



*Brick Detailing*



*Precast Stone Detailing*



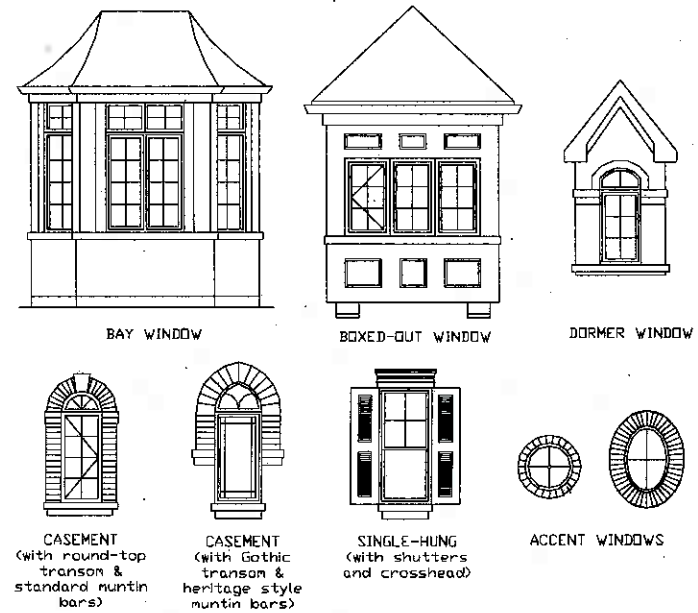
*Stucco Detailing*

*Examples of Architectural Detailing*

3.8 Windows

Ample fenestration is required for publicly exposed elevations to enhance the dwelling's appearance and to promote "eyes on the street".

- Window sizes have proportions and details consistent with the architectural style of the dwelling, including integrated muntin bars where appropriate. Vertical, rectangular window proportions are preferred to reflect traditional architectural styles.
- All windows facing the street or other publicly exposed areas shall be thermally-sealed, double-glazed casement or single-hung type. Windows on low-exposure elevations may be slider type provided the glass is set within a sash.
- Sills and lintels should be consistent with the architectural style of the dwelling.
- Bay windows should be used at appropriate locations and designed in a manner consistent with the architectural style of the dwelling.
- At siding and stucco finishes, window and door apertures must have a 100 mm min. wide casing.
- Where shutters are used, they should be half the width of the window.
- Window acoustic performance must meet or exceed the noise attenuation requirements of any applicable noise reports.



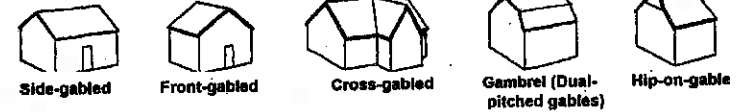
*Examples of Window Styles*

**3.9 Roofs**

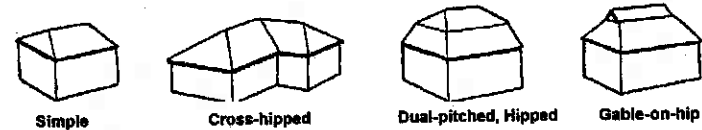
Roofs play a significant role in the massing of the individual dwelling and in the overall built form of the neighbourhood.

- A variety of roof forms consistent with the architectural style of the dwelling together with roof embellishments such as gables and dormers can help to achieve an interesting roofscape.
- Roof slopes should be characteristic of the architectural style of the dwelling. For example, Victorian architecture should employ steep roofs while Georgian architecture is typically designed with lower roof pitches.
- Alternate designs for a given model should have differing roof designs.
- Within the design of a streetscape, attention shall be paid to the relationships of adjacent roof forms to ensure appropriate transitions.
- Main roof pitch for 2-storey dwellings is:
  - front and rear facing slopes: 6:12 minimum;
  - side slopes in profile to the street: 7.75:12 minimum;
- Bungalows shall have a main roof pitch of 7.75:12 min. (both front to back and on sides) for massing compatibility with 2-storey dwellings.
- Roof overhangs shall be a minimum of 150 mm and not exceed 300 mm unless necessary.
- All plumbing stacks, gas flues and roof vents should be located on the rear slope of the roof wherever possible and should be prefinished to match the roof colour.
- Where skylights are proposed, they should be located on the rear or side slope of the roof and have a flat profile (e.g. Velux type).
- The use of upgraded textured shingles is encouraged.

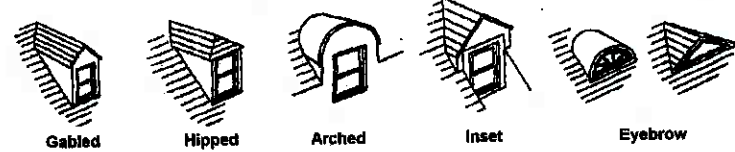
**GABLED FAMILY**



**HIPPED FAMILY**



**DORMERS**



*Examples of Roof Types*



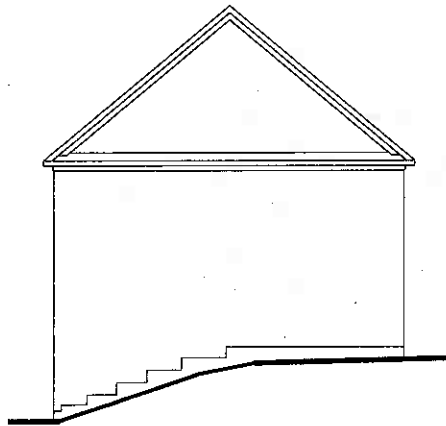
*Steep roof forms will be encouraged throughout the community*



*The use of upgraded textured shingles is encouraged*

### 3.10 Foundation Walls

- Exposed concrete foundation walls are prohibited.
- Grading shall be coordinated with dwelling foundation design and construction to ensure that no more than 300 mm of foundation walls above grade is exposed.
- Where sloping finished grades occur, finished wall materials and foundations shall be stepped accordingly to minimize exposed foundation walls. Special care shall be taken for sides of projecting garages, porches/porticos, front and flanking dwelling elevations.



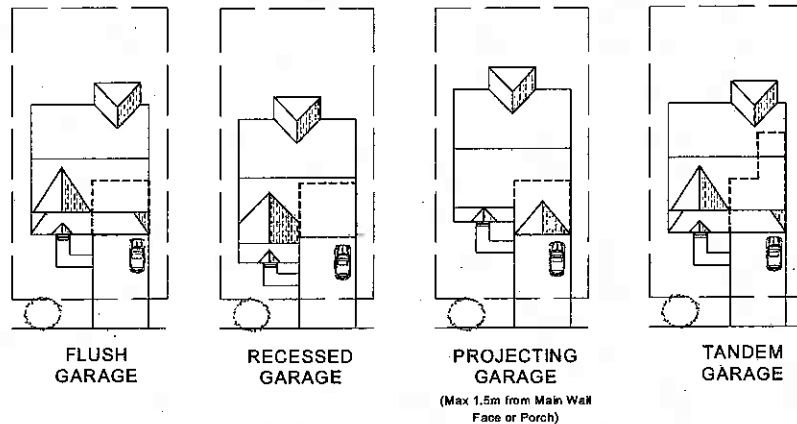
*Foundation walls should be stepped  
to follow sloping grade*

## 4.0 Design Criteria For Garages

### 4.1 General Criteria For Attached Garages

While it is important for these Guidelines to accommodate the needs of a car-oriented society, one of the prime objectives in creating a safe, attractive and livable neighbourhood is to minimize the visual impact of the garage on the residential streetscape. The following general design criteria for the treatment of attached garages shall apply:

- Attached garages shall be complementary in character and quality to the principal dwelling.
- Attached garages shall not dominate the massing of the dwelling. This can be achieved by:
  - giving the habitable portion of the dwelling a larger and more dominant mass;
  - integrating the garage into the main massing of the dwelling;
  - positioning the main front wall and porch face closer to the street than the garage.
- A variety of typical attached garage options will be encouraged including:
  - garages recessed behind the porch or main wall face;
  - garages flush with the porch or main wall face;
  - garages with limited projections in front of the porch or main wall face;



*A variety of attached garage options should be provided*



*Attached Garages Should Not Dominate the Dwelling Facade*

- The use of tandem garages is encouraged in order to limit the width of the garage, yet provide parking and storage opportunities.
- Garage doors shall be sectional (roll-up), panelled and have a variety of header/lintel treatments above.
- Glazing in overhead garage doors is required.
- Variety among garage door paneling patterns/styles is encouraged.
- The use of single bay garage doors separated by a pier, rather than a single 16' wide garage door is encouraged for 2-car garages.
- Upgraded garage door styles are required throughout the neighbourhood. The streetscape should include a combination of garage door styles to avoid repetition and dominance by a single door type.



*A variety of upgraded garage doors styles is required*

**4.2 Projection Criteria For Attached Garages**

- The maximum garage projection in front of the ground level main wall of the house and/or porch face shall be 1.5 metres. Projecting garages will be permitted on up to 25% of any street block. The majority of dwellings shall be designed with the garage flush with or set behind the ground floor main front wall or porch.
- Where a second storey habitable room is located above at least 60% of the garage's width, it shall not be set back more than 2.5 m.
- Dwelling designs with the second storey wall face flush with the garage wall face below should be avoided unless an appropriate design treatment is provided to create a visual break (i.e. a boxed-bay window; an intermediate roof; or other elements appropriate to the architectural style of the dwelling).

**4.3 Width Criteria For Attached Garages**

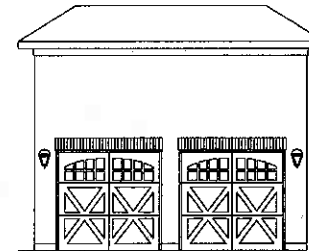
- On lots with frontage 12.0m to 15.0m metres and greater, interior dimensions shall be a minimum 2.6 metres width by 5.7 metres length for single car garages and 5.2 metres width by 5.7 metres length for double car garages, as per the applicable zoning by-law. The maximum interior width of the garage shall be 6.0m.

**4.4 Dropped Garage Conditions**

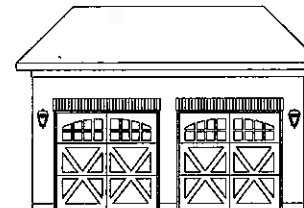
Dropped garages generally occur where rear-to-front sloping grade conditions exist. This often creates "top-heavy" garage massing resulting from additional wall height between the garage door opening and the soffit. Where the slab of the garage drops more than 600mm (2'-0") below what is indicated on the working drawings, an alternative design treatment must be submitted for architectural review and shown on the streetscape.

Alternative design treatments for dropped garages shall include:

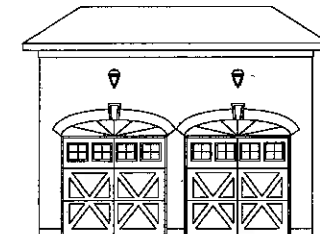
- lowering the garage roof;
- lowering or extending decorative gable louvres/windows;
- providing additional detailing or brick banding and soldier coursing;
- adding a habitable scale window above the garage doors;
- increasing the height of the garage door;
- providing arched headers above the garage doors;
- positioning light fixtures above the garage doors;



DROPPED GARAGE SLAB  
(UNACCEPTABLE)



TREATED BY LOWERING  
GARAGE ROOF



ACCEPTABLE ALTERNATIVE TO  
LOWERING GARAGE ROOF

*Example of dropped garage conditions*

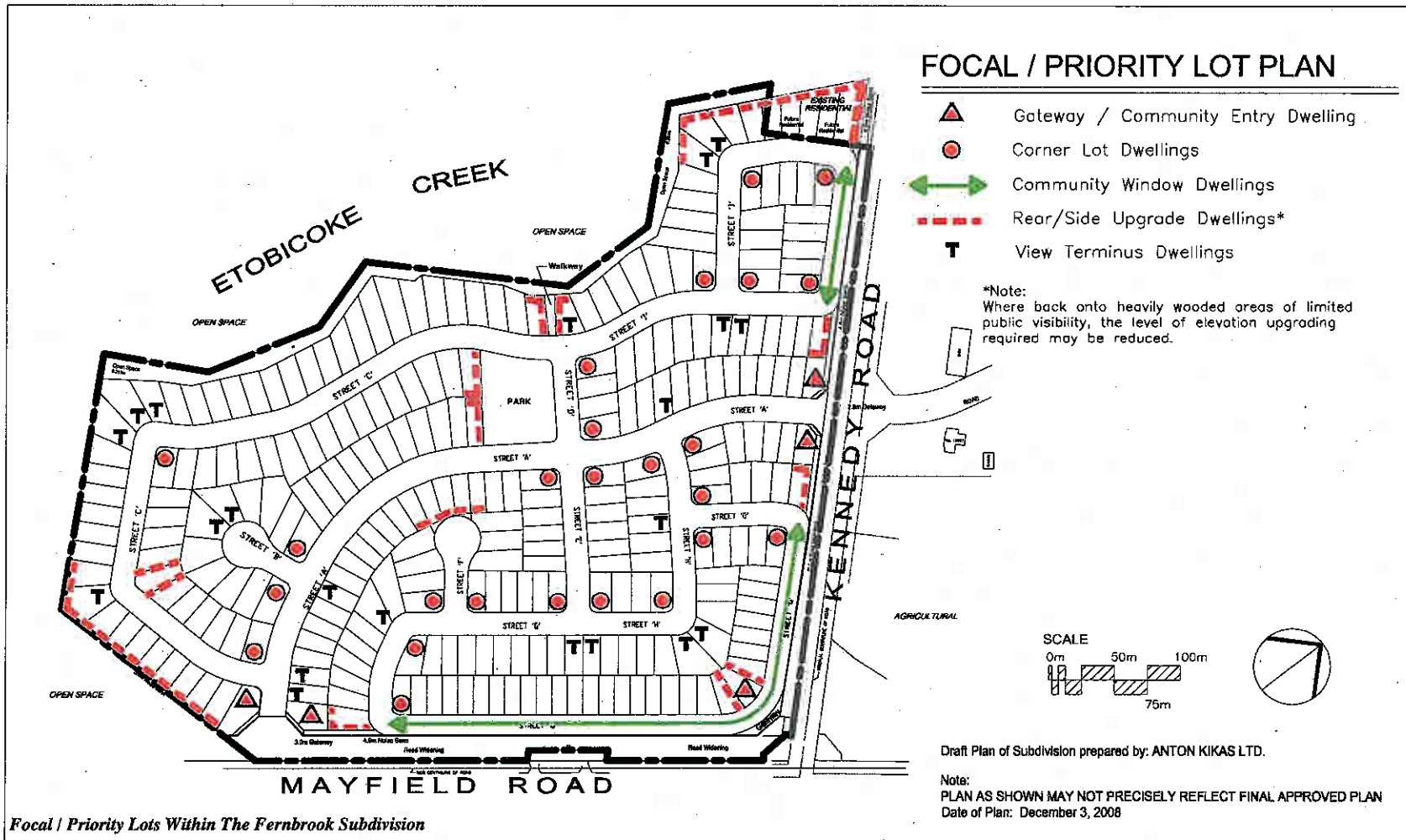
### 5.0 FOCAL/PRIORITY LOTS

Focal Lots are those which are located prominently within the neighbourhood. Special consideration for the siting, architecture and landscaping of buildings on these lots is required so they can act as landmarks and help to establish visual reference points within the neighbourhood.

Bungalows are not permitted on certain Priority Lots (Gateway Lots, Corner Lots, View Terminus Lots, Lots Flanking Mayfield or Kennedy Roads, Community Window Lots).

Focal lots will include:

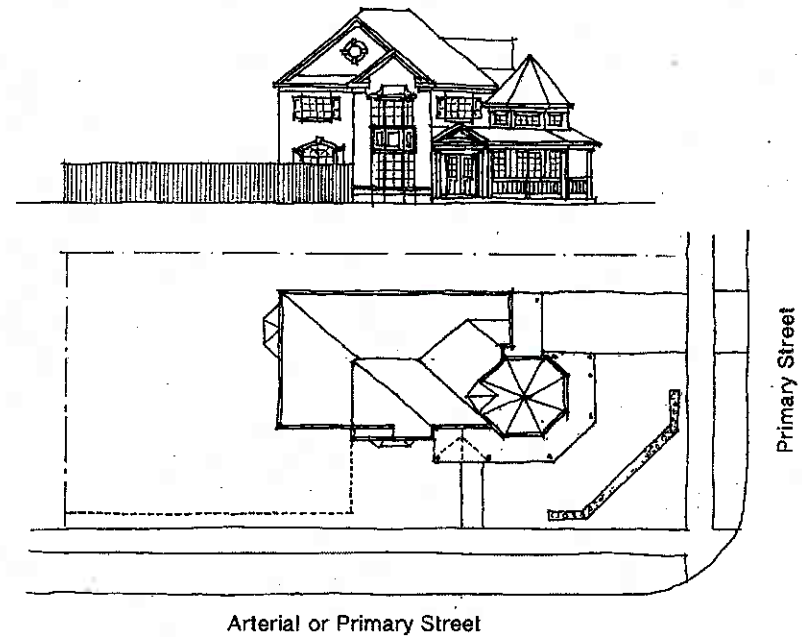
- gateways / community / neighbourhood entry lots;
- corner lots;
- community window lots;
- view terminus lots;
- park / open space lots;



**5.1 Gateway / Community / Neighbourhood Entry Lots**

Gateway / Community / Neighbourhood Entry Lots are located at the main entrances into the Fernbrook Subdivision from Mayfield Road and Kennedy Road. The lot adjacent to the intersection of Mayfield Road and Kennedy Road will also function as a Gateway / Community / Neighbourhood Entry Lot. Dwellings on these lots will play an important role in expressing the image, character and quality of the neighbourhood, as well as the entire Mayfield West Community to residents, visitors and passersby.

- A higher degree of architectural design quality will be required for all elevations of buildings sited on Gateway / Community / Neighbourhood Entry Lots.
- Distinctive architectural elements, such as a wraparound porch or other similarly dominant design features may be employed to emphasize the gateway dwelling's landmark qualities.
- The primary façade containing the main entrance shall be located facing the flanking street.
- The garage shall be oriented to the short side of the lot facing the internal street.
- The dwelling design, materials and colours shall be co-ordinated with any adjacent gateway entry features. The use of stone should be incorporated into the design of these dwellings.
- Dwellings on Gateway / Community / Neighbourhood Entry Lots will be enhanced through the use of landscaping.
- Dwellings and porches shall be appropriately set back from any Gateway entry feature to avoid conflicts.
- Noise attenuation measures, where required, shall be placed in such a manner so as to enclose a maximum of 25% of the flanking building elevation.



*Conceptual Images of Gateway / Community / Neighbourhood Entry Dwelling*



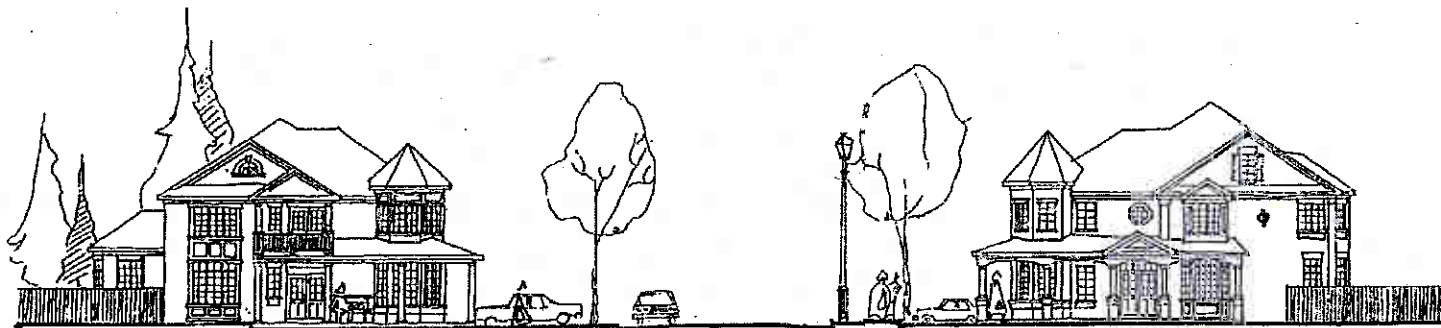
**5.2 Corner Lots (other than Gateway Lots)**

Dwellings on corner lots are among the most visible within the streetscape since they act as gateways to the residential development blocks. High quality designs shall be provided which address the flanking and visible rear elevations in a manner consistent with the front elevation.

- The main entrance should be oriented to face the flankage or long side of the lot. Similar, high quality architectural detailing shall be applied to both facades visible from the public street(s).
- Driveways shall be located as far from the intersection as possible.
- Wraparound porches, bay windows, generous fenestration, wall articulation and other architectural features are encouraged on the flankage side to create a positive pedestrian presence along the street.
- A walkway leading from the flanking side entrance to the sidewalk or curb is encouraged to maximize pedestrian accessibility.
- Privacy fencing should be provided which encloses the rear yard of corner lot dwellings. It should not extend beyond a point  $\frac{1}{4}$  of the house length from the rear corner of the house, thereby exposing at least  $\frac{3}{4}$  of the flankage elevation.
- Fencing height, design, material and colour should be consistent throughout the neighbourhood and should be co-ordinated with any other community fencing.



*Corner Lot Dwellings should be designed to address both street frontages*

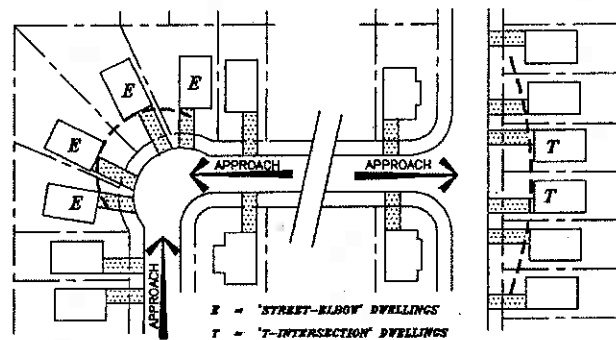


*Conceptual Streetscape Showing Corner Lot Dwellings*

**5.3 View Terminus Lots**

View Terminus Dwellings occur at the top of a 'T' intersection (where one road terminates at a right angle to the other), at the terminus of a cul-de-sac and at a street elbow or significant bend in the street. Dwellings in these locations play an important visual role within the streetscape by terminating a long view corridor.

- Driveways shall be located to the outside of a pair of View Terminus Dwellings to increase landscaping opportunities and reduce the prominence of the garage.
- A greater setback from adjacent dwellings is encouraged where lot depth permits.
- Corner lot dwellings opposite 'T' Intersection dwellings should frame the view from the street.
- A distinct architectural element should be provided to terminate the view.
- Enhanced front yard or boulevard landscaping shall be provided as set out in the approved Landscape Plan.



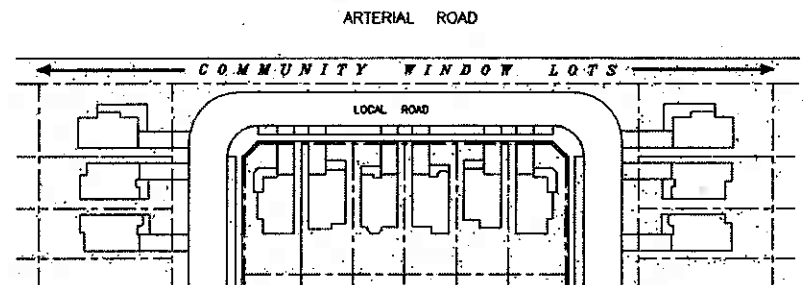
**VIEW TERMINUS DWELLINGS**

*View Terminus Dwellings*

**5.4 Community Window Lots**

Community window lots either flank onto Mayfield Road / Kennedy Road or have frontage on a single-loaded road facing Mayfield Road / Kennedy Road. Dwellings in these locations serve to represent the neighbourhood's image and identity. Due to their high degree of public visibility, community window dwellings will be required to attain a high level of architectural detailing and siting criteria, including:

- Dwellings with front projecting garages shall be minimized.
- Use of upgraded building materials, such as stone, shall be required.
- Exterior colour packages shall be carefully co-ordinated to ensure a rich harmony within the streetscape.
- Community window dwellings shall have a high degree of architectural detailing and will be encouraged to have a covered front entry feature, such as a porch or portico.
- Dwellings which flank onto an external arterial road, will require side and rear elevation upgrades and are encouraged to have a porch and entry facing the arterial road, unless constrained by setbacks and/or noise attenuation features. The treatment of these lots should be similar to corner lots.
- High berming and solid fencing between the public sidewalk along arterial roads and the parallel service roads shall be avoided. Berming and fencing will be required in some flankage conditions and should be designed to screen the rear yards but not obscure the dwelling's side elevation from public view.



*Community Window Dwellings*

**5.5 Park / Open Space Lots**

Park / Open Space Lots are located adjacent to the park or highly exposed open space features within the neighbourhood.

- Enhanced architecture consistent with the level of quality of the front elevation is required for any elevations exposed to these features.
- Additional fenestration and architectural detailing shall be provided.
- Driveways and garages shall be located away from the feature for abutting lots.
- Design variety among exposed rear elevations shall be provided to promote an attractive publicly visible rearscape.

- Rear or side elevations facing densely treed woodlots or open space areas not subject to a high degree of public exposure will not require enhanced design treatment.
- Where noise attenuation fencing obscures the ground level rear elevation, only the visible portion of the elevation must have enhanced design treatment.
- Fencing should be buffered with the use of soft landscaping elements to enhance the rear streetscape. Refer to approved Landscape Plan.



*Conceptual image of rear elevations adjacent to Park / Open Space areas*



*Conceptual image of rear elevations for lots requiring noise attenuation fencing*

### 5.6 Rear / Side Upgrading

- Dwellings adjacent to public walkways, the existing residential properties along Mayfield Road or other areas of public exposure will require appropriate enhanced architectural treatment to the exposed elevations similar to the requirements stated for Park Lots and Reverse Frontage Lots.
- Driveways and garages should be located away from these features.
- The requirement for rear / side upgrading shall also apply when the severe stepping of units along the street causes exposure to the side wall of the dwelling.



*Conceptual image of side elevation adjacent to Park / Open Space / Walkway areas*

## 6.0 IMPLEMENTATION

### 6.1 COMPLIANCE

Performance standards and design objectives within these guidelines are in addition to requirements of the Zoning By-law, Conditions of Draft Approval, Subdivision Agreements, Town of Caledon Development Standards, Policies and Guidelines, Noise Report, Ontario Building Code and all other applicable agreements and legislation. Approvals by the Control Architect do not release the builder from complying with the requirements of the Municipality, the Project Engineer or any other approval authority. It is the builder's complete responsibility to verify conformance with all required authorities. Developers and builders are required to comply with these Guidelines throughout the design, marketing and construction processes.

*Only those dwelling designs which have been given approval by the Design Control Architect shall be offered for sale.*

These guidelines and their interpretation by the Design Control Architect are not intended to discourage design creativity or innovation. Proposed designs which vary slightly from with the guidelines may be considered by the Control Architect, based on their merits, and may be approved where the spirit and intent of the guidelines is preserved or enhanced.

### 6.2 ROLE OF THE DESIGN CONTROL ARCHITECT

The role of the Design Control Architect is to review the builder's submissions in a fair and timely manner. The design review process is summarized as follows:

- Orientation meeting with the Builder / Developer.
- Model design review and approval.
- Siting review and approval.
- Monitoring for compliance.

The Control Architect will review and approve site plans, elevations and other materials for compliance with the Architectural Vision & Design Guidelines, as they relate to the exterior appearance of the dwelling only. The Control Architect does not check for compliance with structural, zoning or building code matters. Once approved, the Control Architect will stamp (with a stamp for this specific purpose and not a seal of practice) and sign the final plans. Building permit applications will only be processed by the Municipality when the stamped and signed plans are received.

### 6.3 PRELIMINARY REVIEW PROCESS

- Preliminary model design sketches which are in conformity with these Guidelines and which demonstrate sufficient design quality, variety and the use of appropriate exterior materials will be submitted to the Control Architect for review. They should clearly depict internal planning, entry conditions, building elevations, fenestration, exterior details and materials.
- Exterior building materials and colours shall be submitted at the time of preliminary model review.
- Submissions for preliminary review and approval should include:
  - Site Plans & Floor Plans
  - Exterior Elevations & Details
  - Special Dwelling Units or Lots (when applicable)
  - Typical Streetscape Elevations (when applicable)
  - Landscaping if integral to lot & dwelling design
  - Corner Lot Fencing Locations (when applicable)
  - Materials & Colours
- Floor plans are reviewed and approved in order to support approval of the exterior design. Floor plans will have a dashed line with dimensions indicating the second floor wall face where it varies from the first floor wall line.
- Sale of models cannot commence until after preliminary approval is given by the Control Architect.
- The applicant should allow up to 5 working days for comments.

### 6.4 FINAL REVIEW AND APPROVAL

#### 6.4.1 Working Drawings

- Working drawings must depict exactly what the builder intends to construct (i.e. roof pitch, exterior materials, window styles, column styles, architectural detailing, etc.).
- All exterior details and materials must be clearly shown on the drawings.

- Unit working drawings will be required for special elevations (i.e. upgraded rear / side), walkout lots and grade-affected garage conditions.
- A master set of all front, flanking and exposed rear elevations shall be submitted to the Design Control Architect for approval. This should be on 1 sheet for each dwelling type if possible.

#### **6.4.2 Site Plans**

- Engineer certified site plans are to be submitted to the Control Architect at a minimum scale of 1:250 and may be submitted on single 8-1/2" x 14" sheets.
- In addition to the required grading details, the proposed siting of each unit must clearly show model and elevation type and indicate the need for rear or side upgrades, where applicable.

#### **6.4.3 Streetscape Drawings**

- To assist in the review process a streetscape drawing (blackline) must accompany each request for siting approval.
- Streetscape drawings are to accurately represent the proposed dwellings in correct relation to each other and to the proposed finished grade.
- In the review of streetscapes, minor elevational changes may be required.
- The onus is on the Builder to ensure that these required changes are implemented in the construction of the dwellings.

#### **6.4.4 Exterior Colour Packages**

- Prior to the submission of site plans, the Builder will be required to submit typed colour schedules and sample boards which include the colour, type and manufacturer of all exterior materials.
- Colour package selections for individual lots and blocks should be submitted at the same time as site plans and streetscapes.

### **6.5 SUBMISSION REQUIREMENTS**

- The Builder is required to submit to the Control Architect for final review and approval, the following:
  - 6 sets of engineer approved site plans;
  - 4 sets of working drawings;
  - 3 sets of streetscapes;
  - 2 sets of colour schedules;
  - 1 set of colour sample boards;
- The Control Architect will retain one set of the foregoing other than the colour sample boards.
- The applicant should allow up to 5 working days for final approvals.
- Any minor redline revisions made by the Control Architect to site plans, working drawings, streetscapes and colour schedules must be incorporated on the originals by the Builder's Design Architect.
- Any revisions to an existing approval requested by the Builder will be considered on their merits and if acceptable will be subject to re approval by the Control Architect.
- It is the Builders' complete responsibility to ensure that all plans submitted for approval fully comply with these Guidelines and all applicable regulations and requirements including zoning and building code provisions.
- The Builder is responsible for the pick-up and delivery of all materials to and from the Control Architect's office and the Town as necessary.

**6.6 TOWN OF CALEDON APPROVAL**

- All site plans, working drawings, streetscapes and colour packages must be submitted for review and approved by the Control Architect and the Project Engineer (site plans only), as required, prior to submission to the Town of Caledon for building permit approval.
- Building permits will not be issued unless all plans bear the required Final Approval stamp of the Control Architect and Project Engineer (site plans only).
- Approvals by the Control Architect and the Project Engineer do not release the Builder from complying with the requirements and approvals of the Town of Caledon and/or any other governmental agency.

**6.7 MONITORING FOR COMPLIANCE**

- The Control Architect will conduct periodic site inspections to monitor development.
- Any significant visible deficiencies or deviations in construction from the approved plans which are considered by the Control Architect to be not in compliance with the Architectural Review Guidelines will be reported in writing to the Builder and Chief Building Official at the Town of Caledon.
- The Builder will respond to the Control Architect in writing within 7 days of notification of their intention to rectify the problem after which the Developer and the Town of Caledon will be informed of the Builder's response or lack of response.
- The Developer and/or Town of Caledon may take appropriate action to secure compliance.

**6.8 PERIODIC REVIEW**

- Town of Caledon staff will undertake periodic review of developments approved and constructed under these Guidelines. Staff will collaborate with the Control Architect to ensure that the plans and construction are in compliance with the Guidelines.

**6.9 DISPUTE RESOLUTION**

Where there is a dispute between the Control Architect and the Builder concerning the interpretation or application of these guidelines or the failure to process plans expeditiously, then the following dispute resolution procedure shall apply:

- The proponent shall notify the Control Architect and the Town of Caledon of the specific reasons and basis for the dispute.
- The Control Architect shall promptly respond in writing to the Town of Caledon and the proponent.
- If the proponent is not satisfied with the Control Architect's response, it may request in writing for the Town to intercede and state its position on the matters in dispute. The Town of Caledon shall be the sole and final arbiter in such circumstances.